

## SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO.	<u>1</u>	DATED	<u>2/17/2022</u>	ADDENDUM NO.	_____	DATED	_____
ADDENDUM NO.	_____	DATED	_____	ADDENDUM NO.	_____	DATED	_____
ADDENDUM NO.	_____	DATED	_____	ADDENDUM NO.	_____	DATED	_____

Number

Description

- 1 Revised Notice To Bidders No. 3897; Revised Bid Items; Amendment EBSx Download Required.

TOTAL ADDENDA: 1

(Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE \_\_\_\_\_

Contractor

BY \_\_\_\_\_

Signature

TITLE \_\_\_\_\_

ADDRESS \_\_\_\_\_

CITY, STATE, ZIP \_\_\_\_\_

PHONE \_\_\_\_\_

FAX \_\_\_\_\_

E-MAIL \_\_\_\_\_

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of \_\_\_\_\_ and the names, titles and business addresses of the executives are as follows:

President

Address

Secretary

Address

Treasurer

Address

The following is my (our) itemized proposal.

SP-0491-00(003)/ 108799301000 & SP-0493-00(001)/ 108799302000

Hinds County(ies)

Revised 01/26/2016

# MISSISSIPPI DEPARTMENT OF TRANSPORTATION

**SECTION 904 - NOTICE TO BIDDERS NO. 3897**

**CODE: (SP)**

**DATE: 02/17/2022**

**SUBJECT: Scope of Work**

**PROJECT: SP-0491-00(003) & SP-0493-00(001) / 108799301 & 302 – Hinds County**

The contract documents do not include an official set of construction plans but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled “Standard Drawings”.

A general description of the work required on the project is as follows:

## SR 890

Approximately 1.5 miles of SR 890 shall be milled and overlaid beginning at the Beginning of State Maintenance (BOP Station 100+00) and ending at the End of State Maintenance near the Bolton Corp. Limit (EOP Station 178+00).

From B.O.P. to Sharon Street, the existing lanes, paved shoulders, and downtown parking area shall be milled two inches (2”).

From Sharon Street to E.O.P. in eastbound lane, the existing lane and paved shoulder shall be milled two inches (2”). In the westbound lane, the existing lane to one foot (1’) beyond the edge stripe on paved shoulder shall be milled two inches (2”). The remaining asphalt paved shoulder will not be milled/overlaid due to the presence of water.

The project shall be overlaid with 2” & variable of 12.5-mm, ST, asphalt keying the new mix into existing westbound lane left in-place.

## SR 855

Approximately 0.75 miles of SR 855 shall be milled and overlaid beginning at the Beginning of State Maintenance near Texas Street (BOP Station 10+00) and ending at End of State Maintenance north of I-20 (EOP Station 48+85).

From B.O.P. to Sharon Street, the existing lanes, paved shoulders, and downtown parking area shall be milled two inches (2”).

Failed areas shall be repaired full depth with 12.5-mm, ST, asphalt per the attached detail.

## **SR 890 from Station 100+00 (BOP) to Station 178+00**

Work in this area shall consist of milling 2” and inlaying with 2” of 12.5-mm, ST, asphalt. Failed/ JRCP joints areas listed in the provided table shall be repaired full depth with 12.5-mm, ST, asphalt; see attached detail.

**SR 855 from Station 10+00 (BOP) to Station 48+85**

Work in this area shall consist of milling 2" and inlaying with 2" of 12.5-mm, ST, asphalt. Failed areas listed in the provided table shall be repaired full depth with 12.5-mm, ST, asphalt. Guardrails shall be replaced. (See General Notes and table in proposal).

**GENERAL NOTES:**

**JOINT REPAIR**

Prior to the milling and paving operations, failed areas and failed JRCP joints in the existing pavement shall be repaired as per the attached table and typical sections. Pay item 403-B: 12.5-mm, ST, Leveling asphalt will be used for all failed area locations. Full depth saw cuts shall be used for all failed area locations. Any granular base deemed unsuitable by the Engineer shall be removed as directed and backfilled with 12.5-mm ST, Leveling asphalt. Payment for the excavation of the granular base will be made using the pay item 203-G: Excess Excavation. A list of the failed areas is shown in the attached table. Pavement repairs shall be completed as a continuous operation in order to minimize the traffic impact. Lane closures shall be in place until the failed area has been completely repaired. Night time lane closures will NOT be allowed.

**MILLING**

Milling/paving shall not begin until an approved asphalt mix design has been received, nor until such time that, in the opinion of the Engineer, weather conditions have been consistently suitable enough to allow placement of the asphalt pavement after the milling operations.

The Reclaimed Asphalt Pavement (RAP) material removed by the milling operation shall become the property of the Contractor.

Milling and paving operations shall be performed such that a -2% slope from centerline is provided in normal crown roadway sections. Superelevation (SE) through curves shall be maintained as it currently exists or improved as directed. Where slope correction is required correction shall be made by milling, paving, or combination thereof as directed by the Engineer. Milling correction: Mill the outside edge of pavement to a depth of 1½" on a 2% slope towards the centerline. Paving Correction: Mill to depth of 1½" on existing slope and 2¼" and variable on centerline and 1½" on outside edge. Combination Method: Combination of both methods as directed by the Engineer to achieve the desired slope. In super elevated areas where correct SE exist, milling shall transition to thickness through curves. Where correct SE does not exist, milling shall transition at curves to correct SE as directed by the Engineer.

Milling operations shall be performed in accordance with the Contract Documents and the Standard Specifications. Variable width and length transitions may be required for ties at ramps, local roads, project limits.

Milling of driveway pads shall be conducted in a manner to prevent gouging or otherwise affecting the roadway pavement structure and slope. Milling of driveway pads shall not be performed in simultaneous path with main line milling.

Traffic will be allowed to travel on the milled surface for three (3) days. Traffic will be allowed to run on all milled local roads for three (3) days unless otherwise stated. The Contractor will be assessed a penalty of **\$5,000 per calendar day** afterwards until the milled surfaces are covered with the next lift of asphalt. Approved mix designs shall be on hand prior to milling. Milling operations will not commence until such time that, in the opinion of the Engineer, weather conditions have been consistently suitable enough to allow the placement of the Asphalt Pavement after the milling operations. Night time lane closures will NOT be allowed.

## **PAVING**

Prior to mainline milling and paving operations, failed areas in the existing pavement shall be removed and backfilled with 12.5-mm, ST, Leveling asphalt as per the attached typical sections and details. Asphalt shall be placed in multiple lifts with a maximum lift thickness of three inches (3"). Any granular/chemically treated/stone/etc. base or subgrade material deemed unsuitable by the Engineer shall be removed as directed and backfilled with 12.5-mm, ST, Leveling asphalt. Payment for the excavation of the granular base and subgrade will be made using pay item 203-G: Excess Excavation. A list of the failed areas is shown in the attached tables. Pavement repairs shall be completed as a continuous operation in order to minimize traffic impacts. Lane closures shall remain in place until the failed area has been completely repaired. Lane closures shall not be left unattended. Night time lane closures will NOT be allowed.

If traditional excavation methods are used, the removal area shall first be saw cut full depth including concrete, where applicable, to create a neat line and prevent damage to the adjacent pavement structure. Payment for saw cuts will be made using the appropriate items. If milling techniques are used, the area will not require saw cuts but care should be exercised to create a neat removal line and to prevent damaged to the adjacent pavement structure. If saw cuts are used in conjunction with milling, payment will be made using the appropriate pay items. Payment will not be made for saw cuts that are not performed.

Publicly maintained roads and streets should be paved to the existing right-of-way and in accordance with the attached drawings.

Privately owned entrances shall be paved to the shoulder line per the included typical drawing unless otherwise directed. Pad dimensions shall match the existing lengths and widths unless otherwise directed. Pads shall be shaped horizontally and vertically to prevent excessive drop-offs. Any new driveway pads deemed necessary by the Engineer shall be placed according to specifications.

## **GRANULAR SHOULDER MATERIAL**

Where applicable, the existing shoulders shall be raised to match the new pavement elevation by placing variable depth granular material. The shoulders shall be graded and pulled up on a daily basis to eliminate drop-offs in excess of two inches (2)". Placement of the granular material on the finished asphalt course shall not be permitted. The existing shoulder shall be scarified to allow incorporation of the new shoulder material. The material shall be bladed, rolled, and compacted to a finished slope of four percent (4%) in normal crown sections. Placement of this material shall be performed to provide a uniform and compacted shoulder with a minimum depth and width of material placed.

Shoulders with adequate shoulder material in place shall be bladed to a slope of four percent (4%) in normal crown sections. The cost of blading will not be measured for separate payment and such costs shall be included in the price of other items bid. Crushed concrete will not be allowed if crushed stone is used for shoulder material.

Granular material (Class 5, Group "C") shall be provided around driveway pads as directed to prevent shoulder drop-offs and shall be placed in a timely manner. Drop-offs exceeding 2½" shall be corrected within two (2) calendar days of the placement of the pad.

#### **TEMPORARY AND PERMANENT PAVEMENT MARKINGS**

Temporary traffic stripe will be required immediately after the milling and/or required overlay and prior to opening area to traffic. Temporary stripe shall be placed in the same location and configuration as the permanent stripe except that it may be offset as required for milling and paving operations. If temporary stripe is offset, the Contractor shall conduct operations in a manner to insure the final temporary stripe is placed at the required location of the permanent stripe. If removal of temporary offset stripe is required in order to achieve the correct location and alignment of permanent stripe, the cost of removal will be absorbed in other items bid. Placing double temporary centerline will not be allowed.

Temporary striping shall conform to finished stripe specifications for alignment, neatness, and straightness.

The use of short strips of traffic tape will not be allowed unless approved by the Engineer.

Temporary stripe of the appropriate color on parking areas shall be placed within 24 hours of both the milling operation and the following paving operation as directed by the Engineer. Blue ADA stripe will be required for corresponding ADA parking spots. The ADA symbols may be omitted until final thermoplastic is placed.

The use of short strips of traffic tape will not be allowed unless approved by the Engineer. All permanent striping shall be double drop thermoplastic, 90-mil thickness unless otherwise specified in Section 626. Edge lines shall be placed to accommodate the lane widths shown on the attached applicable typical sections unless prevented by field conditions.

#### **GUARDRAIL**

Guardrails shall be replaced at the locations shown on the attached table. Removal of guardrail shall consist of removal of bridge end section, w-beam/thrie beam, terminal end section, posts, and all other appurtenances. All guardrail removed shall be replaced the same day and prior to reopening the adjacent lane of traffic. Voids created by the removal of posts, concrete anchors, footings, etc. shall be backfilled and compacted in accordance with Section 203 of the Standard Specifications.

Asphalt shall be extended two feet (2') behind the guardrail per the attached detail. The area to be paved shall be bladed to accommodate three inches (3") of asphalt pavement. The elevation of the finished surface of the asphalt pavement shall provide for the required MASH guardrail

height (see Standard Drawings).

The existing guardrail bridge end sections at Stations 36+27, 37+24, and both at 41+19 shall be replaced in-kind. The existing connection brackets are to be preserved, and new w-beam shapes are to be installed as needed. Transitions to MASH compliant guardrail heights shall be made over the length of the bridge end section (25-foot pay length). The existing guardrail pads shall be milled and paved to the face of the guardrail. The elevation of the finished surface of the asphalt pavement shall provide for the required MASH guardrail height (see Standard Drawings). Holes or voids created by the removal and installation of new guardrail posts shall be backfilled with suitable granular material. Guardrail lengths are based on terminal end length of 37.5'. If terminal of length other than this is used, an adjustment in w-beam length is required.

### **TRAFFIC CONTROL**

The Contractor shall erect and maintain construction signing and provide all signs and traffic control devices necessary to safely maintain traffic around and through the work areas in accordance with the Traffic Control Plan and the MUTCD. The cost is to be included in the price bid for pay item 618-A: Maintenance of Traffic. Fluorescent orange sheeting shall be used on all construction and traffic control signs except those designated in the plans to be black legend and border on white background.

Standard roadside construction signs, barricades, etc. shall be placed in accordance with the attached tables, drawings, and as directed by the Engineer. W20-1 signs shall be placed on all public road approaches as shown or as directed. Payment for standard roadside construction signs, barricades, etc. will be made using the appropriate pay items.

The Contractor shall on a daily basis, remove all debris from within the roadway and a 30-foot clear zone which, in the opinion of the Engineer, is a hazard to the traveling public. This activity shall begin with the beginning of work or the beginning of the contract time, whichever comes first. No direct payment will be made for the debris removal. The cost shall be included in the prices of other items bid. Failure of the Contractor to remove the debris as prescribed herein shall be just cause for withholding the monthly progress estimate payment or suspending active operations until the debris is satisfactorily removed by the Contractor.

