

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO. <u> 1 </u>	DATED <u> 10/19/2017 </u>	ADDENDUM NO. _____	DATED _____
ADDENDUM NO. _____	DATED _____	ADDENDUM NO. _____	DATED _____
ADDENDUM NO. _____	DATED _____	ADDENDUM NO. _____	DATED _____

Number	Description
1	Revised NTB No. 360; Amendment EBS Download Required.

TOTAL ADDENDA: 1
 (Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

 Contractor

BY _____
 Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

_____ President	_____ Address
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_____ Secretary	_____ Address
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_____ Treasurer	_____ Address
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The following is my (our) itemized proposal.

NH-0014-03(072)/ 107646301000

Greene County(ies)

Revised 01/26/2016

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 360

CODE: (SP)

DATE: 08/14/2017

SUBJECT: Scope of Work

PROJECT: NH-0014-03(072) / 107646301 -- Greene County

The contract documents do not include an official set of construction plans but may by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, "Standard Drawings". All other references to plans in the contract documents and Standard Specifications for Road and Bridge Construction are to be disregarded.

Work on the project shall consist of the following:

US 98
OVERLAY APPROXIMATELY 5.45 MILES OF US 98 FROM
GREENE / PERRY COUNTY LINE EAST TO 1.7 MILES EAST OF SR 57
1046+80.6 TO 1332+76

(A) Prior to the overlay, centerline alignment shall be determined by the Contractor by measuring the existing roadway at 500-foot intervals in tangent sections, and 100-foot intervals in horizontal curves. The existing shoulders shall be clipped and surplus material shall be spread along the edge of the shoulders, fore slopes, or other adjacent areas as directed by the Project Engineer, and will be an absorbed item.

(B) Cold Mill 1.25" & variable on US 98 at all the transitions including E.O.P., B.O.P., bridge approaches, and all tie-ins. Also, cold milling will be required along the curb and gutter sections to ensure a smooth transition to the gutter line as directed by the Project Engineer. All milled areas shall be backfilled with asphalt within seven days of milling. Milled material in the amount of 500 tons shall be stockpiled at the intersection of SR 57 AND US 98 as designated by the Engineer and will become property of MDOT. The milled material will not require weighing but trucks hauling the stockpiled milled material will be measured for LVM and a conversion factor of 1.28 tons per cubic yard used to convert the LVM to tonnage. The source of the 500 tons of stockpiled milled material will not be specified, i.e., the milled material may come from milled removal sections, county road tie-ins, curb and gutter sections, etc. All other milled material shall become property of the Contractor.

(C) Overlay US 98 with 1.25" of 9.5-mm, HT asphalt mixture from the Greene / Perry County Line east 5.45 miles to 1.7 miles east of SR 57. Prior to the overlay, a leveling course of ¾" & variable of 9.5-mm, HT, Leveling asphalt shall be required on the mainline lanes. Smoothness incentive/disincentive will be governed by the specification for MRI (mean roughness index). All incentive/disincentive will be based on theoretical tonnage placed on a 12-foot travel lane. Remove any failed areas on the main facility by milling to the appropriate depth and repair by backfilling with 19-mm, HT, asphalt as directed by the Project Engineer. Removal areas will be marked by MDOT personnel and include but are not limited to areas included in Table 1. Approximately 2,400 tons of 19-mm, HT, asphalt will be used for backfill in removal sections.

Publicly maintained roads or streets shall be surfaced to the existing R.O.W.; privately owned entrances shall be surfaced a distance of 10 feet and variable from edge of pavement. Any site grading at local roads or drives will not be measured for separate payment but will be considered an absorbed item. All crossovers, turn lanes, local roads, guardrail pads, and driveways shall be overlaid with 9.5-mm, ST, asphalt. An estimated 1,050 tons will be required for the cross-overs, 1,265 tons for local roads and turn lanes, and 125 tons for driveway pads. Cross slopes shall be maintained to achieve a uniform cross slope of 2% where practical with plan quantities. The existing super-elevation in horizontal curves is to be maintained as a minimum. The Contractor will mill a 12-inch rumble strip along the center of the 2-foot paved shoulder, six inches (6") from the travel lane, and place the traffic stripe on the inside six inches (6") of the rumble strip. If water stands when project is complete, the Contractor shall correct at no additional cost to the State.

