

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO. <u> 1 </u>	DATED <u> 5/18/2022 </u>	ADDENDUM NO. <u> </u>	DATED <u> </u>
ADDENDUM NO. <u> </u>	DATED <u> </u>	ADDENDUM NO. <u> </u>	DATED <u> </u>
ADDENDUM NO. <u> </u>	DATED <u> </u>	ADDENDUM NO. <u> </u>	DATED <u> </u>

Number	Description
1	Revised Notice To Bidders No. 4222; Revised Notice To Bidders No. 4225; Revised Bid Items; Revised Form OCR-485; Amendment EBSx Download Required.

TOTAL ADDENDA: 1
(Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

Contractor

BY _____
Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

President Address

Secretary Address

Treasurer Address

The following is my (our) itemized proposal.

SP-0039-02(054)/ 108233301000

Hinds County(ies)

Revised 01/26/2016

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 4222

CODE: (SP)

DATE: 05/18/2022

SUBJECT: Scope of Work

PROJECT: SP-0039-02(054) 108233301 – Hinds County

The contract documents do not include an official set of construction plans but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, “Standard Drawings”.

A general description of the work required on the project is to mill and overlay approximately 18.2 miles of SR 18 in Hinds County beginning at Copiah County Line (BOP Station 10+00) and ending south of Raymond (EOP Station 1000+58). Due to recent bridge construction project near Station 52+00, the section for this project from Station 26+22 to Station 74+20 will be omitted. Details of specific work are mentioned in the following sections.

From Station 10+00 (BOP) to Station 969+45

Work in this area shall consist of milling 1½” and inlaying with 2” of 12.5-mm, ST asphalt. Failed areas listed in the provided table shall be repaired full depth with 12.5-mm, ST asphalt (see attached detail). Cross drains listed in the provided table shall be repaired or replaced. Traffic will be allowed to run on the milled surfaces more than five (5) consecutive days.

From Station 969+45 to Station 1000+60

Work in this area shall consist of trench widening 2’ wide by 3½” deep using 19-mm, ST, Trench Widening asphalt where asphalt shoulders are not present (see attached detail). At the Old Port Gibson Road Intersection (Station 990+50 to Station 996+50), the Contractor shall mill 1½” and inlay with 2” of 12.5-mm, ST asphalt. Traffic will be allowed to run on the milled surfaces no more than five (5) consecutive days.

GENERAL NOTES

Milling

Milling/paving shall not begin until an approved asphalt mix design has been received, nor until such time that, in the opinion of the Engineer, weather conditions have been consistently suitable enough to allow placement of the asphalt pavement after the milling operations.

The reclaimed asphalt pavement (RAP) material removed by the milling operation shall become the property of the Contractor.

Where milling is required, the Contractor shall provide outlets in the existing shoulders at sufficient intervals to prevent pooling or standing water on the milled surface; the cost of which shall be absorbed in other items bid.

Milling and paving operations shall be performed such that a -2% slope from centerline is provided in normal crown roadway sections. Superelevation through curves shall be maintained as it currently exists or improved as directed. Where slope correction is required, correction shall be made by milling, paving, or combination thereof as directed by the Engineer. Milling correction: Mill outside edge of pavement to a depth of 1½” on a 2% slope towards the centerline. Paving Correction: Mill to depth of 1½” on existing slope and 2¼” and variable on centerline and 1½” on outside edge. Combination Method: Combination of both methods as directed by the Engineer to achieve the desired slope. In super elevated areas where correct superelevation exists, milling will transition to thickness through curves. Where correct superelevation does not exist, milling will transition at curves to correct SE as directed by the Engineer.

Milling operations shall be performed in accordance with the Contract documents and the Standard Specifications. Variable width and length transitions may be required for ties at ramps, local roads, and project limits.

Milling of driveway pads shall be conducted in a manner to prevent gouging or otherwise affecting the roadway pavement structure and slope. Milling of driveway pads shall not be done simultaneously with mainline milling.

Traffic will be allowed to travel on the milled surface for no more than five (5) consecutive days. Traffic will be allowed to run on all milled local roads for 30 consecutive days unless otherwise stated. This allowance is for the Contractor's convenience, and thus, the Contractor is responsible for any pavement failures or damage sustained during this period. Night time lane closures will NOT be allowed.

