

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO.	<u>1</u>	DATED	<u>3/7/2022</u>	ADDENDUM NO.	_____	DATED	_____
ADDENDUM NO.	_____	DATED	_____	ADDENDUM NO.	_____	DATED	_____
ADDENDUM NO.	_____	DATED	_____	ADDENDUM NO.	_____	DATED	_____

Number

Description

- 1 Revised Advertisement; Revised NTB No. 3908; Revised Bid Items; Revised Progress Schedule; Amendment EBSx Download Required.

TOTAL ADDENDA: 1

(Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

Contractor

BY _____

Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

President

Address

Secretary

Address

Treasurer

Address

The following is my (our) itemized proposal.

IM-0055-02(257)/ 108091301000

Holmes County(ies)

Revised 01/26/2016

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 901 - ADVERTISEMENT

Electronic bids will be received by the Mississippi Transportation Commission at 10:00 o'clock A.M., Tuesday, March 22, 2022, from the Bid Express Service and shortly thereafter publicly read on the Sixth Floor For:

Mill & Overlay approximately 22 miles on I-55 from 0.5 miles north of SR 17 to the Carroll County Line, known as Federal Aid Project No. IM-0055-02(257) / 108091301 in Holmes County.

The attention of bidders is directed to the Contract Provisions governing selection and employment of labor. Minimum wage rates have been predetermined by the Secretary of Labor and are subject to Public Law 87-581, Work Hours Act of 1962, as set forth in the Contract Provisions.

The Mississippi Department of Transportation hereby notifies all bidders that it will affirmatively insure that in any contract entered into pursuant to this advertisement, disadvantaged business enterprises will be afforded full opportunity to submit bids in response to this invitation and will not be discriminated against on the grounds of race, color, sex, age, disability, religion or national origin in consideration for an award.

The award of this contract will be contingent upon the Contractor satisfying the DBE requirements.

The specifications are on file in the offices of the Mississippi Department of Transportation.

Contractors may request permission to bid online at <http://shopmdot.ms.gov> at no cost. Upon approval, Contractors shall be eligible to submit a bid using Bid Express at <http://bidx.com>. Specimen proposals may be viewed and downloaded online at no cost at <http://mdot.ms.gov> or purchased online at <http://shopmdot.ms.gov> at a cost of Ten Dollars (\$10.00) per proposal plus a small convenience fee. Cash or checks will not be accepted as payment.

Bid bond, signed or countersigned by a Mississippi Agent or Qualified Nonresident Agent, with Power of Attorney attached, a Cashier's check or Certified Check for five (5%) percent of bid, payable to STATE OF MISSISSIPPI, must accompany each proposal.

The attention of bidders is directed to the provisions of Subsection 102.07 pertaining to irregular proposals and rejection of bids.

BRAD WHITE
EXECUTIVE DIRECTOR

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 – NOTICE TO BIDDERS NO. 3908

CODE: (SP)

DATE: 3/7/2022

SUBJECT: Scope of Work

PROJECT: IM-0055-02(257) / 108091301 -- Holmes County

The contract documents do not include an official set of construction plans, but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, “Standard Drawings.”

MILL & OVERLAY INTERSTATE 55 FROM
0.50 MILES NORTH OF SR 17 TO THE
THE HOLMES/CARROLL COUNTY LINE
LOG MILE 4.194 TO 26.496
HOLMES COUNTY

In general, the work to be accomplished using the pay items and corresponding specifications set forth in this contract is to mill and overlay approximately 22 miles of I-55 in Holmes County from 0.5 miles north of SR 17 to the Holmes/Carroll County line.

Work on the project shall consist of the following:

1. The Contractor shall erect and maintain construction signing, provide all signs, and traffic handling devices in accordance with the Traffic Control Plan. The cost for this work, unless specific pay items are included, is to be included in the price bid for pay item 618-A: Maintenance of Traffic. All traffic control devices on this project should comply with the latest version of the MUTCD. Fluorescent orange sheeting shall be used on all construction and traffic control signs except for those designated in the plans to be black legend and border on white background. The Contractor will be required to use 42-inch channelizing cones with 6-inch wide reflective tape and 16 pound vertical panel bases for each cone.
2. The random clearing quantity is an estimated quantity based on 50' from the edge of travel lane (edge stripe). The Contractor is responsible for establishing clearing limits and must be approved by the Engineer before any clearing work is done. The costs for staking shall be included in the Random Clearing pay item. Clearing limits may change in certain areas as directed by the Engineer. It is the intent of MDOT to clear as much as possible, but in the event clearing limits will uncover areas that have failures in the slope, which may create severe washing hazards, etc., the Engineer may omit these sections. Grubbing will not be required on this project. The areas will be cleared and stumps will be ground down to ground level. All debris shall be removed from the right of way. Payment for removing debris will be included in the Random Clearing pay item. If any part of any tree encroaches on the 50' of clearing, the entire tree will be removed and paid for under 202-B: Removal of Trees.

3. The Contractor shall remove any failed areas on the main facility as directed by the Project Engineer using the following construction sequence.
 - a) Saw cut through the asphalt and concrete pavement.
 - b) Remove the failed pavement.
 - c) Remove any unsuitable material in the subgrade as directed by the Engineer. Removal of this material will be paid for as excess excavation.
 - d) Backfill and stabilize the excavated subgrade with 19-mm, ST, Leveling, asphalt. No lift of 19-mm asphalt pavement shall be greater than 3½" or as designated by the Engineer. Densities will not be required on this asphalt. Contractor shall compact each lift to refusal or as directed by Engineer.
 - e) Repair the continuous reinforced concrete using the concrete pay items in the contract.
 - f) Bring the asphalt pavement back to grade by placing 19-mm, ST, Leveling, asphalt over the concrete repair.
4. The Contractor shall fine mill asphalt pavement as follows:

Mainline: The mainline shall be milled to a depth of two inches (2") prior to the placement of the 12.5-mm, SMA lift. The outside lane milling width is 14 feet and the median lane width is 16 feet, which includes the 4-foot paved shoulder.

The Contractor shall fine mill the roadway at the B.O.P., E.O.P, concrete curb and gutter sections, bridge ends, shoulders, ramps and other tie-ins as designated by the Project Engineer to ensure smooth transitions of the 1½" of 9.5-mm, SMA, asphalt lift with existing grade.

NOTE: It is the Contractor's responsibility to ensure the drainage of surface water from the milled areas using shoulder cuts.

NOTE: Temporary wedges of full lane width asphalt pavement shall be placed by the Contractor immediately after the cold milling process to allow the safe transition of traffic. The length of the wedges will be three feet (3') for every one-half inch (½") in height. These wedges shall be maintained in a satisfactory condition by the Contractor until the permanent asphalt pavement is placed. All costs for placing and maintaining these wedges shall be included in other pay items. No cold mix shall be allowed.

Local Roads: The local roads at the interchanges shall be milled to a depth of 1½" inside the limits of the Interstate ROW. Included in this milling will be the asphalt tie-ins between local roads and the interstate ramps.

Concrete Ramp Shoulders: The existing asphalt shoulders on the concrete sections of the interstate ramps are from the original construction of the interstate. The contractor shall mill these shoulders to a depth of three inches (3") prior to the placement of two - 1½" lifts of 9.5-mm, ST, asphalt.

5. The Contractor shall place asphalt pavement as follows:

Mainline: The Contractor shall place a 2-inch lift of 12.5-mm, SMA, asphalt on the milled surface having a two percent (2%) cross slope or the appropriate super elevation rate in each direction from the centerline. The asphalt shall be placed in a 14-foot wide pass on outside lane and a 16-foot wide pass on the median lane. (The median pass includes the 4-foot paved shoulder.)

A 1½" lift of 9.5-mm, SMA, asphalt shall then be placed on the roadway having a (2%) cross slope or the appropriate super elevation rate in each direction from the centerline. The asphalt shall be placed in a 14-foot wide pass on outside lane and a 16-foot wide pass on the median lane. (The median pass includes the 4-foot paved shoulder.)

A 1½" lift of 9.5-mm, ST, asphalt shall then be placed on the existing outside shoulder pavement. This asphalt will be placed in an 8-foot wide pass and have a (4%) cross slope or the appropriate super elevation rate.

A 1" lift of OGFC asphalt shall then be placed on the roadway having a (2%) cross slope or the appropriate super elevation rate in each direction from the centerline. The asphalt shall be placed in a 14-foot wide pass on both the outside lane and the median lane.

Local Roads: The Contractor shall place 1½" of 9.5-mm, ST, asphalt on the milled surface of the local roads and ramp tie-ins as directed by the Engineer.

Acceleration/Deceleration Lanes & Ramps: The Contractor shall place 1½" of 9.5-mm, HT, asphalt on the acceleration/deceleration lanes and ramps as directed by the Engineer. The concrete section of the ramps will not be overlaid and will remain concrete.

Concrete Ramp Shoulders: The Contractor shall place two - 1½" lifts of 9.5-mm, ST, asphalt on the milled surface of the shoulders as directed by the Engineer. The cross slope of the shoulders should be (4%) or the appropriate super elevation rate. The finished surface of the shoulder asphalt should tie-in flush with the concrete surface and in such a way that water will drain properly.

NOTE: Any work to control the laydown equipment for proper placement of the asphalt in the super elevated curves on this project shall be absorbed by the Contractor at no additional cost to the State.

6. The Contractor shall mill a rumble strip on the mainline shoulders as per the OGFC Rumble Strip Detail Sheet.
7. The Contractor shall install guardrail on the project using the pay items in the contract. Removed guardrail will become property of the Contractor. Guardrail pads are not to be paved.

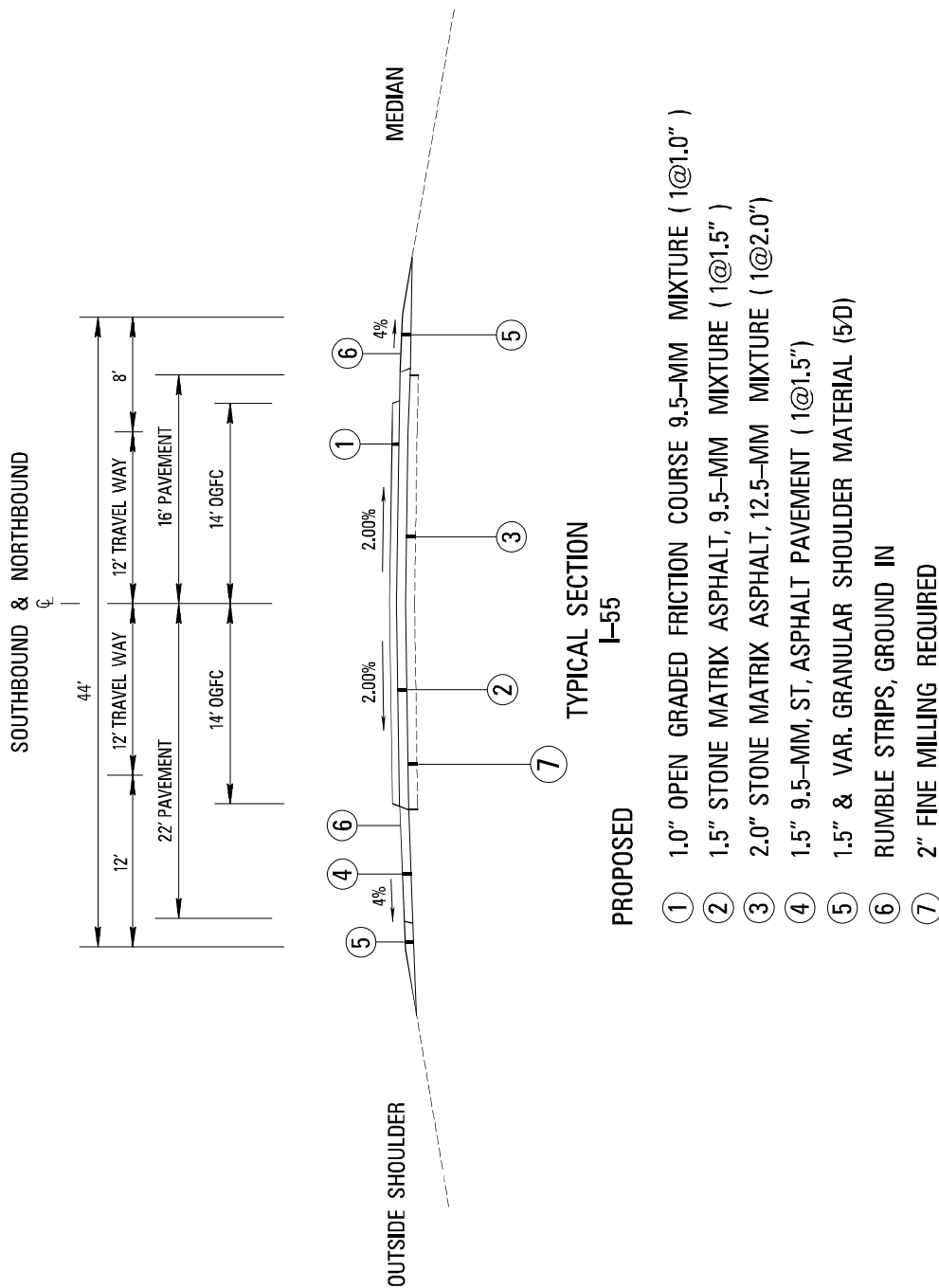
NOTE: On the over pass bridges (147.4, 149.0, 153.0, 156.8, and 160.9), the original Type D bridge end sections are in place, which contains a stub-out section of "w" beam from the concrete bridge rail (Bridge Anchorage). This stub-out may remain in place if it does not

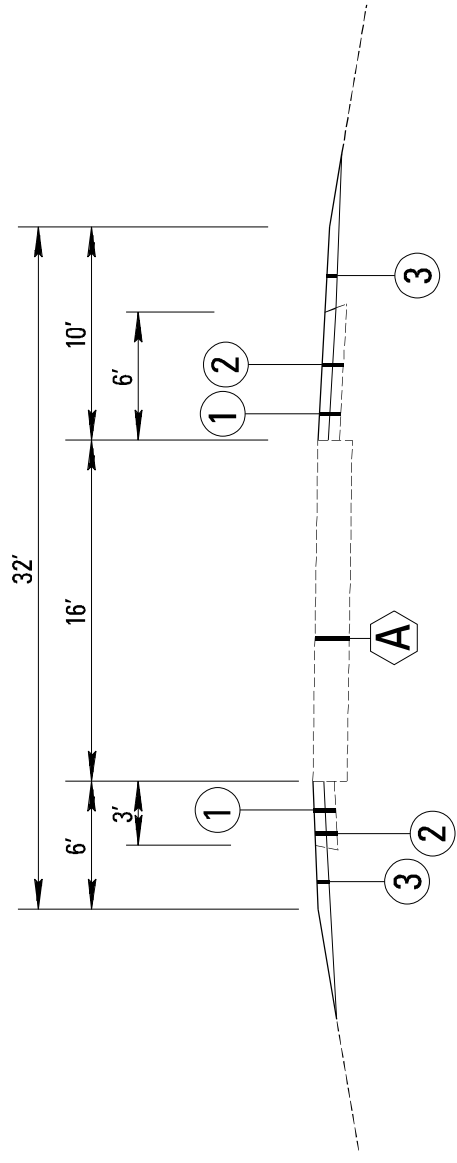
interfere with the installation of the new bridge end section. If removal is necessary, the Contractor shall cut the stub-out back to a distance that it does not interfere. Costs for this removal will be absorbed in the removal of guardrail pay item.

8. Temporary striping shall conform to finished stripe specifications for alignment, reflectivity, straightness, and neatness. Temporary stripe shall be placed as needed for safe movement of traffic. All permanent pavement markings are to be hot thermoplastic on both asphalt and concrete surfaces. All thermoplastic stripe will be placed using the extrusion method. The removal of stripe pay item is for the removal of existing stripe on concrete surfaces prior to the placement of thermoplastic stripe.
9. The existing granular material shoulders shall be raised to match the new pavement elevation by grading existing material and/or placing any needed granular material. The shoulders shall be bladed and dressed to a finished slope of 4%.

Note: Any existing low shoulders or at any time there is a differential in excess of two inches (2"), the Contractor shall raise the shoulder grade up to the current asphalt grade. The Contractor may pull up existing shoulder material if possible or place new granular material. Incidental work such as removing vegetation, shaping and compacting shoulders including the base for paved aprons, and other incidental work that is necessary to complete the work will not be measured for separate payment and the cost will be included in other items bid.

10. Raised pavement markers shall be placed at 80-foot intervals in tangents and 40-foot intervals in curves along the centerline or roadway. Removal of existing raised pavement markers and rumble bars shall be done prior to asphalt placement and shall be considered an absorbed item of work.
11. The maximum length of lane closures shall be three (3) miles with at least one (1) mile between lane closures.
12. Clearing operations must be complete, for the entire direction of travel, prior to beginning asphalt operations in that same direction. Any newly placed asphalt damaged as a result of clearing operations shall be removed and replaced at the Contractor's expense.



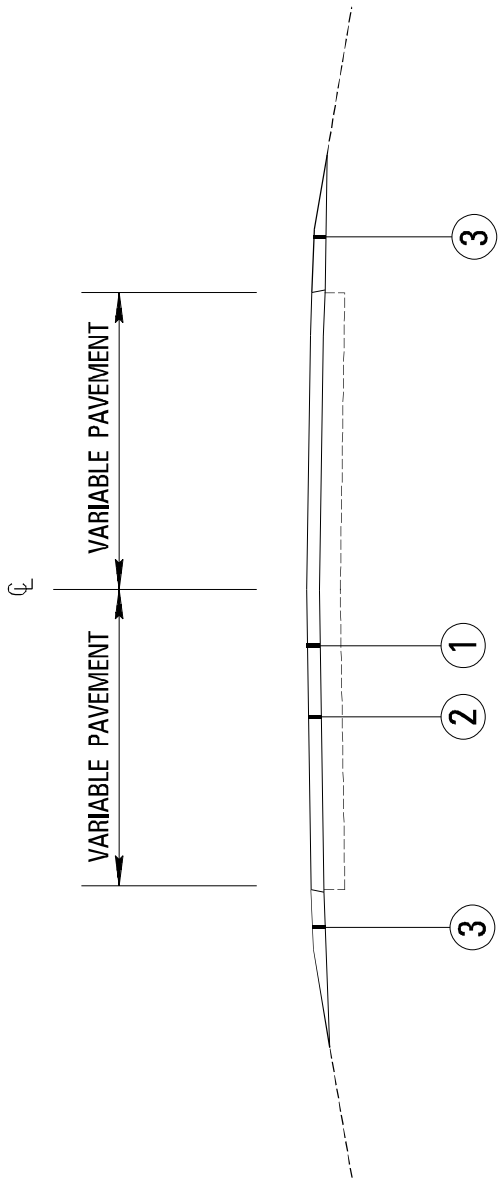


TYPICAL SECTION
INTERSTATE RAMP

A EXISTING 8" CONTINUOUSLY REINFORCED CONCRETE PAVEMENT

PROPOSED

- 1** 3.0" MILLING
- 2** 3.0" 9.5 MM, ST, ASPHALT PAVEMENT (2@1.5")
- 3** 1.5" & VAR. GRANULAR SHOULDER MATERIAL (5D)



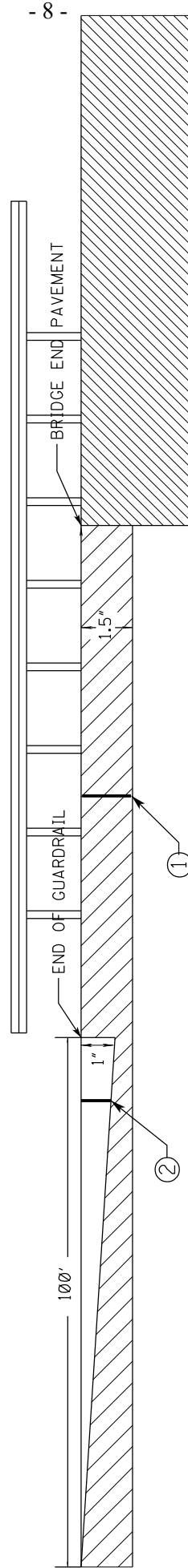
**TYPICAL SECTION
LOCAL ROADS**

PROPOSED

- ① 1.5" MILLING
- ② 1.5" 9.5 MM, ST, ASPHALT PAVEMENT (1@1.5")
- ③ 1.5" & VAR. GRANULAR SHOULDER MATERIAL (5D)

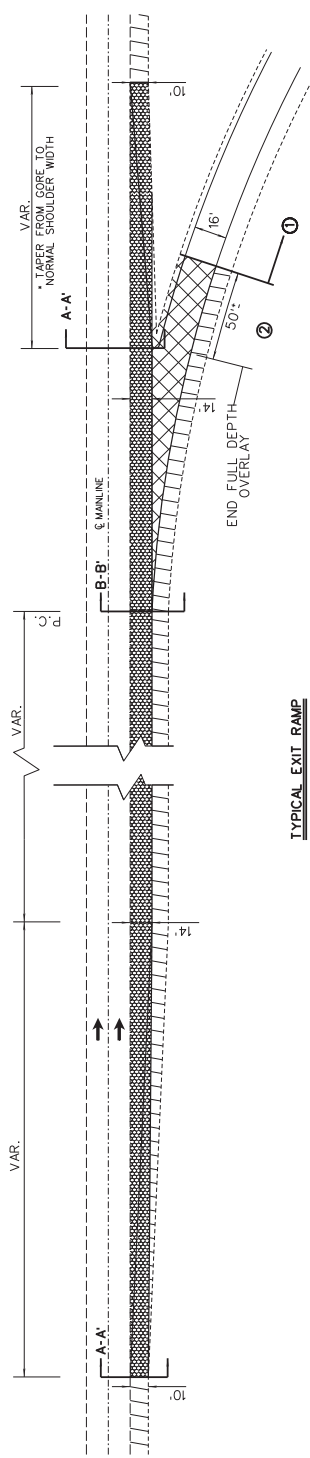
108091/301000
HOLMES COUNTY

OGFC TRANSITION AT
BRIDGE ENDS DETAIL

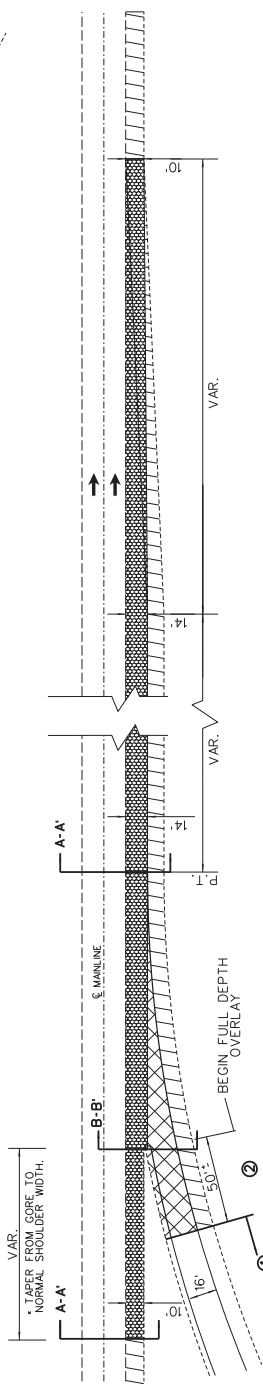


- ① 1.5" FINE MILLING AND REPLACE WITH 1.5" SMA ASPHALT PAVEMENT, SMA (9.5mm MIXTURE)(1@1.5")
- ② 1" AND VARIABLE FINE MILLING & FILL WITH 1" OPEN GRADE FRICTION COURSE.

