

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO. <u>1</u>	DATED <u>2/26/2024</u>	ADDENDUM NO. _____	DATED _____
ADDENDUM NO. <u>2</u>	DATED <u>2/28/2024</u>	ADDENDUM NO. _____	DATED _____
ADDENDUM NO. _____	DATED _____	ADDENDUM NO. _____	DATED _____

Number	Description
1	Potponed til March 5, 2024; Amendment EBSx Download Required.
2	Revised Table of Contents; Revised Advertisement; Revised NTB No. 5583; Added NTB Nos. 5585 & 5586; Amendment EBSx Download Required.

TOTAL ADDENDA: 2
 (Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

 Contractor

BY _____
 Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

_____ President	_____ Address
_____ Secretary	_____ Address
_____ Treasurer	_____ Address

The following is my (our) itemized proposal.

NHPP-7326-03(001) / 109000301

Rankin County(ies)

Revised 01/26/2016

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION
TABLE OF CONTENTS**

PROJECT: NHPP-7326-03(001)/109000301 - Rankin

Section 901 - Advertisement

Section 904 - Notice to Bidders

#1	Governing Specification, w/ Supplement
#2	Status of ROW, w/ Attachments
#3	Final Cleanup
#13	Safety Edge
#14	Railway-Highway Provision, w/Supplement
#113	Tack Coat
#246	Kansas City Southern Construction Requirements
#296	Reduced Speed Limit Signs
#445	Mississippi Agent or Qualified Nonresident Agent
#446	Traffic on Milled Surface in Urban Areas
#516	Errata and Modifications to the 2017 Standard Specifications
#1225	Early Notice to Proceed
#1226	Material Storage Under Bridges
#1241	Fuel and Material Adjustments
#1963	Guardrail Pads
#2206	MASH Compliant Devices
#2273	Mississippi Special Fuel Tax Law
#2278	Smoothness Tolerances
#2782	DBE Pre-Bid Meeting
#2812	Traffic Signal and ITS Components
#2954	Reflective Sheeting for Signs
#3599	Standard Drawings w/Supplement
#3676	Asphalt Gyrotory Compactor Internal Angle Calibration
#3875	General ITS Requirements
#4113	Unique Entity ID Requirement For Federal Funded Projects
#4702	App for Traffic Control Report
#5086	Detail of Square Tube Sign Posts
#5551	Federal Bridge Formula
#5581	Contract Time
#5582	Specialty Items
#5583	Scope of Work
#5584	Lane Closure Restrictions
#5585	Underground Utilities
#5586	Temporary Construction Signs
#5605	Disadvantaged Business Enterprise In Federal-Aid Highway Construction, w/ Supplement

906	Required Federal Contract Provisions -- FHWA 1273, w/Supplements
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Section 907 - Special Provisions

907-101-1	Definitions and Terms
907-102-2	Bidding Requirements and Conditions

PROJECT: NHPP-7326-03(001)/109000301 - Rankin

907-105-2	Control of Work
907-106-1	Control of Materials
907-108-4	Subletting of Contract
907-109-4	Measurement and Payment
907-618-11	Work Zone Law Enforcement
907-632-1	Traffic Signal Cabinet Assemblies
907-641-3	Radar Vehicle Detection
907-700-1	Materials and Tests
907-701-3	Hydraulic Cement
907-702-4	Bituminous Materials
907-703-2	Gradation
907-705-1	Stone Riprap
907-707-3	Joint Materials
907-711-2	Plain Steel Wire
907-712-1	Fence and Guardrail
907-714-3	Miscellaneous Materials
907-718-1	Timber and Dimension Lumber
907-720-2	Acceptance Procedure for Glass Beads
907-721-4	Materials for Signing
907-899-1	Railway-Highway Provisions

Section 905 - Proposal, Proposal Bid Items, Combination Bid Proposal

Certification of Performance - Prior Federal-Aid Contracts

Certification Regarding Non-Collusion, Debarment and Suspension

SAM.GOV Registration and Unique Entity ID

Section 902 - Contract Form

Section 903 - Contract Bond Forms

Form -- OCR-485

Progress Schedule

(REVISIONS TO THE ABOVE WILL BE INDICATED ON THE SECOND SHEET
OF SECTION 905 AS ADDENDA)

02/28/2024 10:50 AM

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 901 - ADVERTISEMENT

Electronic bids will be received by the Mississippi Transportation Commission at 10:00 o'clock A.M., Tuesday, March 05, 2024, from the Bid Express Service and shortly thereafter publicly read in the Construction Division For:

Mill & Overlay approximately 2.5 miles of SR 468 from US 80 to north of 2nd Street, known as Federal Aid Project No. NHPP-7326-03(001) / 109000301 in Rankin County.

The attention of bidders is directed to the Contract Provisions governing selection and employment of labor. Minimum wage rates have been predetermined by the Secretary of Labor and are subject to Public Law 87-581, Work Hours Act of 1962, as set forth in the Contract Provisions.

The Mississippi Department of Transportation hereby notifies all bidders that it will affirmatively insure that in any contract entered into pursuant to this advertisement, disadvantaged business enterprises will be afforded full opportunity to submit bids in response to this invitation and will not be discriminated against on the grounds of race, color, sex, age, disability, religion or national origin in consideration for an award.

The award of this contract will be contingent upon the Contractor satisfying the DBE requirements.

The specifications are on file in the offices of the Mississippi Department of Transportation.

Contractors may request permission to bid online at <http://shop.mdot.ms.gov> at no cost. Upon approval, Contractors shall be eligible to submit a bid using Bid Express at <http://bidx.com>. Specimen proposals may be viewed and downloaded online at no cost at <http://mdot.ms.gov> or purchased online at <http://shop.mdot.ms.gov> at a cost of Ten Dollars (\$10.00) per proposal plus a small convenience fee. Cash or checks will not be accepted as payment.

Bid bond, signed or countersigned by a Mississippi Agent or Qualified Nonresident Agent, with Power of Attorney attached, a Cashier's check or Certified Check for five (5%) percent of bid, payable to STATE OF MISSISSIPPI, must accompany each proposal.

The attention of bidders is directed to the provisions of Subsection 102.07 pertaining to irregular proposals and rejection of bids.

BRAD WHITE
EXECUTIVE DIRECTOR

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION – 904 – NOTICE TO BIDDERS NO. 5583

CODE: (SP)

DATE: 01/05/2024

SUBJECT: Scope of Work

PROJECT: NHPP-7326-03(001) / 109000301 - Rankin County

The contract documents do not include an official set of plans, but may by reference include some Standard Drawings or Special Drawings.

Work on this project shall consist of milling and overlaying approximately 2.5 miles of SR 468 from just west of Second Street (BOP Station 10+00) to just north of US 80 (EOP Station 143+45). The existing pavement on SR 468 consists of 11” of asphalt over untreated clay gravel granular material, with 12’ and variable travel lanes and 3’ to 10’ shoulders.

The existing asphalt roadway shall be fine milled 2” and overlaid with 2” of 12.5-mm, HT, Polymer Modified asphalt.

Local paved public roads shall be fine milled 2” and overlaid with 2” of 12.5-mm, HT, asphalt to the end of the existing asphalt pavement, end of MDOT maintenance or to right-of-way, or as directed. After the paving operation, any material bladed aside for this area shall be pulled back to the asphalt pavement edge as directed by the Engineer and all cost shall be absorbed.

Existing asphalt/concrete driveway connections shall be milled and replaced with new asphalt connections using 12.5-mm, HT, asphalt.

GENERAL NOTES

MILLING

Milling and paving operations shall not begin until an **approved** asphalt mix design has been received, nor until such time that, in the opinion of the Engineer, weather conditions have been consistently suitable enough to allow placement of the asphalt pavement after the milling operations.