Paving

Prior to mainline milling and paving operations, failed areas in the existing pavement shall be removed and backfilled with 12.5-mm, ST, Leveling asphalt as per the attached typical sections and details. Asphalt shall be placed in multiple lifts with a maximum lift thickness of 3”. Any granular/chemically treated/stone/etc. base or subgrade material deemed unsuitable by the Engineer shall be removed as directed and backfilled with 12.5-mm, ST, Leveling, asphalt. Payment for the excavation of the granular base and subgrade will be made using pay item 203-G: Excess Excavation. A list of the failed areas is shown in the attached tables. Pavement repairs shall be completed as a continuous operation in order to minimize traffic impacts. Lane closures shall remain in place until the failed area has been completely repaired. Lane closures shall not be left unattended. Night time lane closures will NOT be allowed.

If traditional excavation methods are used, the removal area shall first be saw cut full depth, including concrete where applicable, to create a neat line and prevent damage to the adjacent pavement structure. Payment for saw cuts will be made using the appropriate items. If milling techniques are used, the area will not require saw cuts. Care should be exercised to create a neat removal line and to prevent damaged to the adjacent pavement structure. If saw cuts are used in conjunction with milling, payment will be made using the appropriate pay items. Payment will not be made for saw cuts that are not performed.

Publicly maintained roads and streets should be paved to the existing right-of-way and in accordance with the attached drawings.

Privately owned entrances shall be paved to the shoulder line per the included typical drawing unless otherwise directed. Pad dimensions shall match the existing lengths and widths unless otherwise directed. Pads shall be shaped horizontally and vertically to prevent excessive drop-offs. Any new driveway pads deemed necessary by the Engineer shall be placed according to specifications.

Granular Shoulder Material

Where applicable, the existing shoulders shall be raised to match the new pavement elevation by placing variable depth granular material. The shoulders shall be graded and pulled up daily to eliminate drop-offs in excess of 2". Placement of the granular material on the finished asphalt course shall not be permitted. The existing shoulder shall be scarified to allow incorporation of the new shoulder material. The material shall be bladed, rolled, and compacted to a finished slope of four percent (4%) in normal crown sections. Placement of this material shall be performed to provide a uniform and compacted shoulder with a minimum depth and width of material placed. Shoulders with adequate shoulder material in place shall be bladed to a slope of four percent (4%) in normal crown sections. The cost of blading will be an absorbed item and shall be included in the price of other items bid. Crushed concrete will not be allowed if crushed stone is used for shoulder material.

Granular material, crushed stone, shall be provided around driveway pads as directed to prevent shoulder drop-offs and shall be placed in a timely manner. Drop-offs exceeding 2½" shall be corrected within two (2) calendar days of the placement of the pad.

Any material excavated from the existing shoulder during pavement trench widening operations or as a result of shoulder blading shall be used on the existing shoulder to match the new pavement elevation and any surplus material shall be spread along the edge of the shoulders, fore slopes, or other adjacent areas as directed by the Engineer and will be an absorbed item. Material which cannot be suitably placed in adjacent areas and deemed to be excess excavation by the Engineer shall be removed from the project site. Payment for removal of excess material will be made using pay item 203-G: Excess Excavation. Asphalt placed as fill material on the shoulders shall be removed; the cost of which shall be absorbed in other items bid.

Temporary And Permanent Pavement Markings

Temporary traffic stripe will be required immediately after the required milling overlay and prior to opening area to traffic. Temporary stripe shall be placed in the same location and configuration as the permanent stripe except that it may be offset as required for milling and paving operations. If temporary stripe is offset, the Contractor shall conduct operations in a manner to insure the final temporary stripe is placed at the required location of the permanent stripe. If removal of offset temporary stripe is required in order to achieve the correct location and alignment of permanent stripe, the cost of removal will be absorbed in other items bid. Placing double temporary centerline will not be allowed.

Temporary striping shall conform to finished stripe specifications for alignment, neatness, and straightness.

The use of short strips of traffic tape will not be allowed unless approved by the Engineer.

Temporary raised pavement markers shall be placed along the centerline of the roadway in any areas expected to be dormant for more than 90 days and/or as directed by the Engineer.

All permanent striping shall be double drop thermoplastic, 90-mil thickness unless otherwise specified in Subsection 626.03.1.2. Edge lines shall be placed to accommodate the lane widths shown on the attached applicable typical sections unless prevented by field conditions.