STATE	PROJECT NO.
MISS.	MM-0059-02(100)



TYPICAL EXIT RAMP



TYPICAL ENTRANCE RAMP

LEGEND

--- EDGE OF PAVEMENT
 --- PAVED SHOULDER LINE

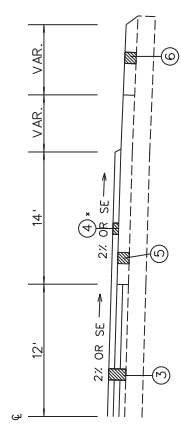
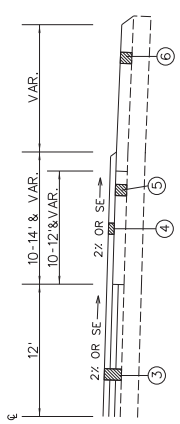
MILL AND INLAY PER TYPICAL SECTION

MILL AND INLAY PER TYPICAL SECTION

1" - OGFC (10'1")
 MIX REQ'D

PAVING DETAILS

- 1 MILL AND OVERLAY OR OVERLAY AS PER APPLICABLE TYPICAL SECTIONS
- 2 MILL AND INLAY AS PER APPLICABLE TYPICAL SECTIONS
- 3 MAINLINE ASPHALT OVERLAY AS PER APPLICABLE TYPICAL SECTION
- 4 1" - OGFC MIX REQ'D (10'1")
- 5 PER TYPICAL SECTION
- 6 PER TYPICAL SECTION



* COMPACT OUTER 2' OF OGFC TO 1/2" THICKNESS

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

TYPICAL SECTION

OGFC PAVING DETAILS
 AT INTERCHANGE RAMPS

(PROJECT NUMBER)

(COUNTY)

FILENAME: OGFC AT INTERCHANGE.DGN

DATE

CHECKED

DESIGN TEAM

REVISION

BY

DATE

WORLD NUMBER

SHEET NUMBER

X

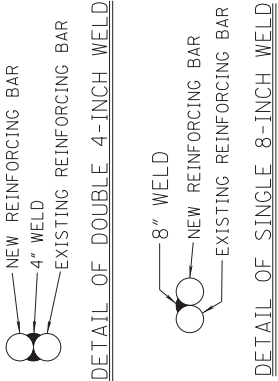
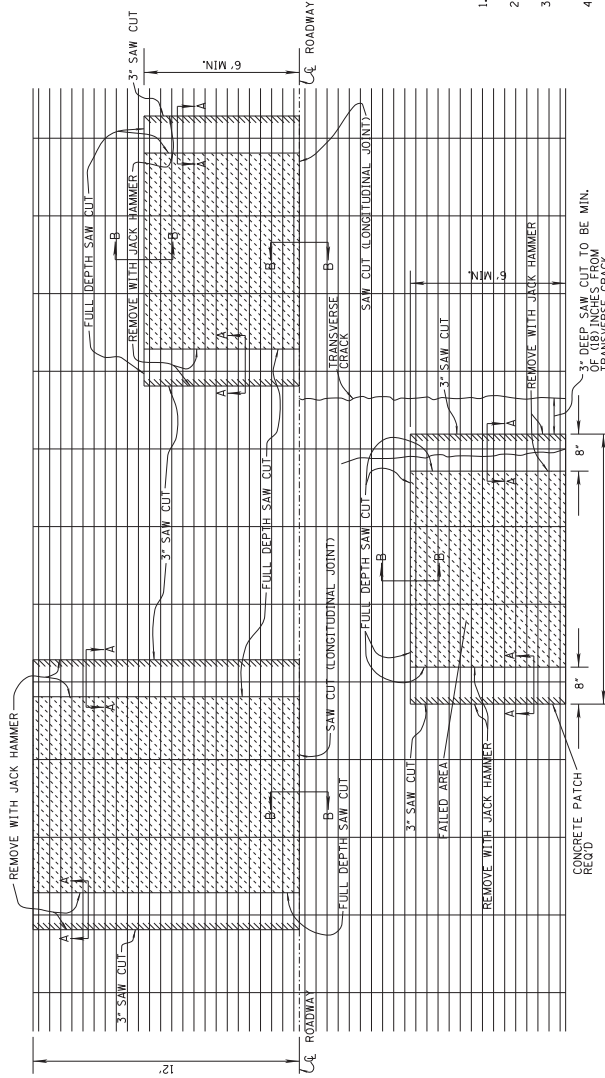
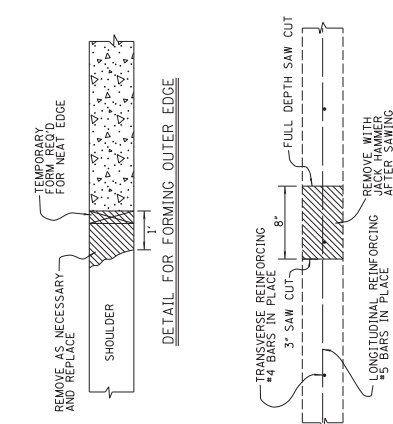
Guardrail												
Station	Station		Removal	w-beam	Terminals Flared	Terminals Non-flared	Br. End Sect. Type D Mod	Delineator White	Delineator Yellow	Type 3 Obj. Marks.	Cab. Anch. Type 1	
			LF	LF	EA	EA	EA	EA	EA	EA	EA	
					I-55							
Box Creek	Bx.Br.# 150.3 A		RT	400	350	1					1	
	Bx.Br.# 150.3 A		LT	350	300	1					1	
Hwy. 12	Br.# 155.0 A	N	LT	215	150	1	1	7		1		
	Br.# 155.0 A	N	RT	215	150	1	1		7	1		
	Br.# 155.0 B	S	LT	215	150	1	1			1		
	Br.# 155.0 B	S	RT	215	150	1	1	7		1		
Jordan Creek	Br.# 162.9 A	N	LT	215	150	1	1	7		1		
	Br.# 162.9 A	N	RT	215	150	1	1		7	1		
	Br.# 162.9 B	S	LT	215	150	1	1		7	1		
	Br.# 162.9 B	S	RT	215	150	1	1	7		1		
West Exit	Br.# 163.5 A	N	LT	215	150	1	1	7		1		
	Br.# 163.5 A	N	RT	203	137.5	1	1		7	1		
	Br.# 163.5 B	S	LT	215	150	1	1		7	1		
	Br.# 163.5 B	S	RT	215	150	1	1	7		1		
RaceTrack Rd.	Br.# 165.7 A	N	LT	196	131.25	1	1	7		1		
	Br.# 165.7 A	N	RT	196	131.25	1	1		7	1		
	Br.# 165.7 B	S	LT	209	143.75	1	1		7	1		
	Br.# 165.7 B	S	RT	209	143.75	1	1	7		1		
Overpass Bridges												
Hwy. 14 Br. # 69.2		NE		215	150		1	1	7	1		
		SE		115	50		1	1	4	1		
		NW		115	50		1	1	4	1		
		SW		215	150		1	1	7	1		
Franklin Rd. Br. #147.4		NE		150	112.5		1	1	6	1		
		SE		100	62.5		1	1	4	1		
		NW		100	62.5		1	1	4	1		
		SW		200	162.5		1	1	7	1		
State Park Rd. Br. # 149.0		NE		215	150		1	1	7	1		
		SE		115	50		1	1	4	1		
		NW		115	50		1	1	4	1		
		SW		215	150		1	1	7	1		
Castalian Sp. Rd. Br. # 153.0		NE		200	162.5		1	1	7	1		
		SE		100	62.5		1	1	4	1		
		NW		100	62.5		1	1	4	1		
		SW		200	162.5		1	1	7	1		
Bowling Gr. Rd. Br. # 156.8		NE		200	162.5		1	1	7	1		
		SE		100	62.5		1	1	4	1		
		NW		100	62.5		1	1	4	1		
		SW		200	162.5		1	1	7	1		
West-Bowling Gr. Rd. Br. # 160.9		NE		200	162.5		1	1	7	1		
		SE		100	62.5		1	1	4	1		
		NW		100	62.5		1	1	4	1		
		SW		200	162.5		1	1	7	1		
Totals =			7798	5538	18	24	40	187	56	40	2	

PUNCH-OUT REPAIR														
Lane	Latitude Dec./Deg.	Longitude Dec./Deg.	Length LF	Width LF	Removal CRCP SY	#5 Tie Bars EA	Long. Saw- Cut LF	F. Depth Saw- Cut LF	8"CRCP SY	Removal of Asphalt SY	Excess Excavation CY	19-mm Asphalt TON		Notes
NB	32.9470	89.9730	12	12	16.0	30	12	24	16.0	17	5	2		
NB	32.9720	89.9651	12	12	16.0	30	12	24	16.0	17	5	2		
NB	32.9762	89.9630	12	12	16.0	30	12	24	16.0	17	5	2		
NB	32.9877	89.9573	12	12	16.0	30	12	24	16.0	17	5	2		
NB	33.0379	89.9358	12	12	16.0	30	12	24	16.0	17	5	2		
NB	33.0642	89.9042	12	12	16.0	30	12	24	16.0	17	5	2		
NB	33.0661	89.9031	12	12	16.0	30	12	24	16.0	17	5	2		
NB	33.07220	89.9006	12	12	16.0	30	12	24	16.0	17	5	2		
NB	33.0867	89.8953	12	12	16.0	30	12	24	16.0	17	5	2		
NB	33.1069	89.8917	12	12	16.0	30	12	24	16.0	17	5	2		
NB	33.1094	89.8892	12	12	16.0	30	12	24	16.0	17	5	2		
NB	33.1206	89.8728	25	12	33.3	50	25	24	33.3	35	11	3		
NB	33.1953	89.8319	12	12	16.0	30	12	24	16.0	17	5	2		
NB	33.2022	89.8267	12	12	16.0	30	12	24	16.0	17	5	2		
NB	33.2053	89.8247	12	12	16.0	30	12	24	16.0	17	5	2		
NB	33.2150	89.8161	12	12	16.0	30	12	24	16.0	17	5	2		
NB	Rest area	Rest area	26	16	46.2	32		32	46.2	17	15	5		located at rest area entrance ramp
SB	33.2186	89.8133	12	12	16.0	30	12	24	16.0	17	5	2		
SB	33.0739	89.9003	12	12	16.0	30	12	24	16.0	17	5	2		
Total =					352	592.0	229.0	464.0	351.6	341	117	36		
					SY	EA	LF	LF	SY	SY	CY	TON		
-														

Notice to Bidders No. 3908-- Cont'd.

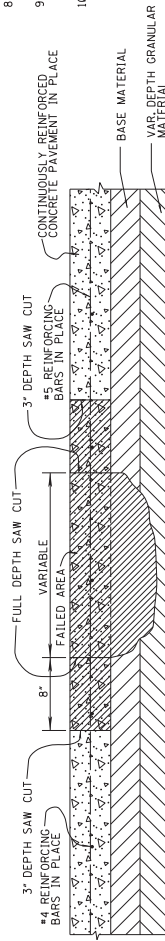
RANDOM CLEARING (ACRE)					
From	To	SB LT Outside Acres	SB RT Median Acres	NB LT Median Acres	NB RT Outside Acres
B.O.P	SR14 (Br. 69.2)	2	2	3	2
SR14 (Br. 69.2)	Franklin Rd. (Br. 147.4)	1	0	0	1
Franklin Rd. (Br. 147.4)	State Park Rd. (Br. 149.0)	1	0	0	1
State Park Rd. (Br. 149.0)	Castalian Springs Rd. (Br. 153.3)	4	4	5	5
Castalian Springs Rd. (Br. 153.3)	SR 12 (Brs. 155.0 A&B)	2	0	0	3
SR 12 (Brs. 155.0 A&B)	Bowling Green Rd.(Br. 156.8)	1	0.5	0.5	1
Bowling Green Rd.(Br. 156.8)	West-Bowling Green Rd. (Br. 160.9)	3	0	0	4
West-Bowling Green Rd. (Br. 160.9)	Emory Rd. (Brs. 163.5 A&B)	4	2	2	2
Emory Rd. (Brs. 163.5 A&B)	E.O.P	2	0	0	2
Total =		20	8.5	10.5	21
				Total(Ac.) =	60

STATE	PROJECT NO.
MISS.	

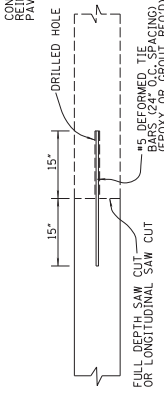
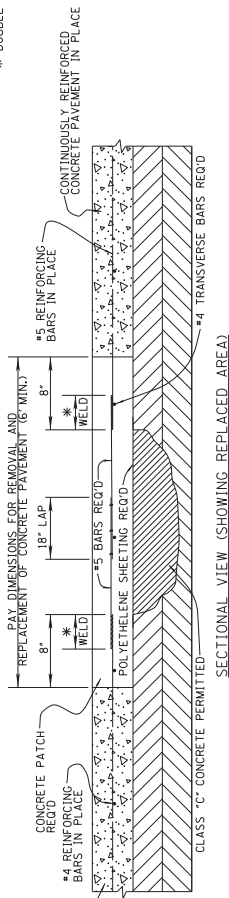


GENERAL NOTES

1. REMOVE EXISTING MATERIALS TO DIMENSIONS DETERMINED BY THE ENGINEER.
2. REMOVAL OF ASPHALT PATCHES AND CONCRETE PAVEMENT WILL BE PAID FOR UNDER THE APPROPRIATE PAY ITEM.
3. REINFORCING BARS TO BE FIELD CUT AS DIRECTED BY THE ENGINEER. COST OF REQUIRED REINFORCING BARS TO BE INCLUDED IN THE BID PRICE OF CONCRETE PAVEMENT.
4. REMOVAL OF FAILED BASE (ABSORBED ITEM) REPAIR BACKFILL WITH CLASS "C" CONCRETE (BASE REPAIR)
5. PAVEMENT EDGE ADJACENT TO SHOULDER SHALL BE FORMED.
6. SEE SHEET NO. 6101 FOR DETAILS NOT SHOWN.
7. POLYETHYLENE SHEETING SHALL BE TWO (2) LAYERS OF 8 MIL THICKNESS (ABSORBED ITEM)
8. REINFORCING BARS WILL BE SUPPORTED AS SHOWN ON SHEET NO. 6101.
9. ALL SAW CUTS (3\"/>



SECTIONAL VIEW (SHOWING AREA TO BE REMOVED)



SECTION B-B

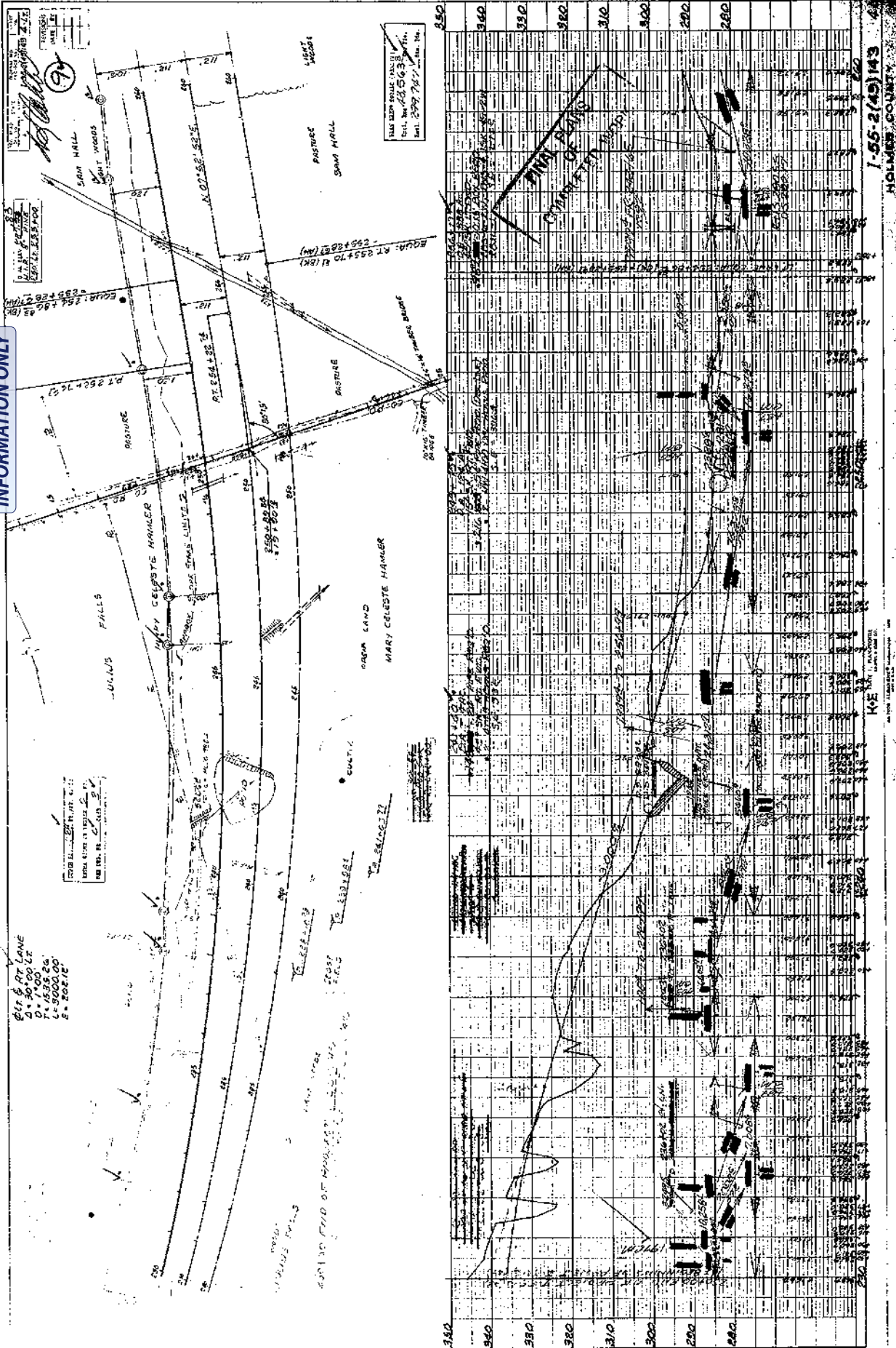
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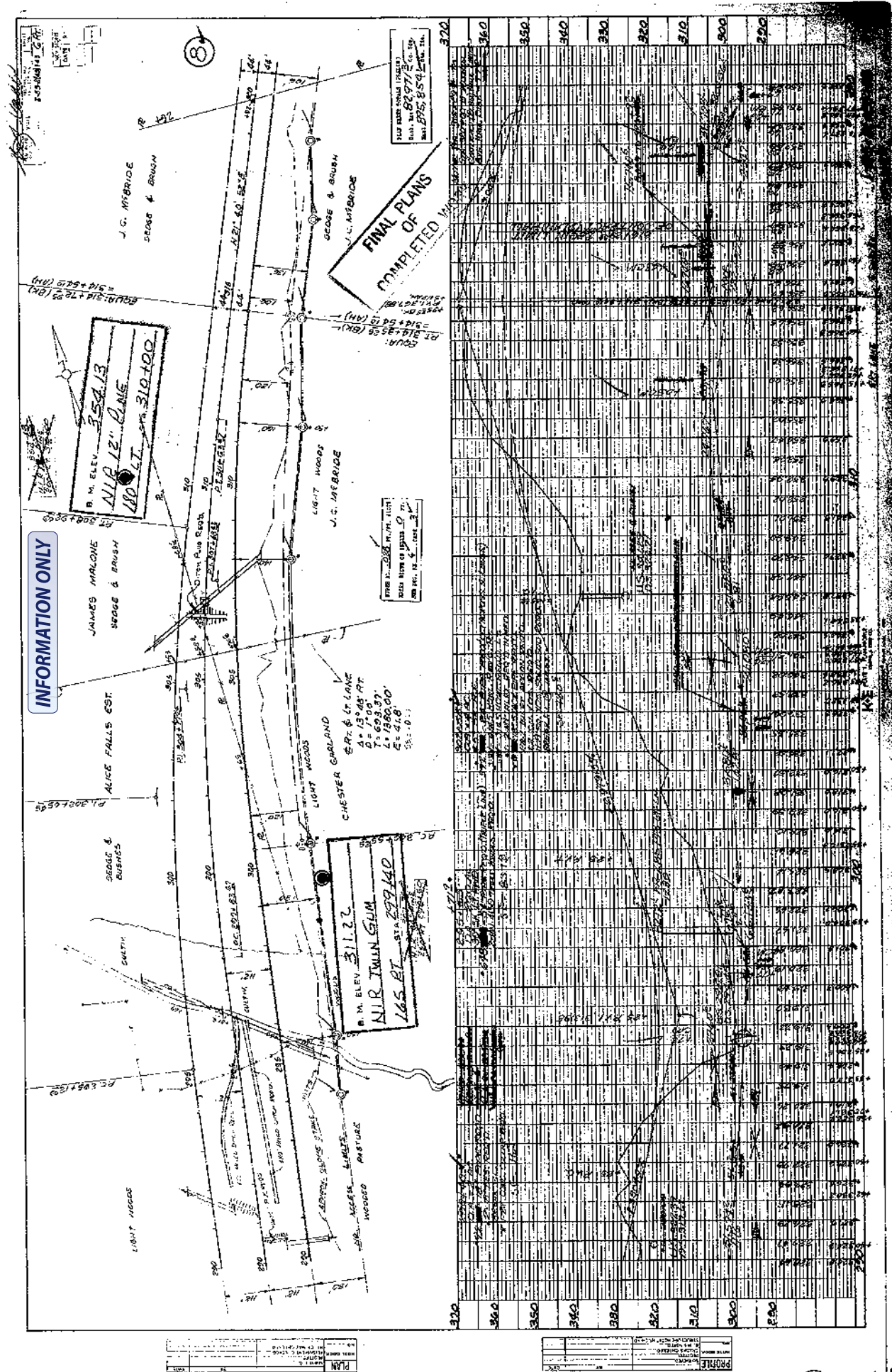
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TYPICAL CRC PAVEMENT REPAIR	
(OPTIONAL WELDING METHOD)	
BY	REVISION
DATE	FILENAME:
DESIGN TEAM	CHECKED
DATE	DATE