The reclaimed asphalt pavement (RAP) material removed by the milling operation shall become the property of the Contractor.

Where milling is required, the Contractor shall provide outlets in the existing shoulders at sufficient intervals to prevent pooling or standing water on the milled surface; the cost of which shall be absorbed in other items bid.

Milling and paving operations shall be performed such that a -2% slope from centerline is provided in normal crown roadway sections. Super elevation through curves shall be maintained as it currently exists or improved as directed. Where slope correction is required, correction will be

made by milling, paving, or combination thereof as directed by the Engineer. Milling correction: Mill outside edge of pavement to a depth of 1½” on a 2% slope towards the centerline. Paving Correction: Mill to depth of 1½” on existing slope and 2¼” and variable on centerline and 1½” on outside edge. Combination Method: Combination of both methods as directed by the Engineer to achieve the desired slope. In super elevated areas where correct SE exist milling will transition to thickness through curves. Where correct SE does not exist milling will transition at curves to correct SE as directed by the engineer.

Milling operations shall be performed in accordance with the Contract documents and the Standard Specifications. Variable width and length transitions may be required for ties at ramps, local roads, project limits.

Milling of driveway pads shall be conducted in a manner to prevent gouging or otherwise affecting the roadway pavement structure and slope. Milling of driveway pads shall not be done in simultaneous path with main line milling.

Traffic on the milled surface shall be limited to five (5) days. The Contractor will be assessed a penalty of **\$5,000 per calendar day** afterwards until the milled surfaces are covered with the next lift of asphalt. Additionally, traffic will be allowed to run on all milled surfaces other than the mainline for 30 days unless otherwise stated, and the Contractor will be assessed a penalty of **\$1,000** per calendar day afterwards until the non-mainline milled surface is covered with the next lift of asphalt. The additional allowance for the non-mainline milled surface is for the Contractor's convenience, and thus, the Contractor is responsible for any pavement failures or damage sustained during this period. Milling and paving of paved shoulders shall conform to Section 406.03.2 of the Standard Specifications.

PAVING

Per Subsection 401.02.3.2, the asphalt mix design shall be submitted to the Engineer at least 10 working days prior to its proposed use.

Prior to mainline milling and paving operations, failed areas in the existing pavement shall be removed and backfilled with 12.5-mm, HT, Leveling asphalt as per the attached typical sections and details. Asphalt shall be placed in multiple lifts with a maximum lift thickness of 3”. Any granular material base or subgrade material deemed unsuitable by the Engineer shall be removed as directed and backfilled with 12.5-mm, HT, Leveling asphalt. Payment for the excavation of the granular base and subgrade will be made using pay item 203-G: Excess Excavation. A list of the failed areas is shown in the attached tables. Pavement repairs shall be completed as a continuous operation in order to minimize traffic impacts. Lane closures shall remain in place until the failed area/shoulder has been completely repaired. Lane closures may not be left unattended except as allowed by the Engineer on multilane projects.

If traditional excavation methods are used, the removal area shall first be saw cut full depth, where applicable, to create a neat line and prevent damage to the adjacent pavement structure. Payment for saw cuts will be made using the appropriate items. If milling techniques are used, the area will not require saw cuts, but care should be exercised to create a neat removal line and to prevent damage to the adjacent pavement structure. If saw cuts are used in conjunction with milling,

payment will be made using the appropriate pay items. Payment will not be made for saw cuts that are not performed.

The surface lift for failed area repair or concrete punchout repair shall have a maximum deviation of 3/8" as determined by a 10 foot straight edge. Any location that deviates more than this tolerance, as determined by the Engineer, shall be corrected at no additional cost to the State.

Prior to paving, the existing 12' lanes shall be widen to 14' by trench widening. This shall be performed by excavating 5" deep by 2' wide and placing two lifts at 2½" of 12.5-mm, HT, Trench Widening asphalt.

Publicly maintained roads and streets should be paved to the existing right-of-way and in accordance with the attached drawings.

Privately owned entrances shall be paved to the shoulder line per the included typical drawing unless otherwise directed. Pad dimensions shall match the existing lengths and widths unless otherwise directed. Pads shall be shaped horizontally and vertically to prevent excessive drop-offs. Any new driveway pads deemed necessary by the Engineer shall be placed according to specifications.

GRANULAR SHOULDER MATERIAL

Where applicable, the existing shoulders shall be raised to match the new pavement elevation by placing variable depth granular material on the existing shoulders. The shoulders shall be graded and pulled up on a daily basis to eliminate drop-offs in excess of 2¼". Placement of the granular material on the finished asphalt course shall not be permitted. The material shall be bladed, rolled, and compacted to a finished slope of four percent (4%). Placement of this material shall be performed to provide a uniform and compacted shoulder with a minimum depth and width of material placed. Shoulders with adequate shoulder material in place shall be bladed to a slope of four percent (4%). The cost of blading will be an absorbed item and is not to be included in the price of pay items bid.

Granular material shall be provided around driveway pads as directed to prevent shoulder drop-offs and shall be placed in a timely manner. Drop-offs exceeding 2¼" shall be corrected within two (2) calendar days of the placement of the pad. Stabilizer aggregate shall be used as directed by the Engineer.

Any material excavated from the existing shoulder shall be used to raise the existing shoulder to match the new pavement elevation and any surplus material shall be spread along the edge of the shoulders, fore slopes, or other adjacent areas as directed by the Engineer and will be an absorbed item. Material which cannot be placed in adjacent areas and deemed to be excess excavations by the Engineer shall be removed from the project site. Payment for removal of excess material will be made using pay item 203-G: Excess Excavation.

TEMPORARY AND PERMANENT PAVEMENT MARKINGS

Temporary traffic stripe will be required immediately after the milling and/or overlay and prior to opening area to traffic. Temporary stripe shall be placed in the same location and configuration as the permanent stripe except that it may be offset as required for milling and paving operations.

If temporary stripe is offset, the Contractor shall conduct operations in a manner to insure the final temporary stripe is placed at the required location of the permanent stripe. If removal of temporary offset stripe is required in order to achieve the correct location and alignment of permanent stripe, the cost of removal will be included in other items bid. Placing double temporary centerline will not be allowed.

Temporary striping shall conform to finished stripe specifications for alignment, neatness, and straightness.

All permanent striping shall be double drop thermoplastic, 90-mil thickness unless otherwise specified in Subsection 626.03.1.2. Edge lines shall be placed to accommodate the lane widths shown on the attached applicable typical sections unless prevented by field conditions.

The use of short strips of traffic tape will not be allowed unless approved by the Engineer.

Temporary raised pavement markers shall be placed along the centerline of the roadway in any areas expected to be dormant for more than 90 days and/or as directed by the Engineer.

Permanent raised pavement markers shall be installed on mainline and local public roads after completion of all paving operations.

Payment for edge stripe on local roads shall be made under pay item 626-G004, Thermoplastic Double Drop Detail Stripe, White when the length of said stripe is less than 150 feet when measured from the end of the radius. If the measured length is greater than 150 feet, payment shall be made under pay item 626-C002: Thermoplastic Double Drop Edge Stripe, Continuous White.

Payment for centerline stripe on local roads shall be made under pay item 626-G005, Thermoplastic Double Drop Detail Stripe, Yellow when the length of said stripe is less than 150 feet when measured from the stop bar. If the measured length is greater than 150 feet, payment shall be made under pay item 626-E001: Thermoplastic Double Drop Traffic Stripe, Continuous Yellow. Centerline stripe shall be omitted on local roads whose width is less than 20 feet.