Rumble stripe shall be placed throughout the project limits in accordance with MDOT Standard Drawings.

Guardrail

Asphalt shall be extended two feet (2') behind the guardrail per the attached detail. The area to be paved shall be bladed to accommodate three inches (3") of asphalt pavement. The elevation of the finished surface of the asphalt pavement shall provide for the required MASH guardrail height (see Standard Drawings).

Traffic Control

The Contractor shall erect and maintain construction signing and provide all signs and traffic control devices necessary to safely maintain traffic around and through the work areas in accordance with the Traffic Control Plan and the MUTCD. The cost shall be included in the price bid for pay item 618-A: Maintenance of Traffic. Fluorescent orange sheeting shall be used on all construction and traffic control signs except those designated in the plans to be black legend and border on white background.

Standard roadside construction signs, barricades, etc. shall be placed in accordance with the attached tables, drawings, and as directed by the Engineer. W20-1 signs shall be placed on all public road approaches as shown or as directed. Payment for standard roadside construction signs, barricades, etc. will be made using the appropriate pay items.

The Contractor shall on a daily basis, remove all debris from within the roadway and a 30-foot clear zone which, in the opinion of the Engineer, is a hazard to the traveling public. This activity shall begin with the beginning of work or the beginning of the contract time, whichever comes first. No direct payment will be made for the debris removal; the cost is to be included in the prices of items bid. Failure of the Contractor to remove the debris as prescribed herein shall be just cause for withholding the monthly progress estimate payment or suspending active operations until the debris is satisfactorily removed by the Contractor.

Temporary asphalt joints (paper joints) shall be employed at all locations requiring traffic to traverse an uneven, transverse, pavement joint. Paper joints shall be a minimum of nine feet (9') in length and for the full width of the milled/paved surface. Paper joints shall be adequately maintained.

Potholes that may exist or occur in the existing pavement shall be patched in a timely manner as required. Patching of potholes shall be considered an absorbed item.

Miscellaneous Notes

It shall be the responsibility of the Contractor to protect existing structures such as pipes, inlets, aprons, bridges, etc. from damage which might occur during construction. The Contractor shall replace or repair, as directed by the Engineer, any structures damaged by the Contractor during the life of the contract. No payment will be made for replacement or repair of damaged items.

Cross drain repairs and replacement shall take place prior to the overlay operation following the attached table. All pipe joints shall be wrapped in 24-inch wide Type V Geotextile. All pickup holes shall be plugged and covered with Type V Geotextile; the cost of which shall be absorbed in other items bid.

The cross drain repair at Station 121+60 to 121+75 will involve a full remove and replace of the existing pipe which shall be completed as a continuous operation in order to minimize traffic impacts and will require special traffic control according to the current edition of the Manual on Uniform Traffic Control Devices. Lane closures shall remain in place until the failed area has been completely repaired. Lane closures shall not be left unattended. Night time lane closures will NOT be allowed. In lieu of lift holes for this pipe, the producer may cast an approved lifting device during the manufacturing process. Should a lifting device be included with the pipe, the Contractor shall cut off or grind down the lifting device flush with the pipe surface after placement of the pipe. The area around the lifting device shall be coated with a sealer approved by the Engineer. Work related to the lifting device will be considered incidental and absorbed in other items bid.

Riprap shall be required at both the in fall and outfall of the replaced pipe, some of which may need to be hand placed due to field conditions. The furnished rock shall come from a pre-approved source and be visually approved prior to placement.