Temporary asphalt joints (aka paper joints) shall be employed at all locations requiring traffic to traverse an uneven, transverse, pavement joint. Paper joints shall be a minimum of nine feet (9') in length and for the full width of the milled/paved surface. Paper joints for 1-inch OGFC joints shall be a minimum of three feet (3') in length. Paper joints shall be adequately maintained.

Potholes that may exist or occur in the existing pavement shall be patched in a timely manner as required. Patching of potholes shall be considered an absorbed item.

### **MISCELLANEOUS NOTES**

It shall be the responsibility of the Contractor to protect existing structures such as pipes, inlets, aprons, bridges, etc. from damage which might occur during construction. The Contractor shall replace or repair, as directed by the Engineer, any structures damaged by the Contractor's during the life of the contract. No payment will be made for replacement or repair of damaged items.

Any signs that are in conflict with construction of this project shall be removed and relocated by the Contractor as directed by the Engineer. The cost of which shall be absorbed in other items bid.

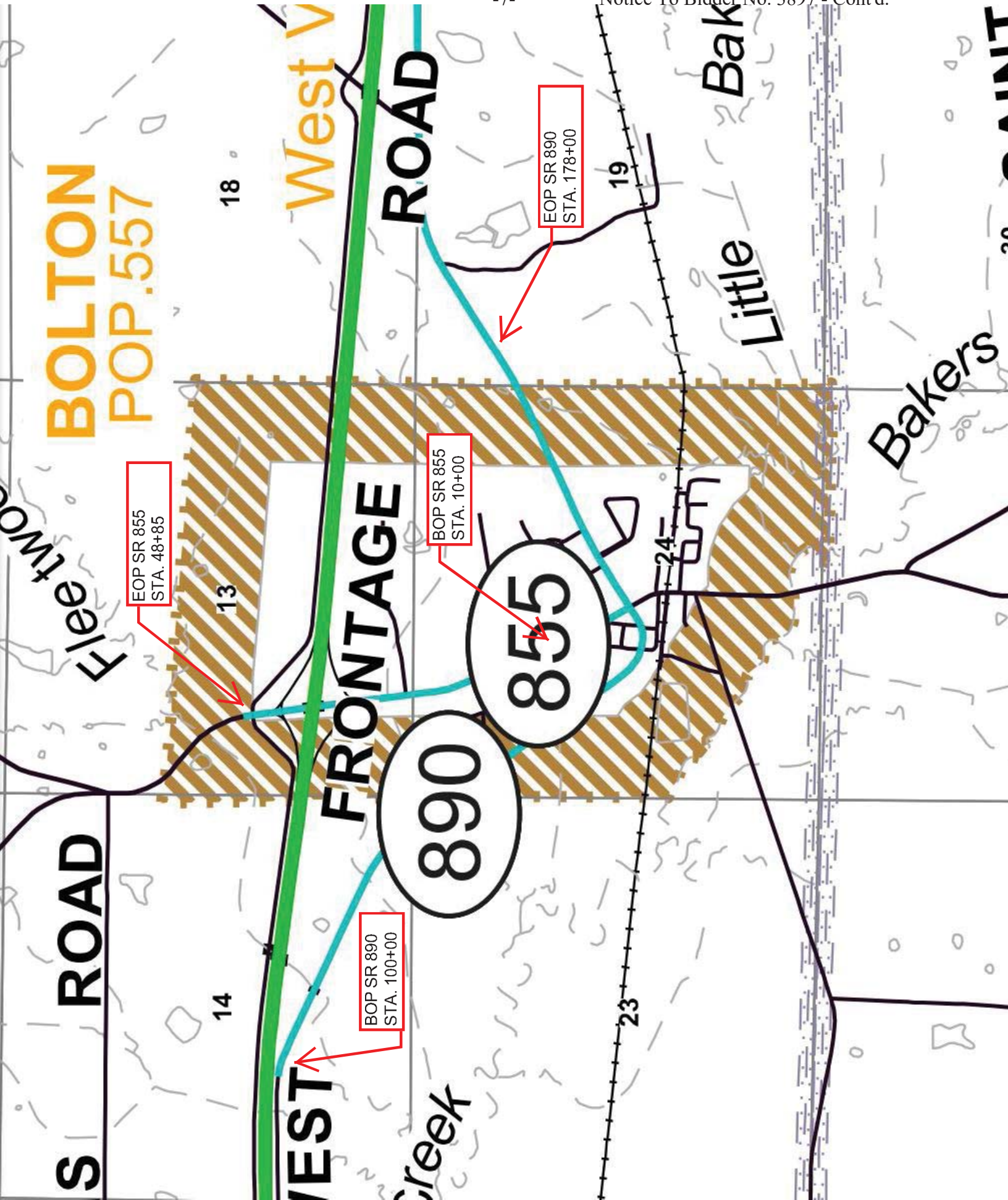
Removal of existing raised pavement markers shall be included in the prices of other items bid.

Incidental work such as removing vegetation, shaping and compacting shoulders, removing and resetting signs and/or mailboxes, removing excess asphalt material, project clean-up, and other items of incidental work necessary to complete the project will not be measured for separate payment and will be considered included in the prices of items bid.

Prior to the final inspection, bridges, islands, and areas with curb shall be swept/cleaned. Care should be taken to prevent milled asphalt, asphalt debris, vegetative/granular debris, etc. from entering drainage structures or clogging other drainage ways. Disposal of material will not be measured for separate payments.

Following the overlaying operation, the transverse joints in the pavement shall be sawed and sealed within seven (7) days. The details for sawing and sealing transverse joints for this section are in the Standard Specifications. The width of the sawing and sealing operation will be 14 feet on each side of centerline, unless otherwise directed by the Engineer, to prevent "sympathy cracking." It is the responsibility of the Contractor to locate and mark all existing joints that are to be sawed and sealed prior to the milling operation. The Contractor is to notify the Department when this is to take place so that they can oversee the work and determine the width that each joint will be sawed and sealed.







STATE	PROJECT NO.
MISS	SP-0493-00(001)

SUMMARY OF QUANTITIES (SHEET 1)

PAY ITEM NO.	PAY ITEM	UNIT	HINDS : 108799-302000	
			Prelim	Final
202-B009	Removal of Asphalt Pavement, Failed Areas	SY	448	
202-B158	Removal of Guard Rail, Including Rails, Posts and Terminal Ends	LF	664	
203-G001	Excess Excavation, FM, AH	CY	81	
304-B003	Granular Material, Class 5, Group C	TON	239	
403-A003	12.5-mm, ST, Asphalt Pavement	TON	1,869	
403-B003	12.5-mm, ST, Asphalt Pavement, Leveling	TON	145	
406-D001	Fine Milling of Bituminous Pavement, All Depths	SY	16,178	
407-A001	Asphalt for Tack Coat	GAL	2,022	
503-C010	Saw Cut, Full Depth	LF	1,090	
606-B002	Guard Rail, Class A, Type 1, 'W' Beam	LF	500	
606-E001	Guard Rail, Terminal End Section	EA	4	
606-G001	Special Sections, Bridge Connector	EA	4	
618-A001	Maintenance of Traffic	LS	1	
619-A1001	Temporary Traffic Stripe, Continuous White	MI	5	
619-A2001	Temporary Traffic Stripe, Continuous Yellow	MI	5	
619-A5001	Temporary Traffic Stripe, Detail	LF	11,566	
619-A6002	Temporary Traffic Stripe, Legend	LF	5,988	
907-619-B001	Temporary Portable Rumble Strips	LF	66	
619-D1001	Standard Roadside Construction Signs, Less than 10 Square Feet	SF	16	
619-D2001	Standard Roadside Construction Signs, 10 Square Feet or More	SF	96	
619-G4001	Barricades, Type III, Double Faced	LF	24	
620-A001	Mobilization	LS	1	
626-B002	6" Thermoplastic Double Drop Traffic Stripe, Continuous White	MI	2	
626-E001	6" Thermoplastic Double Drop Traffic Stripe, Continuous Yellow	MI	2	
626-G004	Thermoplastic Double Drop Detail Stripe, White	LF	1,859	
626-G005	Thermoplastic Double Drop Detail Stripe, Yellow	LF	657	
626-H002	Thermoplastic Double Drop Legend, White	LF	824	
627-K001	Red-Clear Reflective High Performance Raised Markers	EA	318	
627-L001	Two-Way Yellow Reflective High Performance Raised Markers	EA	97	
630-F006	Delineators, Guard Rail, White	EA	32	
630-G004	Type 3 Object Markers, OM-3R or OM-3L	EA	4	

	By	MISSISSIPPI DEPARTMENT OF TRANSPORTATION		
	Revision	SUMMARY OF QUANTITIES		
		PROJ NO: SP-0493-00(001)	Working Number	
		COUNTY: HINDS	SQ-1	
	Date	FILENAME: 855 SQS	Sheet Number	1
		Design Team	Checked	Date

STATE	PROJECT NO.
MISS	SP-0491-00(003)

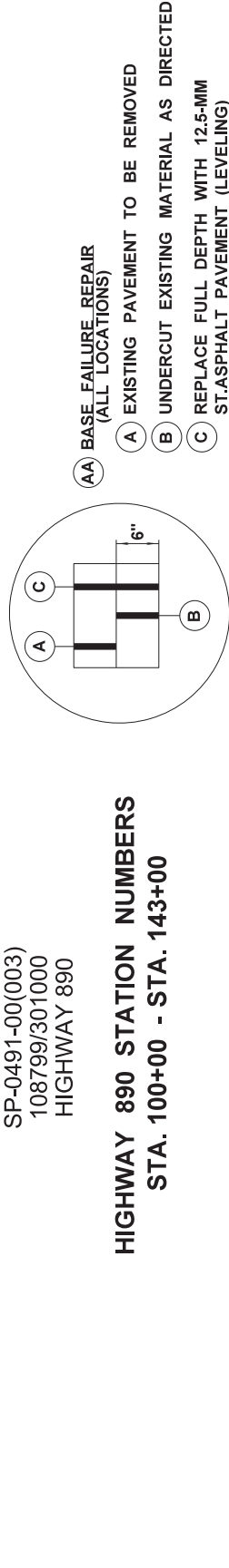
SUMMARY OF QUANTITIES (SHEET 1)

PAY ITEM NO.	PAY ITEM	UNIT	HINDS : 108799-301000	
			Prelim	Final
202-B069	Removal of Concrete Pavement w/ Variable Depth Overlay	SY	263	
203-G001	Excess Excavation, FM, AH	CY	44	
304-B003	Granular Material, Class 5, Group C	TON	246	
403-A003	12.5-mm, ST, Asphalt Pavement	TON	2,754	
403-B003	12.5-mm, ST, Asphalt Pavement, Leveling	TON	106	
406-D001	Fine Milling of Bituminous Pavement, All Depths	SY	23,838	
407-A001	Asphalt for Tack Coat	GAL	2,411	
413-D002	Cleaning and Filling Joints	LF	660	
413-E001	Sawing and Sealing Transverse Joints in Asphalt Pavement	LF	8,134	
503-C010	Saw Cut, Full Depth	LF	560	
618-A001	Maintenance of Traffic	LS	1	
619-A1001	Temporary Traffic Stripe, Continuous White	MI	9	
619-A2001	Temporary Traffic Stripe, Continuous Yellow	MI	9	
619-A5001	Temporary Traffic Stripe, Detail	LF	4,440	
619-A6002	Temporary Traffic Stripe, Legend	LF	1,980	
907-619-B001	Temporary Portable Rumble Strips	LF	66	
619-D1001	Standard Roadside Construction Signs, Less than 10 Square Feet	SF	16	
619-D2001	Standard Roadside Construction Signs, 10 Square Feet or More	SF	244	
619-G4001	Barricades, Type III, Double Faced	LF	24	
620-A001	Mobilization	LS	1	
626-B002	6" Thermoplastic Double Drop Traffic Stripe, Continuous White	MI	3	
626-E001	6" Thermoplastic Double Drop Traffic Stripe, Continuous Yellow	MI	3	
626-G001	Thermoplastic Detail Stripe, Blue-ADA	LF	48	
626-G004	Thermoplastic Double Drop Detail Stripe, White	LF	1,376	
626-G005	Thermoplastic Double Drop Detail Stripe, Yellow	LF	194	
907-626-H001	Thermoplastic Legend, Blue-ADA Handicap Symbol	EA	1	
626-H002	Thermoplastic Double Drop Legend, White	LF	1,102	
627-J001	Two-Way Clear Reflective High Performance Raised Markers	EA	196	
627-L001	Two-Way Yellow Reflective High Performance Raised Markers	EA	195	