Guardrail

Guardrails shall be replaced at the locations shown on the attached table. Removal of guardrail shall consist of removal of w-beam/thrie beam, terminal end section, posts, and all other appurtenances. All guardrail removed shall be replaced the same day and prior to reopening the adjacent lane of traffic. Voids created by the removal of posts, concrete anchors, footings, etc. shall be backfilled and compacted in accordance with Section 203 of the Standard Specifications.

An asphalt guardrail pad shall be constructed at Station 43+00 prior to the placement of the new guardrail. Asphalt shall be extended under the guard rail and two feet (2') behind guard rail post as per the attached detail. The area to be paved shall be bladed to accommodate 4" of 12.5-mm, HT, asphalt. The elevation of the finished surface of the asphalt pavement shall provide for the required MASH guardrail height (see Standard Drawings). The cost of blading will be an absorbed item and should be included in the price of other items bid. Material which cannot be placed and blended in adjacent areas and deemed to be excess excavation by the Engineer shall be removed under pay item 203-G: Excess Excavation.

Guardrail lengths are based on terminal end length of 37.5'. If a terminal of length other than this is used, an adjustment in w-beam length is required.

All dimensions and spacings for bridge rail connectors shall be verified in the field by the Contractor prior to fabrication.

Permanent Signs

Permanent signs as listed on the attached tables shall be replaced. Unless otherwise listed in the attached tables, existing posts, anchors, angles/bars, and other components are to be reused. The Contractor shall use new bolts, screws, washers, nuts, etc. of the required sizes in the installation of signs. If required as part of the sign replacement activities, all post, pipe, and I-beam lengths in these plans are estimated. Post lengths for all signs shall be verified in the field by the Contractor prior to fabrication. Installation dates shall be clearly written in bold black markings on the back bottom half off all signs with a permanent marking stick that is waterproof, fade resistant, and marks on wet or dry surfaces. Removal of sign, post, and footing and backfilling will be paid using the removal of sign pay item.

Traffic Signals

Vehicle loop detectors at desired locations shall be replaced with radar detection sensors. Radar units shall be installed per manufacturer's recommendations. Existing EPAC controllers shall be replaced with new controllers and existing EPAC controllers shall be salvaged and delivered to MDOT Signal Shop (601-359-1493). It is the responsibility of the Contractor to coordinate delivery of existing EPAC controllers with MDOT personnel to MDOT signal shop. Contractor shall also be responsible for transferring existing controller data to the new controllers. Contractor may remove existing detection loop cable, if necessary. Cable quantities may be adjusted based on radar locations per manufacturer recommendations. Costs of the removal of vehicle loop detection cable shall be included in the cost of other items bid.

In order to prevent long term disruptions of normal signal timing operations, the signal work shall be completed prior to milling/paving activities in the applicable areas. Concurrent milling/paving and signal replacement operations will be allowed provided the established signal operations are not affected.

TRAFFIC CONTROL

The Contractor shall erect and maintain construction signing and provide all signs and traffic control devices necessary to safely maintain traffic around and through the work areas in accordance with the Traffic Control Plan and the MUTCD. The cost shall be included in the price bid for pay item 618-A: Maintenance of Traffic. Fluorescent orange sheeting shall be used on all construction and traffic control signs except those designated in the plans to be black legend and border on white background.

Standard roadside construction signs, barricades, etc. shall be placed in accordance with the attached tables, drawings, and as directed by the Engineer. W20-1 signs shall be placed on all public road approaches as shown or as directed. Payment for standard roadside construction signs, barricades, etc. will be made using the appropriate pay items.

The Contractor shall on a daily basis, remove all debris from within the roadway and a 30-foot clear zone which, in the opinion of the Engineer, is a hazard to the traveling public. This activity shall begin with the beginning of work or the beginning of the contract time, whichever comes first. No direct payment will be made for the debris removal. The cost shall be included in the prices of other items bid. Failure of the Contractor to remove debris as prescribed herein shall be just cause for withholding the monthly progress estimate payment or suspending active operations until the debris is satisfactorily removed by the Contractor.

Temporary asphalt joints (aka paper joints) shall be employed at all locations requiring traffic to traverse an uneven, transverse, pavement joint. Paper joints shall be a minimum of nine feet (9') in length and for the full width of the milled/paved surface. Paper joints shall be adequately maintained.

Potholes that may exist or occur in the existing pavement shall be patched in a timely manner. Patching of potholes shall be considered an absorbed item.

MISCELLANEOUS NOTES

It shall be the responsibility of the Contractor to protect existing structures such as pipes, aprons, signs, utilities, etc. from damage occurring during construction. The Contractor shall replace or repair, as directed by the Engineer, any structures damaged by the Contractor during the life of the Contract. No payment will be made for replacements and or repairs resulting from such damages.

Any signs that conflict with construction of this project shall be removed and relocated by the Contractor as directed by the Engineer; the cost of which is to be absorbed in other items bid.

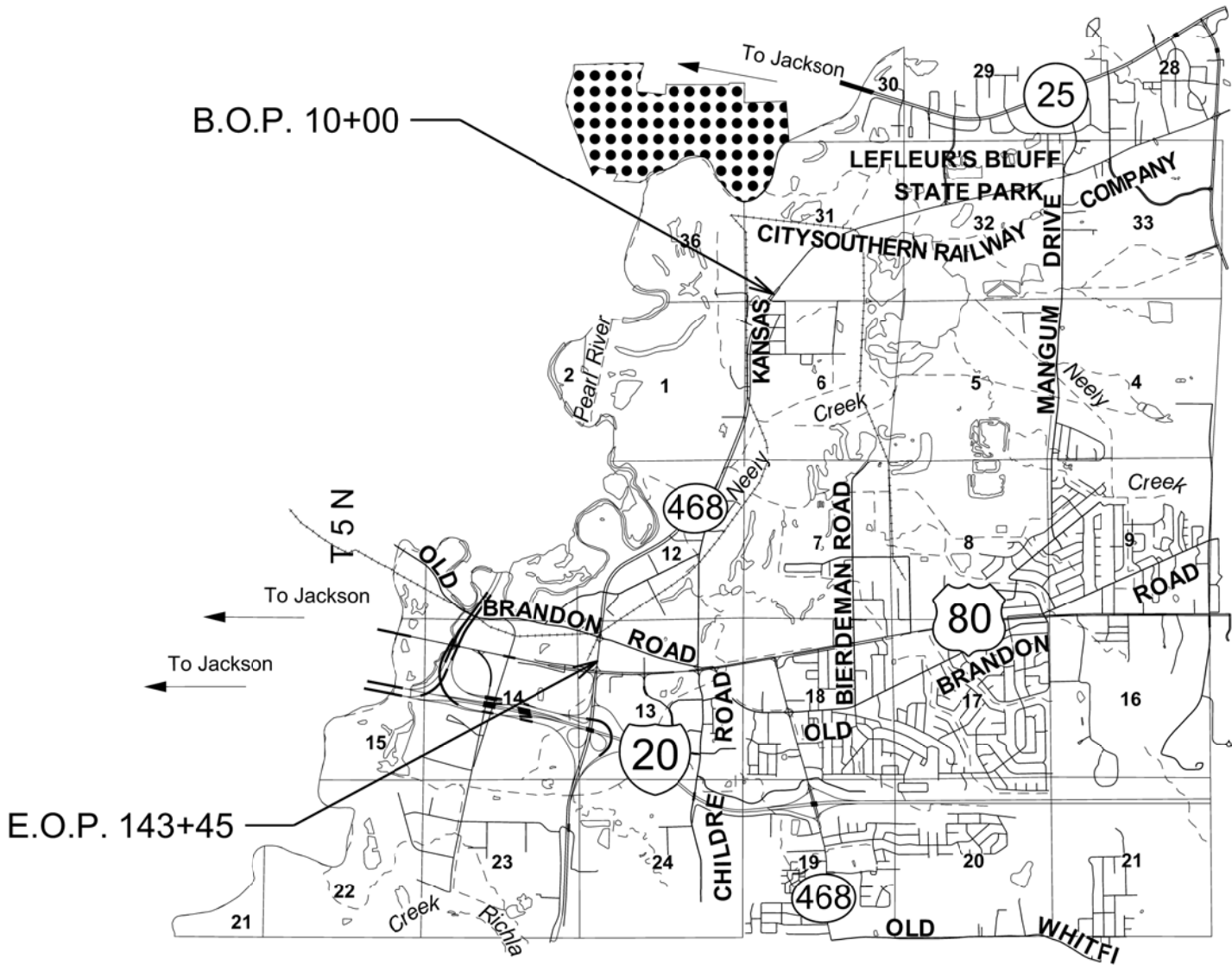
Costs of the removal of existing raised pavement markers shall be included in the prices for other items bid.