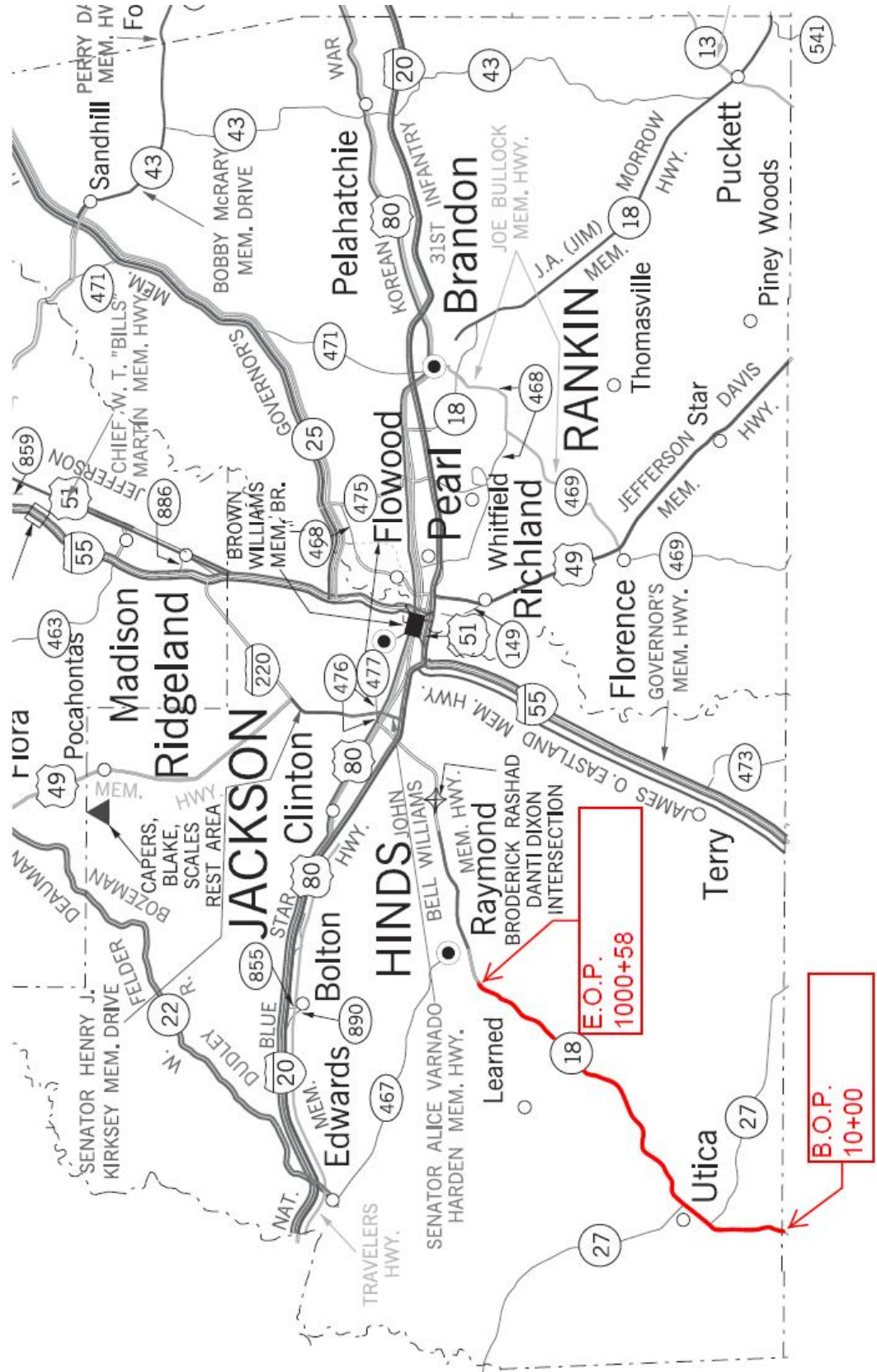
Any signs that are in conflict with construction of this project shall be removed and relocated by the Contractor as directed by the Engineer, the cost of which is to be absorbed in other items bid.

Removal of existing raised pavement markers shall be included in the prices for other items bid.

Incidental work such as removing vegetation, shaping and compacting shoulders, removing and resetting signs and/or mailboxes, removing excess asphalt material, project clean-up, and other items of incidental work necessary to complete the project will not be measured for separate payment and will be considered included in the prices of items bid.