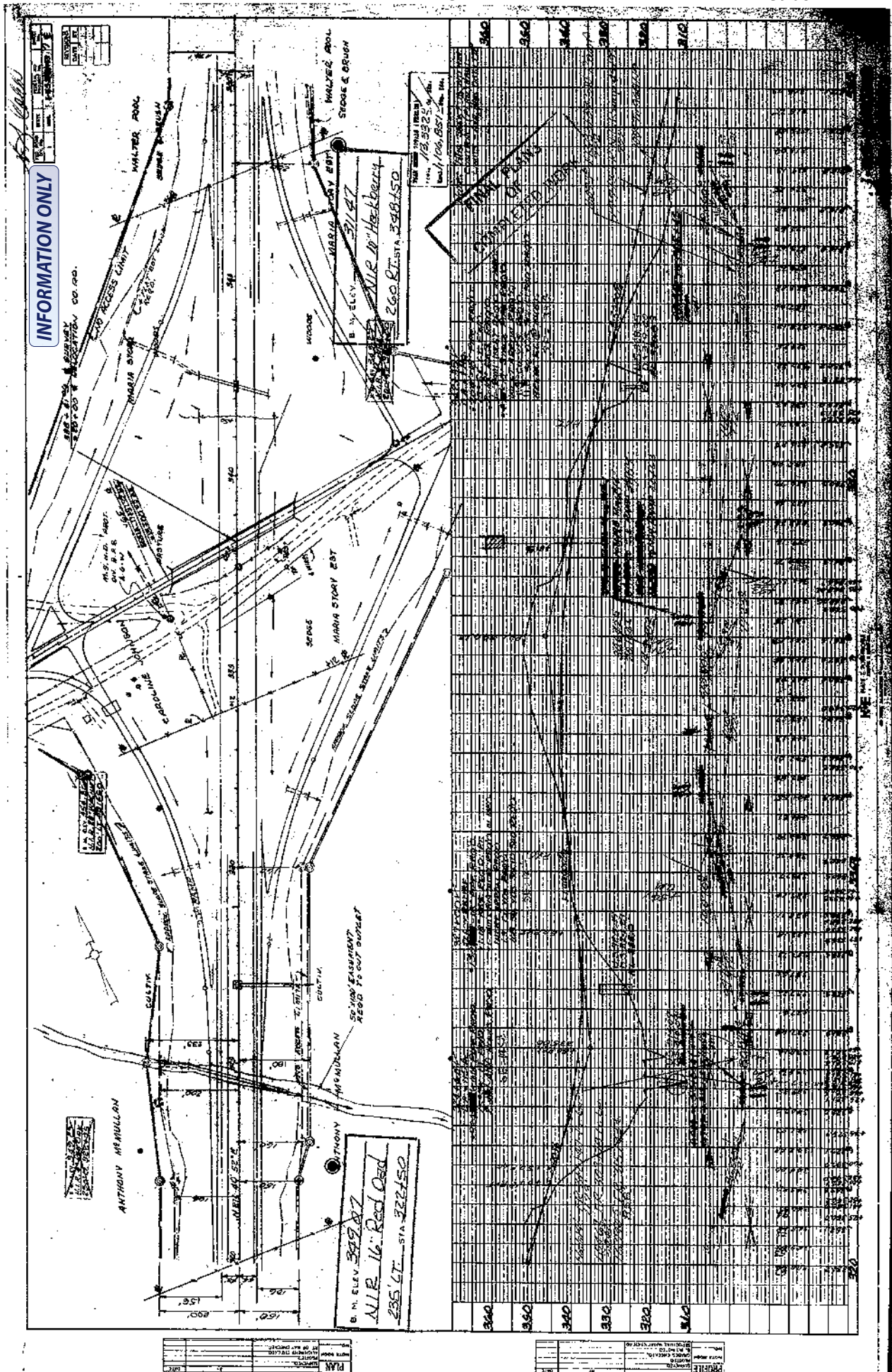


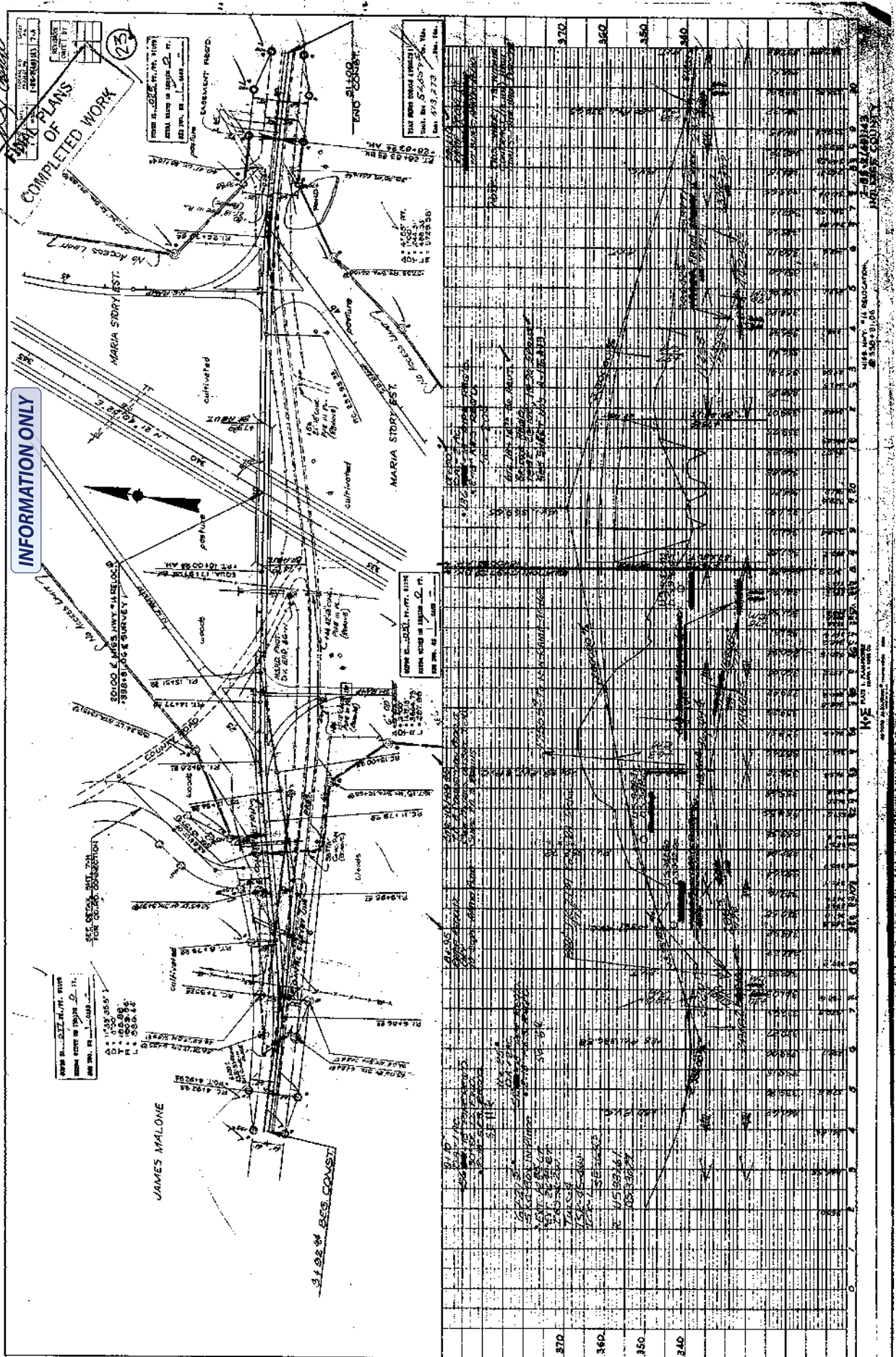
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PC-1B
SHEET NUMBER

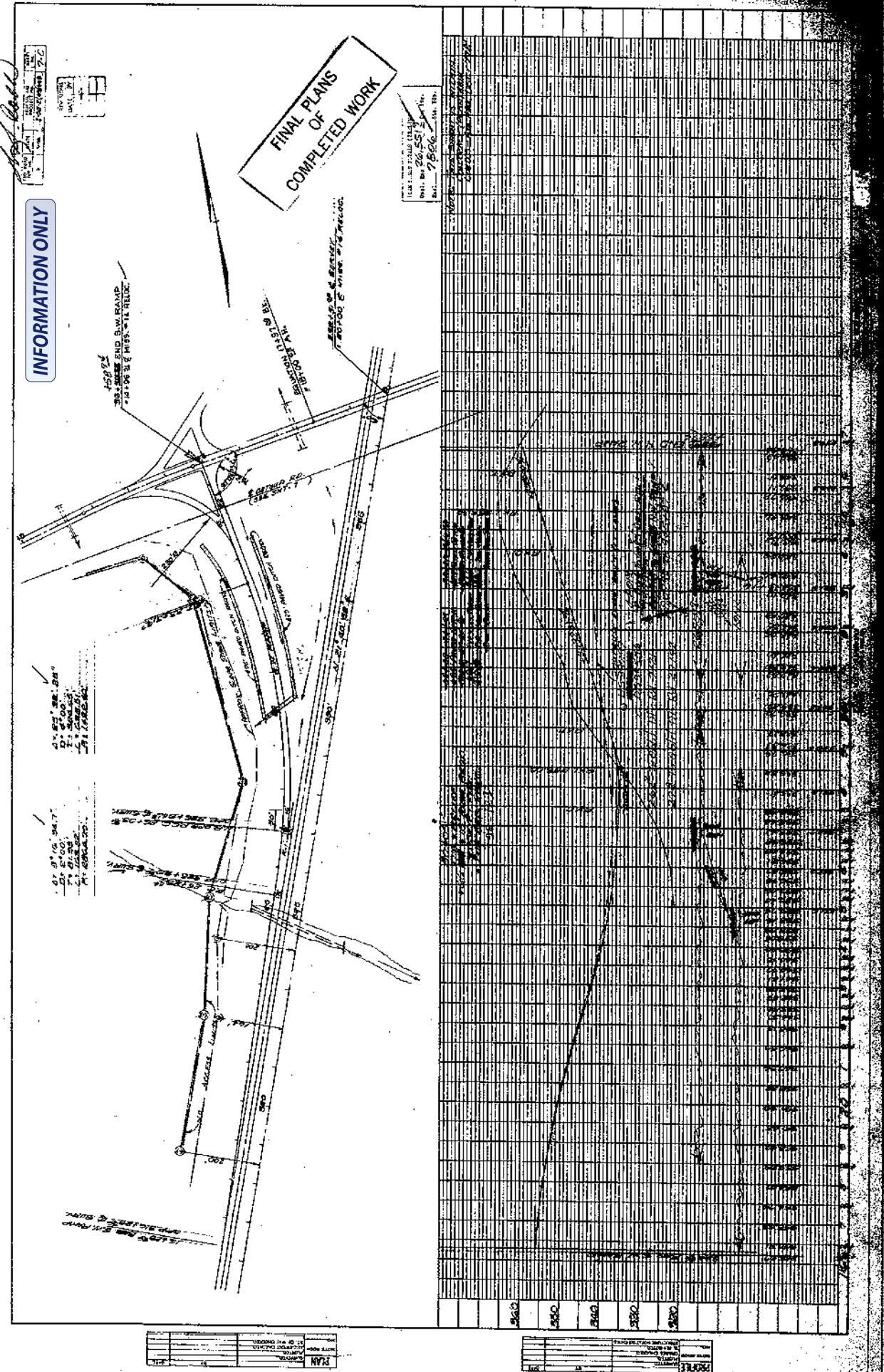
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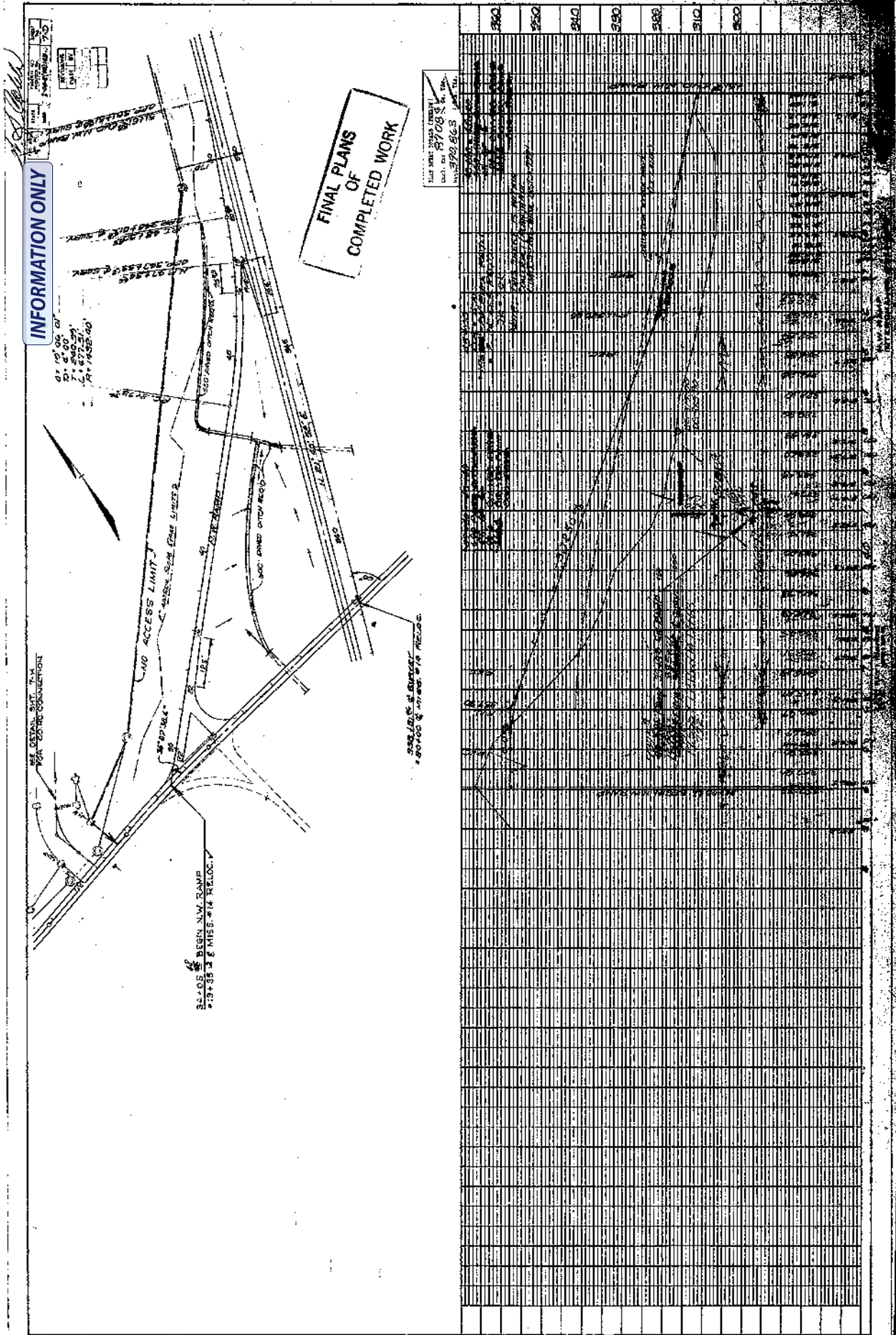