	By	MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
	Revision	SUMMARY OF QUANTITIES	
		PROJ NO: SP-0491-00(003)	Working Number
		COUNTY: HINDS	SQ-1
	Date	FILENAME: SQS 108799-301000	Sheet Number
		Design Team	1
		Checked	Date

HINDS COUNTY  
SP-0491-00(003)  
108799/301000  
HIGHWAY 890

**HIGHWAY 890 STATION NUMBERS**  
**STA. 100+00 - STA. 143+00**



**EXISTING**

- 1- 4" - 5 1/4" and variable HMA over 6" plain or reinforced joint concrete pavement
- 2- Cement-Treated Clay Gravel
- = Overlay with 2" 12.5mm, Mix, ST
- = Repair area
- = Existing asphalt
- = Cement-Treated Clay Gravel

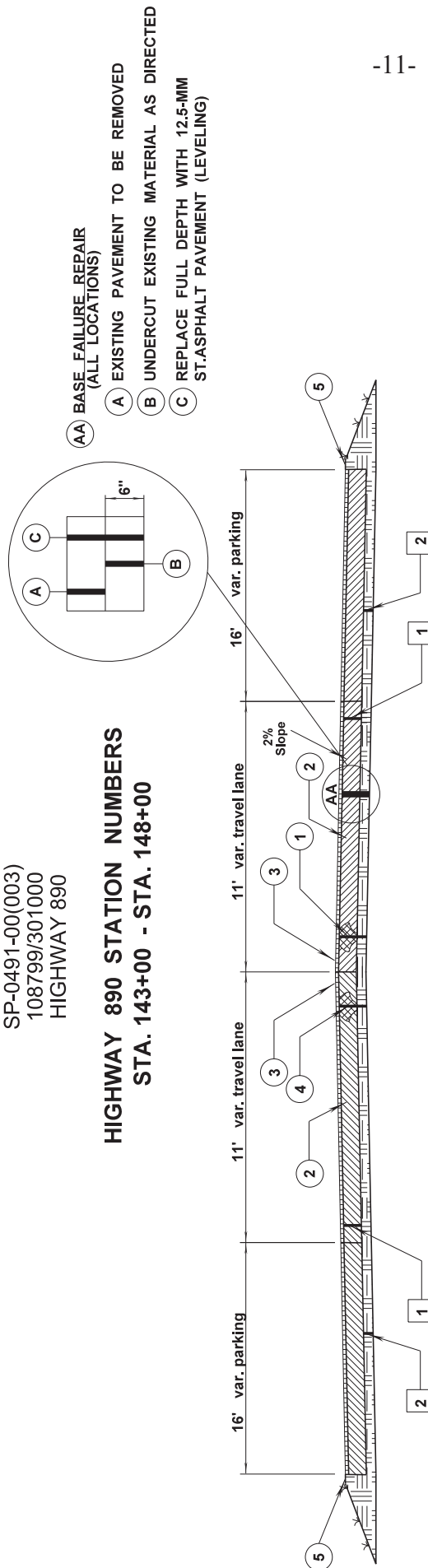
**PROPOSED**

- 1 Repair failed areas with 12.5mm, Mix ST Leveling full depth.
- 2 Mill existing asphalt 2" and variable.
- 3 Overlay with 2" and variable of 12.5mm Mix, ST.
- 4 Repair any failed JRPC joints full depth to 3' width on either side of joint (6' total width) by removal of existing concrete. Repair with 12.5mm mix, ST LEVELING
- 5 Place 2" & Var. Granular Material on shoulder as directed by the Engineer

\* In some areas, concrete was previously removed. Pavement consists of 10" +/- asphalt pavement.

HINDS COUNTY  
 SP-0491-00(003)  
 108799/301000  
 HIGHWAY 890

# **HIGHWAY 890 STATION NUMBERS** **STA. 143+00 - STA. 148+00**



## **EXISTING**

- 1 - 4" - 5 1/4" and variable HMA over 6" plain or reinforced joint concrete pavement
- 2 - Cement-Treated Clay Gravel
- = Overlay with 2" 12.5mm, Mix, ST
- = Repair area
- = Existing asphalt
- = Cement-Treated Clay Gravel

## **PROPOSED**

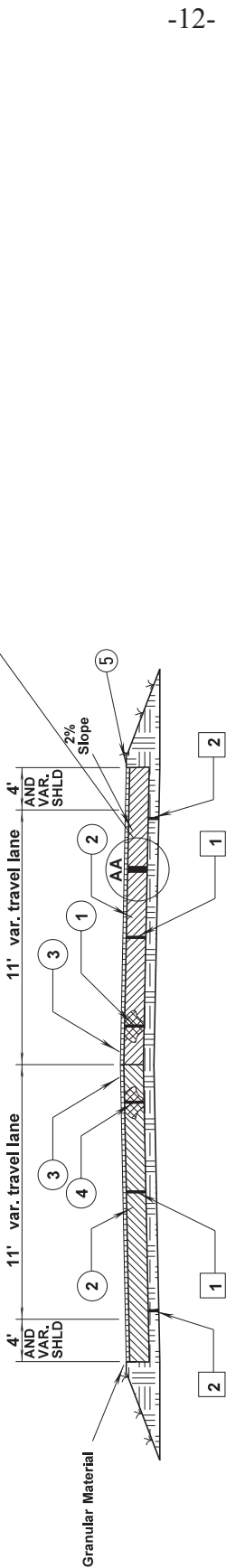
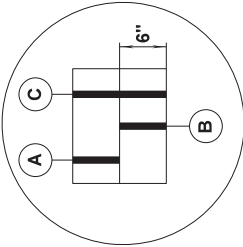
- 1 Repair failed areas with 12.5mm, Mix ST Leveling full depth.
- 2 Mill existing asphalt 2" and variable.
- 3 Overlay with 2" and variable of 12.5mm Mix, ST.
- 4 Repair any failed JRC joints full depth to 3' width on either side of joint (6' total width) by removal of existing concrete. Repair with 12.5mm mix, ST LEVELING
- 5 Place 2" & Var. Granular Material on shoulder as directed by the Engineer

\* In some areas, concrete was previously removed. Pavement consists of 10" +/- asphalt pavement.

HINDS COUNTY  
SP-0491-00(003)  
108799/301000  
HIGHWAY 890

**HIGHWAY 890 STATION NUMBERS  
STA. 148+00 - STA. 158+00**

- AA) BASE FAILURE REPAIR  
(ALL LOCATIONS)
- A) EXISTING PAVEMENT TO BE REMOVED
- B) UNDERCUT EXISTING MATERIAL AS DIRECTED
- C) REPLACE FULL DEPTH WITH 12.5-MM ST.ASPHALT PAVEMENT (LEVELING)



**EXISTING**

- 1 - 4" - 5 1/4" and variable HMA over 6" plain or reinforced joint concrete pavement\*
- 2 - Cement-Treated Clay Gravel
- 3 - Overlay with 2" 12.5mm,Mix, ST
- 4 - Repair area
- 5 - Existing asphalt
- 6 - Cement-Treated Clay Gravel

**PROPOSED**

- 1 Repair failed areas with 12.5mm, Mix ST Leveling full depth.
- 2 Mill existing asphalt 2" and variable.
- 3 Overlay with 2" and variable of 12.5mm Mix, ST.
- 4 Repair any failed JRCP joints full depth to 3' width on either side of joint (6' total width) by removal of existing concrete. Repair with 12.5mm mix, ST LEVELING
- 5 Place 2" & Var. Granular Material on shoulder as directed by the Engineer

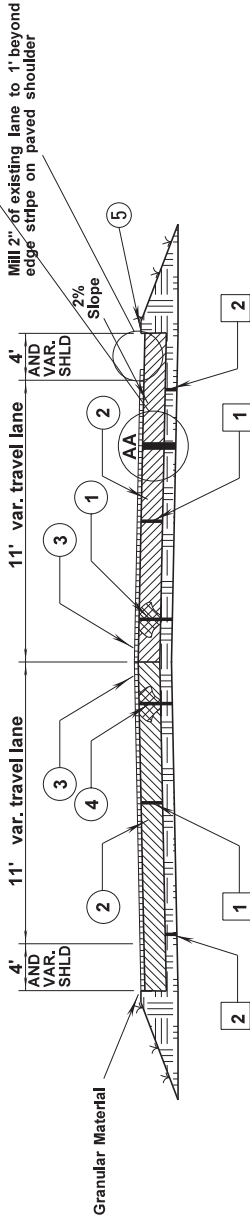
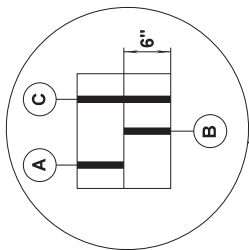
\* In some areas, concrete was previously removed. Pavement consists of 10" +/- asphalt pavement.



HINDS COUNTY  
SP-0491-00(003)  
108799/301000  
HIGHWAY 890

**HIGHWAY 890 STATION NUMBERS**  
**STA. 158+00 - STA. 178+00**

- AA BASE FAILURE REPAIR  
(ALL LOCATIONS)
- A EXISTING PAVEMENT TO BE REMOVED
- B UNDERCUT EXISTING MATERIAL AS DIRECTED
- C REPLACE FULL DEPTH WITH 12.5-MM ST.ASPHALT PAVEMENT (LEVELING)



**EXISTING**

- 1 - 4" - 5 1/4" and variable HMA over 6" plain or reinforced joint concrete pavement\*
- 2 - Cement-Treated Clay Gravel
- 3 - Overlay with 2" 12.5mm,Mix, ST
- 4 - Repair area
- 5 - Existing asphalt
- 6 - Cement-Treated Clay Gravel

**PROPOSED**

- 1 Repair failed areas with 12.5mm, Mix ST Leveling full depth.
- 2 Mill existing asphalt 2" and variable.
- 3 Overlay with 2" and variable of 12.5mm Mix, ST.
- 4 Repair any failed JRCP joints full depth to 3' width on either side of joint (6' total width) by removal of existing concrete. Repair with 12.5mm mix, ST LEVELING
- 5 Place 2" & Var. Granular Material on shoulder as directed by the Engineer

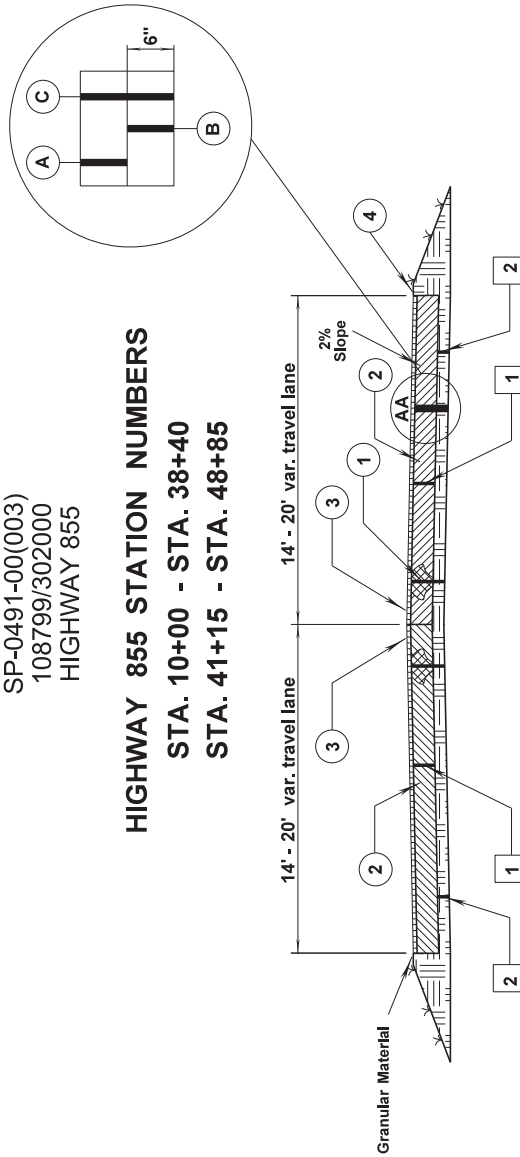
\* In some areas, concrete was previously removed. Pavement consists of 10" +/- asphalt pavement.