Incidental work such as removing vegetation, shaping and compacting shoulders, removing and resetting signs and/or mailboxes, removing excess asphalt material, project clean-up, and other items of incidental work necessary to complete the project will not be measured for separate payment and all costs will be considered included in the prices of items bid.

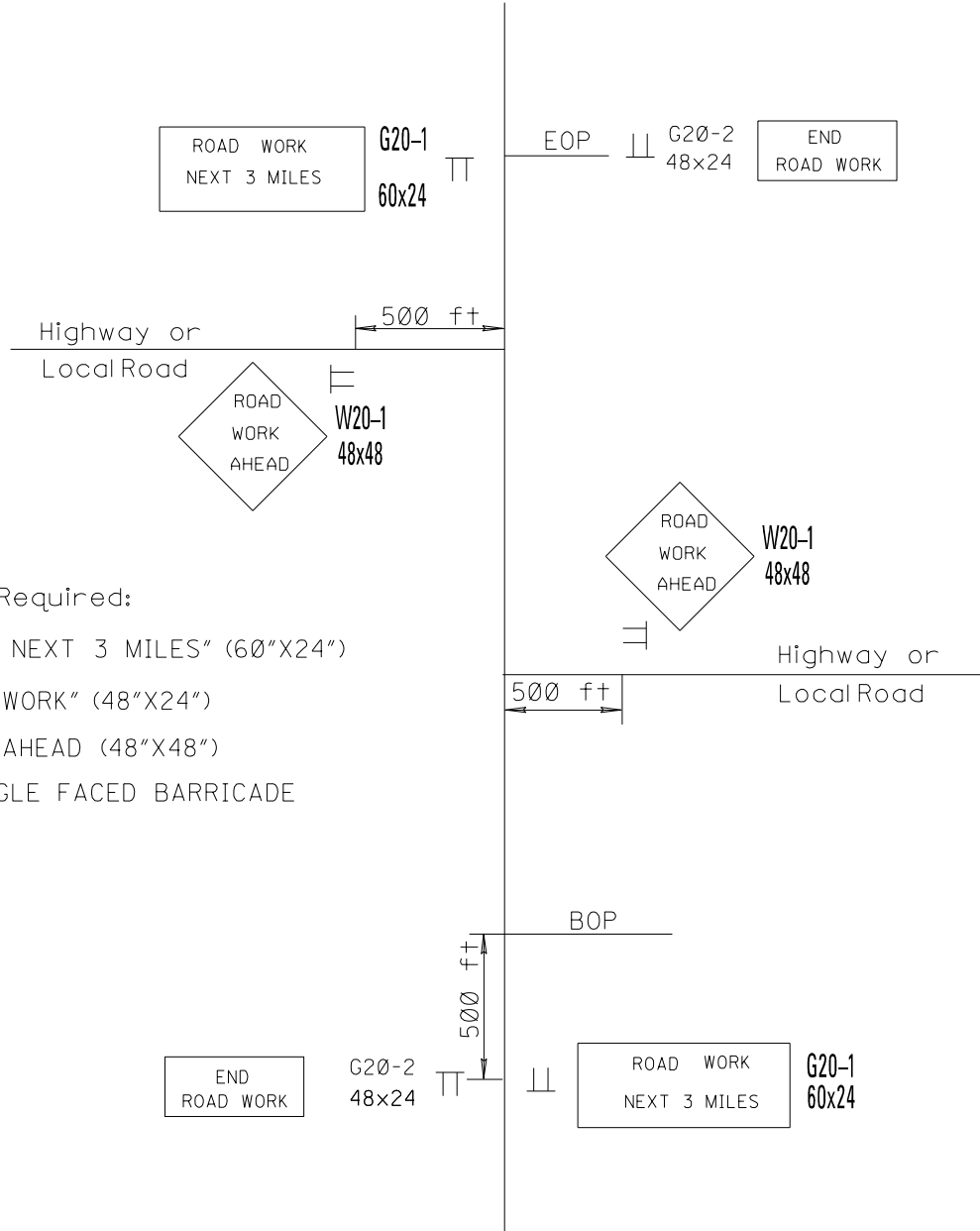
Prior to the final inspection, islands, and areas with curb shall be swept/cleaned. Care should be taken to prevent milled asphalt, asphalt debris, vegetative/granular debris, etc. from entering drainage structures or clogging other drainage ways. Disposal of material will not be measured for separate payments.

There is a railroad crossing located just south of the intersection of SR 468 and Old Brandon Road at Station 133+20. The Contractor will be required to comply with all applicable Railway-Highway Provisions.

SR 468 - RANKIN COUNTY PROJECT LOCATION MAP



SR 468 - RANKIN COUNTY CONSTRUCTION SIGNING

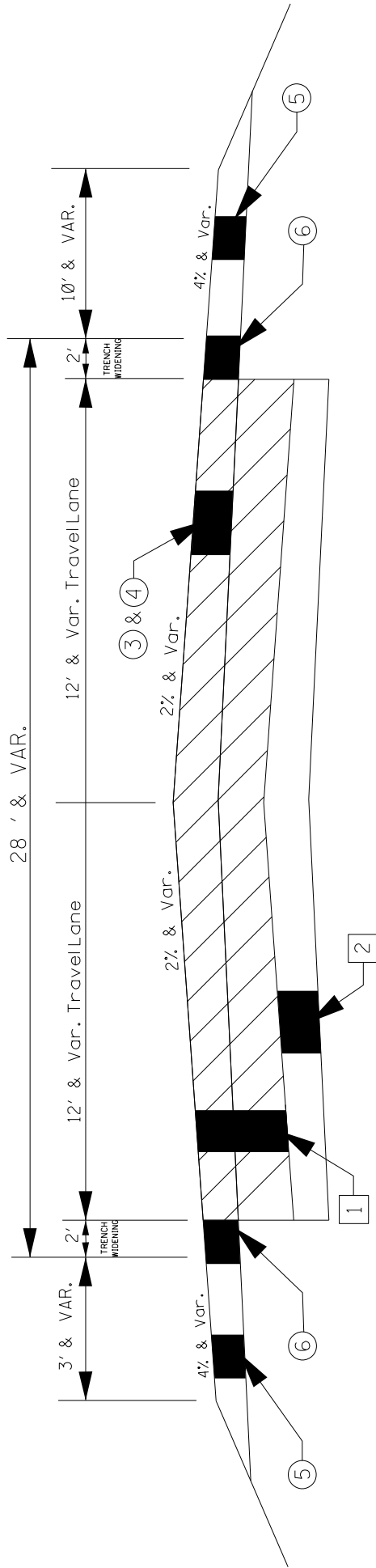


Traffic Control Signs Required:

- 4 - G20-1 "ROAD WORK NEXT 3 MILES" (60"x24")
- 4 - G20-2 "END ROAD WORK" (48"x24")
- 16 - W20-1 ROAD WORK AHEAD (48"x48")
- 8 - 619-G TYPE 3 SINGLE FACED BARRICADE

- NOTES: ① One (1) W20-1 "ROAD WORK AHEAD" Sign is Required at each Local Road, Street or Highway Entering the Project.
- ② G20-1 and G20-2 signs mounted on Type III Single Faced Barricade.
- ③ Placement of W20-1 signs on intersecting roads may vary from typical shown as conditions warrant.