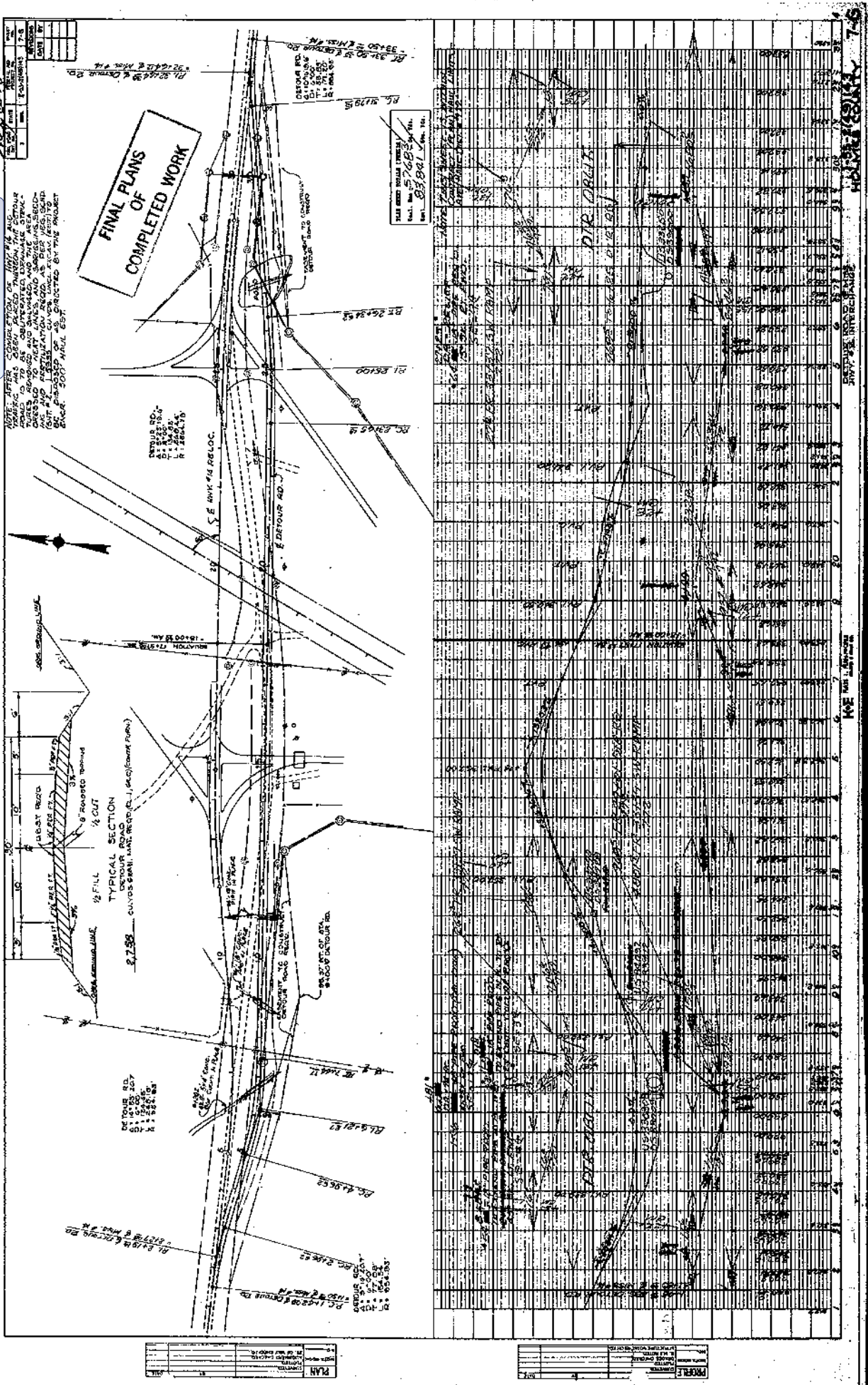


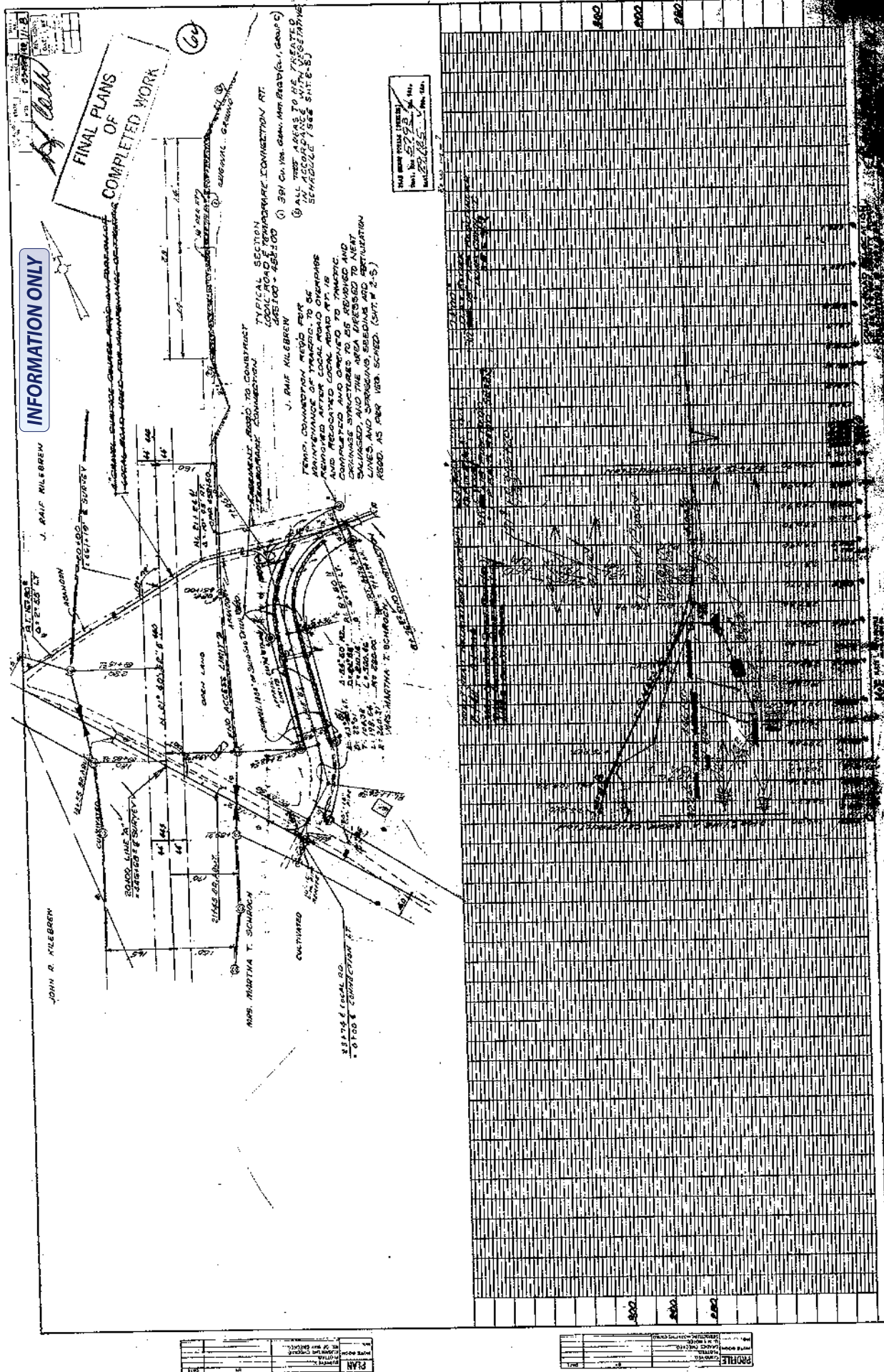


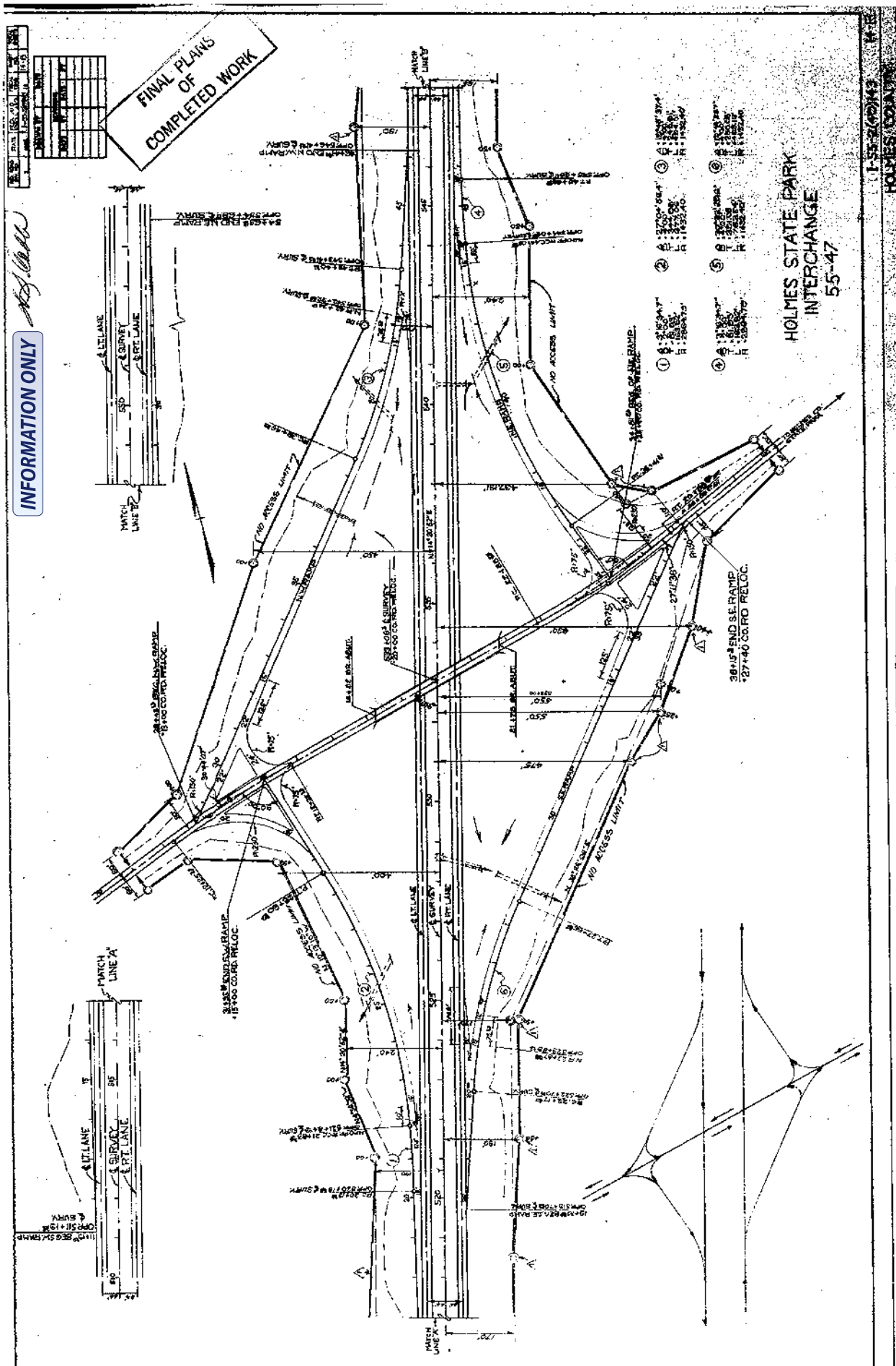




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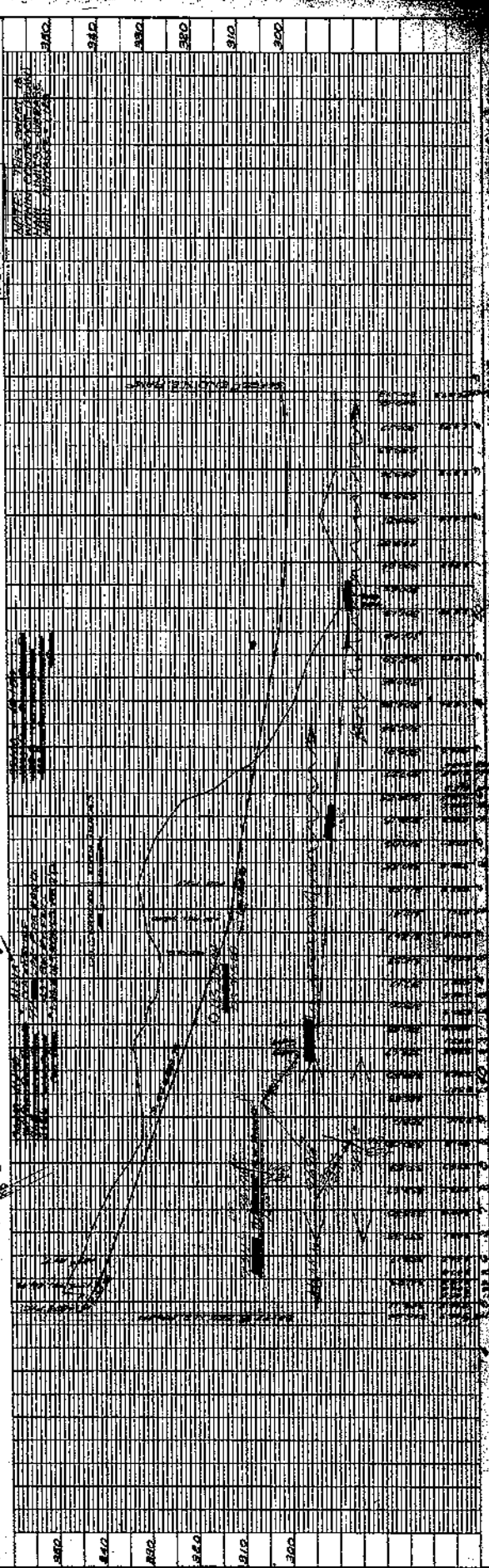
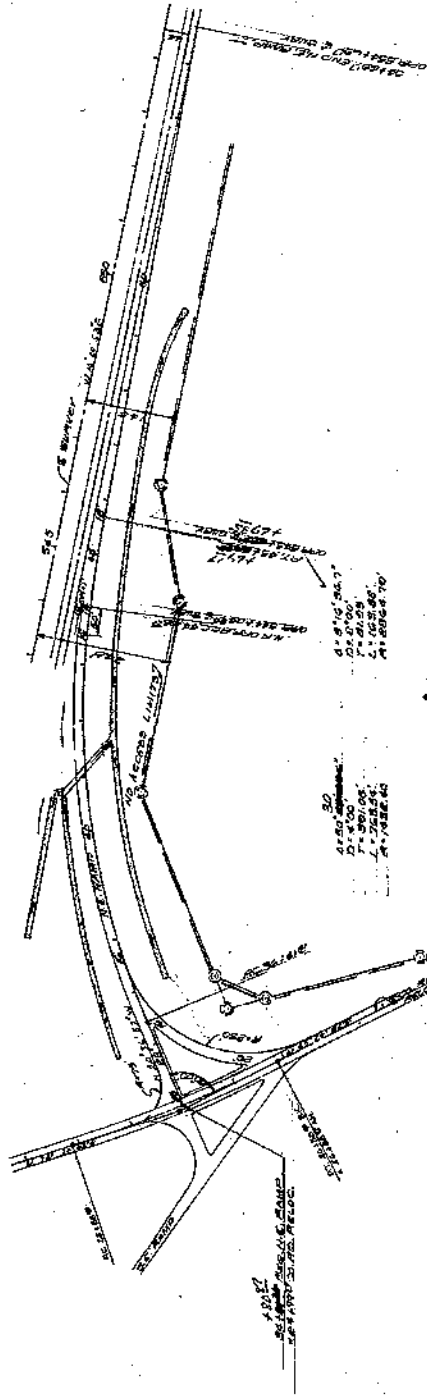