HINDS COUNTY  
 SP-0491-00(003)  
 108799/302000  
 HIGHWAY 855

# HIGHWAY 855 STATION NUMBERS

STA. 10+00 - STA. 38+40

STA. 41+15 - STA. 48+85



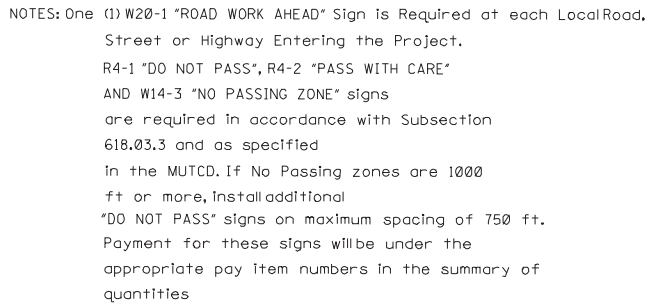
- (AA) BASE FAILURE REPAIR (ALL LOCATIONS)
- (A) EXISTING PAVEMENT TO BE REMOVED
- (B) UNDERCUT EXISTING MATERIAL AS DIRECTED
- (C) REPLACE FULL DEPTH WITH 12.5-MM ST.ASPHALT PAVEMENT (LEVELING)

## EXISTING

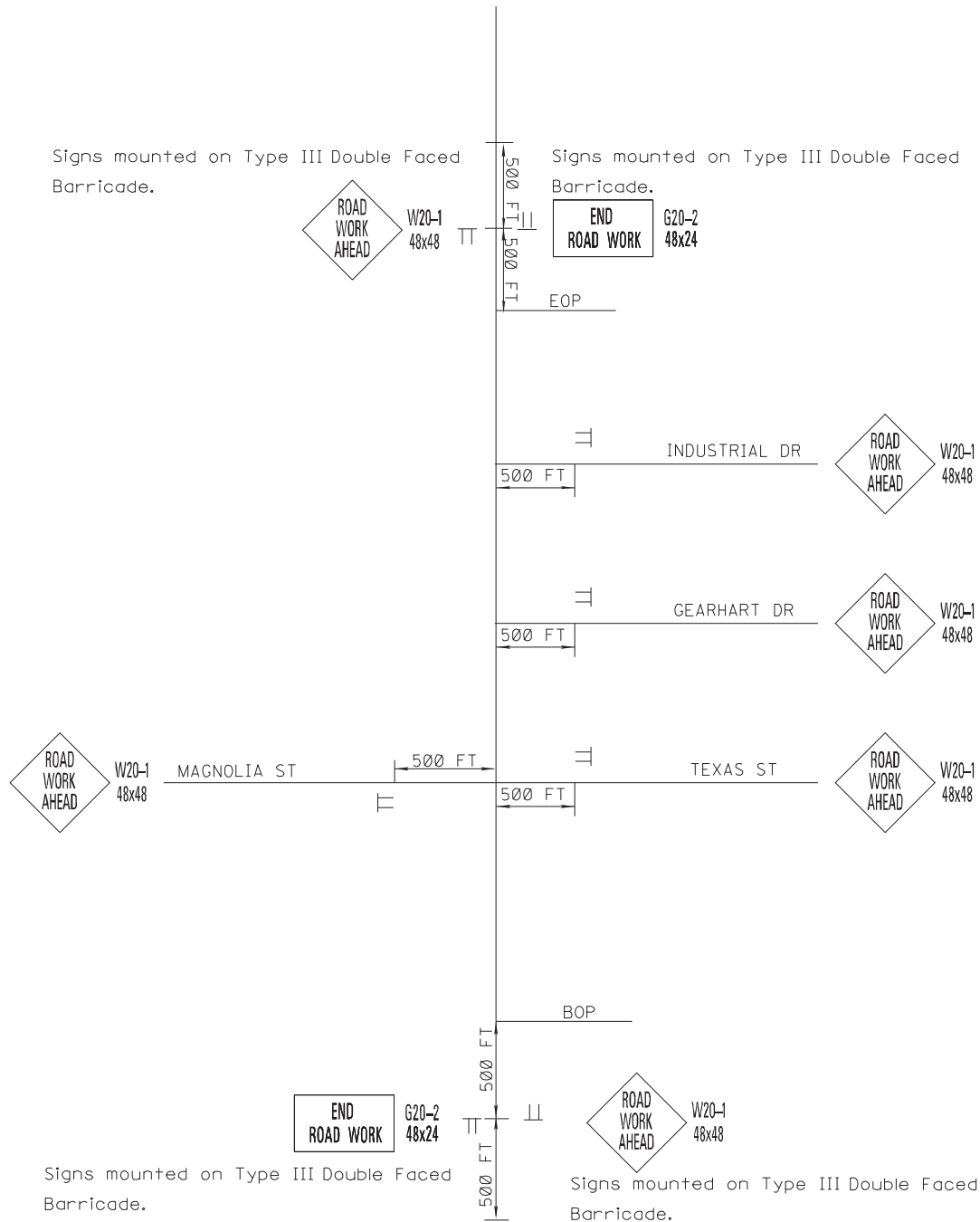
- 1 - 4" - 5 1/4" and variable HMA
- 2 - Cement-Treated Clay Gravel
- 3 - Overlay with 2" 12.5mm,Mix, ST
- 4 - Repair area
- 5 - Existing asphalt
- 6 - Cement-Treated Clay Gravel

## PROPOSED

- 1 Repair failed areas with 12.5mm, Mix ST Leveling full depth.
- 2 Mill existing asphalt 2" and variable.
- 3 Overlay with 2" and variable of 12.5mm Mix, ST.
- 4 Place 2" & Var. Granular Material on shoulder as directed by the Engineer



-16-      Notice To Bidder No. 3897 - Cont'd.  
 CONSTRUCTION SIGNING DETAIL  
 HWY 855 HINDS COUNTY



## ESTIMATED

### Traffic Control Signs Required:

- 2 - W20-1 "ROAD WORK AHEAD"
- 2 - G20-2 "END ROAD WORK"
- 8 - W20-1 "ROAD WORK AHEAD"
- 10 - R4-1 "DO NOT PASS"
- 0 - R4-2 "PASS WITH CARE"
- 10 - W14-3 "NO PASSING ZONE"
- 24 - TYPE III DBL. FACE BARRICADES

NOTES: One (1) W20-1 "ROAD WORK AHEAD" Sign is Required at each Local Road, Street or Highway Entering the Project.

R4-1 "DO NOT PASS", R4-2 "PASS WITH CARE" AND W14-3 "NO PASSING ZONE" signs are required in accordance with Subsection 618.03.3 and as specified in the MUTCD. If No Passing zones are 1000 ft or more, install additional "DO NOT PASS" signs on maximum spacing of 750 ft. Payment for these signs will be under the appropriate pay item numbers in the summary of quantities

<b>108799-3010000 SP-0491-00(003) 890 THROUGH THE TOWN OF BOLTON</b>						
<b>SR 890 - 12.5-mm, ST, Asphalt Pavement, Leveling</b>						
<b>Station Start:</b>	<b>Station End:</b>	<b>Width:</b>	<b>Length:</b>	<b>Square yards:</b>	<b>Weight (tons):</b>	
128+82	129+16	22	34	83.10	55	
153+95	154+05	22	10	24.45	17	
161+08	161+20	22	12	29.33	20	
163+06	163+12	22	6	14.67	10	
167+65	167+78	22	13	31.78	21	
168+05	168+15	22	10	24.45	17	
171+90	172+05	11	15	18.33	13	
172+85	173+00	22	15	36.67	25	
				<b>Total Tons:</b>	<b>106</b>	



<b>108799-3010000 SP-0491-00(003) 890 THROUGH THE TOWN OF BOLTON</b>					
<b>SR 890 - Sawing and Sealing Transverse Joints in Asphalt Pavement</b>					
<b>Name:</b>	<b>Start Start:</b>	<b>Station End:</b>	<b>Linear Feet Paid:</b>	<b>Descriptive:</b>	
413-E001	100+00	146+00	5290	230 cuts at 23' sections	
413-E001	146+46	177+95	2844	158 cuts at 18' sections	
		<b>Total:</b>	<b>8134</b>		

108799-301000 SP-0491-00(003) 890 THROUGH THE TOWN TO BOLTON		
SR 890 - Cleaning and Filling Joints		
STA	DESCRIPTION	Linear Feet
100+60	LT	11
101+00	LT	11
101+58	LT	11
107+12	LT	11
128+85	RT	11
128+90	LT	11
128+99	LT	11
134+71	LT	11
134+92	LT	11
135+74	LT + RT	22
135+93	LT + RT	22
137+12	LT + RT	22
148+19	LT + RT	22
150+75	LT + RT	22
150+85	LT + RT	22
161+62	LT + RT	22
162+12	LT + RT	22
168+29	LT	11
173+70	LT + RT	22
174+18	LT + RT	22
174+52	LT + RT	22
174+74	LT + RT	22
174+90	LT + RT	22
175+00	LT	11
175+10	LT + RT	22
175+20	LT + RT	22
175+43	LT + RT	22
175+52	LT + RT	22
175+70	LT + RT	22
175+95	LT + RT	22
176+19	LT + RT	22
176+38	LT + RT	22
176+43	LT + RT	22
176+66	LT + RT	22
176+81	RT	11
176+83	LT + RT	22
	Total:	660

108799-3010000 SP-0491-00(003) 890 THROUGH THE TOWN OF BOLTON						
SR 890-Removal of Concrete Overlayed w/ Asphalt Pavement, All Depths						
Station Start:	Station End:	Width:	LENGTH	ASPHALT DEPTH (ft)	Square yards	
128+82	129+16	22	34	1	83	
153+95	154+05	22	10	1	24	
161+08	161+20	22	12	1	29	
163+06	163+12	22	6	1	15	
167+65	167+78	22	13	1	32	
168+05	168+15	22	10	1	24	
171+90	172+05	11	15	1	18	
172+85	173+00	22	15	1	37	
				Total SY	263	

108799-3010000 SP-0491-00(003) 890 THROUGH THE TOWN OF BOLTON						
SR 890 - Excess Excavation, FM, AH						
Station Start:	Station End:	Width:	LENGTH	Depth, Feet	Cubic Yards	
128+82	129+16	22	34	0.5	14	
153+95	154+05	22	10	0.5	4	
161+08	161+20	22	12	0.5	5	
163+06	163+12	22	6	0.5	2	
167+65	167+78	22	13	0.5	5	
168+05	168+15	22	10	0.5	4	
171+90	172+05	11	15	0.5	3	
172+85	173+00	22	15	0.5	6	
				<b>Total CY</b>	<b>44</b>	

108799-3010000 SP-0491-00(003) 890 THROUGH THE TOWN OF BOLTON				
SR 890 - Sawing, Full Depth				
Station Start:	Station End:	Width:	LENGTH	
128+82	129+16	22	34	
153+95	154+05	22	10	
161+08	161+20	22	12	
163+06	163+12	22	6	
167+65	167+78	22	13	
168+05	168+15	22	10	
171+90	172+05	11	15	
172+85	173+00	22	15	
		TOTAL LF	560	



108799-3020000 SP-0491-00(003) 855 FROM TEXAS ST TO END OF MAINTENANCE NORTH OF I-20													
GUARDRAIL QUANTITIES													
STATION	GUARDRAIL			TERMINAL END SECTION	Cable Anchor TYPE I (EA)	BRIDGE END SECTION		TYPE "D" Modified (EA)	SPECIAL SECTIONS SPECIAL DESIGN BRIDGE CONNECTOR	DELINEATORS		Type 3 Object Markers	REMOVAL ITEMS
	(W-BEAM) (LF)	TRANS. SECT. (EA)	THRIE BEAM (LF)			TYPE "I" (EA)	TYPE "C" (EA)			WHITE	YELLOW		
36+27 RT	175			1					1	10		1	216 RIGHT
37+24 LT	75			1					1	6		1	116 LEFT
41+19 RT	175			1					1	6		1	116 RIGHT
41+19 LT	75			1					1	10		1	216 LEFT
TOTAL =	500	0	0	4	0	0	0	0	4	32	0	4	664
	L.F.	EA	L.F.	EA	EA	EA	EA	EA	EA	EA	EA	EA	L.F.

\* ALL TERMINAL END SECTIONS ARE 37.5'

\* REMOVAL OF ALL GUARDRAIL (BRIDGE END SECTIONS, W-BEAM, TYPE-I CABLE ANCHORAGE, TERMINAL END SECTIONS, ETC.) WILL BE PAID UNDER PAY ITEM 202-B REMOVAL OF GUARD RAIL

\* REMOVAL OF GUARDRAIL DELINEATORS ARE CONSIDERED INCIDENTAL TO THE REMOVAL OF GUARDRAIL AND WILL NOT BE MEASURED AS A SEPARATE PAY ITEM.