(D) Remove and install guardrail and terminal end sections at the Leaf River Bridge. Guardrail shall be installed in accordance with the attached drawing. The installation of terminal end sections, installed as per the manufacturer's recommendations, shall be MASH or NCHRP Report 350 Test Level 3 (TL-3) compliant. All terminal end sections shall be marked with a Sanford Mean Streak grease pen (or equivalent) to identify the type installed. The Contractor shall furnish the Project Engineer two (2) copies of the manufacturer's installation instructions prior to beginning guardrail operations. Any site grading and all fill material necessary at the guardrail locations will not be measured for separate payment but will be absorbed in other guardrail pay items. Guardrail pads shall be overlaid prior to placing guardrail and will be paid for under Pay Item 403-A: 9.5mm, ST, Asphalt Pavement. Seeding, fertilizer and vegetative mulch are required on all disturbed areas at the guardrail site and will be considered an absorbed item. Application rate for Bermuda grass seed will be 25 pounds per acre. Application rate for combination fertilizer (13-13-13) will be 1,000 pounds per acre. Application rate for mulch shall be two (2) tons per acre.

(E) Raise the existing shoulders to match the new pavement elevation by placing 1¾" & variable depth 825B crushed stone on the shoulders as per the typical drawing. Placement of the crushed stone on the finished surface course shall not be permitted. The material shall be bladed, rolled and compacted to a finished slope of 4% where practical. Shoulders with existing adequate shoulder material in place shall be bladed to a slope of 4%, the cost of which shall be included in the prices of other items bid.

(F) Temporary striping shall conform to finished stripe specifications for alignment, neatness, reflectivity, and straightness. All permanent pavement markings are to be hot thermoplastic. Edge lines will be placed so as to maintain the original lane width. On all bridges and concrete sections of highway, old traffic stripe shall be removed and replaced with thermoplastic. Special care should be taken for the placement of thermoplastic detail stripe along the edge of pavement at turn-outs on all local roads and along tapers where detail stripe is required as per Typical PMD-1.

(G) Raised pavement markers will be placed as per sheet PM-1 of the Standard Drawings. Any removal of existing raised pavement markers or rumble bars shall be considered an absorbed item. Only flexible adhesive shall be allowed for placement of raised pavement markers meeting the requirements of Subsection 720.03.7.7 of the Standard Specifications.

The Contractor shall erect and maintain construction signing, and provide all signs and traffic handling devices, and shall provide two portable R16-3 signs per work zone or lane closure in addition to signs required by standard drawings in accordance with Manual Uniform Traffic