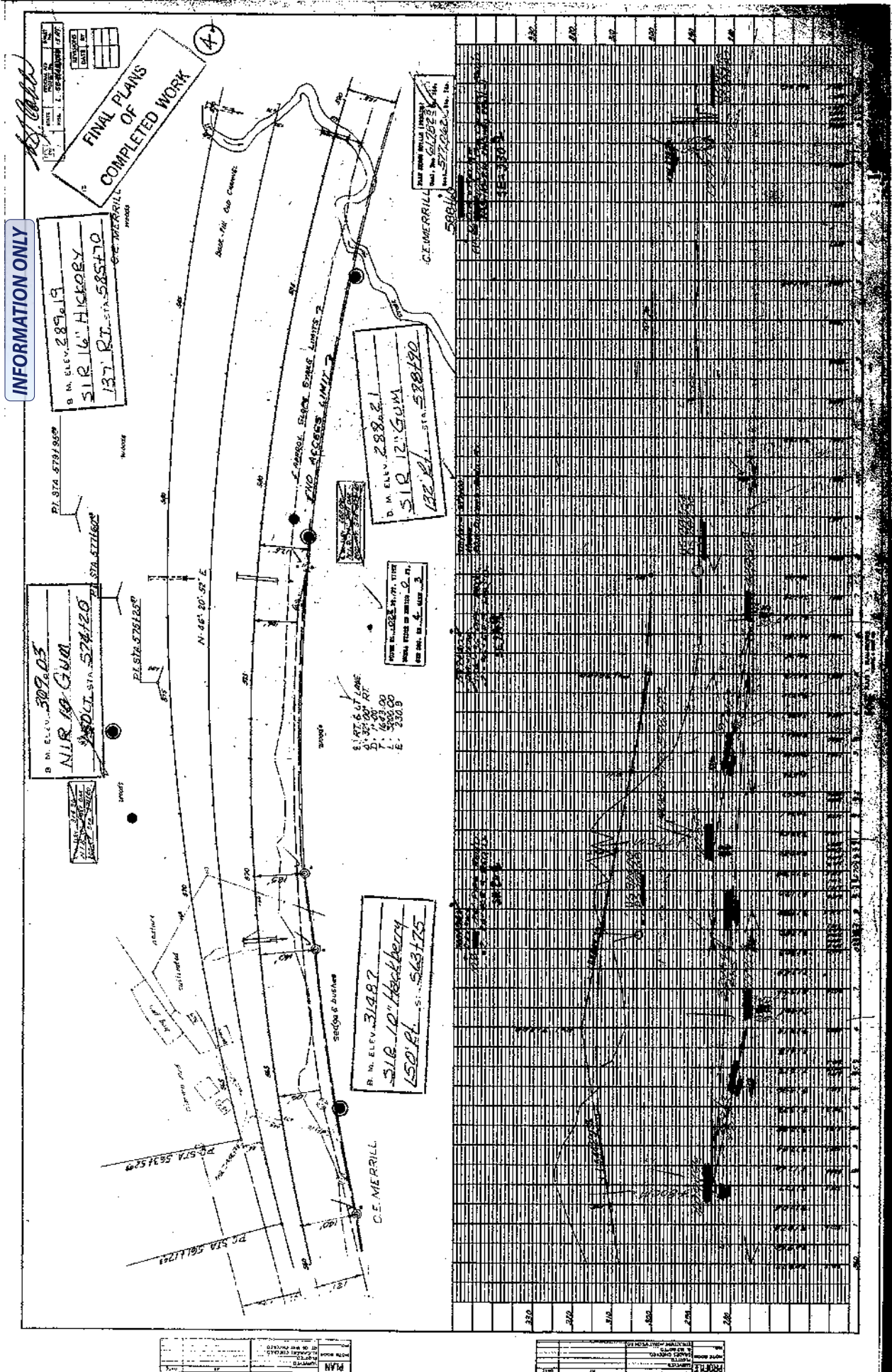


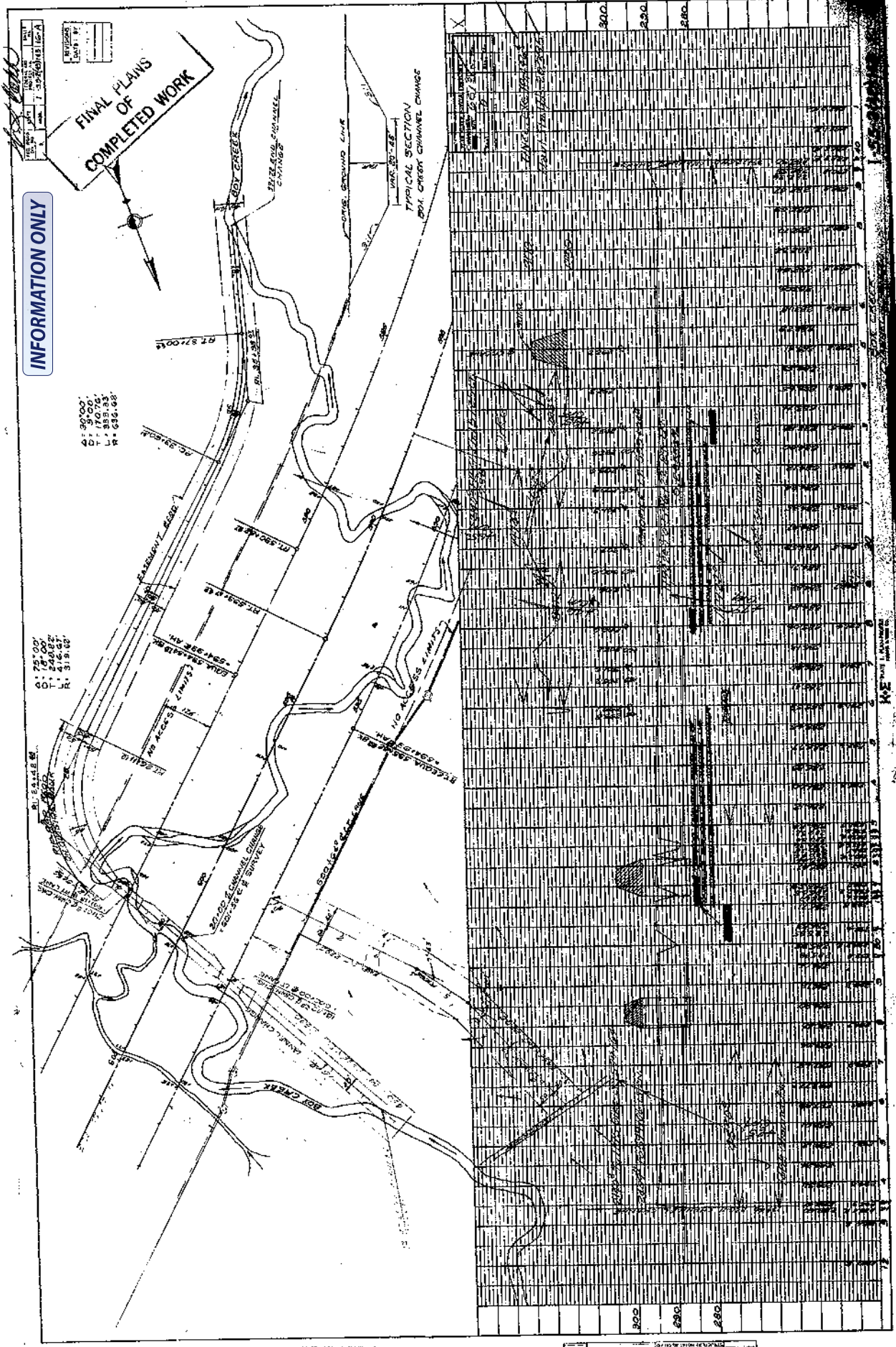


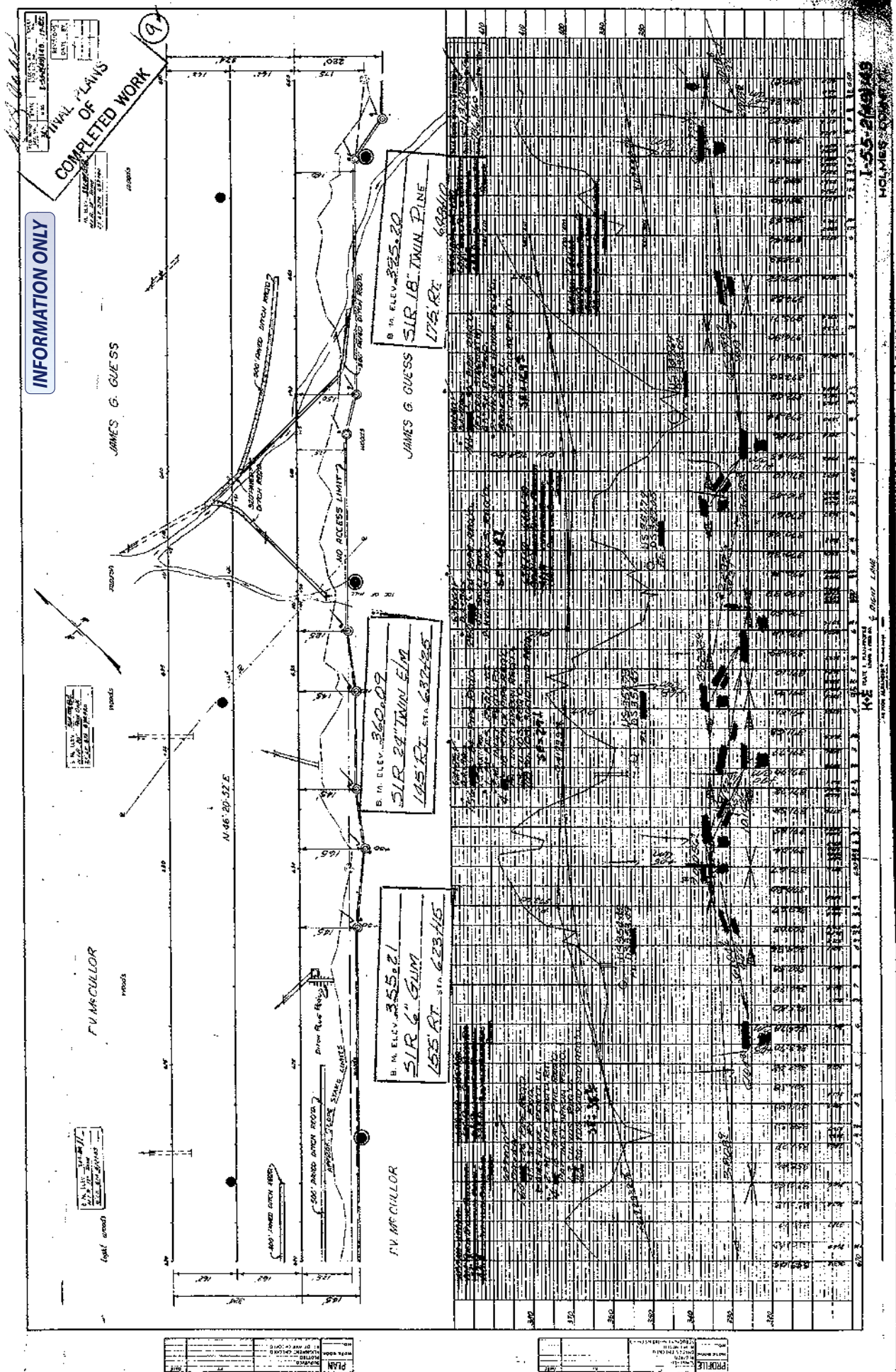
INFORMATION ONLY

FINAL PLANS
OF
COMPLETED WORK

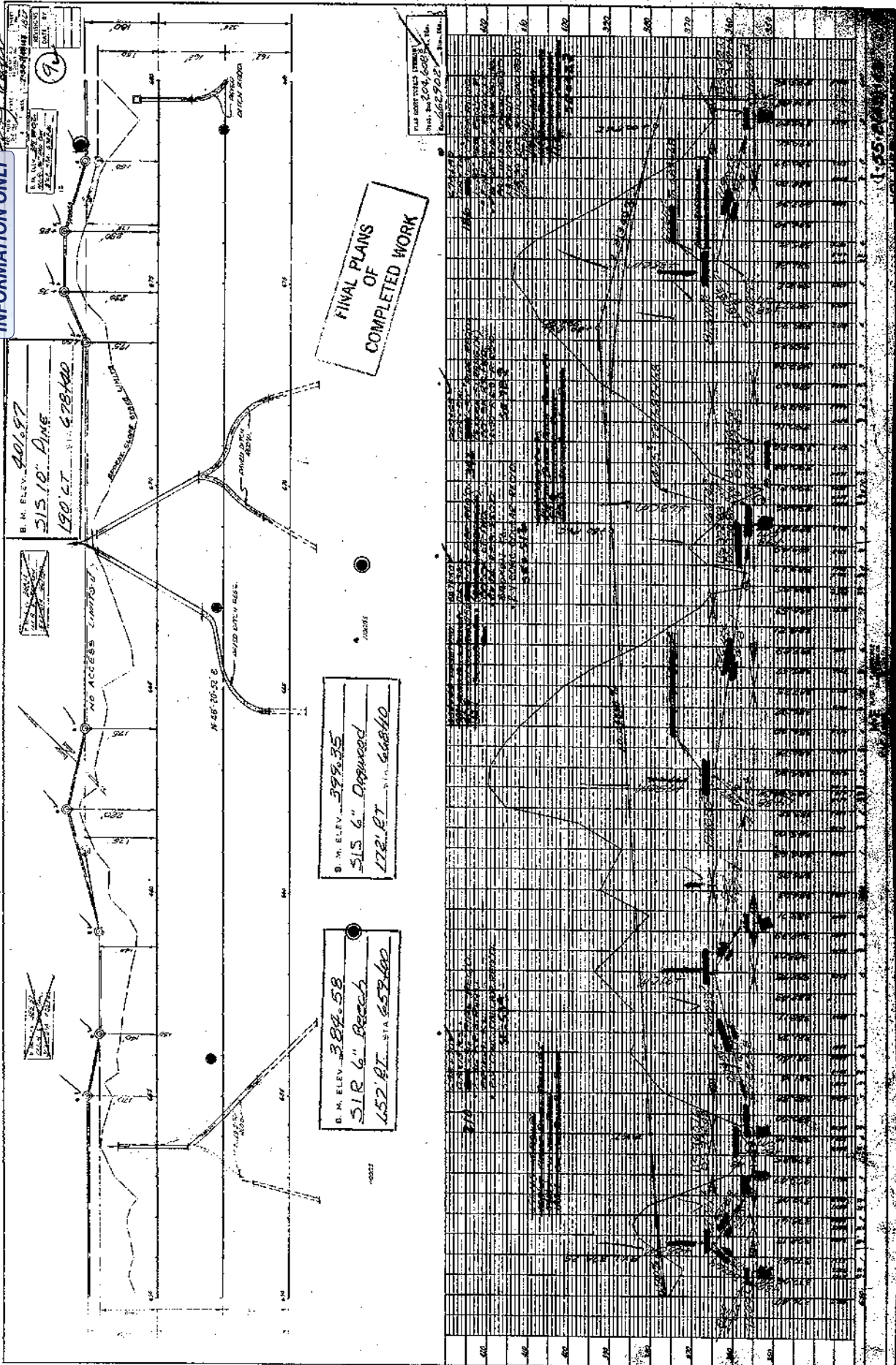


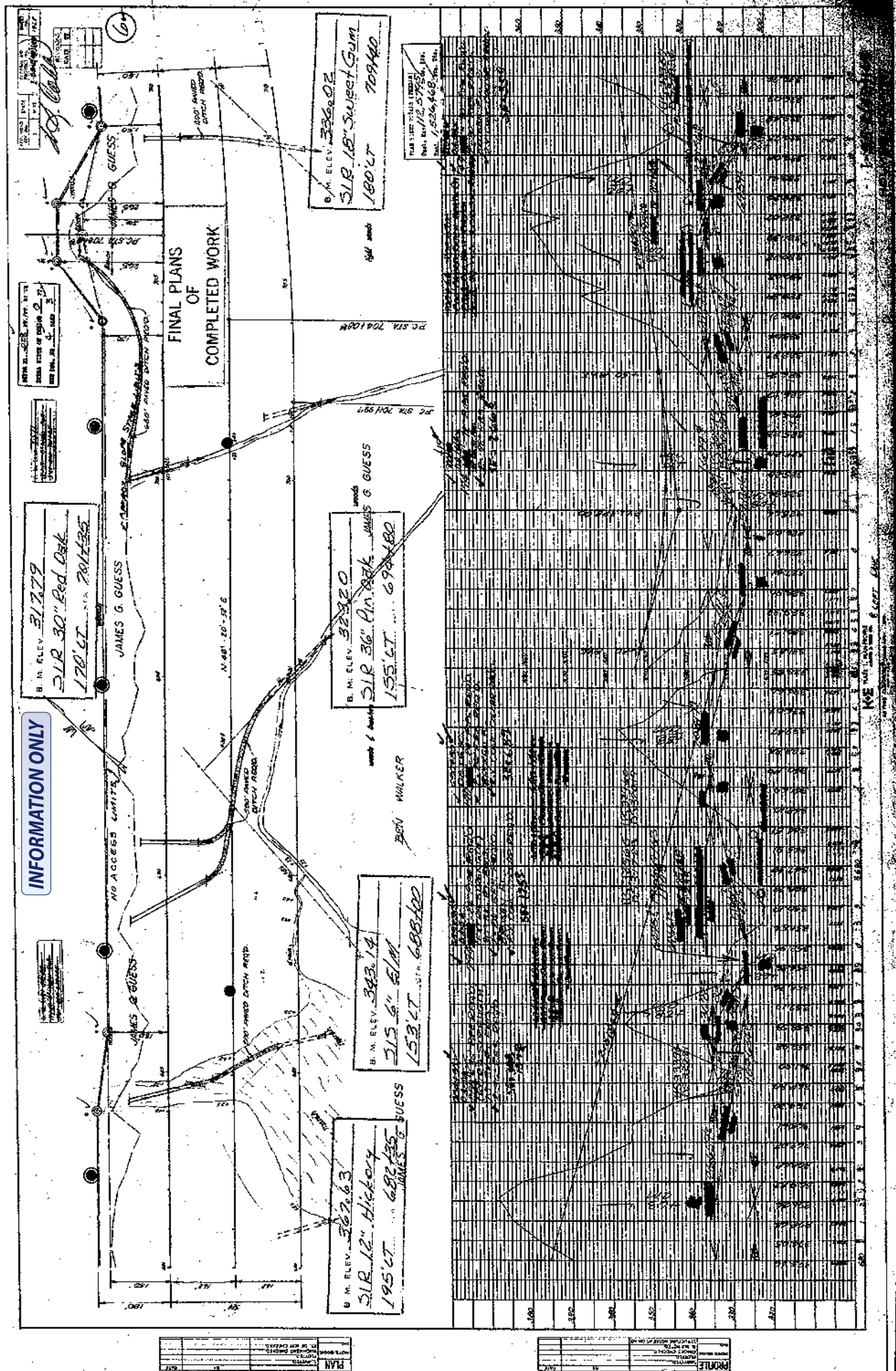




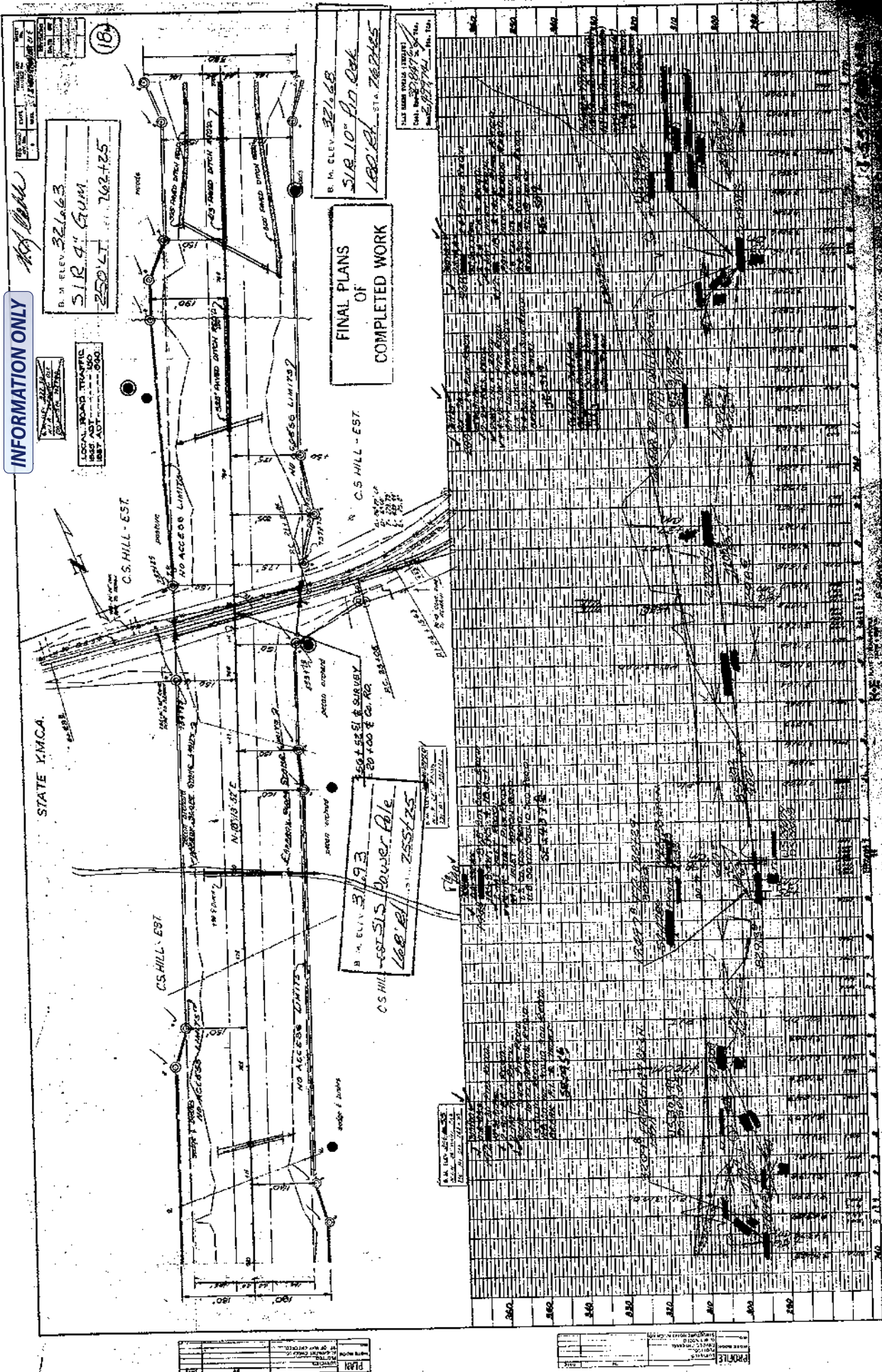


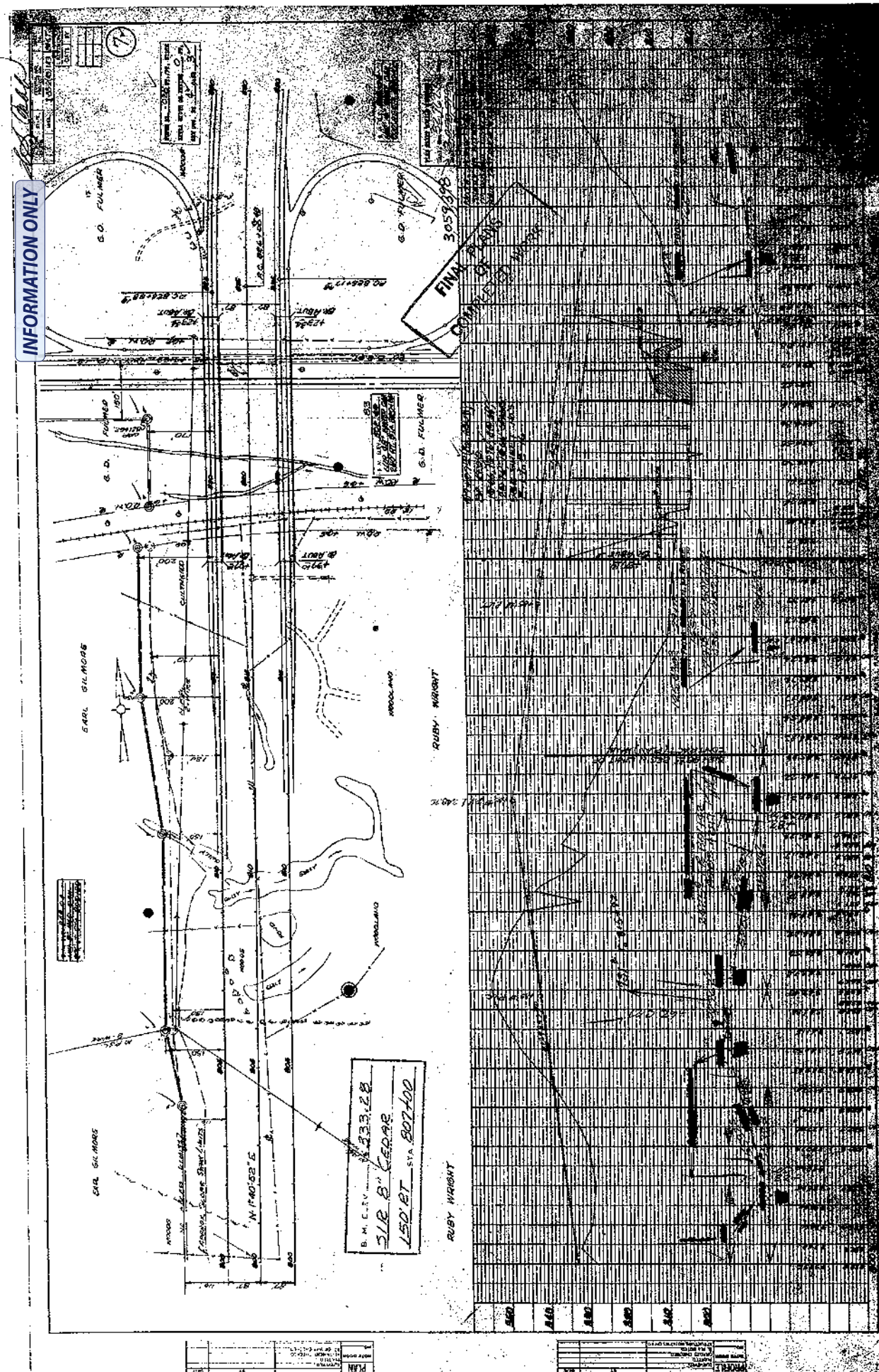
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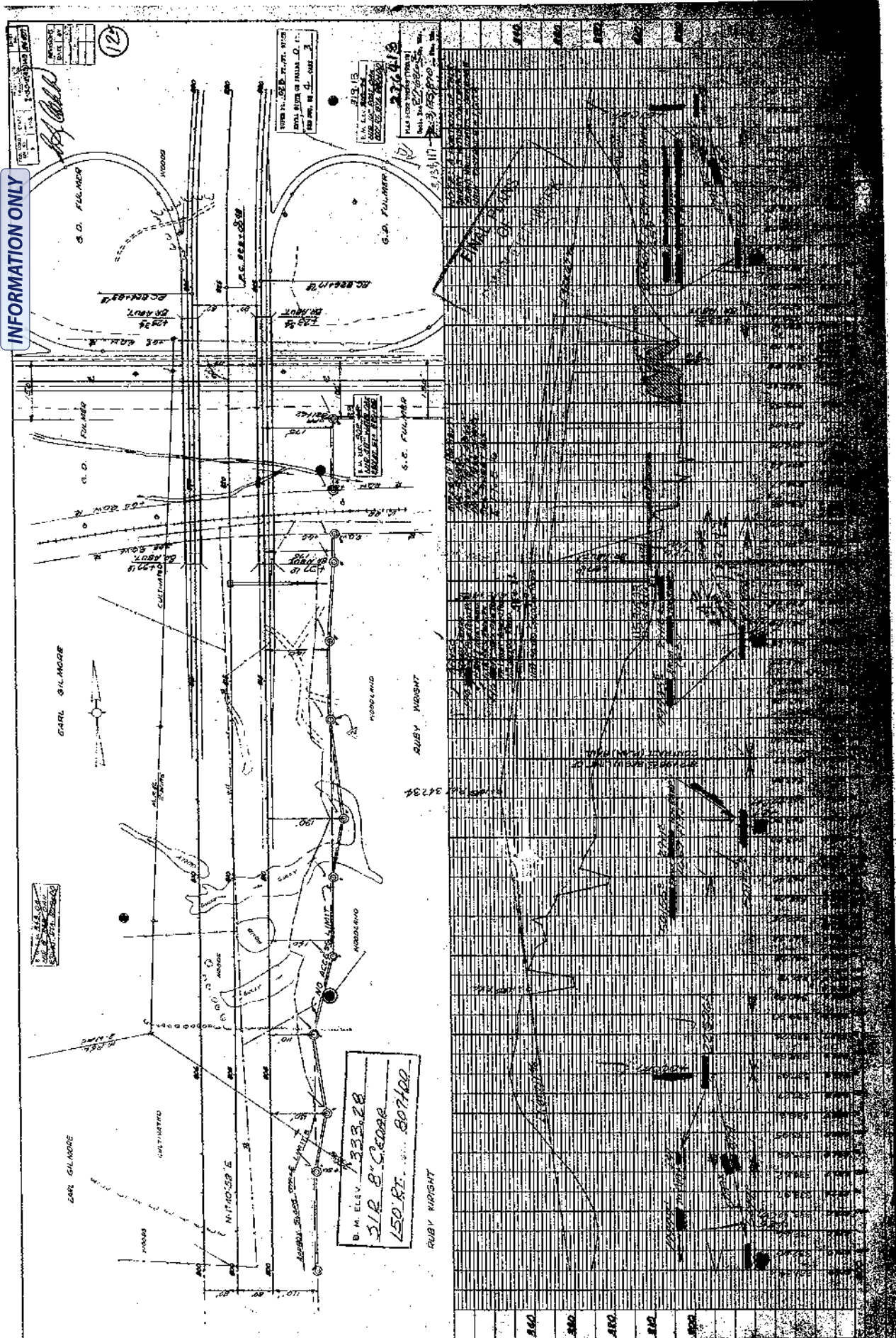


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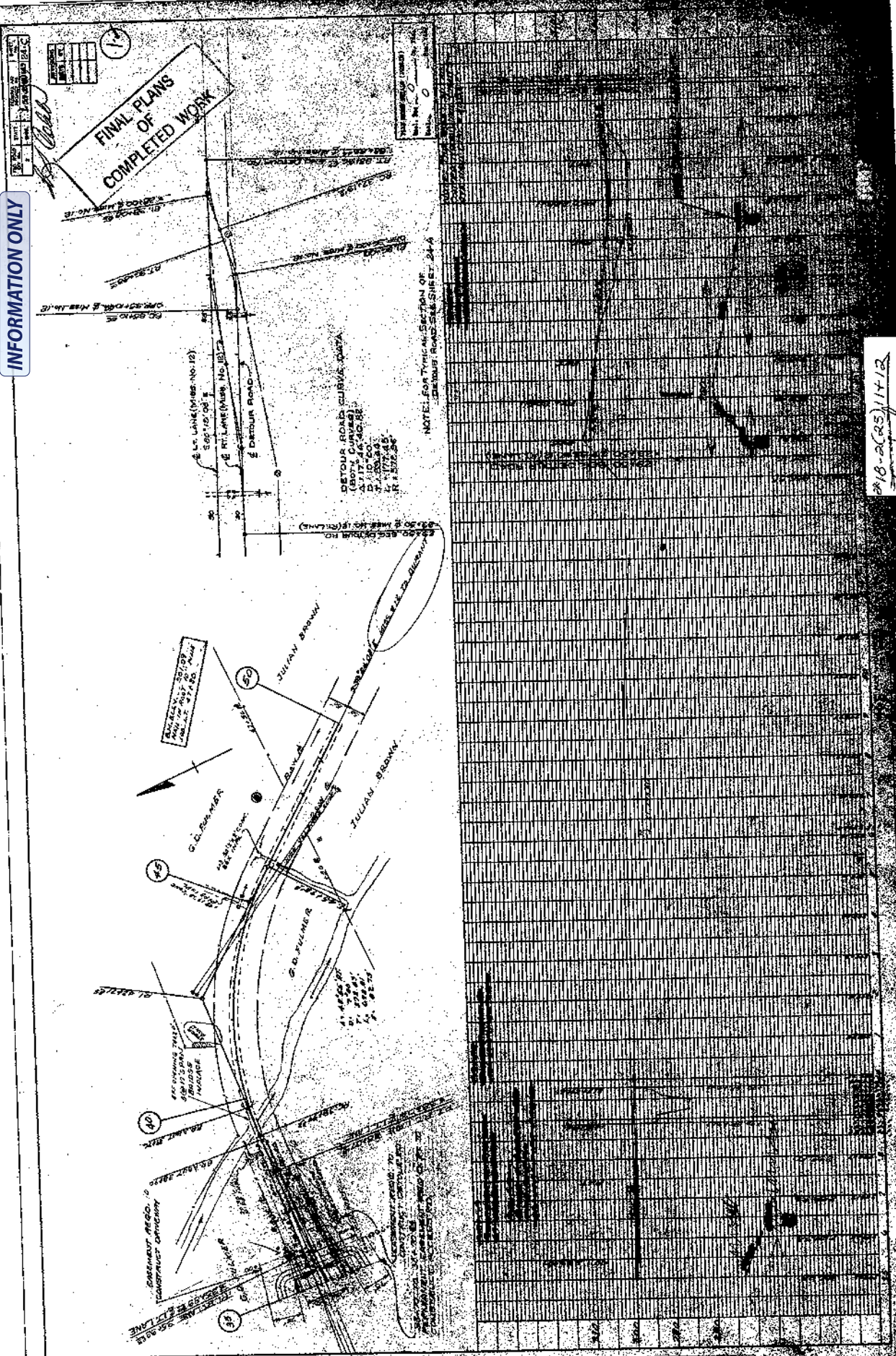




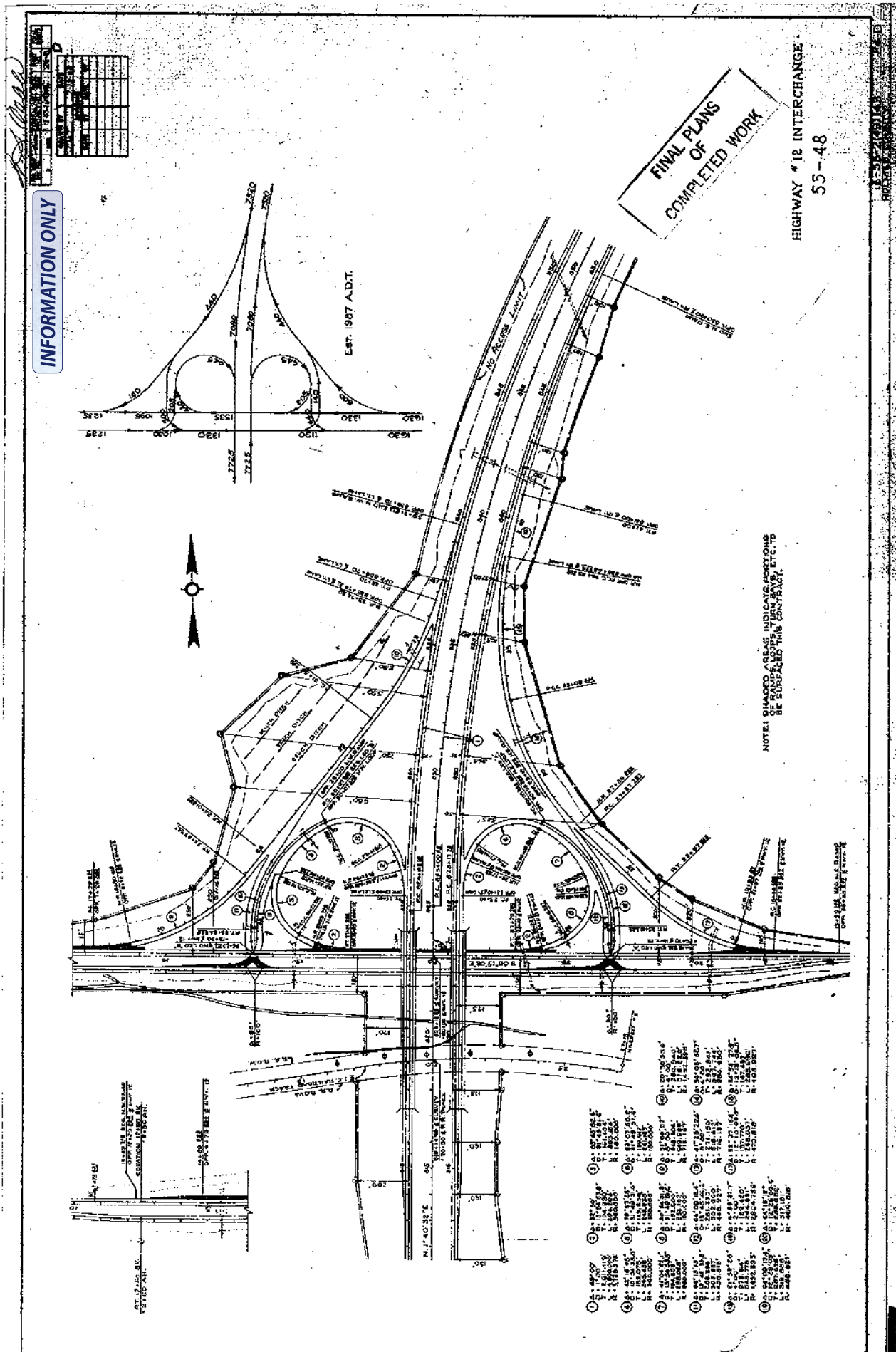
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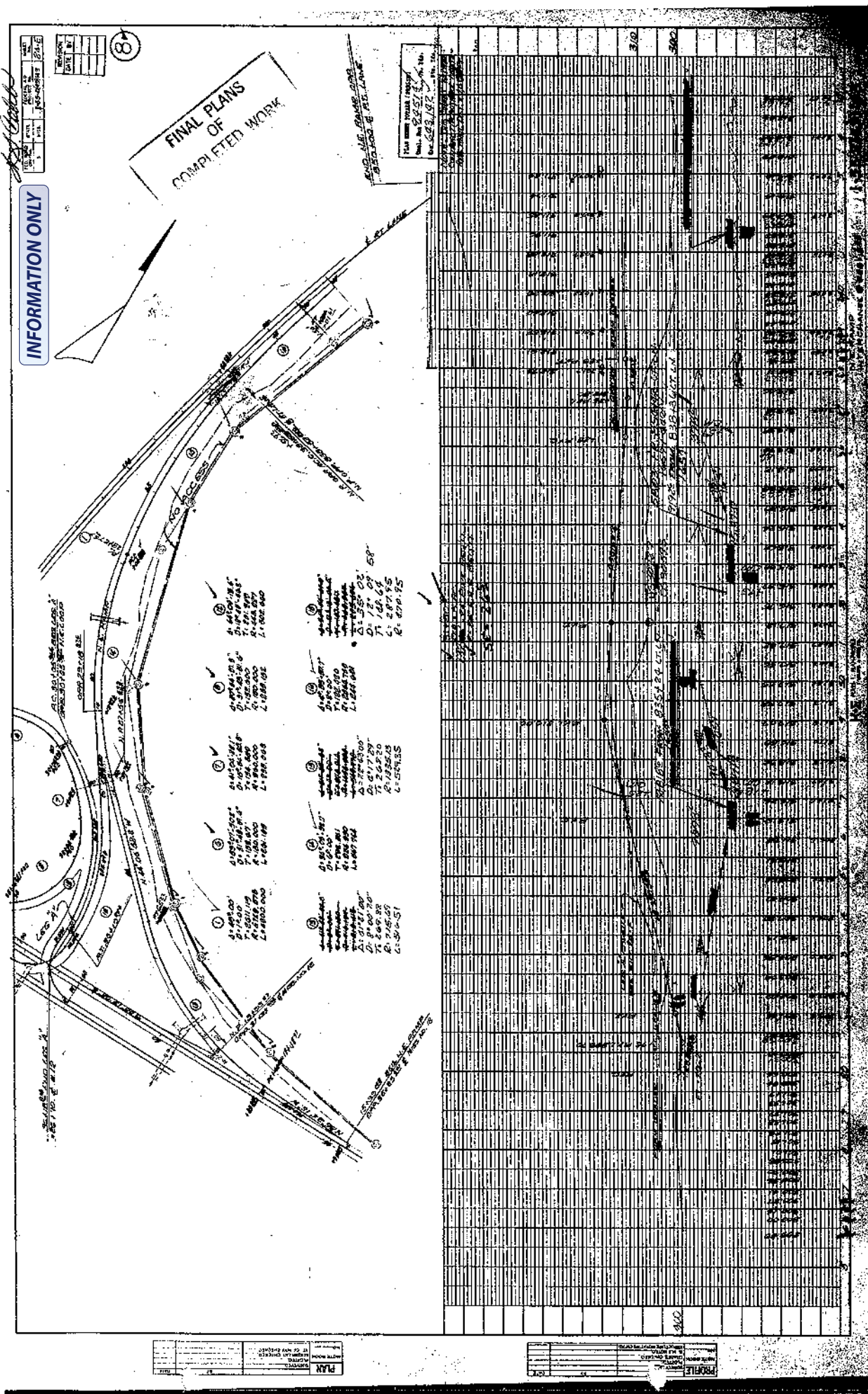