\*REMOVAL OF OBJECT MARKERS WILL NOT BE MEASURED AS A SEPARATE PAY ITEM AS SHALL BE ABSORBED IN OTHER ITEMS

<b>108799-3020000 SP-0491-00(003) 855 FROM TEXAS ST TO END OF MAINTENANCE</b>						
<b>SR 855 - Asphalt Leveling for Failed Areas</b>						
<b>Name:</b>	<b>Station Start:</b>	<b>Station End:</b>	<b>Width:</b>	<b>LENGTH:</b>	<b>Square Yards:</b>	<b>Weight (tons):</b>
RL	11+80	13+03	8	123	109	34
RL	13+70	13+87	8	17	15	5
RL	16+25	16+37	8	12	11	4
RL	16+70	16+96	8	26	23	7
LL	21+16	20+87	18	29	58	18
LL	20+22	19+86	10	36	40	13
LL	19+09	19+04	8	5	4	2
LL	18+49	18+44	8	5	4	2
LL	17+34	17+24	8	10	9	3
LL	17+08	16+89	8	19	17	6
LL	16+64	16+50	8	14	12	4
LL	16+14	15+84	8	30	27	9
LL	15+71	15+56	8	15	13	5
LL	14+99	14+54	18	45	90	28
LL	12+40	12+23	8	17	15	5
					<b>Total Tons:</b>	<b>145</b>

108799-3020000 SP-0491-00(003) 855 FROM TEXAS ST TO END OF MAINTENANCE NORTH OF I-20						
SR 855 - Excess Excavation, FM, AH						
Location	Station Start	Station End	Width	Length	Depth, Feet	Cubic Yards
RL	11+80	13+03	8	123	0.5	19
RL	13+70	13+87	8	17	0.5	3
RL	16+25	16+37	8	12	0.5	2
RL	16+70	16+96	8	26	0.5	4
LL	21+16	20+87	18	29	0.5	10
LL	20+22	19+86	10	36	0.5	7
LL	19+09	19+04	8	5	0.5	1
LL	18+49	18+44	8	5	0.5	1
LL	17+34	17+24	8	10	0.5	2
LL	17+08	16+89	8	19	0.5	3
LL	16+64	16+50	8	14	0.5	3
LL	16+14	15+84	8	30	0.5	5
LL	15+71	15+56	8	15	0.5	3
LL	14+99	14+54	18	45	0.5	15
LL	12+40	12+23	8	17	0.5	3
					<b>TotalCY</b>	<b>81</b>

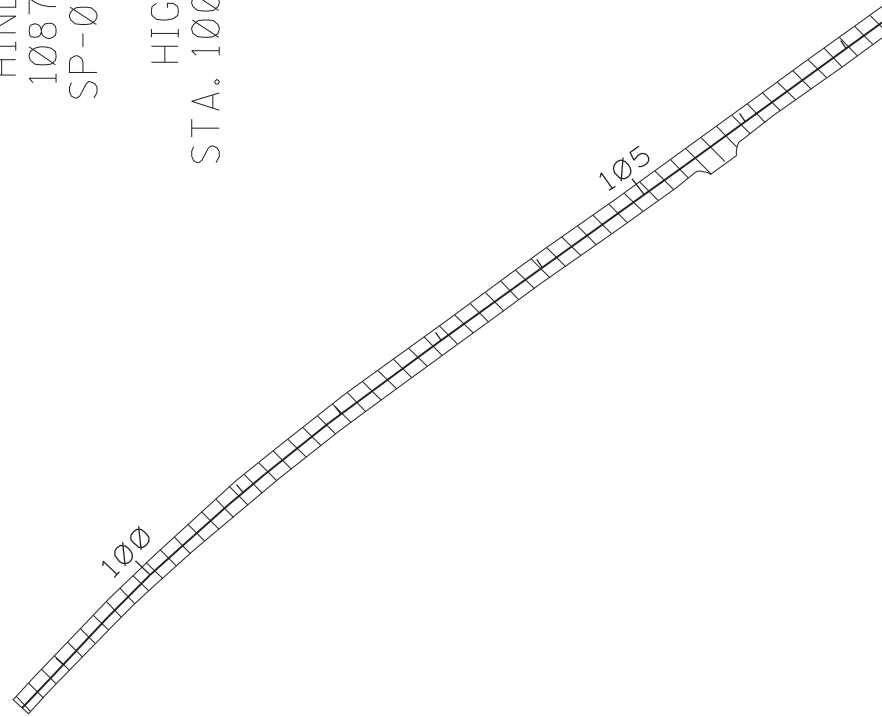
108799-3020000 SP-0491-00(003) 855 FROM TEXAS ST TO END OF MAINTENANCE NORTH OF I-20			
SR 855 - Saw Cuts, Full Depth			
Station Start:	Station End:	Width:	LENGTH
11+80	13+03	8	123
13+70	13+87	8	17
16+25	16+37	8	12
16+70	16+96	8	26
21+16	20+87	18	29
20+22	19+86	10	36
19+09	19+04	8	5
18+49	18+44	8	5
17+34	17+24	8	10
17+08	16+89	8	19
16+64	16+50	8	14
16+14	15+84	8	30
15+71	15+56	8	15
14+99	14+54	18	45
12+40	12+23	8	17
		<b>TOTAL LF</b>	<b>1090</b>

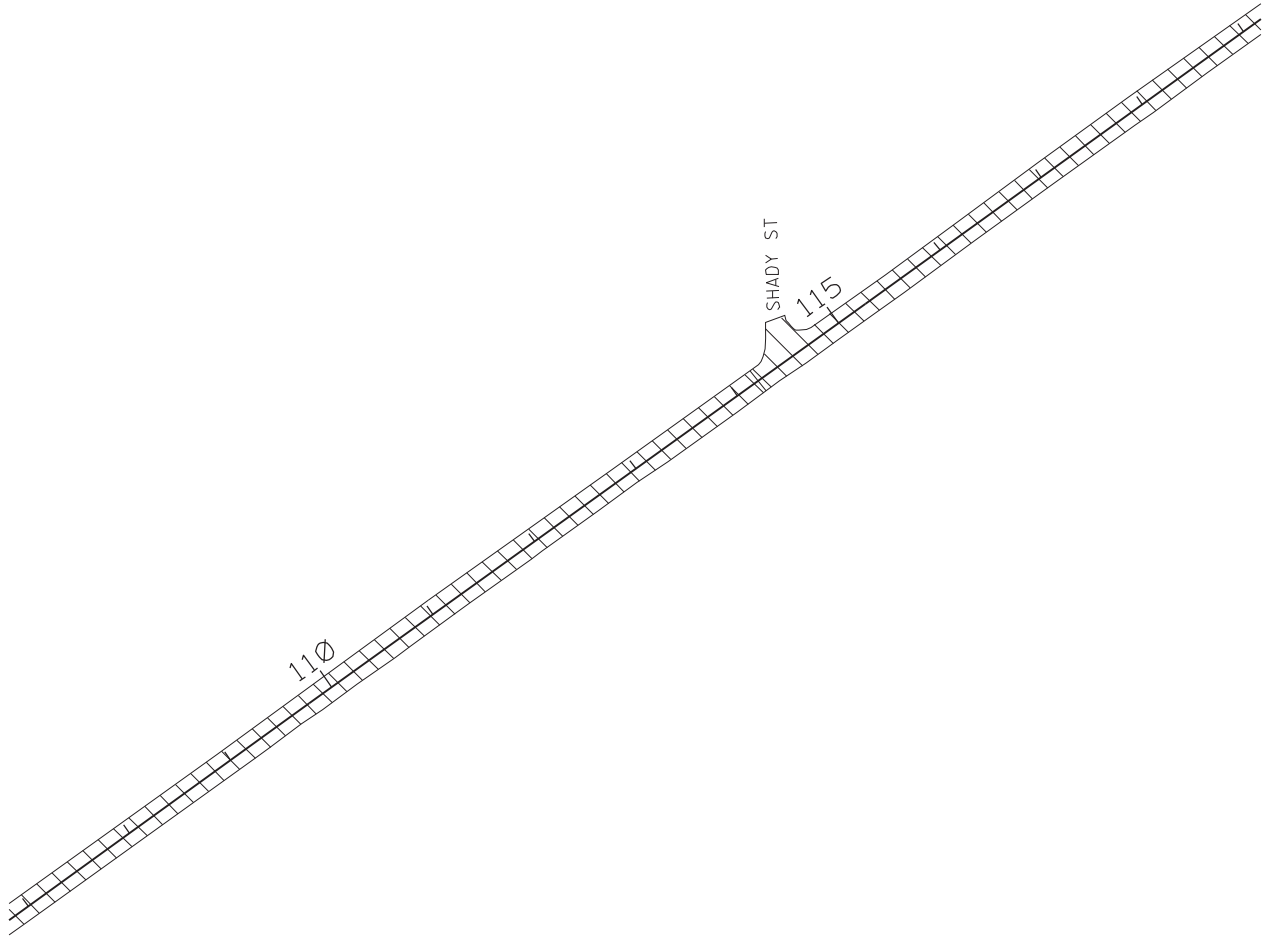
<b>108799-3020000 SP-0491-00(003) 855 FROM TEXAS ST TO END OF MAINTENANCE NORTH OF I-20</b>					
<b>SR 855 - Failed Areas</b>					
<b>Name:</b>	<b>Station Start:</b>	<b>Station End:</b>	<b>Width:</b>	<b>LENGTH</b>	<b>Square Yards</b>
RL	11+80	13+03	8	123	109
RL	13+70	13+87	8	17	15
RL	16+25	16+37	8	12	11
RL	16+70	16+96	8	26	23
LL	21+16	20+87	18	29	58
LL	20+22	19+86	10	36	40
LL	19+09	19+04	8	5	4
LL	18+49	18+44	8	5	4
LL	17+34	17+24	8	10	9
LL	17+08	16+89	8	19	17
LL	16+64	16+50	8	14	12
LL	16+14	15+84	8	30	27
LL	15+71	15+56	8	15	13
LL	14+99	14+54	18	45	90
LL	12+40	12+23	8	17	15
				<b>Total SY:</b>	<b>448</b>



MILLING OVERVIEW  
HINDS COUNTY  
108799/301000  
SP-0491-00(003)

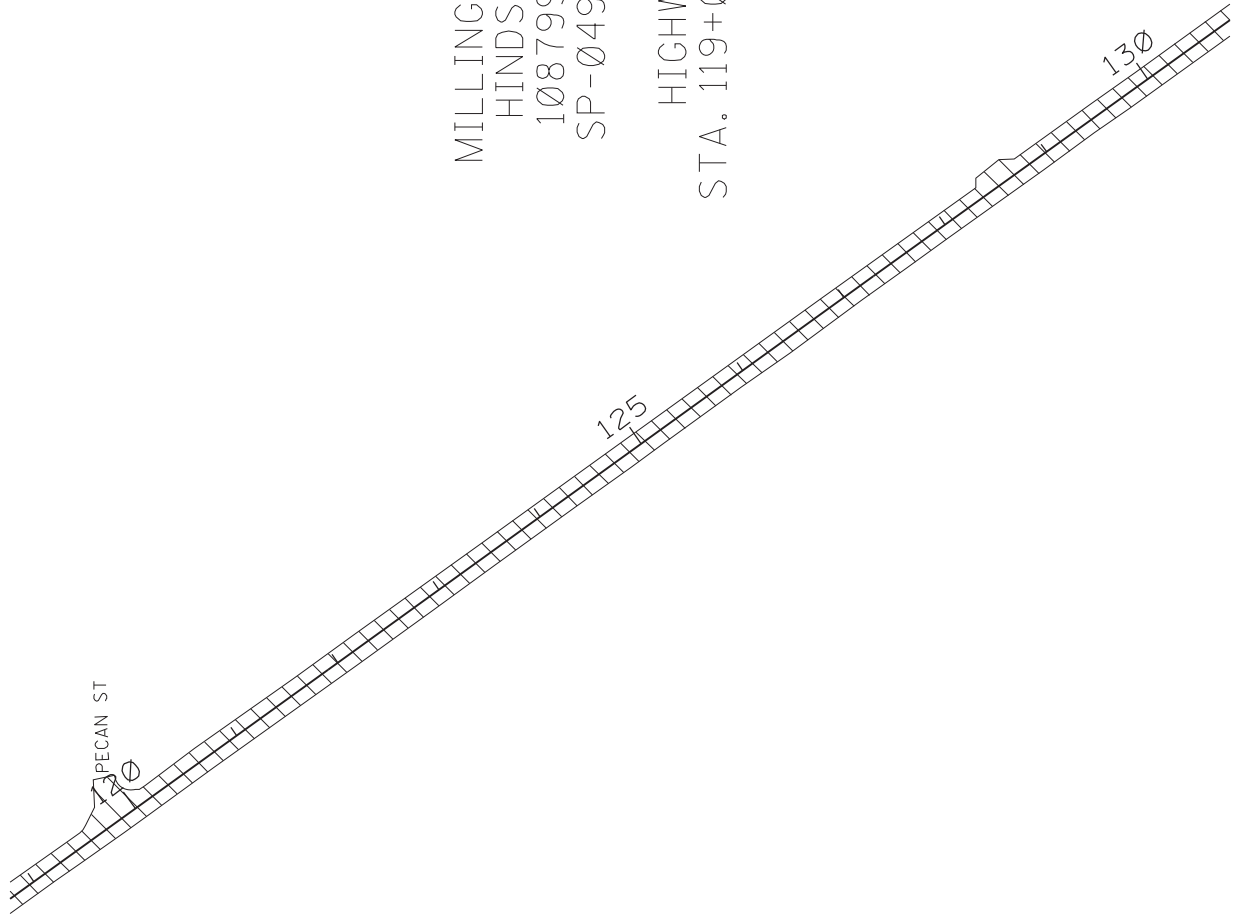
HIGHWAY 890  
STA. 100+00 - 107+00





MILLING OVERVIEW  
HINDS COUNTY  
108799/301000  
SP-0491-00(003)