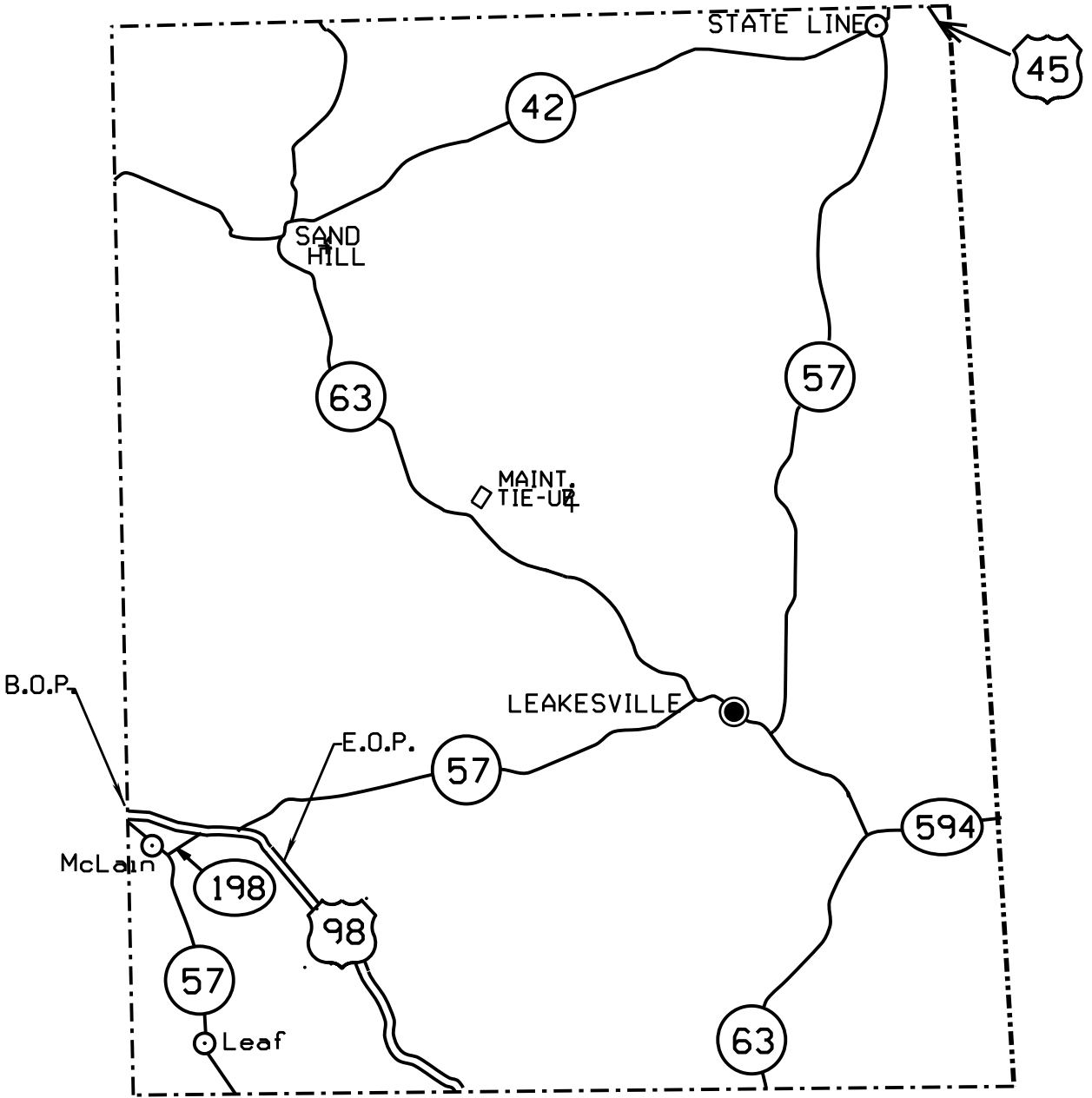
Control Devices (MUTCD). The cost is to be included in the price bid for pay item No. 618-A, Maintenance of Traffic.

If pavement section marking tape is encountered on this project, it shall be located prior to overlaying and placed back in the same location after paving operations have ceased. The section marking tape shall be eight inch (8") High Performance Cold Plastic Detail Stripe (white) and shall be four feet (4') in length. The marking shall be centered across the centerline stripe. The cost of this item shall be absorbed in other bid items.