INFORMATION ONLY



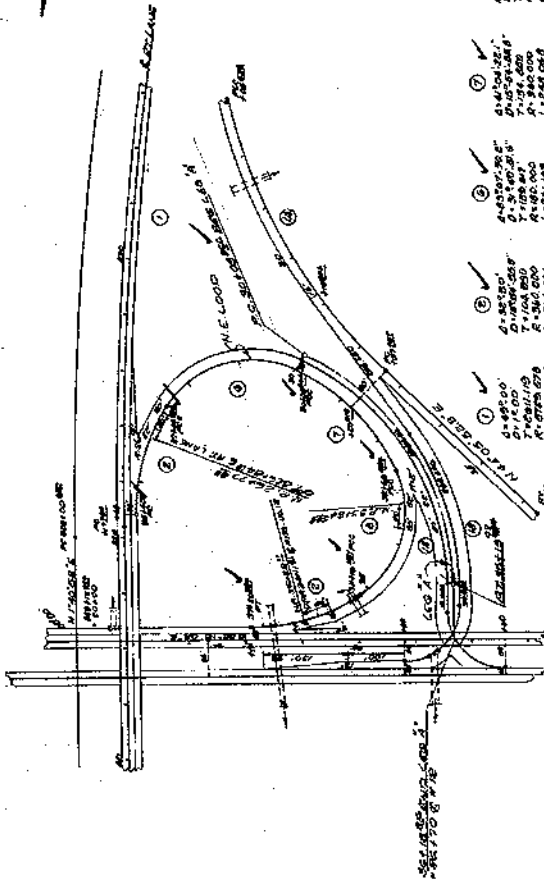
218-2(35)11412
 [Signature]
 11/1/85





INFORMATION ONLY

FINAL PLANS
OF
COMPLETED WORK



①
Δ = 64° 12' 00"
D = 12' 28' 51"
T = 222' 11"
C = 548' 11"
R = 453' 27"

②
Δ = 54° 03'
D = 11' 09' 58"
T = 148' 68"
C = 272' 55"
R = 270' 95"

③
Δ = 60° 14'
D = 12' 28' 51"
T = 222' 11"
C = 548' 11"
R = 453' 27"

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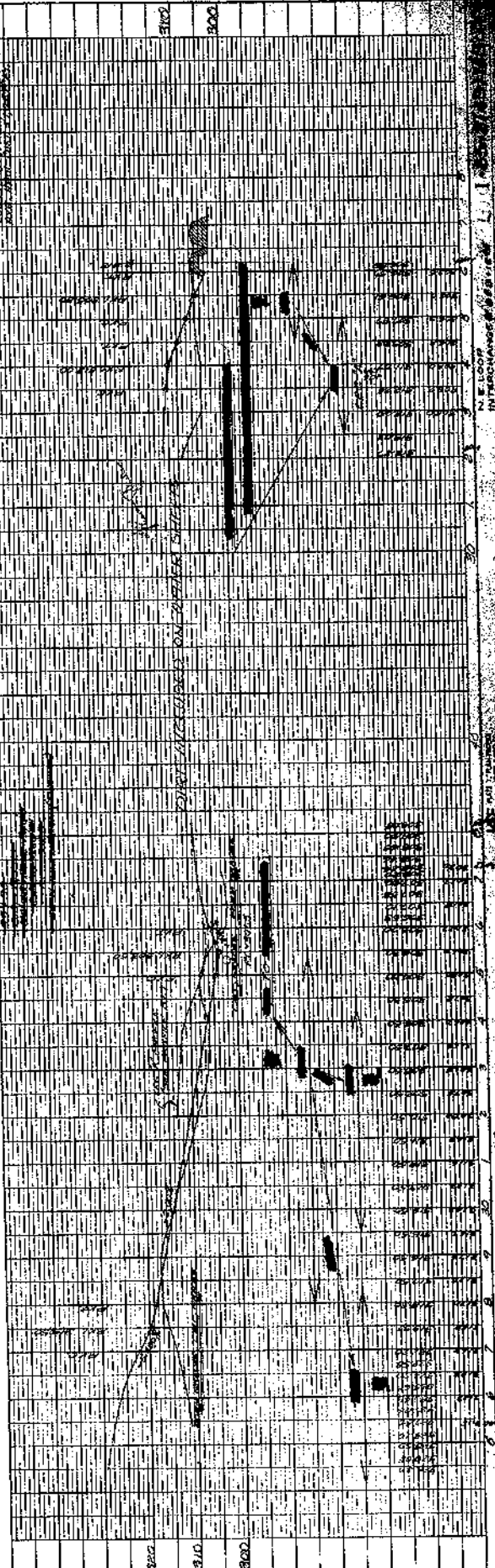
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T = 222' 11"
C = 548' 11"
R = 453' 27"



PLAN	DATE	BY	CHECKED

PROFILE	DATE	BY	CHECKED

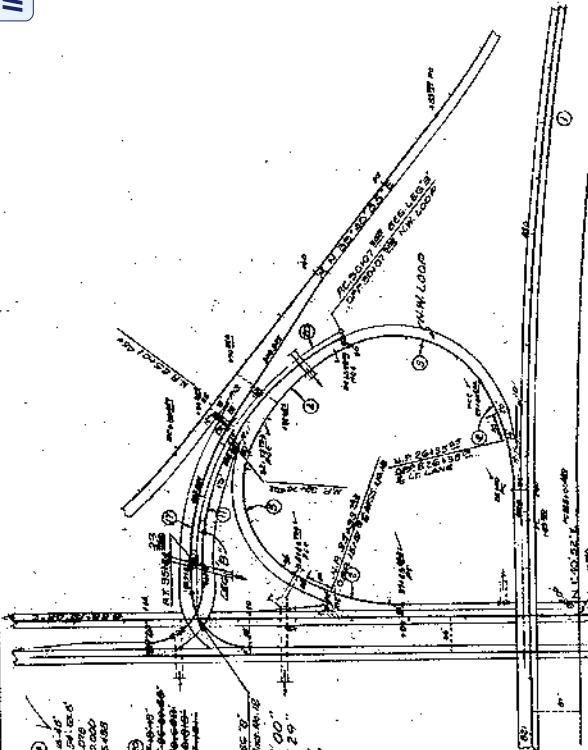
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DATE	BY
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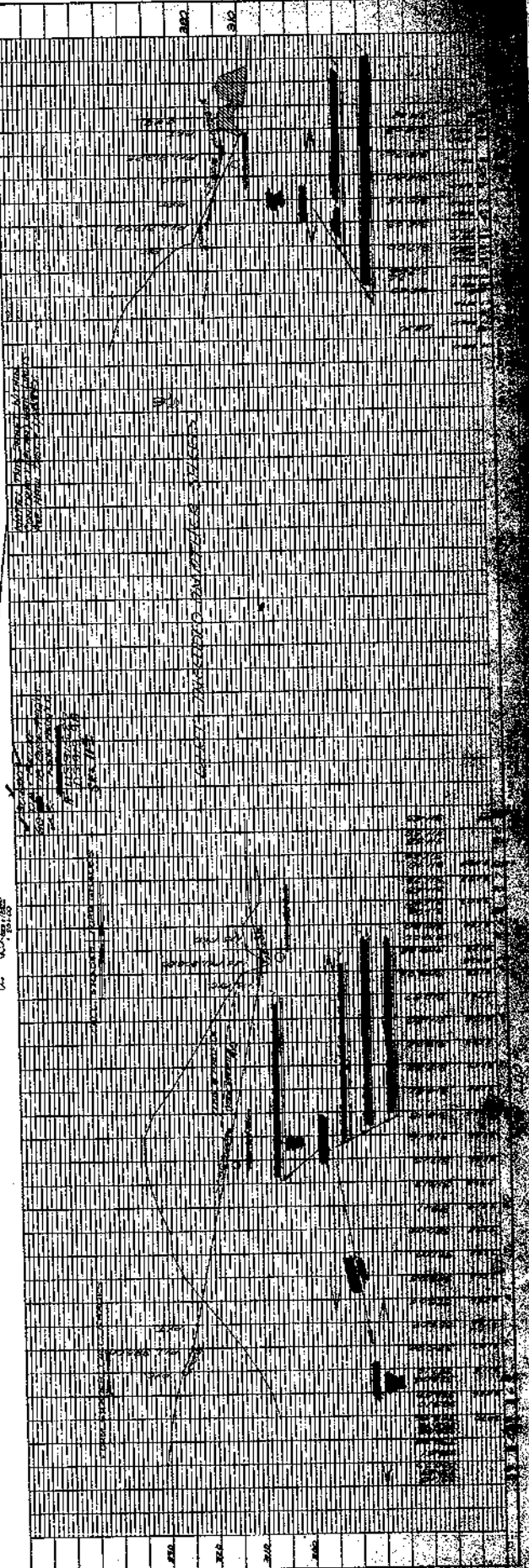
R. J. Webb



FINAL PLANS
OF
COMPLETED WORK



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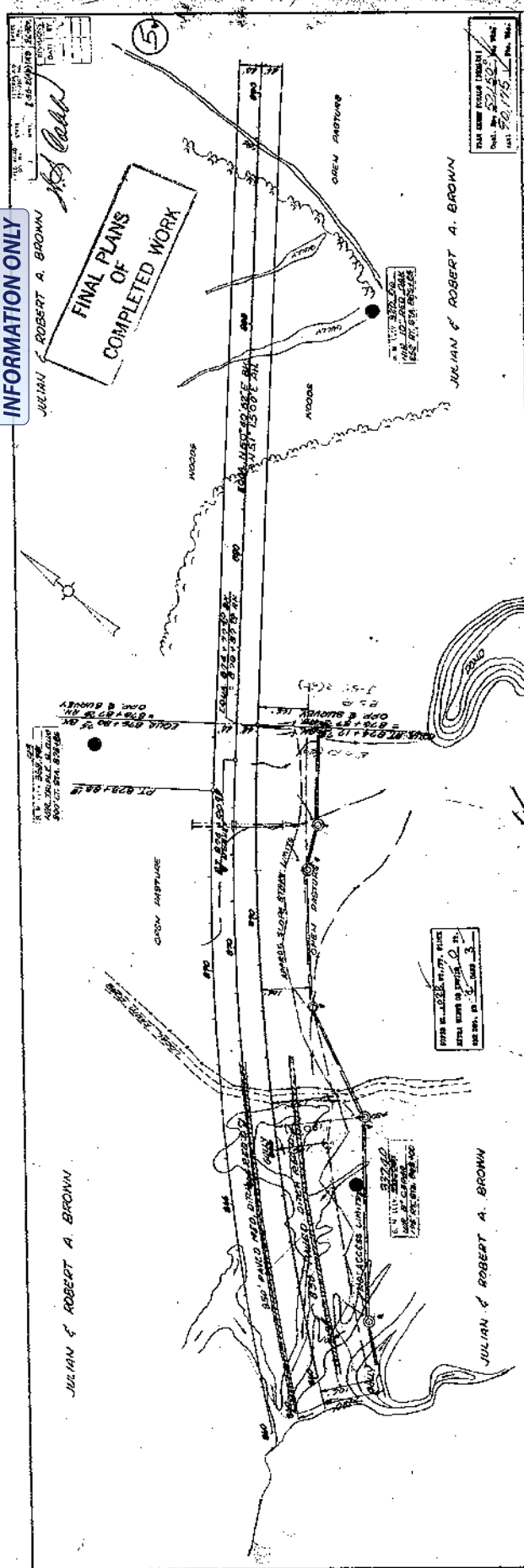
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DATE	BY

INFORMATION ONLY

JULIAN & ROBERT A. BROWN

JULIAN & ROBERT A. BROWN

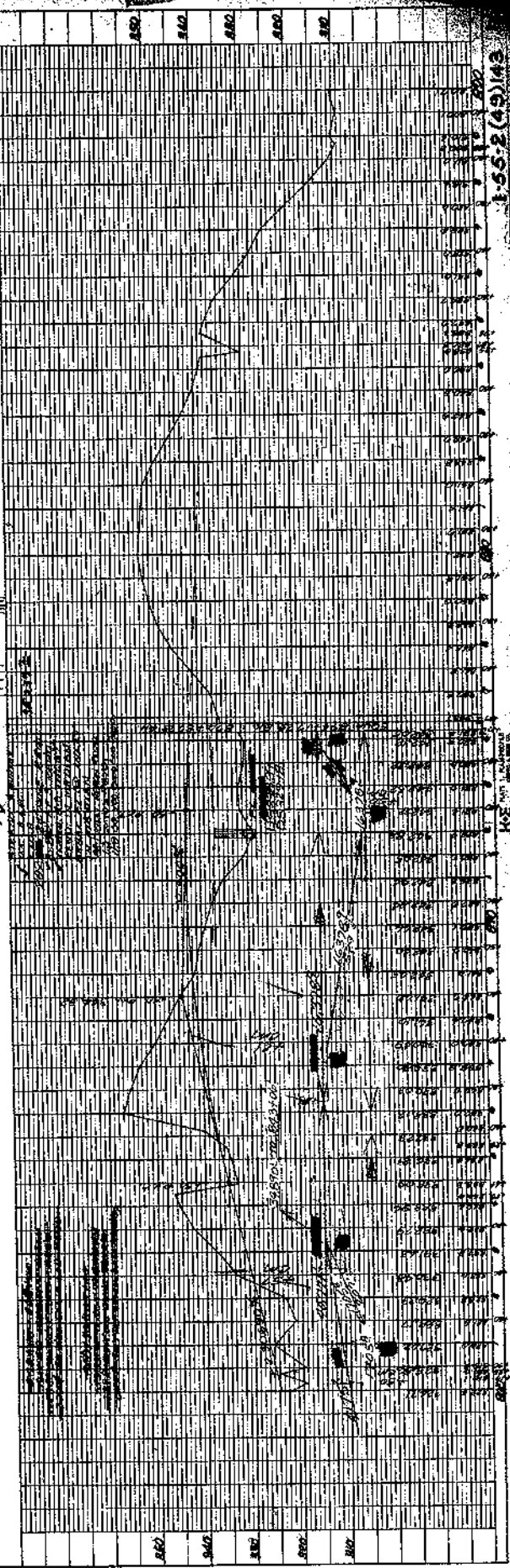
FINAL PLANS
OF
COMPLETED WORK



JULIAN & ROBERT A. BROWN

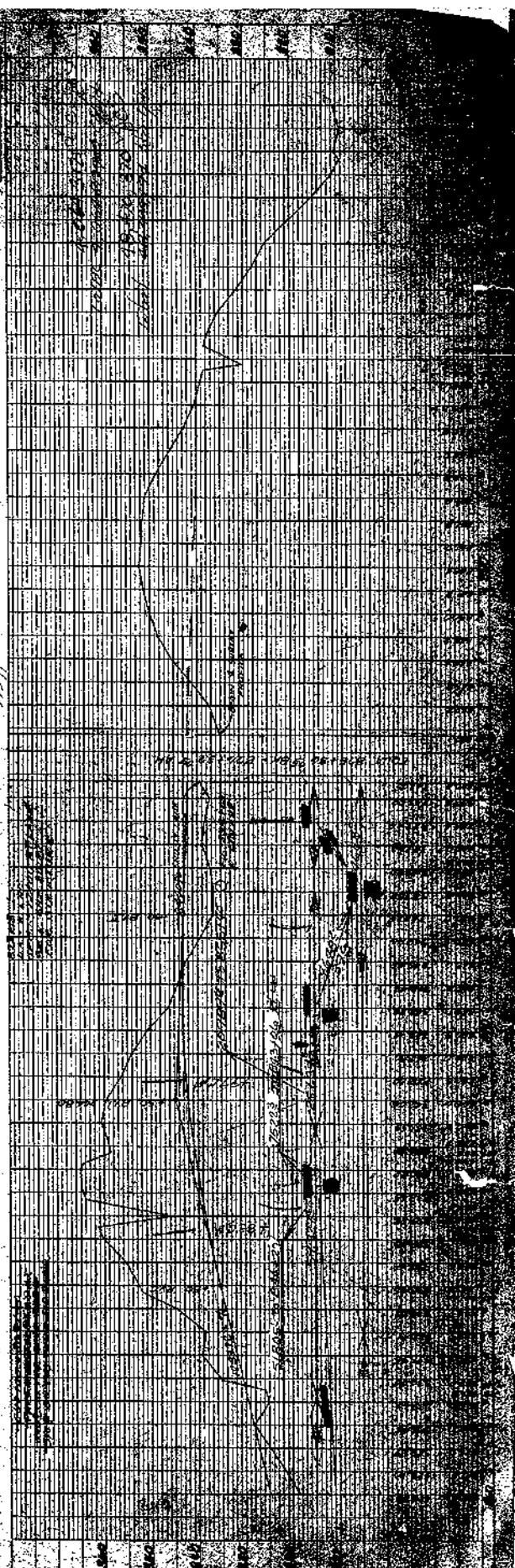
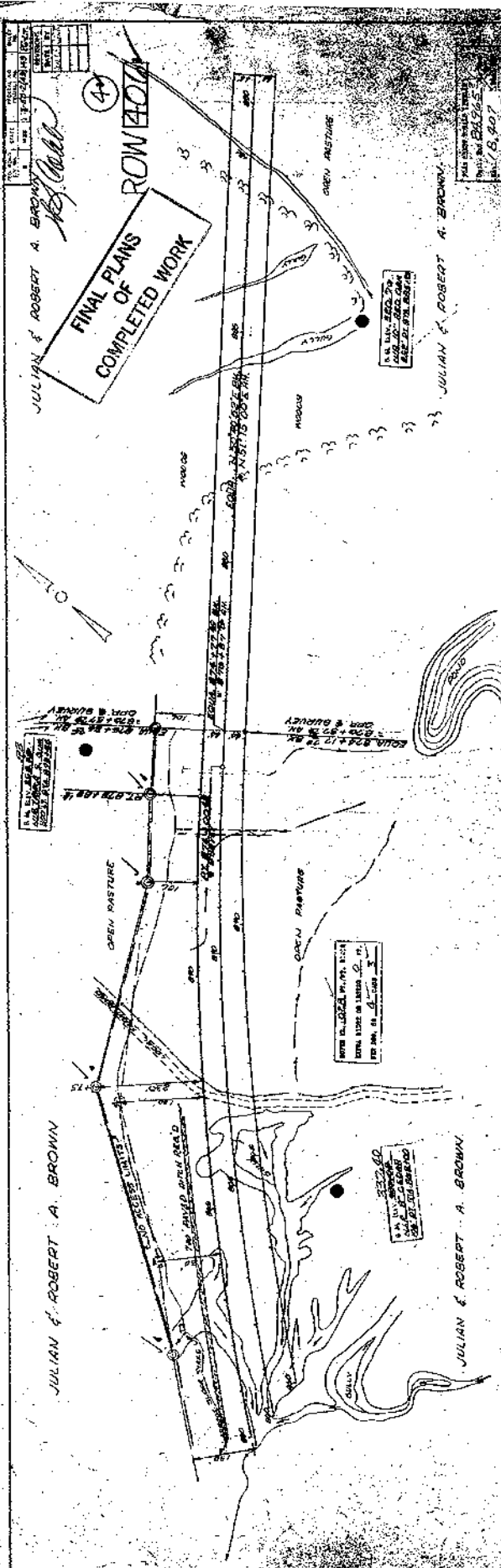
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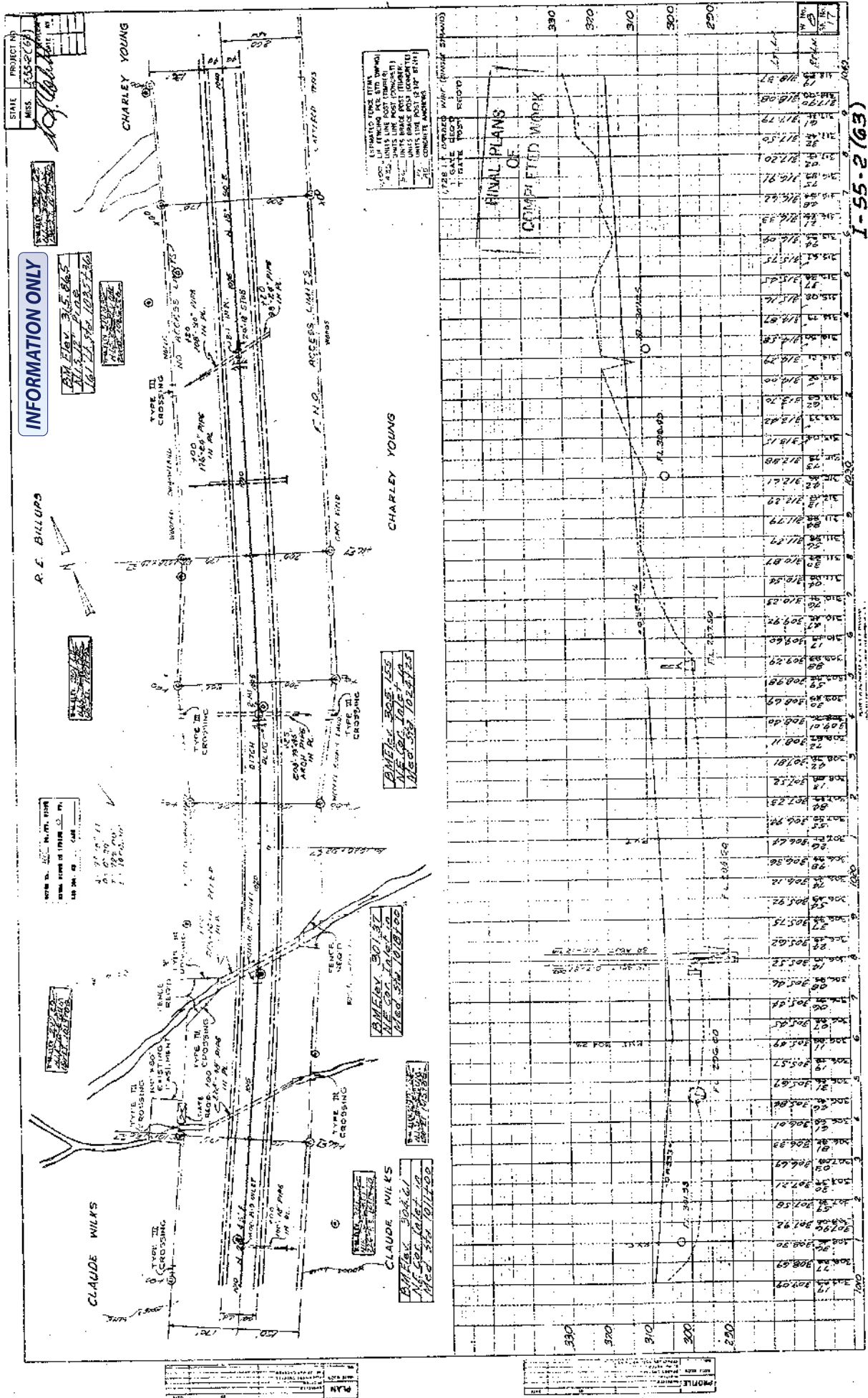
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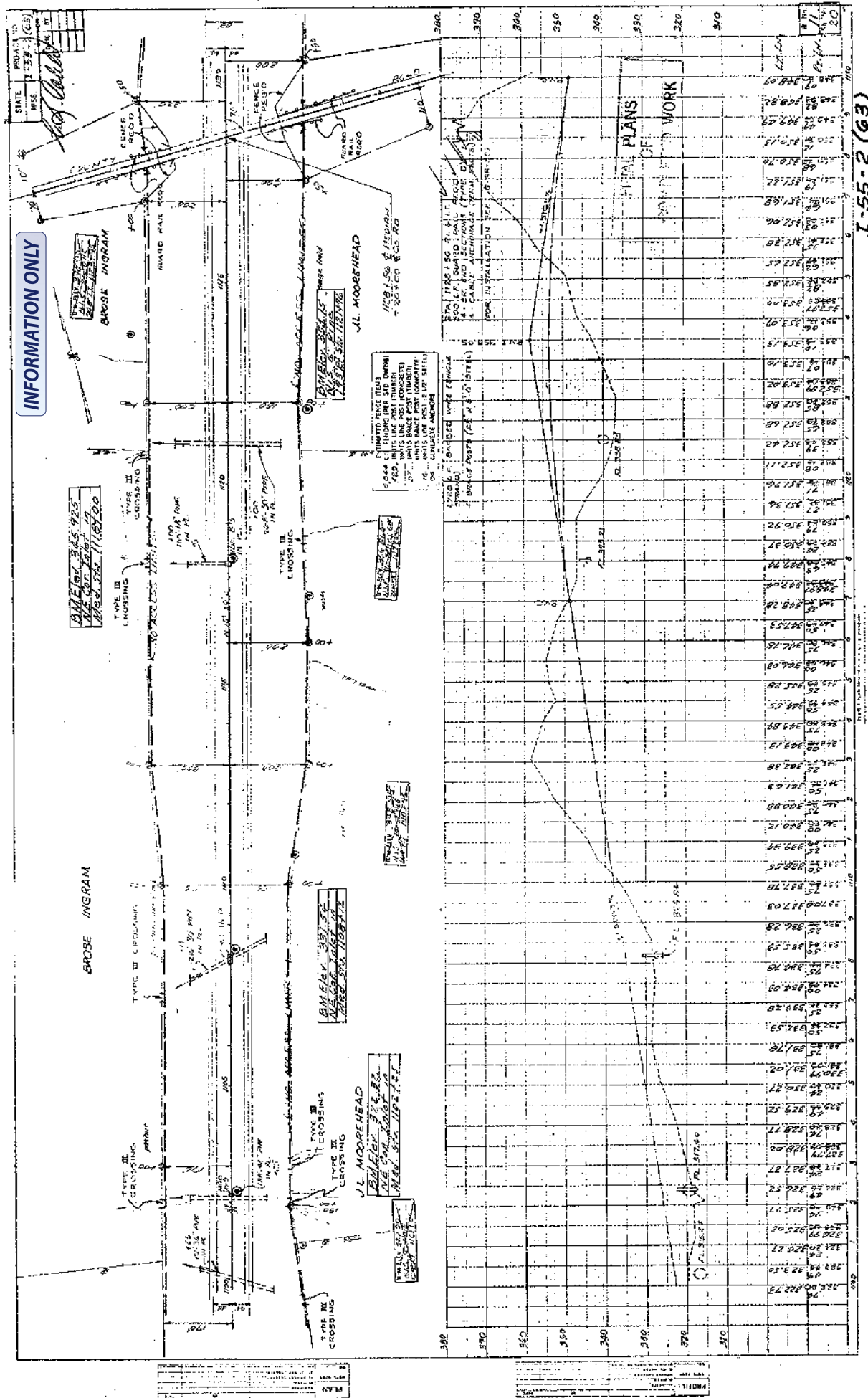