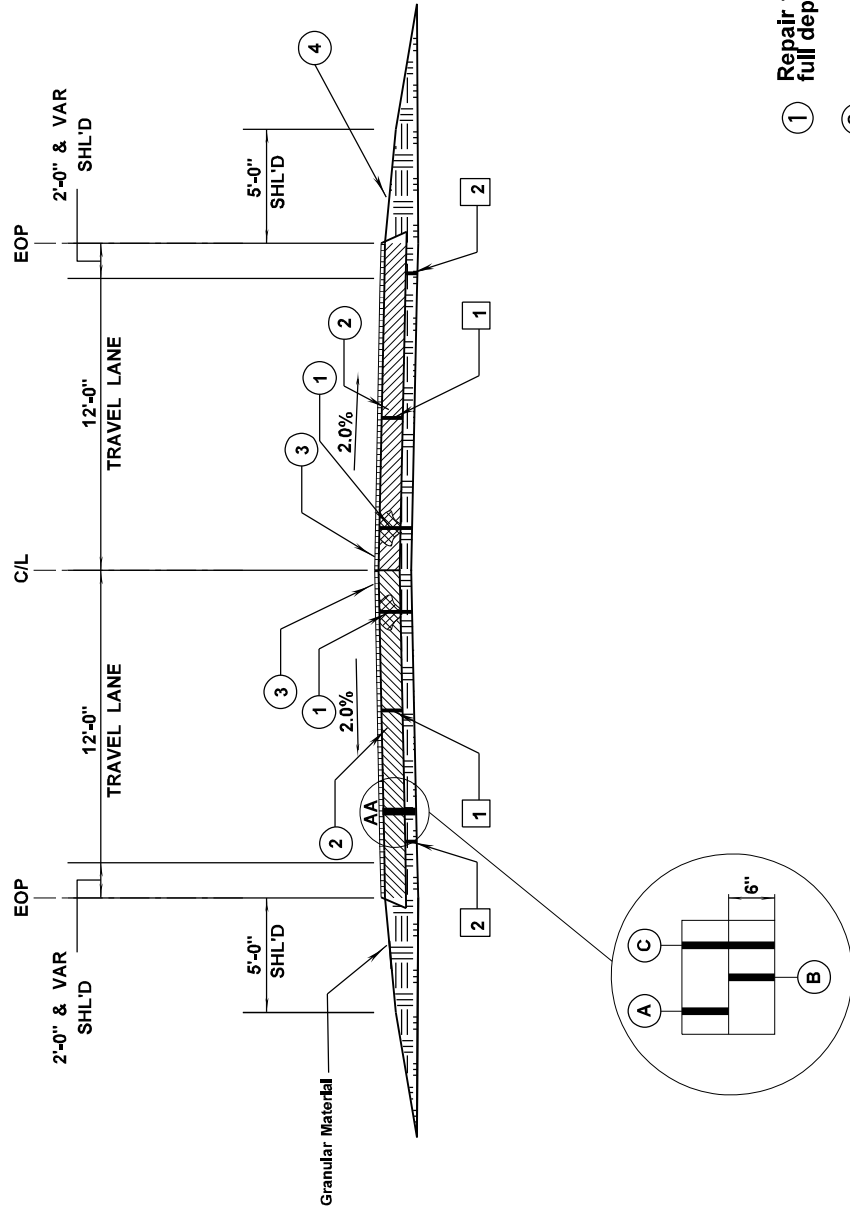
Prior to the final inspection, bridges, islands, and areas with curb shall be swept/cleaned. Care should be taken to prevent milled asphalt, asphalt debris, vegetative/granular debris, etc. from entering drainage structures or clogging other drainage ways. Disposal of material will not be measured for separate payments.

PROJECT MAP
HINDS COUNTY
SP-0039-02(054)
HWY 18



HWY 18 STATION NUMBERS
STA. 10+00 - STA. 158+12
OMIT BRIDGE STA. 26+22 - STA 74+20

**SEE OTHER TYPICALS FOR THE REST
OF THE STATION NUMBERS**



- EXISTING**
- 1 - 5 1/4" - 15" ASPHALT
 - 2 - 8" Topping
 - [Symbol] = Overlay with 2" 12.5mm, Mix, ST
 - [Symbol] = Repair area
 - [Symbol] = Existing asphalt
 - [Symbol] = Cement-Treated Clay Gravel

- PROPOSED**
- 1 Repair failed areas with 12.5mm, Mix ST Leveling full depth.
 - 2 Mill existing asphalt 1 1/2" and variable.
 - 3 Overlay with 2" and variable of 12.5mm Mix, ST. Place 2" and Variable Granular Material on shoulder as directed by the Engineer
 - 4