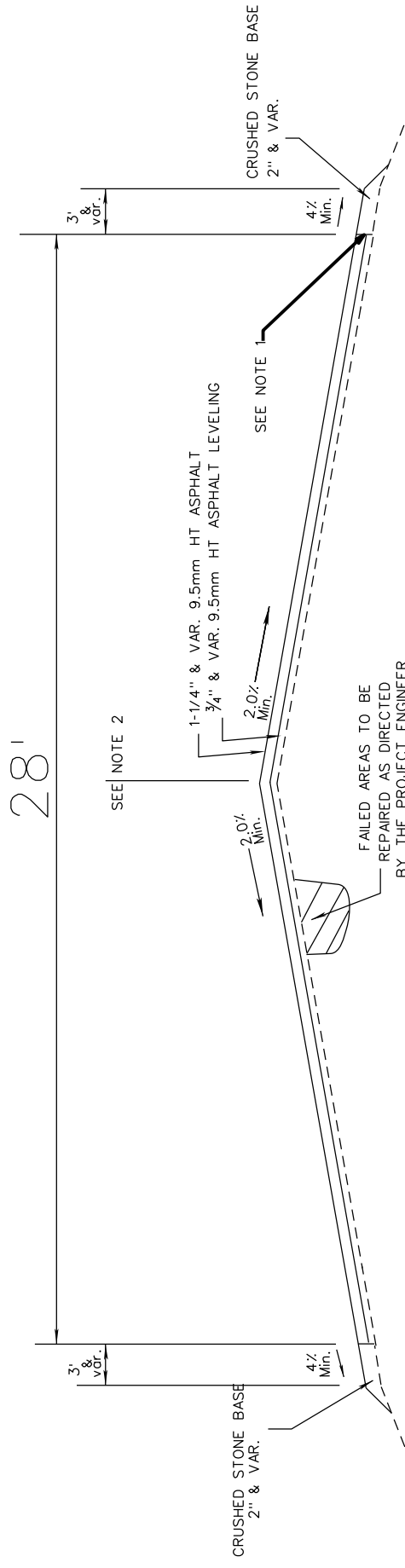
Incidental work such as removing vegetation, shaping and compaction of shoulder, removing excess asphalt material, project clean-up, and other incidental work necessary to complete the project will not be measured for separate payment, but will be included in other bid items, and must be performed during the operating hours for this project.

TABLE 1

Removal Locations			
Lane	Station	Length	Width
EB RT	1083+79 to 1089+21	542	14
EB RT	1096+52 to 1101+65	513	14
EB RT	1120+95 to 1121+81	86	14
EB RT	1138+04 to 1139+05	101	14
EB RT	1140+91 to 1143+68	277	14
EB RT	1199+48 to 1200+07	59	14
EB RT	1202+04 to 1202+86	82	14
EB RT	1204+80 to 1206+13	133	14
EB RT	1215+47 to 1220+76	529	14
EB RT	1244+27 to 1246+44	217	14
EB RT	1309+57 to 1311+25	166	14
EB RT	1238+08 to 1238+62	54	7
EB RT	1239+79 to 1240+80	101	7
EB RT	1241+66 to 1242+16	50	7
EB RT	1243+67 to 1244+27	60	7
EB RT	1320+99 to 1323+15	216	7
EB CL	1200+07 to 1202+04	197	7
EB CL	1208+94 to 1212+40	346	7
EB CL	1103+28 to 1109+46	618	7
EB CL	1111+12 to 1117+14	602	7
EB CL	1131+25 to 1131+72	43	7
EB CL	1139+05 to 1140+82	177	7
WB LT	1074+23 to 1074+75	52	14
WB LT	1091+16 to 1091+88	72	14
WB LT	1093+79 to 1094+24	45	14
WB LT	1113+87 to 1114+24	37	14
WB LT	1139+19 to 1139+92	73	14
WB LT	1199+95 to 1200+52	57	14
WB LT	1261+04 to 1261+83	79	14