SR 468 - RANKIN COUNTY
 TYPICAL SECTION - MILL & OVERLAY
 STATIONS: 10+00 - 135+67 EAST BOUND



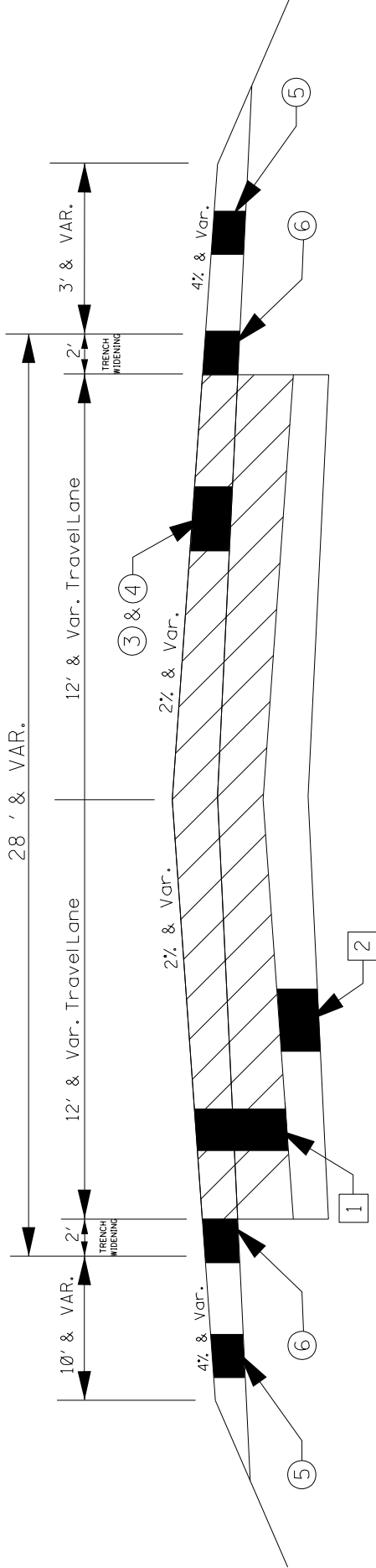
EXISTING

PROPOSED

- 1 11" HMA
- 2 Untreated Clay Gravel Granular Material
- 3 2" Milling (Correct to 2% NormalCrown or SE)
- 4 2" 12.5mm, HT, Asphalt Pavement, Polymer Modified
- 5 Variable Depth Class 5 Group C Granular Material (As Directed)
- 6 Prior to overlay, widen existing 12' lanes to 14' by trench widening, excavate 5" deep by 2' wide and place two 2 1/2" lifts of 12.5 mm, HT, Asphalt Pavement, Trench Widening.

Notes: Prior to milling, repair any failed areas full depth with 12.5 mm, HT, Asphalt Pavement, Leveling.

SR 468 - RANKIN COUNTY
 TYPICAL SECTION - MILL & OVERLAY
 STATIONS: 10+00 - 135+67 WEST BOUND



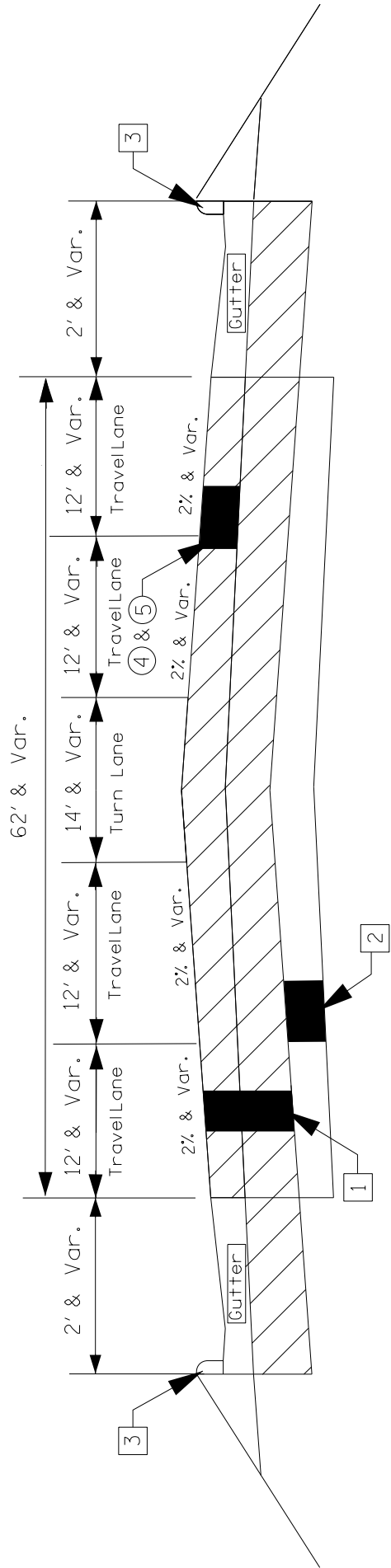
EXISTING

PROPOSED

- 1 11" HMA
- 2 Untreated Clay Gravel Granular Material
- 3 2" Milling (Correct to 2% Normal Crown or SE)
- 4 2" 12.5mm, HT, Asphalt Pavement, Polymer Modified
- 5 Variable Depth Class 5 Group C Granular Material (As Directed)
- 6 Prior to overlay, widen existing 12' lanes to 14' by trench widening. Excavate 5" deep by 2' wide and place two 2 1/2" lifts of 12.5 mm, HT, Asphalt Pavement, Trench Widening.

Notes: Prior to milling, repair any failed areas full depth with 12.5 mm, HT, Asphalt Pavement, Leveling.

SR 468 - RANKIN COUNTY
 TYPICAL SECTION - MILL & OVERLAY
 5 LANE WITH CONCRETE CURB
 STATIONS: 135+67 - 143+45 EAST & WEST BOUND



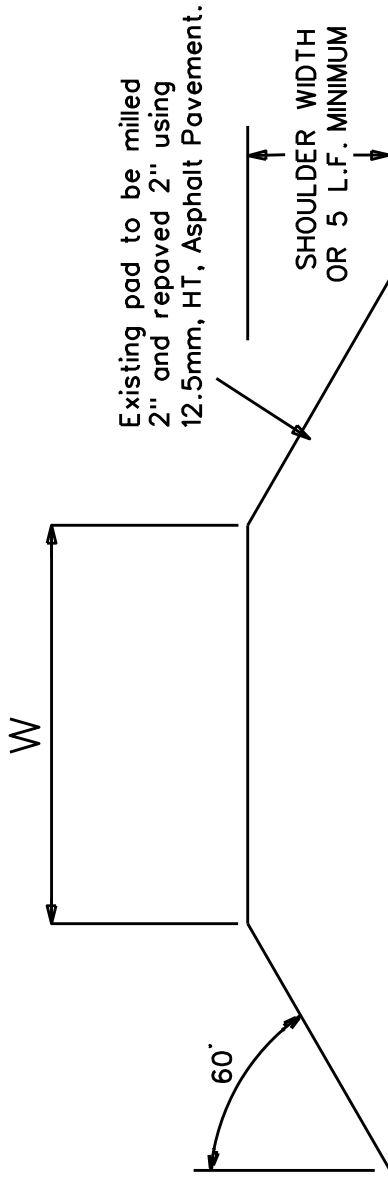
EXISTING

PROPOSED

- 1 11" HMA
- 2 Untreated Clay Gravel Granular Material
- 3 Concrete Curb and Gutter
- 4 2" Milling (Correct to 2% Normal Crown or SE)
- 5 2" 12.5mm, HT, Asphalt Pavement, Polymer Modified

Notes: Prior to milling, repair any failed areas full depth with 12.5 mm, HT, Asphalt Pavement, Leveling.