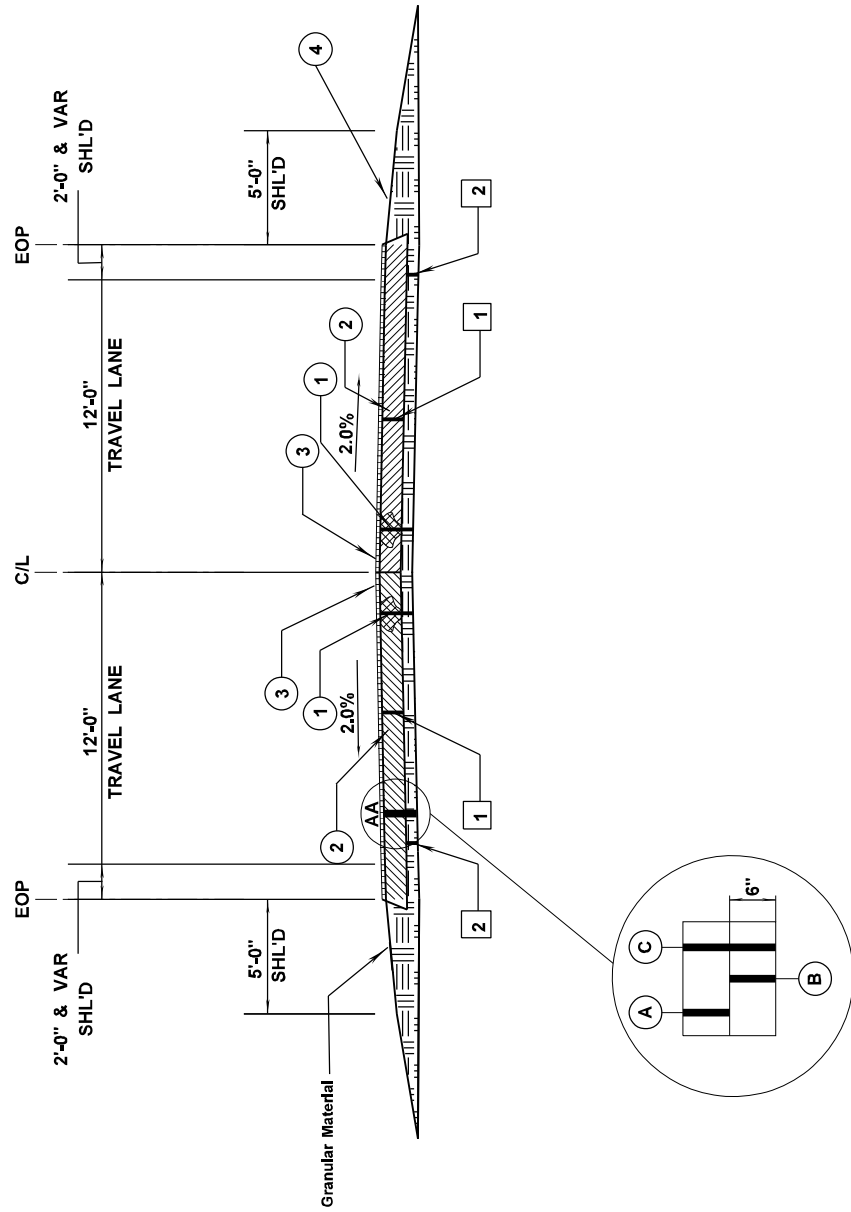
- AA **BASE FAILURE REPAIR (ALL LOCATIONS)**
- A **EXISTING PAVEMENT TO BE REMOVED**
- B **UNDERCUT EXISTING MATERIAL AS DIRECTED**
- C **REPLACE FULL DEPTH WITH HMA 12.5-MM ST. ASPHALT PAVEMENT (LEVELING)**

HINDS COUNTY
108233/301000
HIGHWAY 18

HWY 18 STATION NUMBERS

STA. 158+12 - STA. 260+60

**SEE OTHER TYPICALS FOR THE REST
OF THE STATION NUMBERS**



EXISTING

1 - 5 1/4" - 15" ASPHALT

2 - Cement-Treated Clay Gravel

[Symbol] = Overlay with 2" 12.5mm, Mix, ST

[Symbol] = Repair area

[Symbol] = Existing asphalt

[Symbol] = Cement-Treated Clay Gravel

PROPOSED

- 1 Repair failed areas with 12.5mm, Mix ST Leveling full depth.
- 2 Mill existing asphalt 1 1/2" and variable.
- 3 Overlay with 2" and variable of 12.5mm Mix, ST.
- 4 Place 2" and Variable Granular Material on shoulder as directed by the Engineer