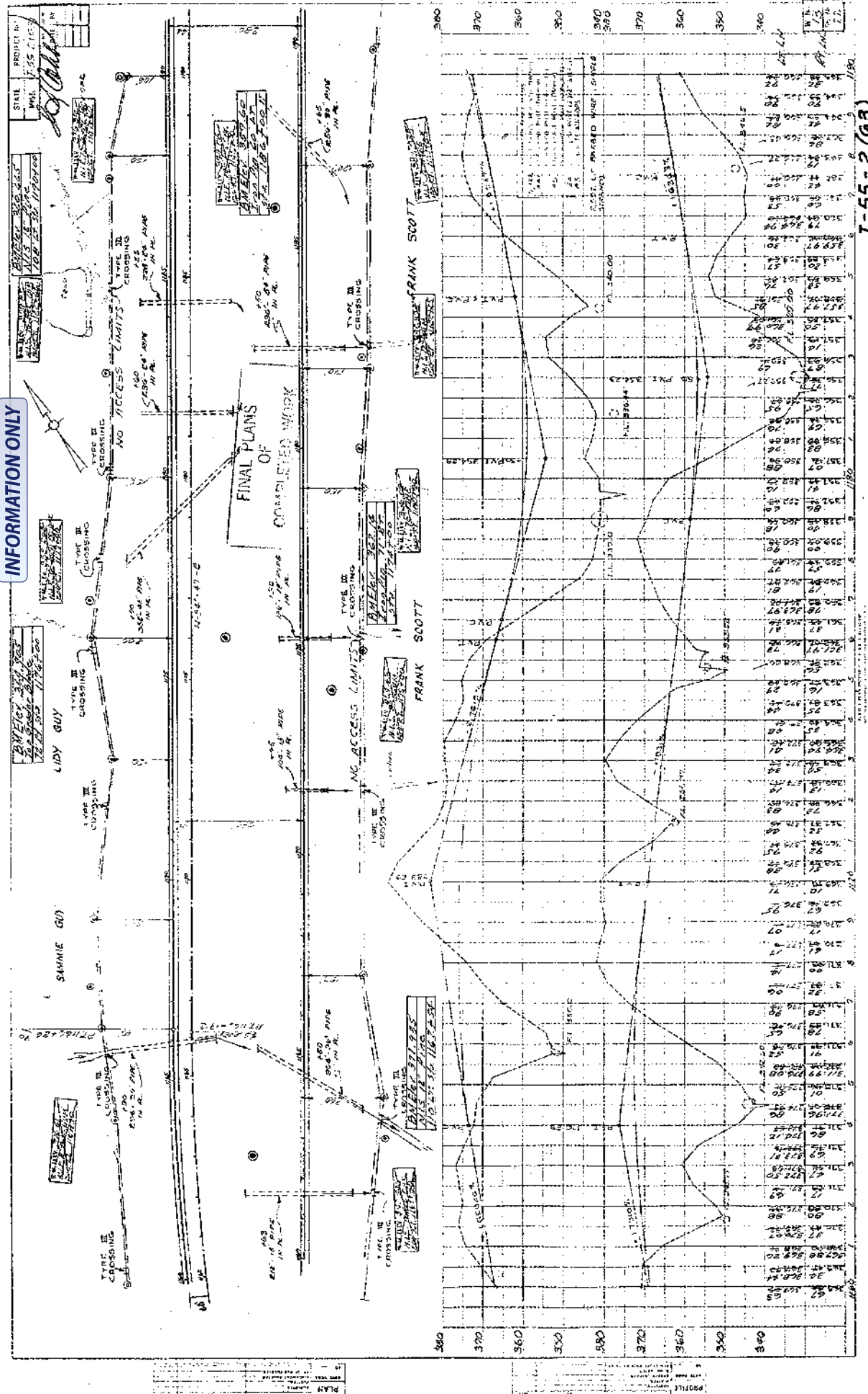
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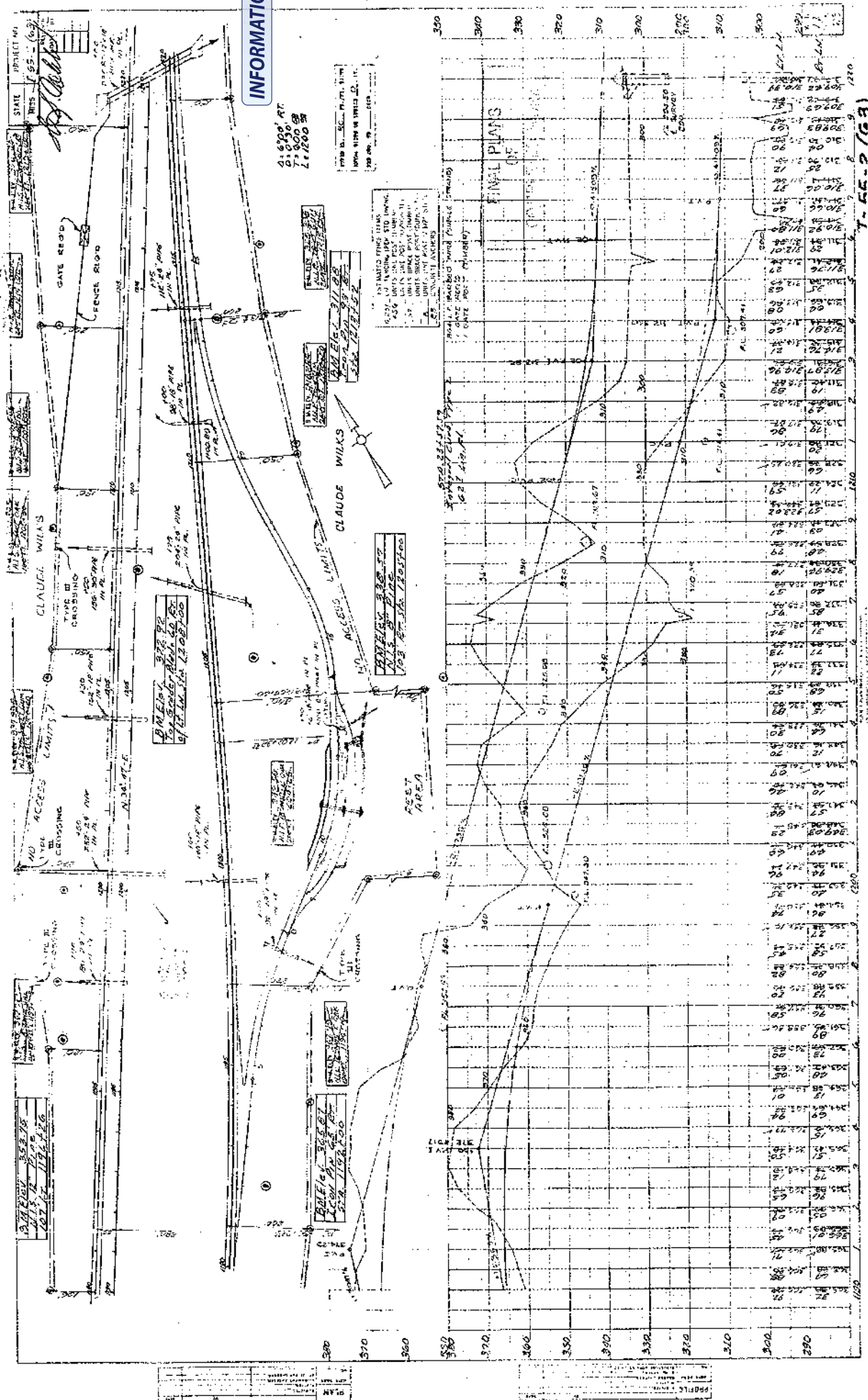