HIGHWAY 890  
STA. 107+00 - 119+00

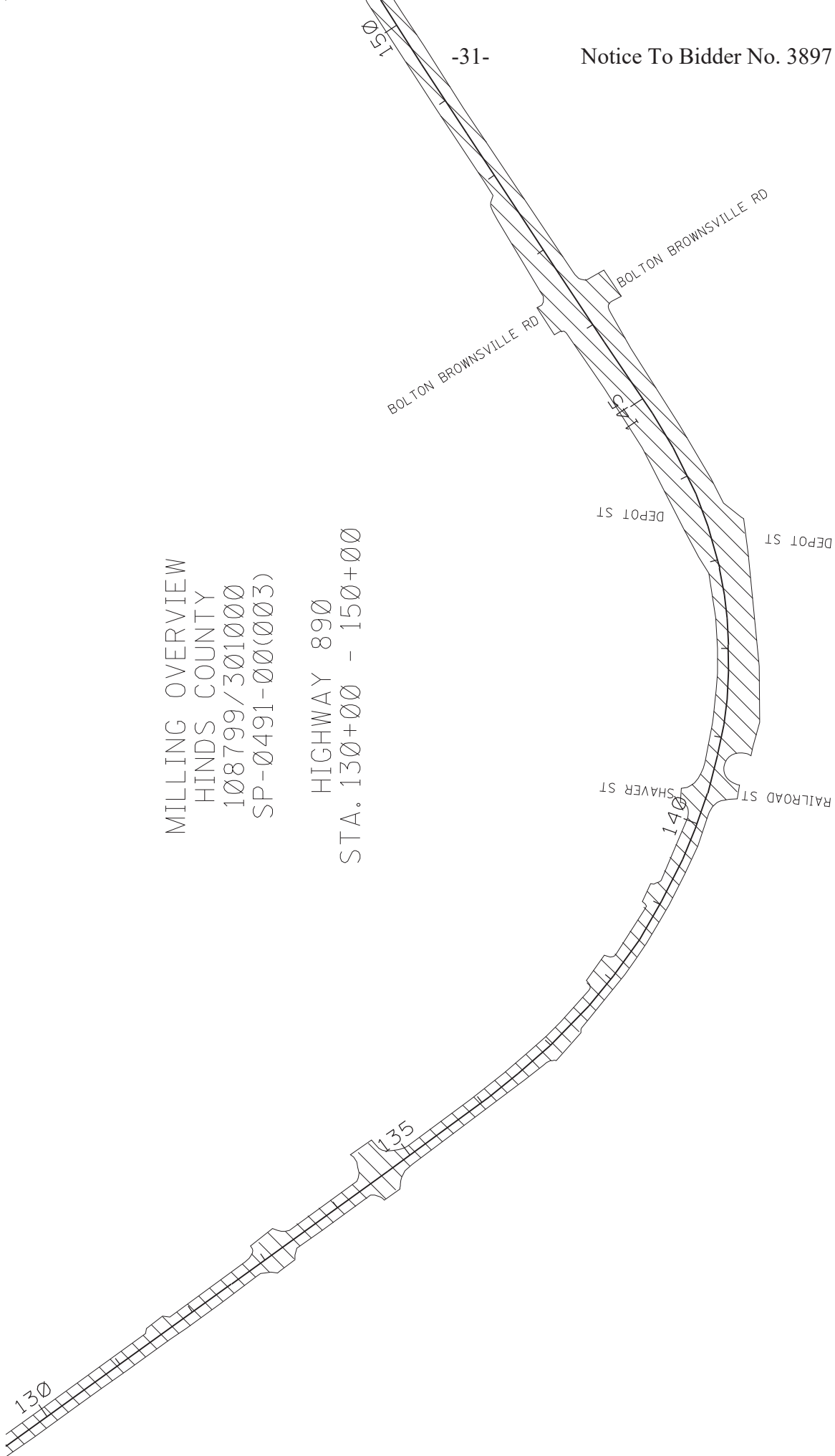


MILLING OVERVIEW  
HINDS COUNTY  
108799/301000  
SP-0491-00(003)

HIGHWAY 890  
STA. 119+00 - 130+00

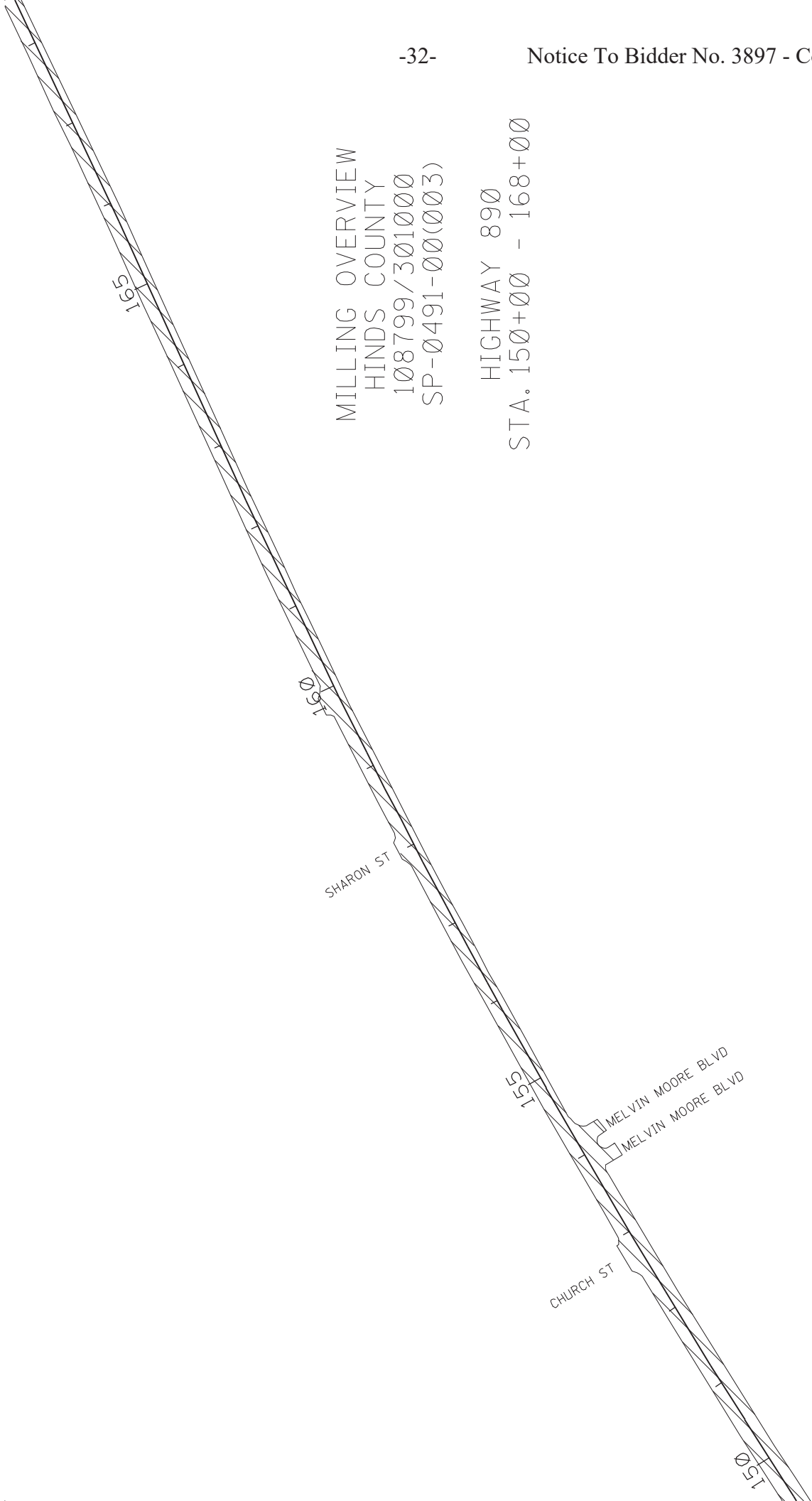
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HINDS COUNTY  
108799/301000  
SP-0491-00(003)

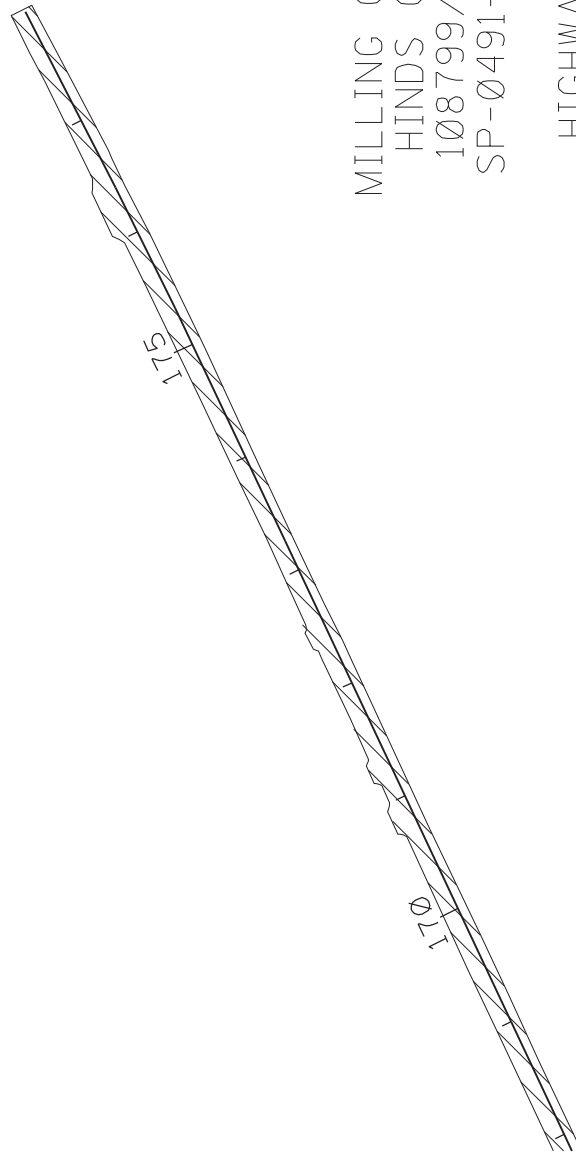
HIGHWAY 890  
STA. 130+00 - 150+00



MILLING OVERVIEW  
HINDS COUNTY  
108799/301000  
SP-0491-00(003)