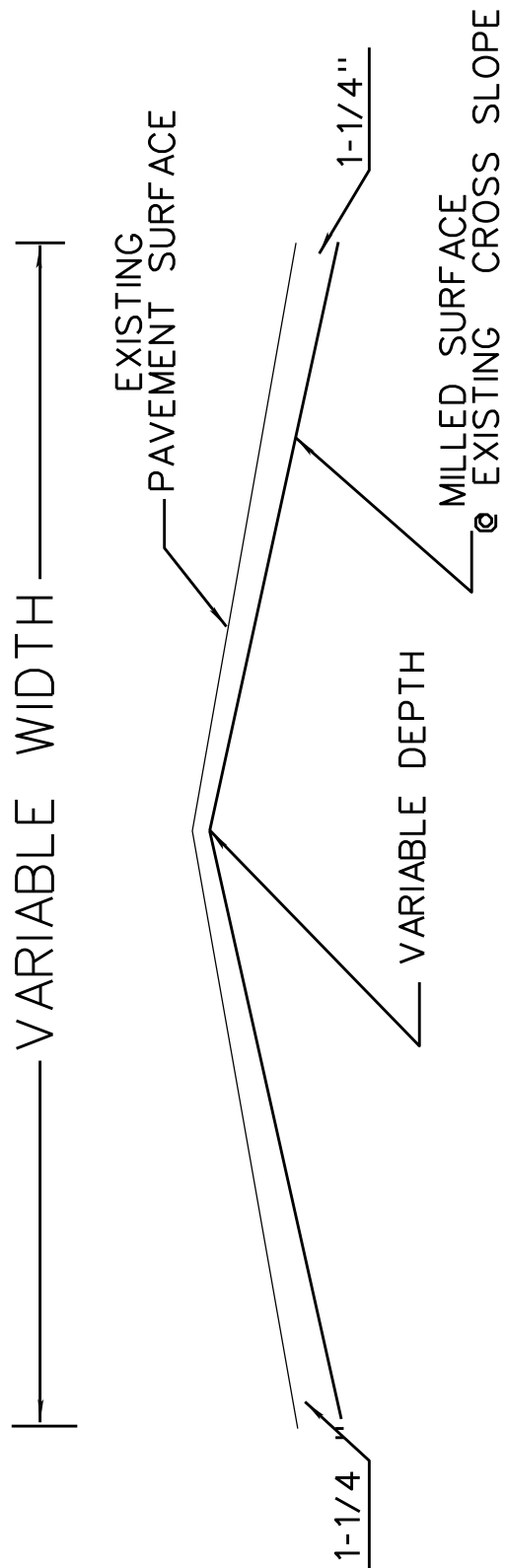
THE ABOVE REFERENCED PROJECT IS FOR LEVELING AND OVERLAYING US 98 FROM THE PERRY - GREENE COUNTY LINE, EAST 5.45 MILES TO 1.7 MILES EAST OF SR 57



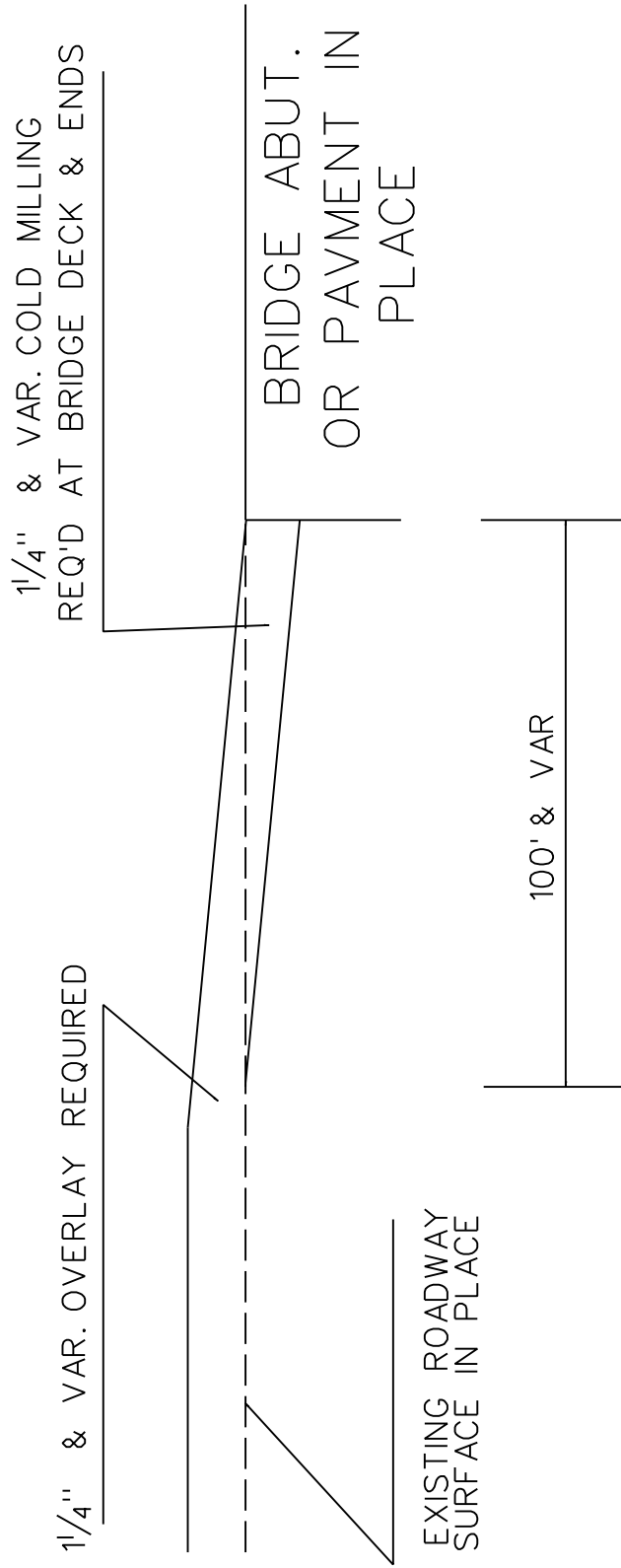
NOTES:

- (1) THE EXISTING PAVEMENT EDGE SHALL BE CUT TO A SMOOTH AND NEAR VERTICAL FACE WITH AN APPROVED CUTTING DEVICE. (NOTO MEASURED FOR SEPARATE PAY.)
- (2) TRUE CENTERLINE PAVEMENT ALIGNMENT SHALL BE DETERMINED BY THE CONTRACTOR BY MEASURING THE EXISTING ROADWAY AT 500ft. INTERVALS IN TANGENT SECTIONS, AND 100ft. INTERVALS IN HORIZONTAL CURVES.

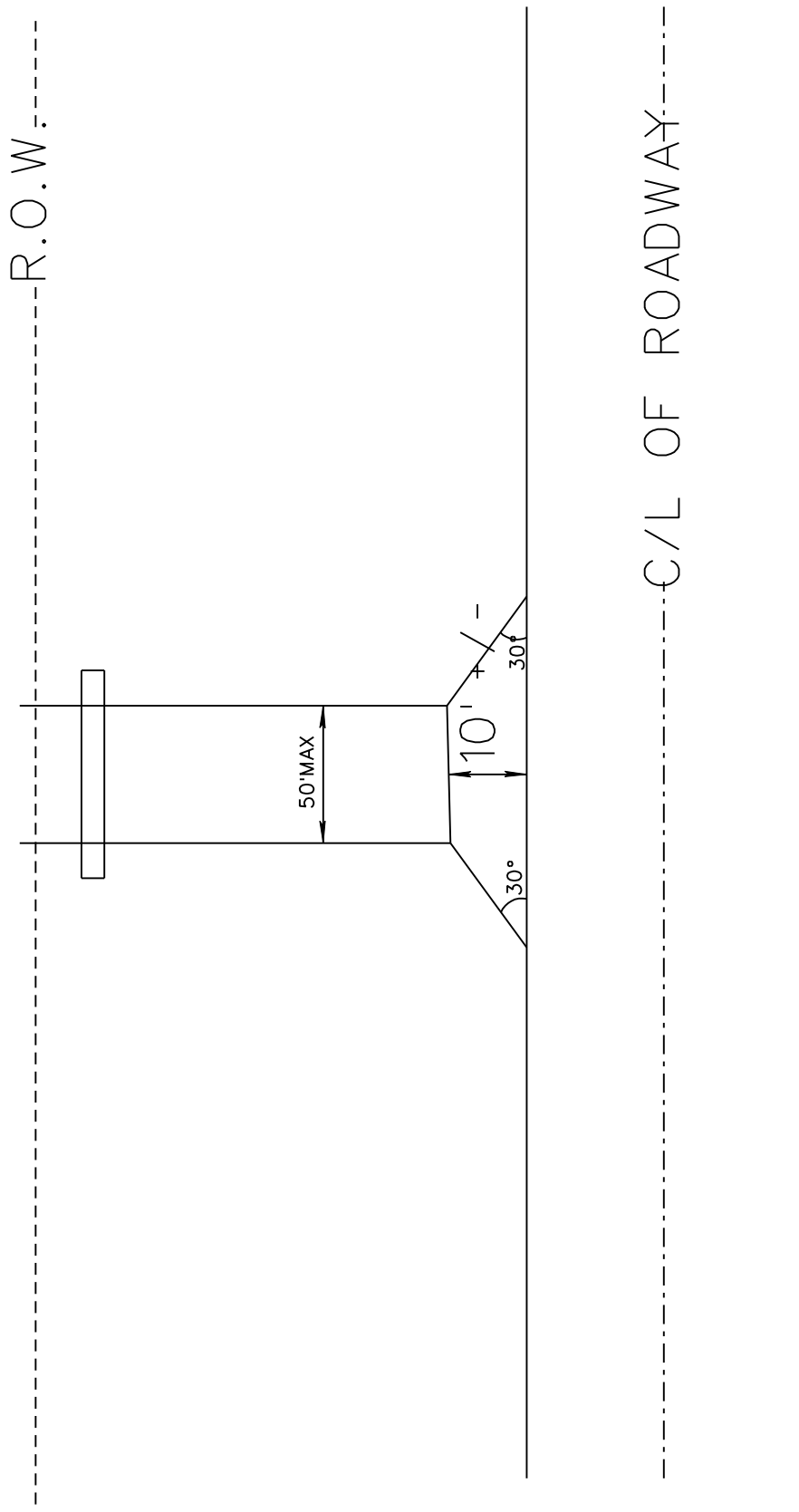