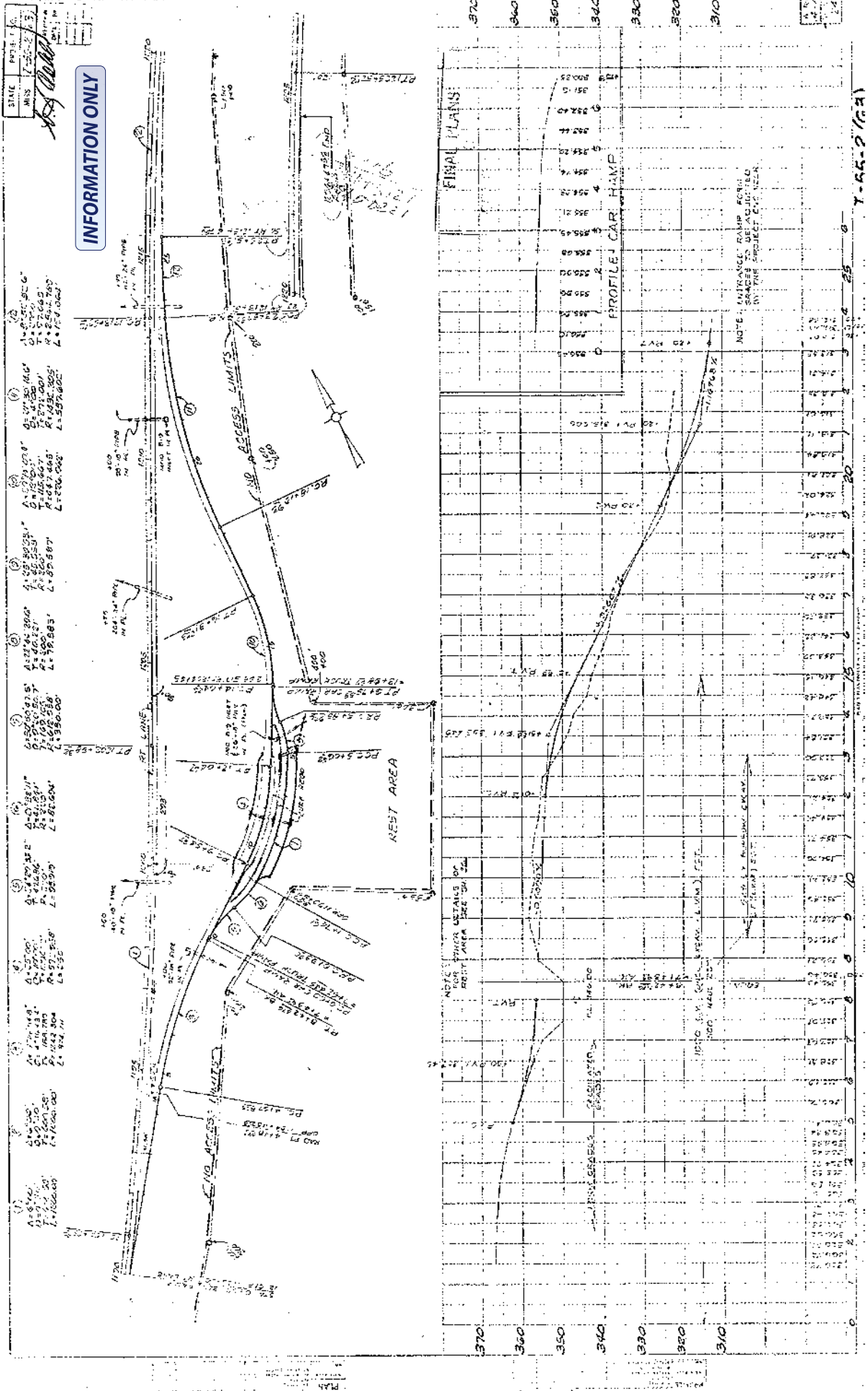


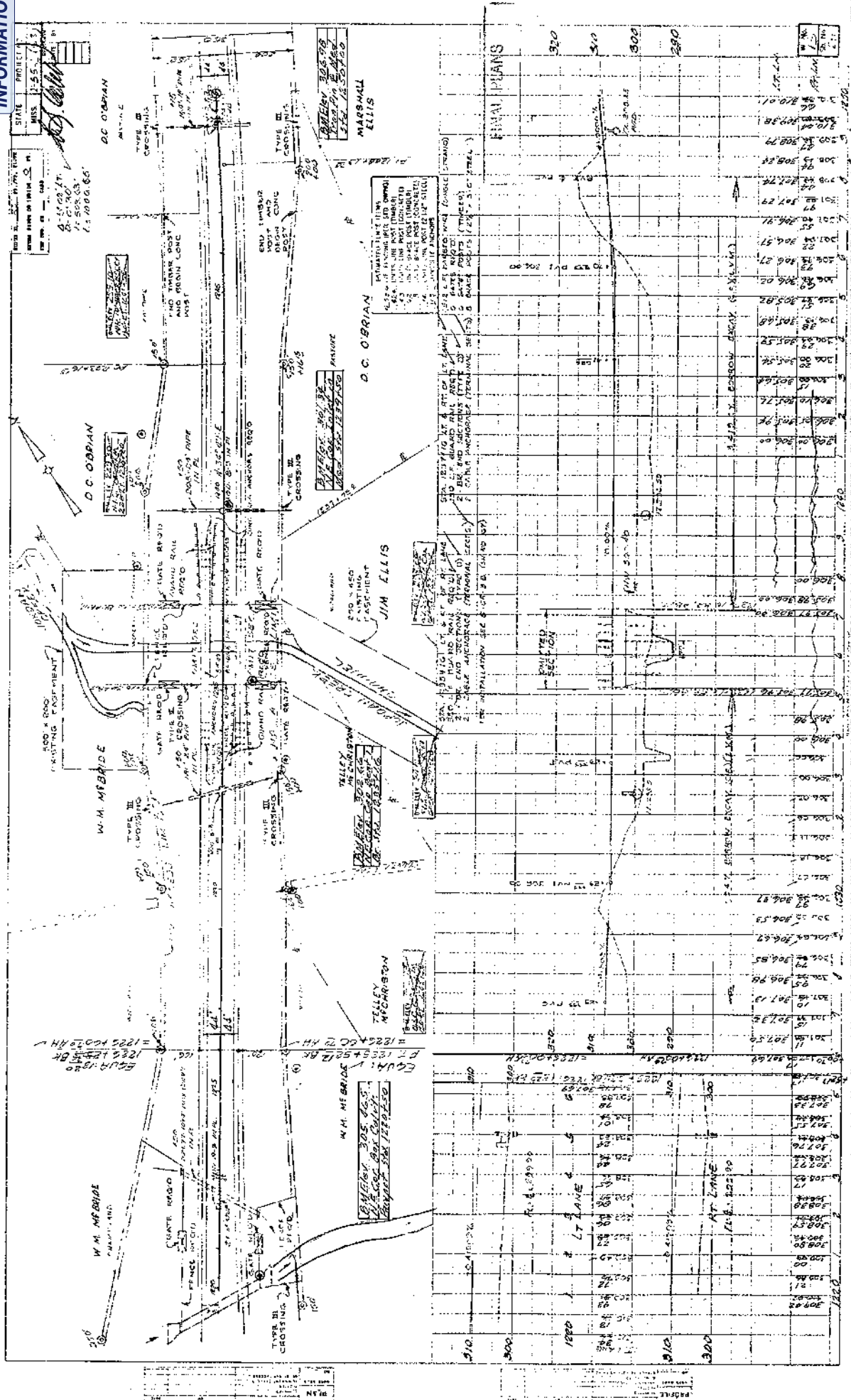


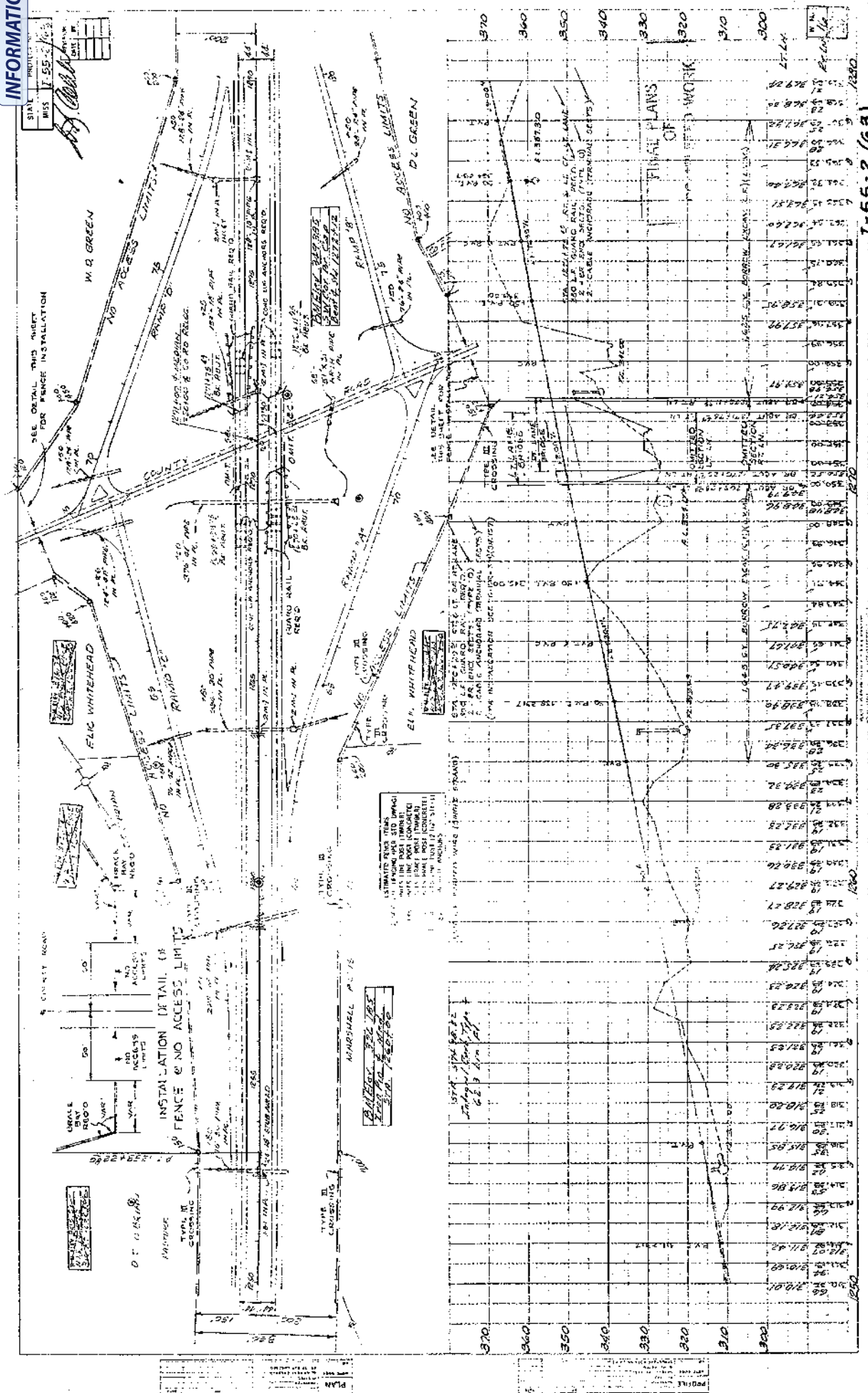


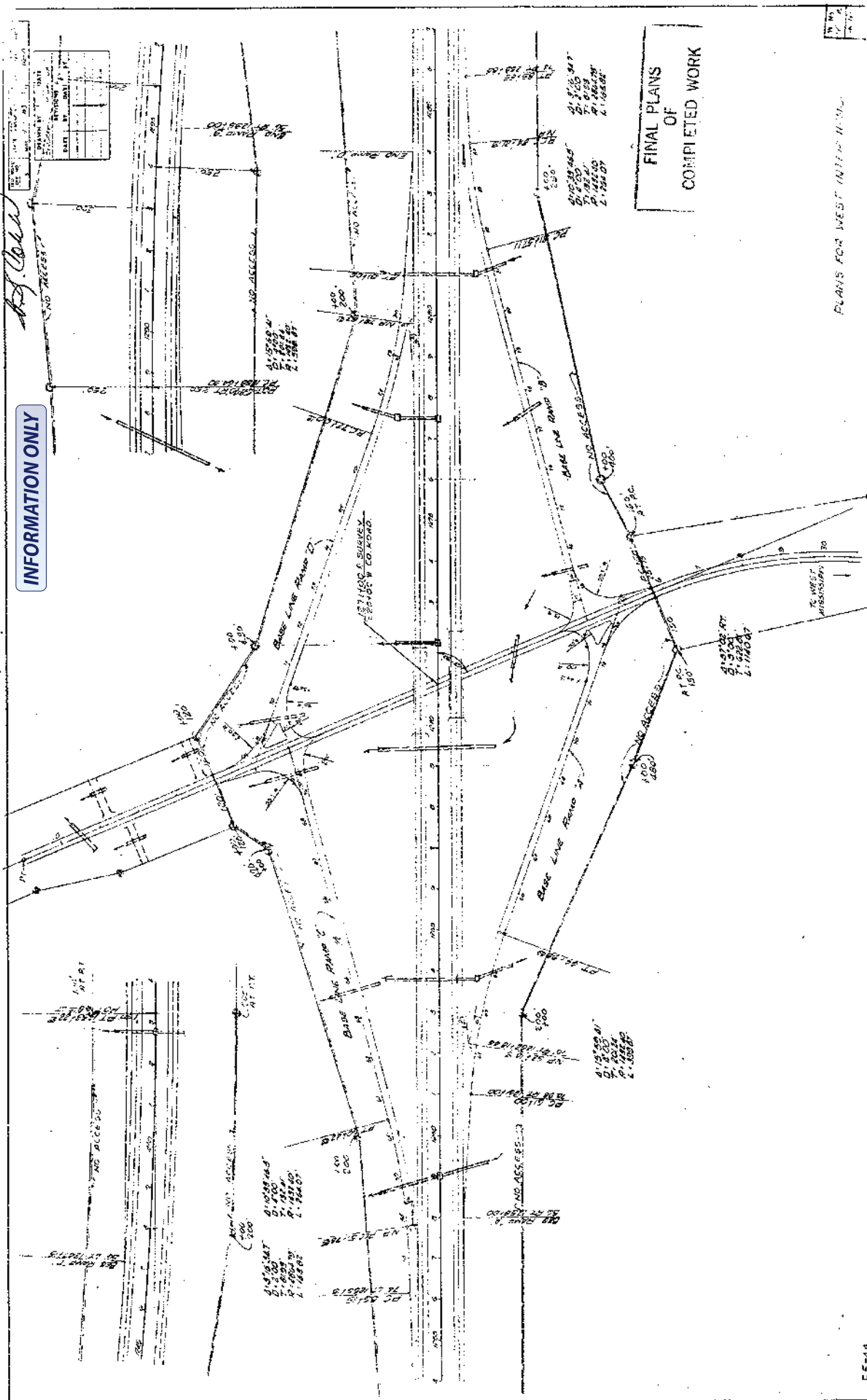
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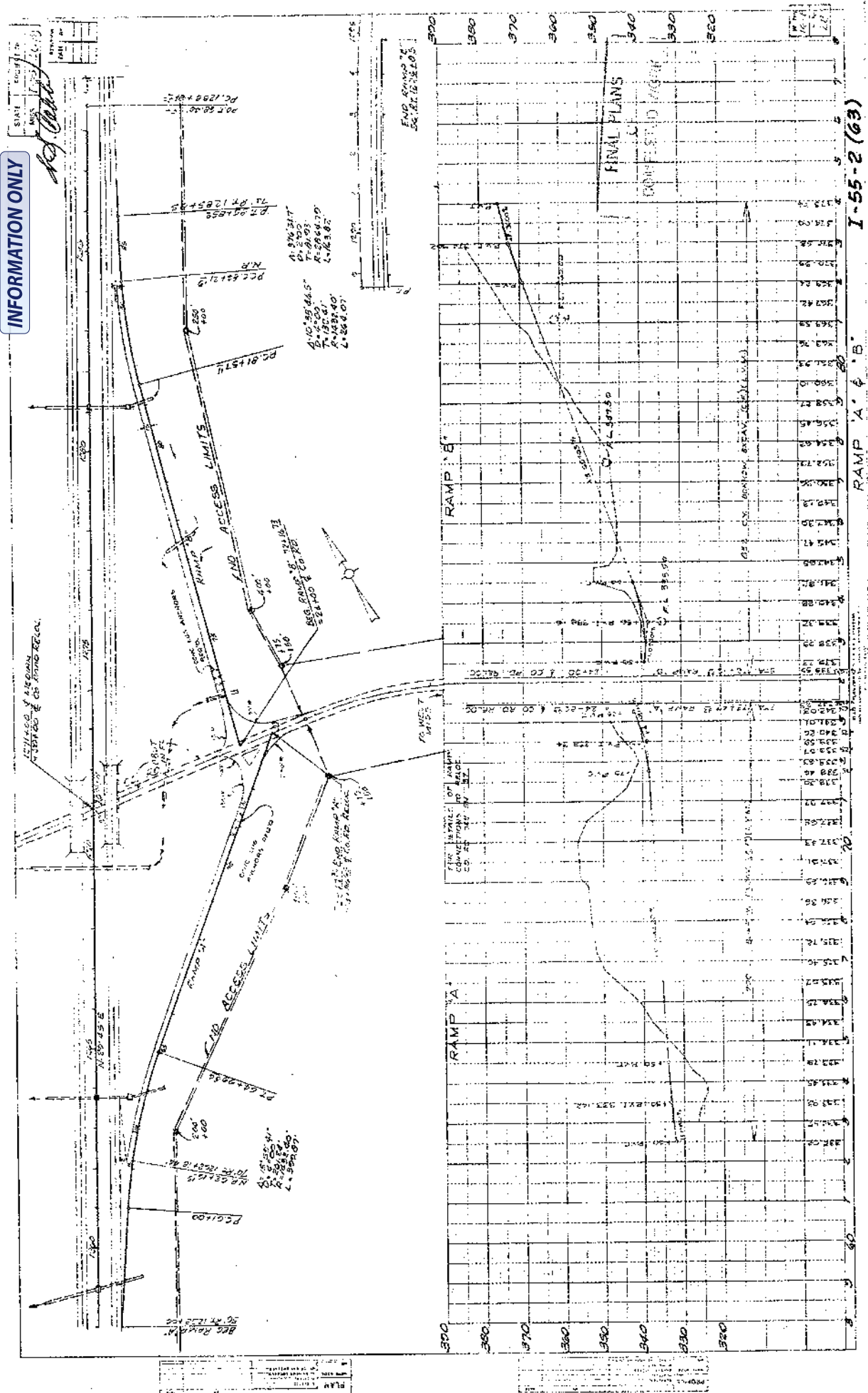


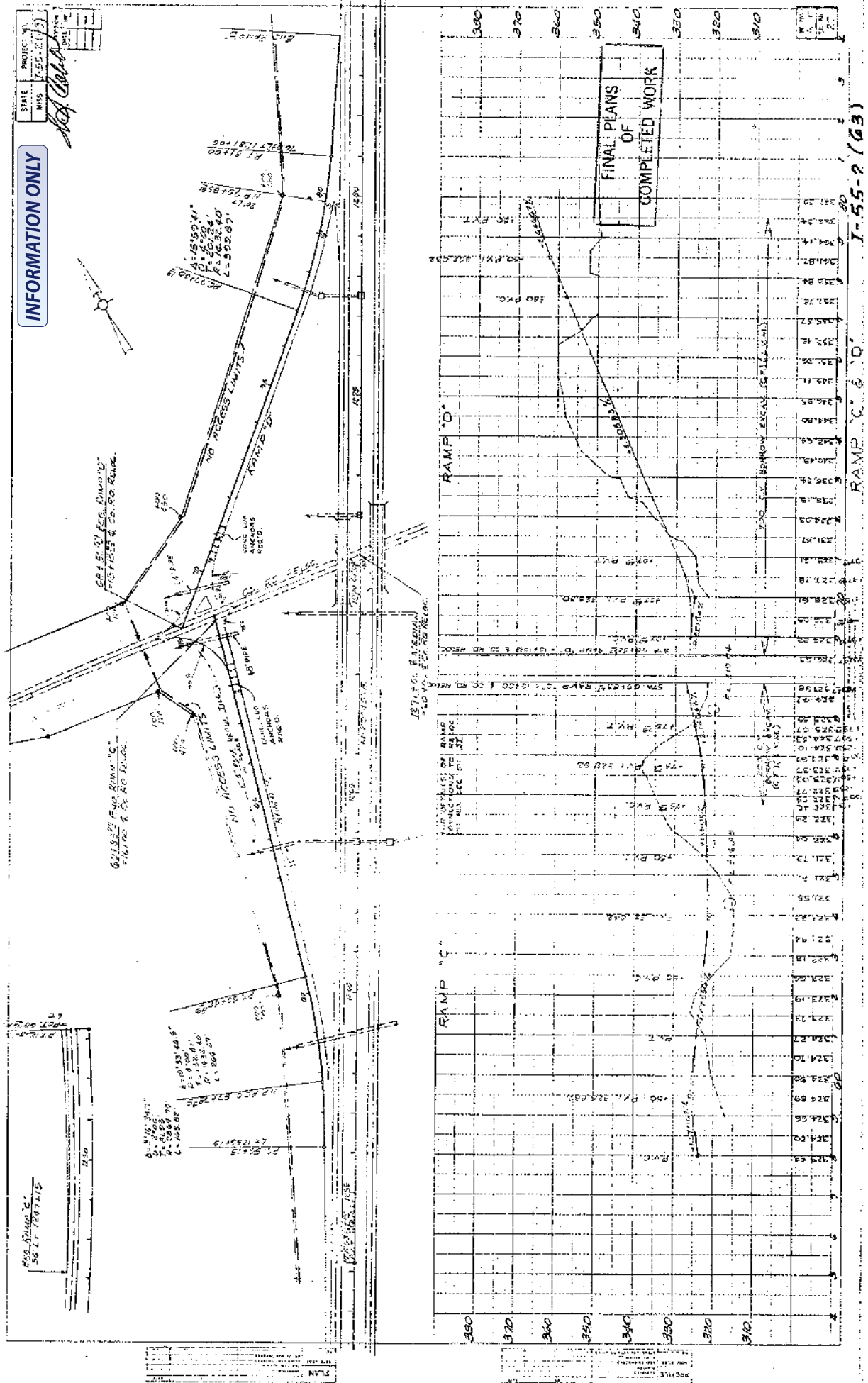


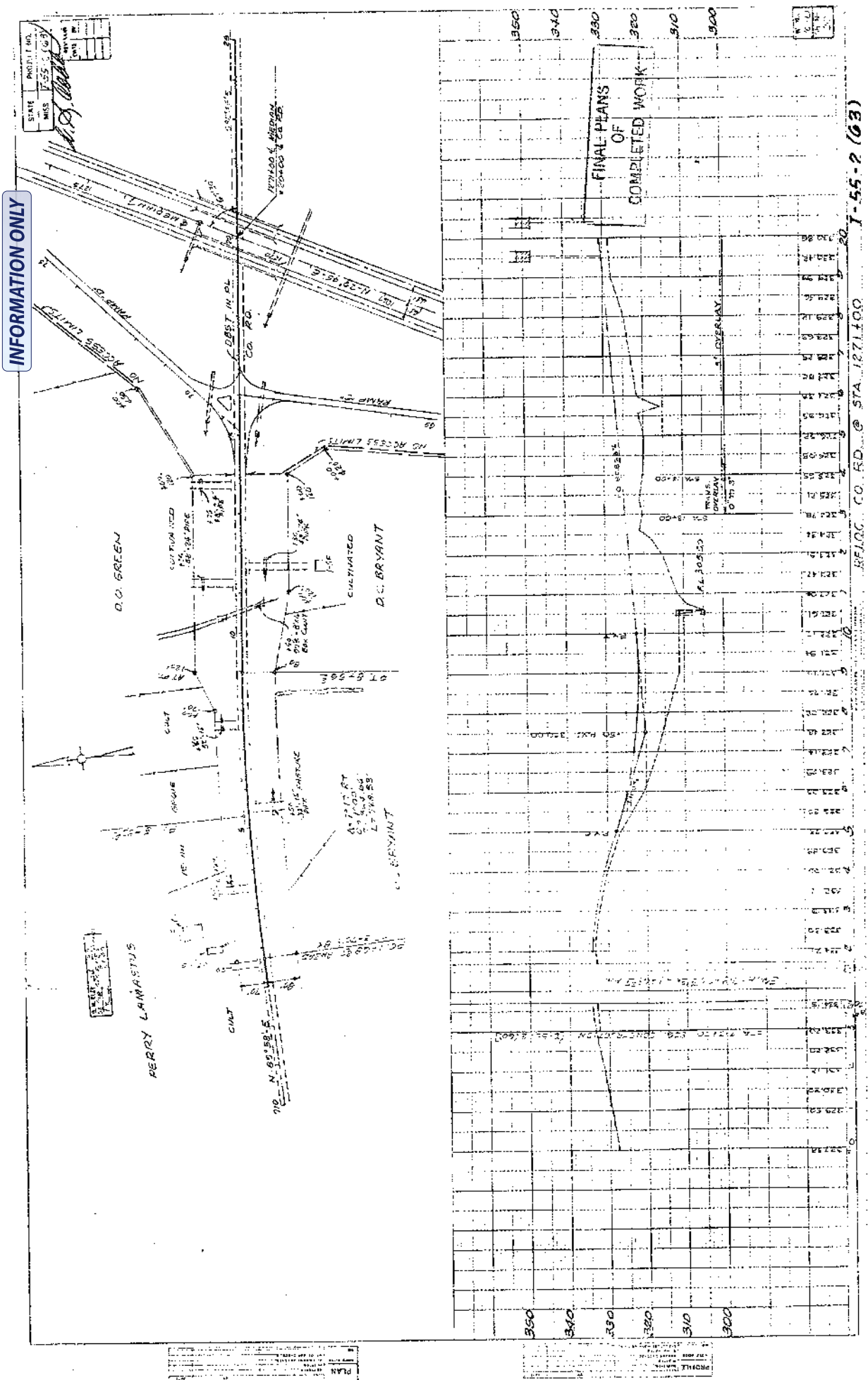






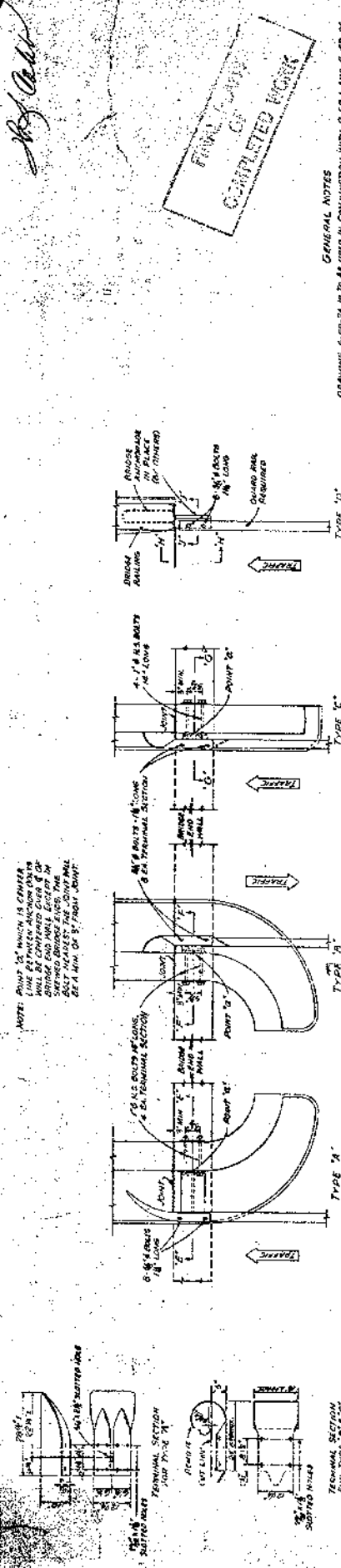




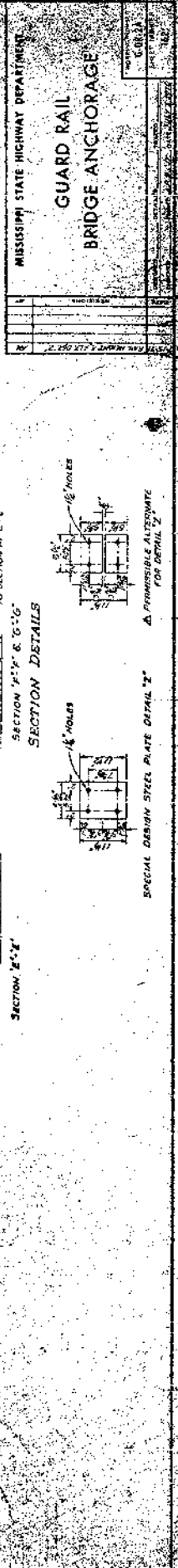
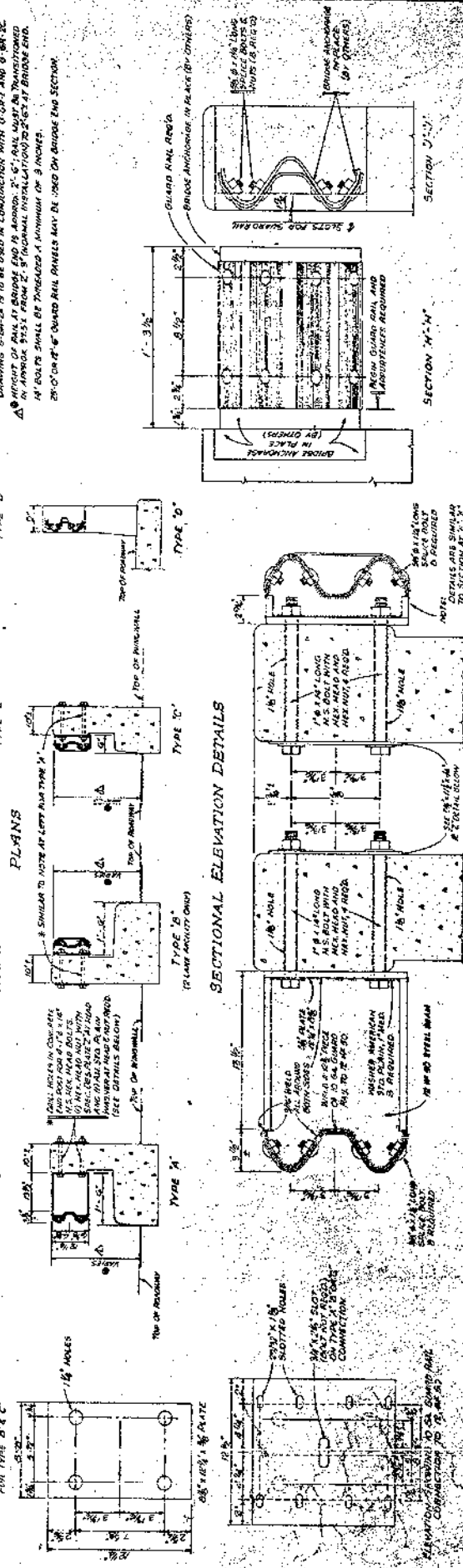


[illegible]

POINT "2" WHICH IS CENTER
LINE BETWEEN ANKERS MUST
BE CENTERED OVER & OF
BRIDGE END WALL EXCEPT IN
SKEWED BRIDGE ENDS. THE
BOLT HEADS THE JOINT WILL
BE A MIN. OF 5" FROM JOINT.



GENERAL NOTES



NO.	DATE	BY	REMARKS
100	10-25-57	J. L. B. & J. L. B.	100

GUARD RAIL
BRIDGE ANCHORAGE

[illegible]

Mill & Overlay approximately 22 miles on I-55 from 0.5 miles north of SR 17 to the Carroll County Line, known as Federal Aid Project No. IM-0055-02(257) / 108091301 in Holmes County.

Line No.	Item Code	Adj Code	Quantity	Units	Description [Fixed Unit Price]
Roadway Items					
0010	201-D002		60	Acre	Random Clearing
0020	202-B007		341	Square Yard	Removal of Asphalt Pavement, All Depths
0030	202-B073		352	Square Yard	Removal of Concrete Pavement, All Depths
0040	202-B136		7,798	Linear Feet	Removal of Guard Rail
0050	202-B240		37,500	Linear Feet	Removal of Traffic Stripe
0052	202-B244		1,000	Each	Removal of Trees
0060	203-G001	(E)	117	Cubic Yard	Excess Excavation, FM, AH
0070	237-A002		3,000	Linear Feet	Wattles, 20"
0080	246-B001		250	Each	Rockbags
0090	304-B004	(GT)	20,000	Ton	Granular Material, Class 5, Group D
0100	403-A013	(BA1)	2,000	Ton	9.5-mm, HT, Asphalt Pavement
0110	403-A015	(BA1)	23,500	Ton	9.5-mm, ST, Asphalt Pavement
0120	403-B006	(BA1)	36	Ton	19-mm, ST, Asphalt Pavement, Leveling
0130	403-S001		88	Mile	Joint Sealant
0140	406-D003		90,000	Ton	Fine Milling of Bituminous Pavement, All Depths
0150	407-A001	(A2)	146,000	Gallon	Asphalt for Tack Coat
0160	413-E001		2,200	Linear Feet	Sawing and Sealing Transverse Joints in Asphalt Pavement
0170	423-A001		85	Mile	Rumble Strips, Ground In
0180	503-A001	(C)	352	Square Yard	8" and Variable Continuously Reinforced Concrete Pavement, Broom Finish
0190	503-B001		229	Linear Feet	Saw Cut, Longitudinal Joints
0200	503-C010		464	Linear Feet	Saw Cut, Full Depth
0210	503-E002		592	Each	Tie Bars, No. 5 Deformed Drilled and Epoxied or Grouted
0220	606-B001		5,538	Linear Feet	Guard Rail, Class A, Type 1
0230	606-C003		2	Each	Guard Rail, Cable Anchor, Type 1
0240	606-D012		40	Each	Guard Rail, Bridge End Section, Type D Modified
0250	606-E005		18	Each	Guard Rail, Terminal End Section, Flared
0260	606-E007		24	Each	Guard Rail, Terminal End Section, Non-Flared
0270	618-A001		1	Lump Sum	Maintenance of Traffic
0280	618-B001		1	Square Feet	Additional Construction Signs [\$10.00]
0290	619-A1001		134	Mile	Temporary Traffic Stripe, Continuous White
0300	619-A2001		192	Mile	Temporary Traffic Stripe, Continuous Yellow
0310	619-A3001		180	Mile	Temporary Traffic Stripe, Skip White

Line No.	Item Code	Adj Code	Quantity	Units	Description [Fixed Unit Price]
0320	619-A5001		95,000	Linear Feet	Temporary Traffic Stripe, Detail
0330	619-A6002		1,000	Linear Feet	Temporary Traffic Stripe, Legend
0340	620-A001		1	Lump Sum	Mobilization
0350	626-A001		45	Mile	6" Thermoplastic Double Drop Traffic Stripe, Skip White
0360	626-B002		47	Mile	6" Thermoplastic Double Drop Traffic Stripe, Continuous White
0370	626-E001		50	Mile	6" Thermoplastic Double Drop Traffic Stripe, Continuous Yellow
0380	626-G004		39,000	Linear Feet	Thermoplastic Double Drop Detail Stripe, White
0390	626-G005		1,400	Linear Feet	Thermoplastic Double Drop Detail Stripe, Yellow
0400	626-H002		700	Linear Feet	Thermoplastic Double Drop Legend, White
0410	627-K001		4,200	Each	Red-Clear Reflective High Performance Raised Markers
0420	627-L001		150	Each	Two-Way Yellow Reflective High Performance Raised Markers
0430	630-F006		187	Each	Delineators, Guard Rail, White
0440	630-F007		56	Each	Delineators, Guard Rail, Yellow
0450	630-G005		40	Each	Type 3 Object Markers, OM-3R or OM-3L, Post Mounted
0460	907-234-A001		2,500	Linear Feet	Temporary Silt Fence
0470	907-402-A002	(BA1)	38,000	Ton	Open Graded Friction Course, 9.5-mm Mixture
0480	907-402-B001	(A3)	58,000	Gallon	Bituminous Tack Coat
0490	907-405-A001	(BA1)	66,000	Ton	Stone Matrix Asphalt, 9.5 mm Mixture
0500	907-405-A002	(BA1)	88,000	Ton	Stone Matrix Asphalt, 12.5 mm Mixture
0510	907-618-M2001		500	Hours	Work Zone Law Enforcement [\$60.00]
0520	907-619-E3001		4	Each	Changeable Message Sign
0530	907-906001		520	Hours	Trainees [\$5.00]

[illegible]

NOTE: THE ANTICIPATED WORKING DAYS SHOWN ON THIS SCHEDULE ARE FOR INFORMATIONAL PURPOSES ONLY. THE ACTUAL WORKING DAY TOTAL AS ASSESSED BY THE PROJECT ENGINEER ON FORM CSD-765 SHALL GOVERN.