HIGHWAY 890  
STA. 150+00 - 168+00



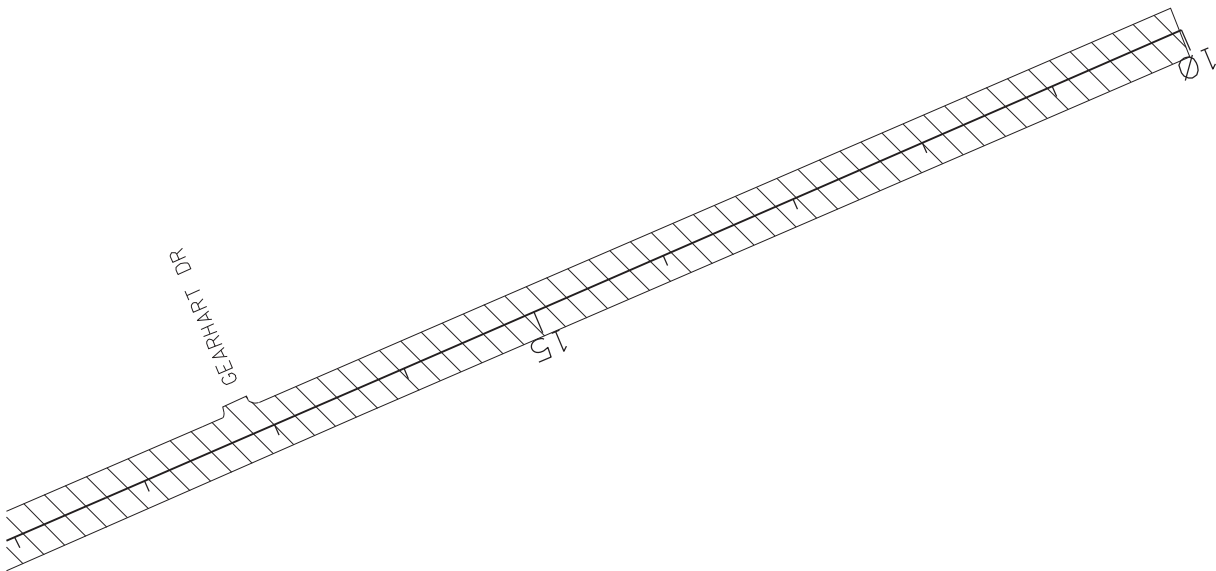


MILLING OVERVIEW  
HINDS COUNTY  
108799/301000  
SP-0491-00(003)

HIGHWAY 890  
STA. 168+00 - 178+00

MILLING OVERVIEW  
HINDS COUNTY  
108799/302000  
SP-0491-00(003)

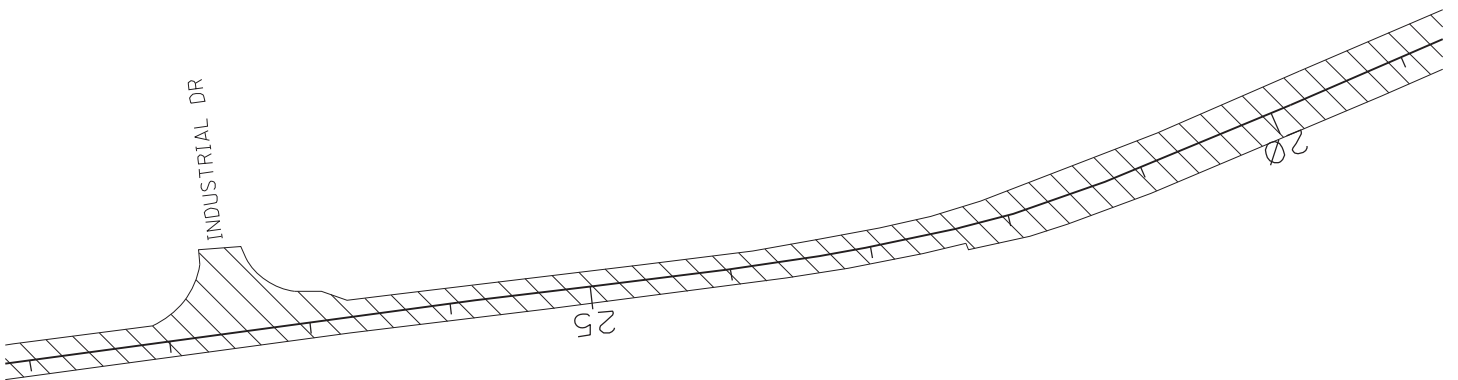
HIGHWAY 855  
STA. 10+00 - 19+00





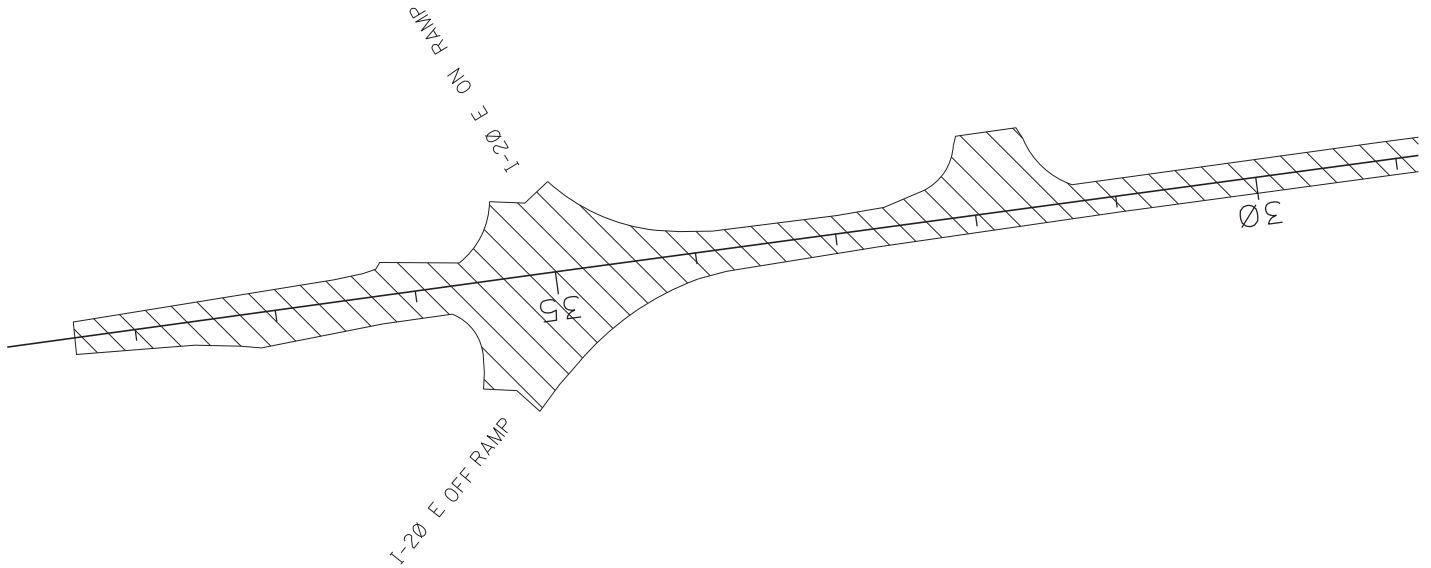
MILLING OVERVIEW  
HINDS COUNTY  
108799/302000  
SP-0491-00(003)

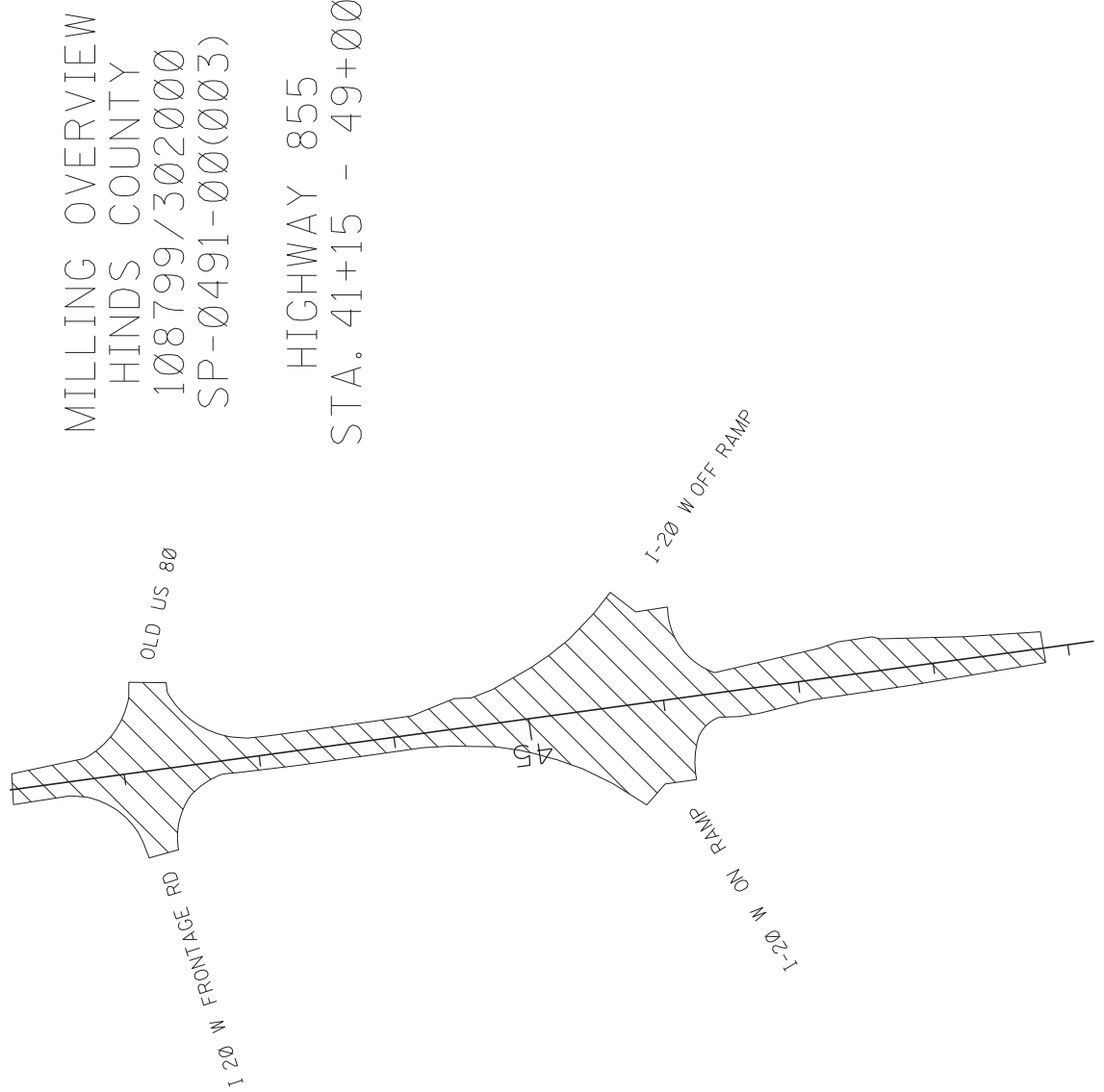
HIGHWAY 855  
STA. 19+00 - 29+00



MILLING OVERVIEW  
HINDS COUNTY  
108799/302000  
SP-0491-00(003)

HIGHWAY 855  
STA. 29+00 - 38+40



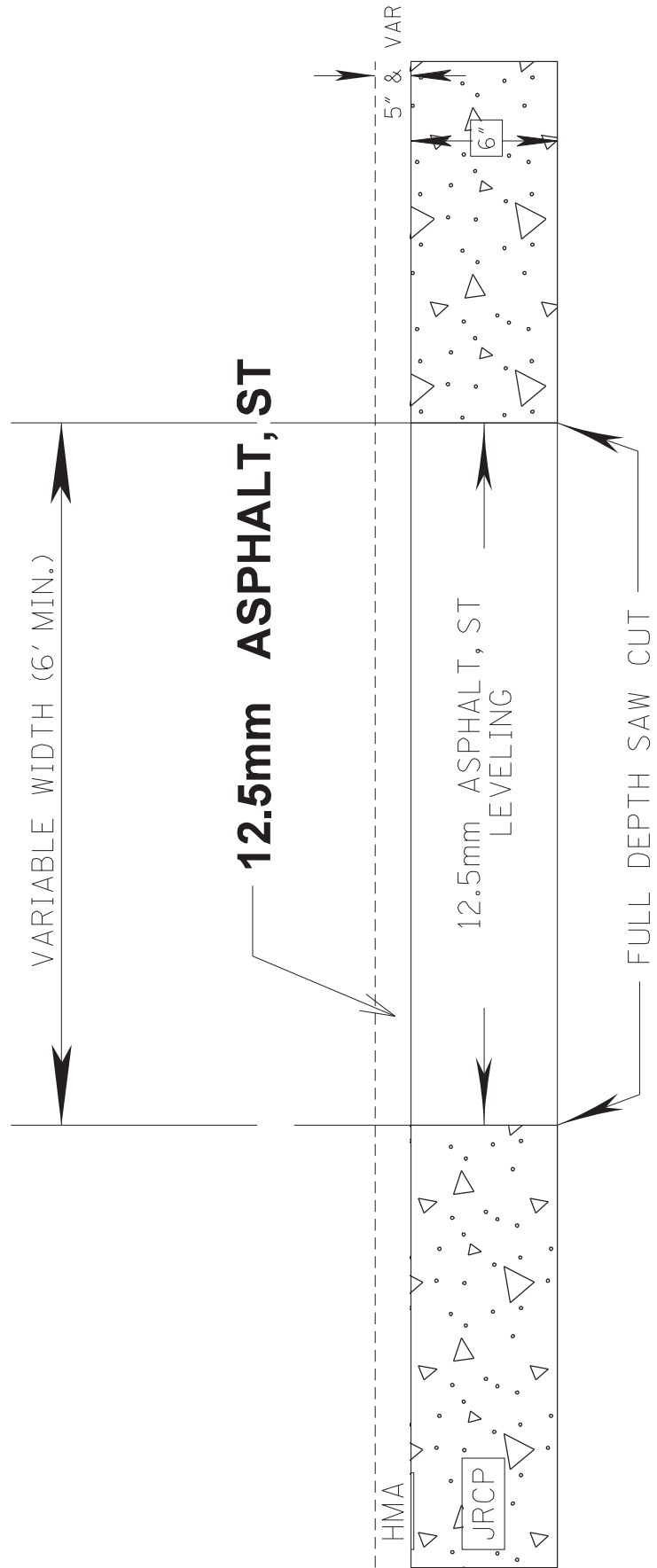


MILLING OVERVIEW  
HINDS COUNTY  
108799/302000  
SP-0491-00(003)