SR 468 - Rankin County DRIVEWAY PAD DETAIL

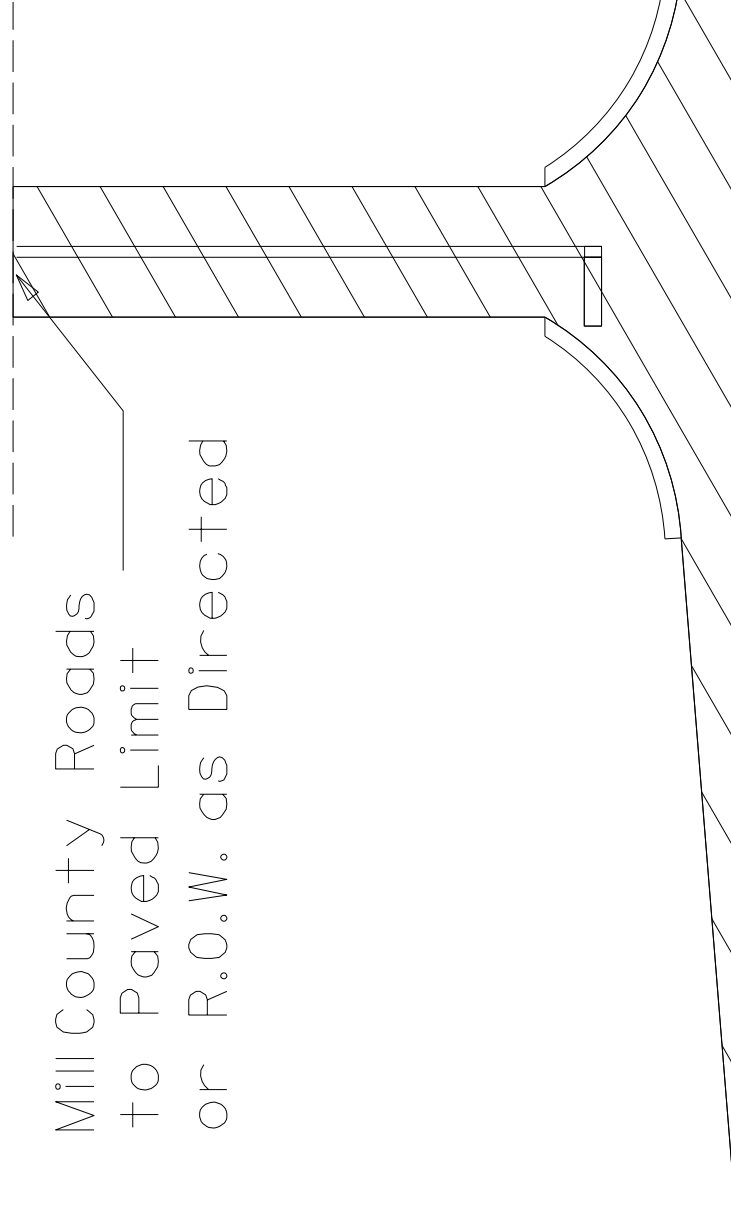


	MAINLINE PAVEMENT

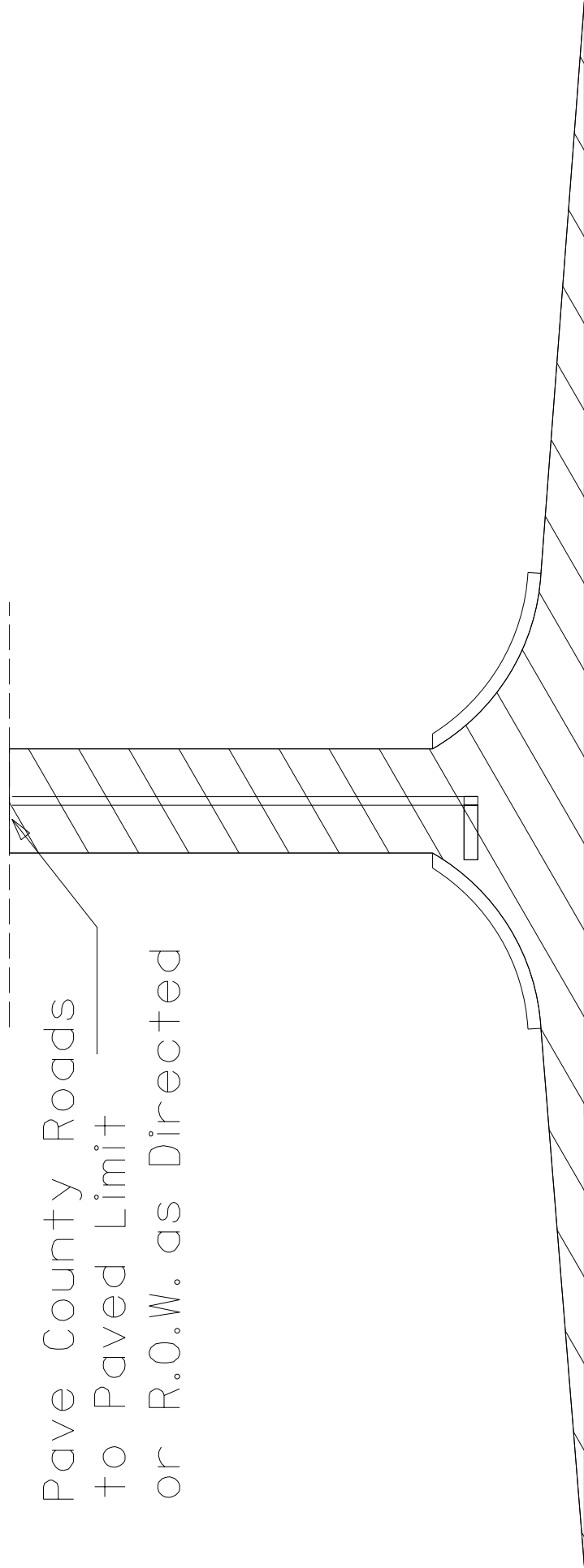
W = 16' MAX RESIDENTIAL
W = 30'-50' COMMERCIAL

NOTE: THE ASPHALT ON THE EXISTING DRIVEWAY/RAMP PADS ARE TO REMAIN IN THEIR CURRENT SIZE AND LOCATION AND MILLED/OVERLAID. IF, IN THE OPINION OF THE ENGINEER, A PAD SHOULD BE MODIFIED OR REPLACED, PAYMENT WILL BE MADE FOR THE WORK USING THE APPROPRIATE PAY ITEMS. GRANULAR MATERIAL AND/OR STABILIZER AGGREGATE SHOULD BE PLACED AROUND THE PADS AS REQUIRED.