TYPICAL MILLING DIAGRAM



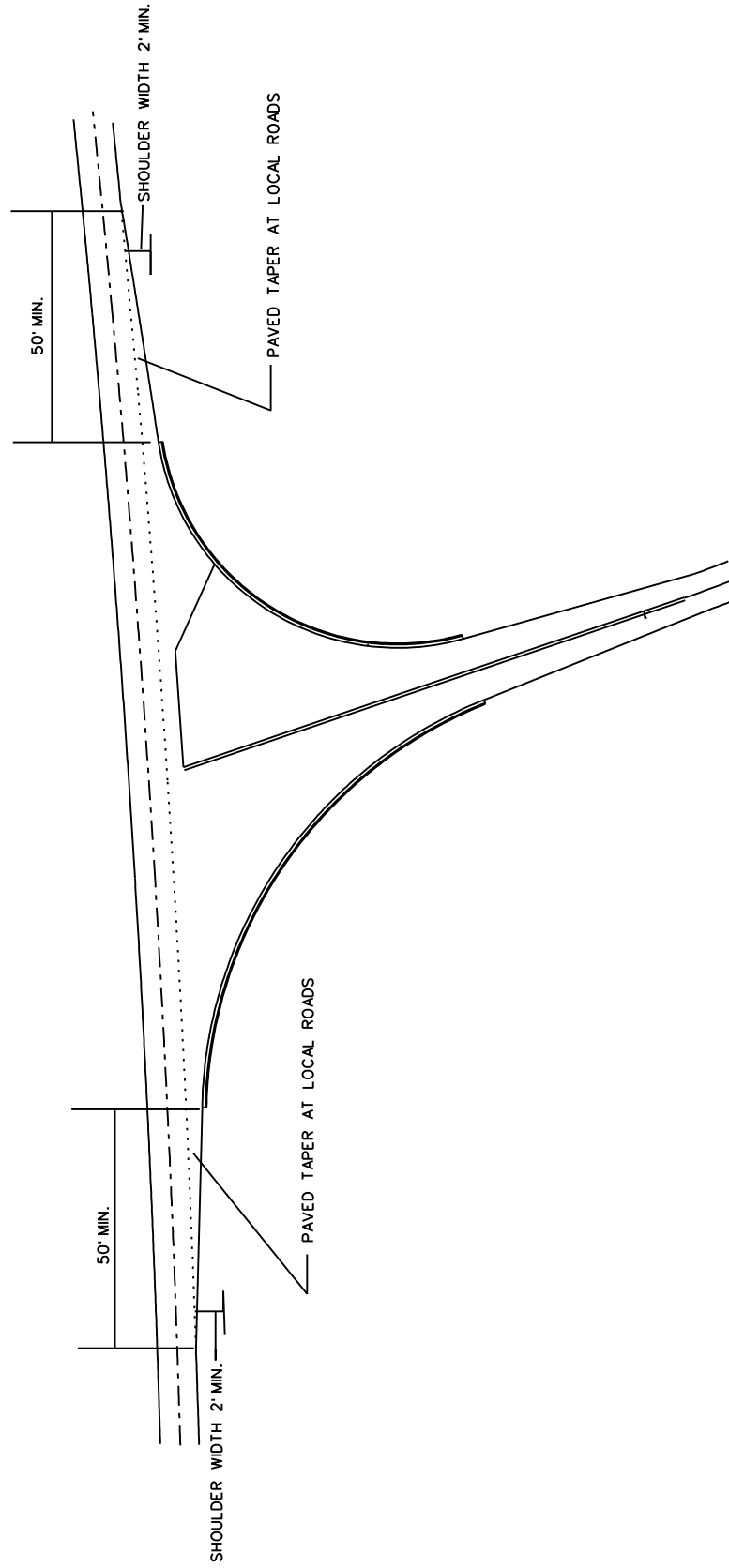
TYPICAL MILLED TRANSITION AT BRIDGE ABUT. OR PAVEMENT IN PLACE



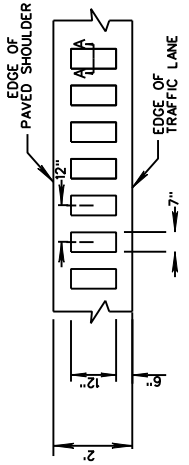
PRIVATE DRIVEWAY DETAIL



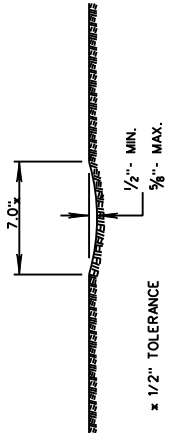
TYPICAL FOR PAVED TAPER AT LOCAL ROADS



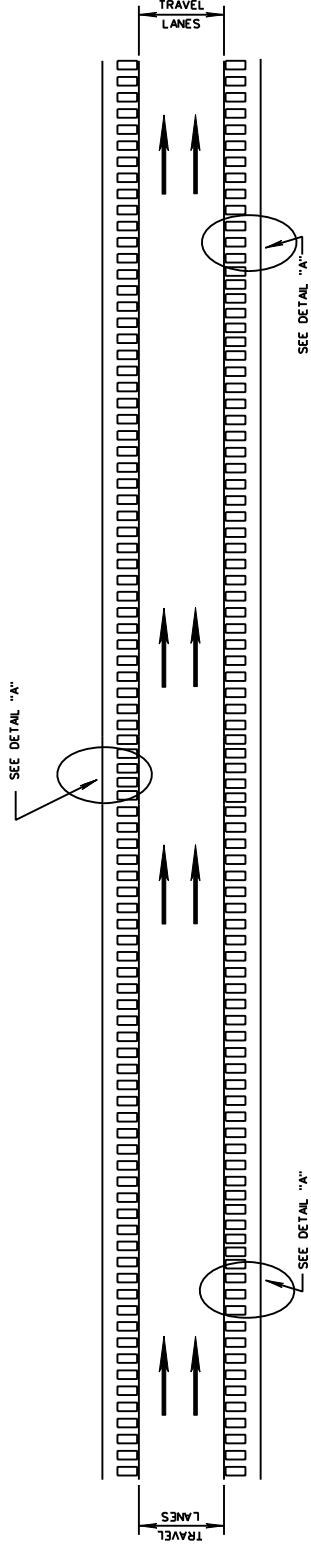
- GENERAL NOTES**
- GROUND-IN RUMBLE STRIPS SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
 - GROUND-IN RUMBLE STRIPS SHALL BE APPLIED TO MAINLINE ONLY.



DETAIL "A"



SECTION "A-A"



PLAN
NOT TO SCALE
DETAIL OF
RUMBLE STRIPS
(GROUND-IN)

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
TYPICAL SECTION	
RUMBLE STRIPS (GROUND-IN)	
WORKING NUMBER	DATE
RS	
SHEET NUMBER	
FILE NAME:	DESIGN TITLE
	DATE
	DESIGNER

TYPICAL STRIPING FOR SIMPLE INTERSECTION AT LOCAL ROADS