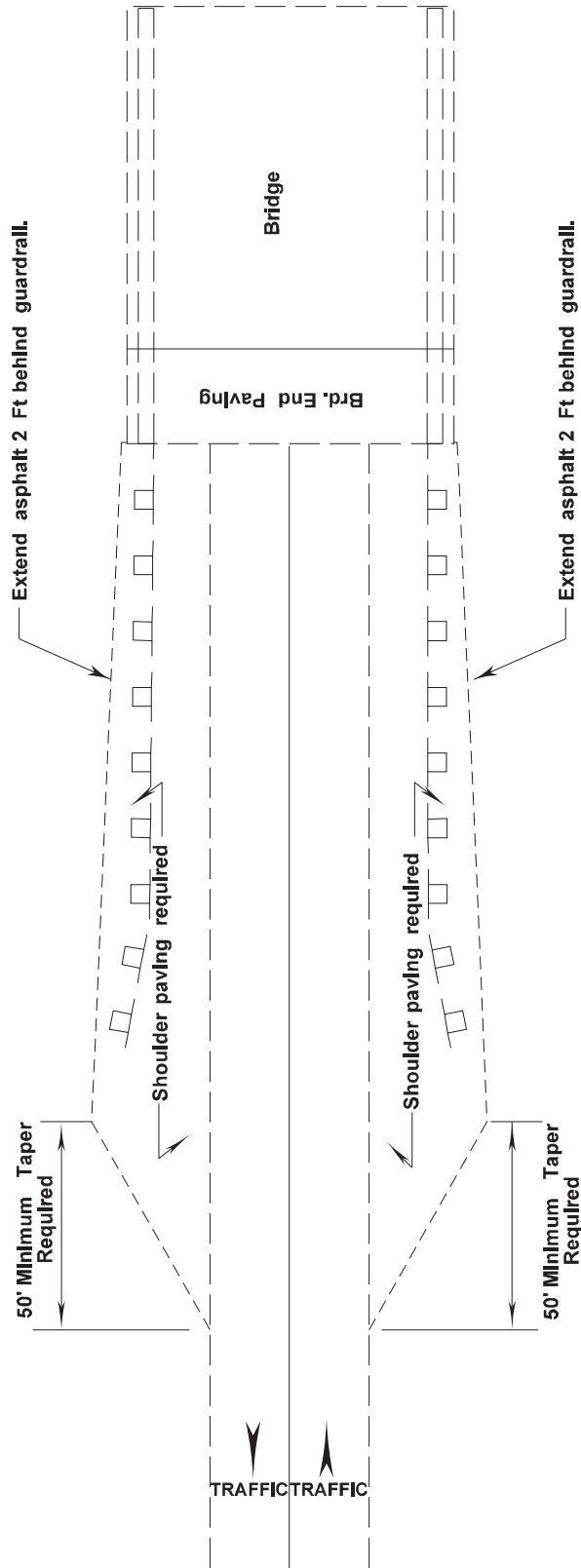
HIGHWAY 855  
STA. 41+15 - 49+00

**HINDS COUNTY**  
**SP-0491-00(003) / 108799/301000**  
**SR 890**

**CONCRETE PAVEMENT REPAIR DETAIL**

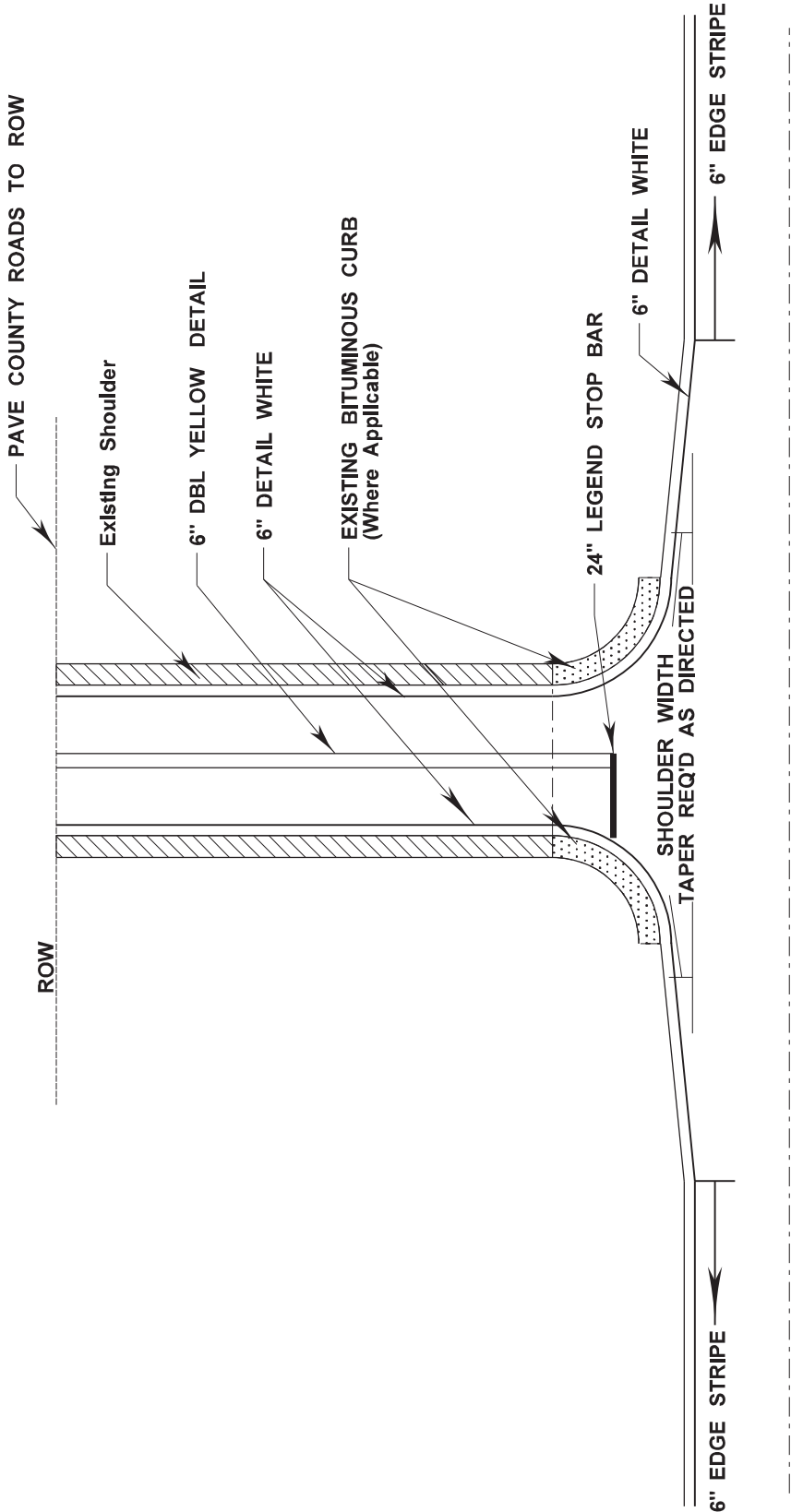


**HINDS COUNTY**  
**SP-0491-00(003) 108799/301000**  
**TYPICAL DETAIL OF ADDITIONAL SHOULDER PAVING**  
**REQUIRED AT GUARDRAIL LOCATIONS**



\*Asphalt Thickness  
See scope of work for additional details

HINDS COUNTY  
SR 890 AND SR 855  
108799/301000 & 108799/302000  
STRIPE DETAIL



Section 905

SP-0491-00(003)/108799301, SP-0493-00(001)/108799302

Proposal (Sheet 2 - 1)

HINDS

Mill & Overlay approximately 1.5 miles on SR 890 from the BOSM to the EOSM near the Bolton Corp. Limit & approximately 0.75 miles on SR 855 from the BOSM to the EOSM north of I-20, known as State Project Nos. SP-0491-00(003) & SP-0493-00(001) / 108799301 & 302 in Hinds County.

Line No.	Item Code	Adj Code	Quantity	Units	Description [Fixed Unit Price]
<b>Roadway Items</b>					
0010	202-B009		448	Square Yard	Removal of Asphalt Pavement, Failed Areas
0020	202-B069		263	Square Yard	Removal of Concrete Pavement w/ Variable Depth Overlay
0030	202-B158		664	Linear Feet	Removal of Guard Rail, Including Rails, Posts and Terminal Ends
0040	203-G001	(E)	125	Cubic Yard	Excess Excavation, FM, AH
0050	304-B003	(GT)	485	Ton	Granular Material, Class 5, Group C
0060	403-A003	(BA1)	4,623	Ton	12.5-mm, ST, Asphalt Pavement
0070	403-B003	(BA1)	251	Ton	12.5-mm, ST, Asphalt Pavement, Leveling
0080	406-D001		40,016	Square Yard	Fine Milling of Bituminous Pavement, All Depths
0090	407-A001	(A2)	4,433	Gallon	Asphalt for Tack Coat
0100	413-D002		660	Linear Feet	Cleaning and Filling Joints
0110	413-E001		8,134	Linear Feet	Sawing and Sealing Transverse Joints in Asphalt Pavement
0120	503-C010		1,650	Linear Feet	Saw Cut, Full Depth
0130	606-B002		500	Linear Feet	Guard Rail, Class A, Type 1, 'W' Beam
0140	606-E001		4	Each	Guard Rail, Terminal End Section
0150	606-G001		4	Each	Special Sections, Bridge Connector
0160	618-A001		1	Lump Sum	Maintenance of Traffic
0170	619-A1001		14	Mile	Temporary Traffic Stripe, Continuous White
0180	619-A2001		14	Mile	Temporary Traffic Stripe, Continuous Yellow
0190	619-A5001		16,006	Linear Feet	Temporary Traffic Stripe, Detail
0200	619-A6002		7,968	Linear Feet	Temporary Traffic Stripe, Legend
0210	619-D1001		32	Square Feet	Standard Roadside Construction Signs, Less than 10 Square Feet
0220	619-D2001		340	Square Feet	Standard Roadside Construction Signs, 10 Square Feet or More
0230	619-G4001		48	Linear Feet	Barricades, Type III, Double Faced
0240	620-A001		1	Lump Sum	Mobilization
0250	626-B002		5	Mile	6" Thermoplastic Double Drop Traffic Stripe, Continuous White
0260	626-E001		5	Mile	6" Thermoplastic Double Drop Traffic Stripe, Continuous Yellow
0270	626-G001		48	Linear Feet	Thermoplastic Detail Stripe, Blue-ADA
0280	626-G004		3,235	Linear Feet	Thermoplastic Double Drop Detail Stripe, White
0290	626-G005		851	Linear Feet	Thermoplastic Double Drop Detail Stripe, Yellow
0300	626-H002		1,926	Linear Feet	Thermoplastic Double Drop Legend, White
0310	627-J001		196	Each	Two-Way Clear Reflective High Performance Raised Markers
0320	627-K001		318	Each	Red-Clear Reflective High Performance Raised Markers

Section 905  
Proposal (Sheet 2 - 2)

SP-0491-00(003)/108799301, SP-0493-00(001)/108799302  
HINDS

Line No.	Item Code	Adj Code	Quantity	Units	Description [Fixed Unit Price]
0330	627-L001		292	Each	Two-Way Yellow Reflective High Performance Raised Markers
0340	630-F006		32	Each	Delineators, Guard Rail, White
0350	630-G004		4	Each	Type 3 Object Markers, OM-3R or OM-3L
0360	907-619-B001		132	Linear Feet	Temporary Portable Rumble Strips
0370	907-626-H001		1	Each	Thermoplastic Legend, Blue-ADA Handicap Symbol