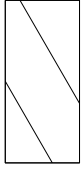
SR 468 - RANKIN COUNTY
MILLING COUNTY ROADS



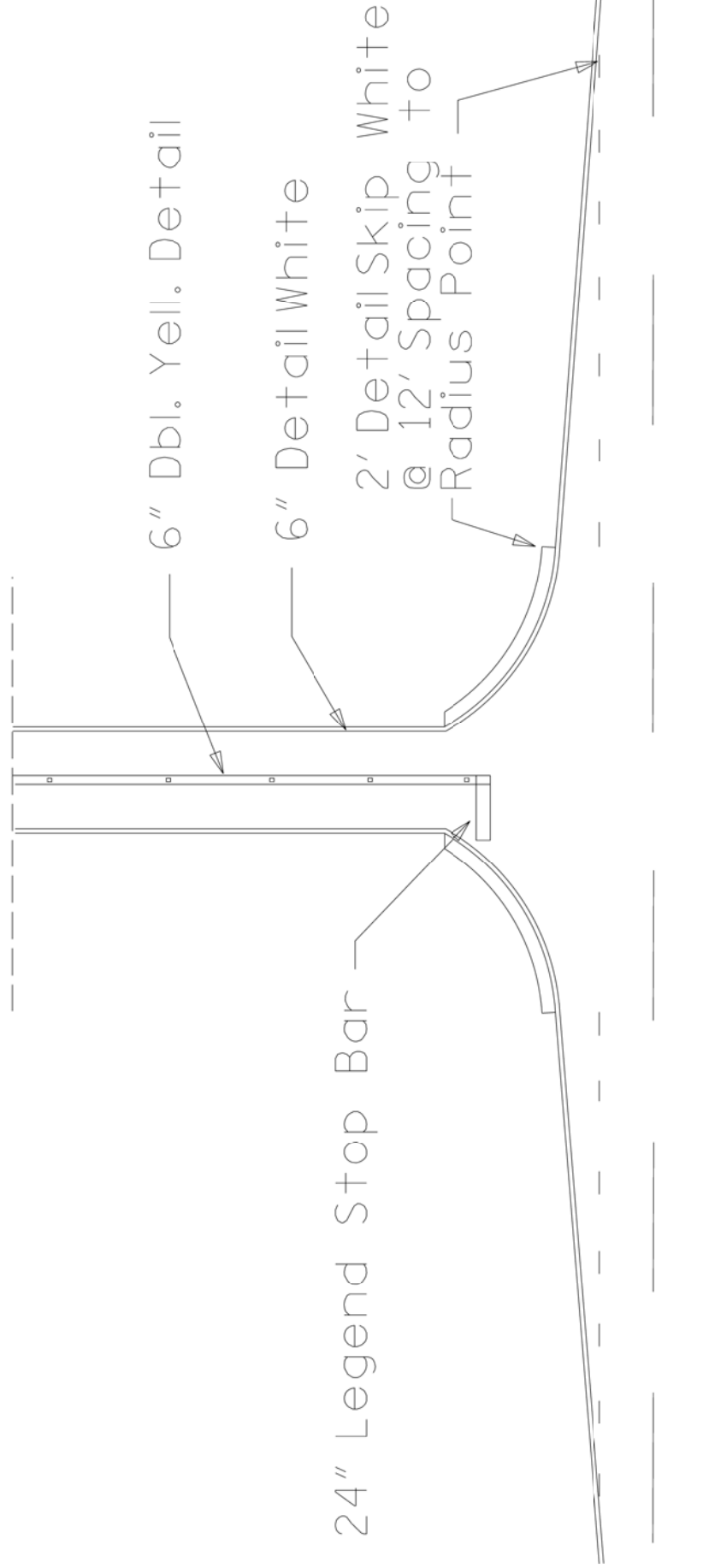
SR 468 - RANKIN COUNTY
PAVING COUNTY ROADS



Pave County Roads
to Paved Limit
or R.O.W. as Directed

 2" Pave Area

SR 468 - RANKIN COUNTY
COUNTY ROAD STRIPING



NOTE: DETAIL SKIP SHALL BE PLACED ON LOCAL ROADS WITH TAPERS.



6.0" Radius, 1.3" Border, White on, Brown;

"Crystal Lake", D 2K; "Mississippi Blues", D 2K;

"Trail Site", D 2K; Standard Arrow Custom 12.0" X 6.1" 180';



6.0" Radius, 1.3" Border, White on, Brown;
"Crystal Lake", D 2K; "Mississippi Blues", D 2K;
"Trail Site", D 2K; Standard Arrow Custom 12.0" X 6.1" 0';

TRAFFIC SIGNAL RADAR DETECTION CHART							
Intersection	Detection Zone Location	Phase #	Detection Zone Size	STOPBAR Radar Unit	Radar Cable (ft)	Existing Controller Type	Existing Pole Configuration
MS 468 at Underwood Dr/ Caterpillar Dr	SB Left Turn Lane	1	6'X50'	1	60	M50 EPAC (Contractor shall be responsible for converting firmware to TS2)	Mast Arm Poles
	SB Thru Lanes	6	6'X50'				
	NB Left Turn Lane	5	6'X50'	1	210		
	NB Thru Lanes	2	6'X50'				
	WB Lanes	3	6'X50'	1	140		
	EB Lanes	4	6'X50'	1	220		
	MS 468 at Old Brandon Road	SB Left Turn Lane	1	6'X50'	1		
SB Thru Lanes		6	6'X50'				
NB Left Turn Lane		5	6'X50'	1	140		
NB Thru Lanes		2	6'X50'				
WB Lanes		8	6'X50'	1	140		
EB Lanes		4	6'X50'	1	230		
Total				8	1300		

RANKIN COUNTY - 109000/301000

Guardrail Quantities															
STATION	LOCATION (LT/RT)	GUARDRAIL		FLARED TERMINAL END SECT. (EA)	TANGENT TERMINAL END SECT. (EA)	Cable Anchor TYPE I (EA)	BRIDGE END SECTION			DELINEATORS		Type 3 Object Markers (EA)	GUARDRAIL REMOVAL (LF)	REMARKS	
		TRANS. SECT. (LF)	THIRIE BEAM (LF)				TYPE "A" (EA)	TYPE "C" (EA)	TYPE "F" (EA)	SPEC. DESIGN BR END CONN. (EA)	WHITE (EA)				YELLOW (EA)
43+00	RT	606.25		1		1					22		678		
TOTAL =		606.25 L.F.	0 EA.	1 EA.	0 EA.	1 EA.	0 EA.	0 EA.	0 EA.	0 EA.	22 EA.	0 EA.	678 L.F.		

* REMOVAL OF ALL GUARDRAIL (BRIDGE END SECTIONS, W-BEAM, TYPE-I CABLE ANCHORAGE, TERMINAL END SECTIONS, ETC.) WILL BE PAID UNDER PAY ITEM 202-B REMOVAL OF GUARD RAIL.
 * REMOVAL OF GUARDRAIL DELINEATORS ARE CONSIDERED INCIDENTAL TO THE REMOVAL OF GUARDRAIL AND WILL NOT BE MEASURED AS A SEPARATE PAY ITEM.
 * ALL GUARDRAIL (METAL RAIL AND METAL POSTS ONLY) ,WOODEN POSTS, ALL BLOCKOUTS, CONCRETE ANCHORS, ETC. WILL BE THE PROPERTY OF THE CONTRACTOR.
 * TOTAL GUARDRAIL LENGTH IS BASED ON A TERMINAL END SECTION 37.5' LONG. IF A TERMINAL END SECTION OF A DIFFERENT LENGTH IS USED, THE LENGTH OF THE W-BEAM MAY HAVE TO BE ADJUSTED.

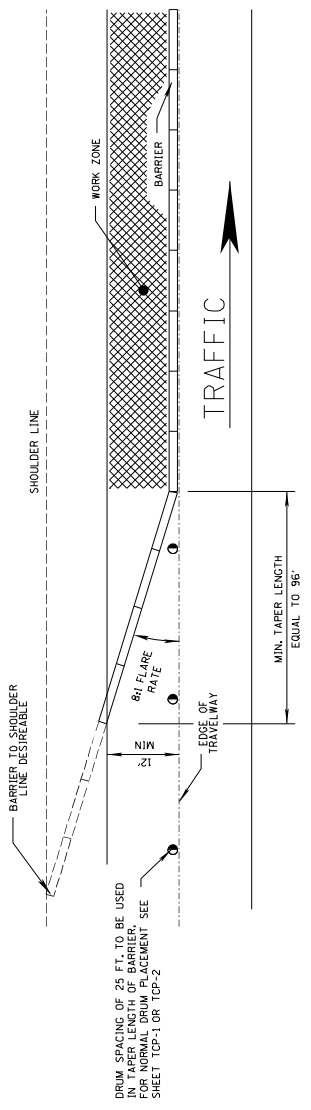
SR 468 - Rankin County
Pave Guardrail Pad

Edge Pavement

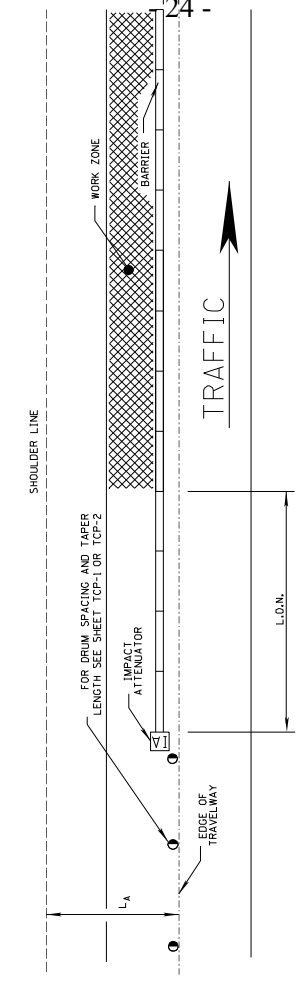


Edge Pavement

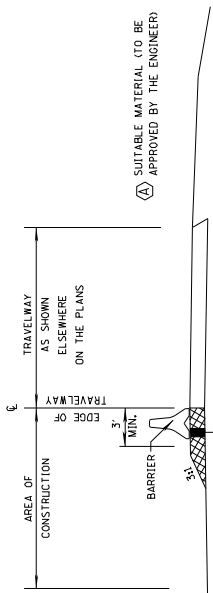




DETAIL OF POSITIVE BARRIER WITH TAPER

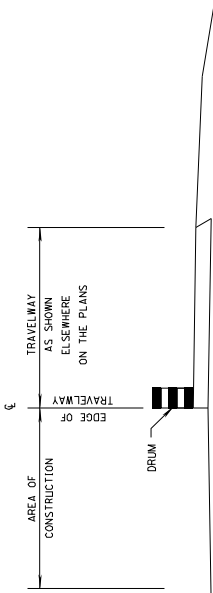


DETAIL OF POSITIVE BARRIER WITH IMPACT ATTENUATOR



ELEVATION VIEW FOR POSITIVE BARRIER

- NOTES:
1. POSITIVE BARRIER IS REQUIRED IN THE AREA OF OPEN PUNCH OUTS THAT ARE WITHIN SIX (6) FEET OF THE TRAVELWAY WHENEVER ACTUAL REPAIR WORK IS NOT BEING PERFORMED WITHIN THE LANE CLOSURE.
 2. MATERIAL USED TO SUPPORT POSITIVE BARRIER MUST BE AT SAME ELEVATION AS PAVEMENT IN ADJACENT TRAVELWAY.
 3. DELINEATORS REQUIRED ON ALL NON-REFLECTIVE BARRIER, AS SHOWN ON WORKING NO. OMB-3.



ELEVATION VIEW FOR DRUM

- NOTES:
1. WHILE WORK IS BEING PERFORMED WITHIN THE LANE CLOSURE DROP-OFFS MUST BE PROTECTED WITH DRUMS, ETC. IN EMERGENCIES EXCAVATED SECTION MAY BE BACKFILLED WITH GRANULAR MATERIAL, STONE OR OTHER APPROVED MATERIAL TO AVOID OVERNIGHT DROP-OFFS.
 2. LANE CLOSURES WITH OPEN PUNCH OUT AREAS MAY NOT BE LEFT UNATTENDED WHEN DRUMS ARE BEING USED FOR LANE CLOSURE

Notice to Bidders No. 55583 - Co

GENERAL NOTES:

1. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER OTHER BID ITEMS.
2. FOR DETAILS OF DRUM PLACEMENT SEE OTHER TRAFFIC CONTROL PLANS.

DESIGN SPEED (mph)	OVER 10,000 veh/day	5,000-10,000 veh/day	1,000-5,000 veh/day	UNDER 1,000 veh/day
70	360	330	290	250
60	300	250	210	200
50	230	190	160	150
40	160	130	110	100
30	110	90	80	70

2. RUNOUT LENGTH (L_R) IS TO BE DETERMINED USING THE FOLLOWING TABLE:

- NOTES:
1. LENGTH OF NEED, L.O.N. = $\frac{L_R \cdot A_{12}}{L_A}$

WHERE: L_A = LATERAL EXTENT OF THE AREA OF CONCERN
 L_R = RUNOUT LENGTH
 L₂ = LATERAL OFFSET FROM EDGE OF TRAVELED WAY TO BARRIER.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
LANE CLOSURE DETAILS FOR GREATER THAN 3 INCH DROPOFF

PROJ. NO.: _____
 COUNTY: _____
 FILE NAME: SDTCP-CADGN
 DESIGN TEAM: _____
 DATE: _____
 REVISION: _____
 SHEET NUMBER: _____

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 5585

CODE: (SP)

DATE: 02/28/2024

SUBJECT: Underground Utilities

PROJECT: NHPP-7326-03(001) / 109000301 – Rankin County

Bidders are hereby advised that utility lines owned and maintained by MDOT may be present within the project limits. These utilities are not located by Mississippi 811. It shall be the Contractor's responsibility to coordinate with MDOT to have the utility lines located and marked prior to beginning work. The Contractor shall give a minimum of three (3) working days of advance notice for locate requests.

Additionally, it shall be the Contractor's responsibility to maintain the utility markings or have the ability to survey the marked utilities and re-establish said utility markings as needed. The Department shall only be responsible for locating and marking the utilities once per Contract.

The contacts for MDOT utility lines are as follows:

Underground Power Lines:

Michael Lee – 601-683-3341 – mlee@mdot.ms.gov

Billy Coward – 601-683-3341 – bcoward@mdot.ms.gov

Underground Communication Lines:

Kerby McFarland – 601-359-7450 – kmcfarland@mdot.ms.gov

Steven Newell – 601-359-7450 – snewell@mdot.ms.gov

Henry Lewis – 601-359-1454 – hlewis@mdot.ms.gov

Underground Signal Lines:

Amrik Singh – 601-359-1454 – asingh@mdot.ms.gov

Kenneth Welch – 601-359-1454 – kwelch@mdot.ms.gov

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 5586

CODE: (SP)

DATE: 02/28/2024

SUBJECT: Temporary Construction Signs

PROJECT: NHPP-7326-03(001) / 109000301 – Rankin County

Bidders are hereby advised of the following regarding the Temporary Construction Signs required:

Should the Bidders elect to install Temporary Construction Signs by first driving short u-channel sections and then bolting the longer, correct height u-channel sections to them, the Bidders are advised that these short sections shall be a minimum of five (5) feet from the ground level when driven and the splice must consist of a minimum of eighteen (18) inches of overlap with a total of four (4) bolts. Bidders are also advised that it is mandatory that these short sections be removed at the completion of the project.