**AA BASE FAILURE REPAIR
(ALL LOCATIONS)**

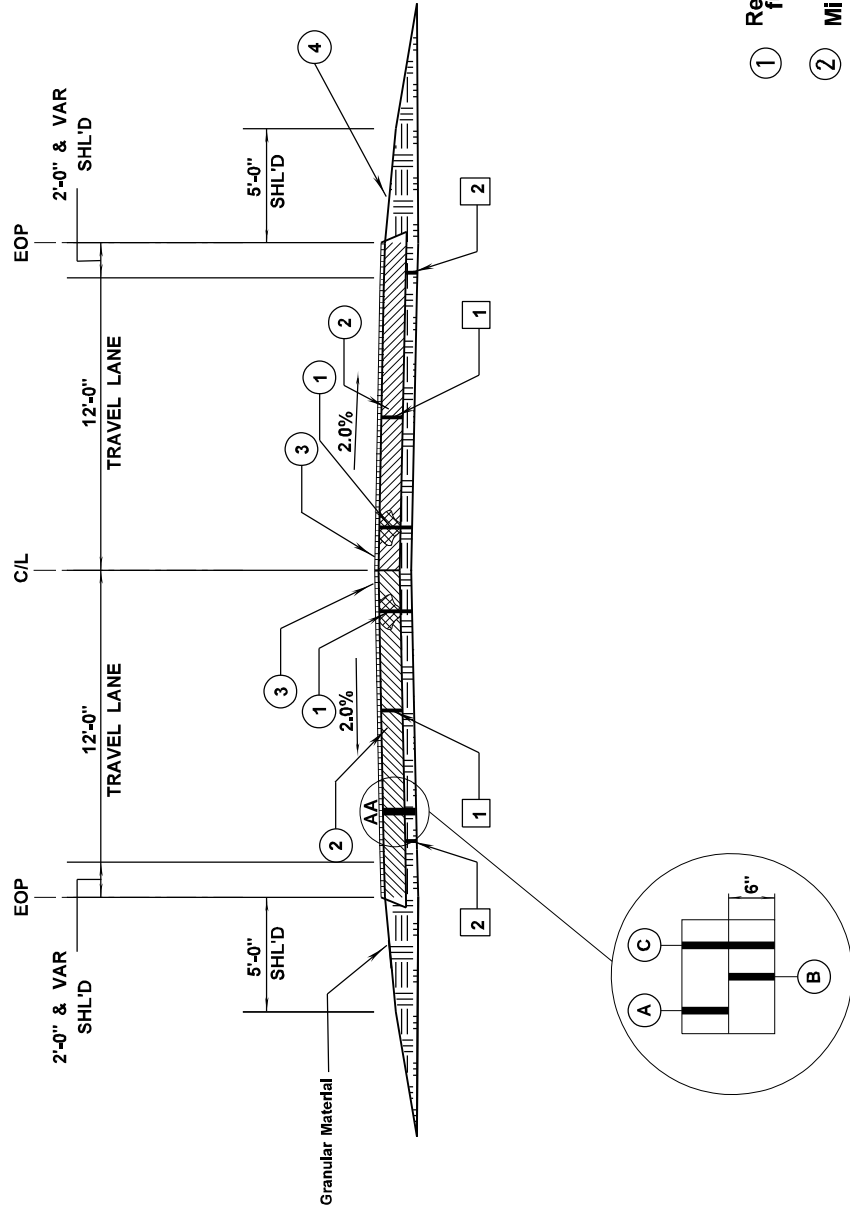
- A EXISTING PAVEMENT TO BE REMOVED
- B UNDERCUT EXISTING MATERIAL AS DIRECTED
- C REPLACE FULL DEPTH WITH HMA 12.5-MM ST. ASPHALT PAVEMENT (LEVELING)

HINDS COUNTY
108233/301000
HIGHWAY 18

HWY 18 STATION NUMBERS

**STA. 260+60 - STA. 969+45
OMIT BRIDGE STA. 915+18 - STA. 916+58**

**SEE OTHER TYPICALS FOR THE REST
OF THE STATION NUMBERS**

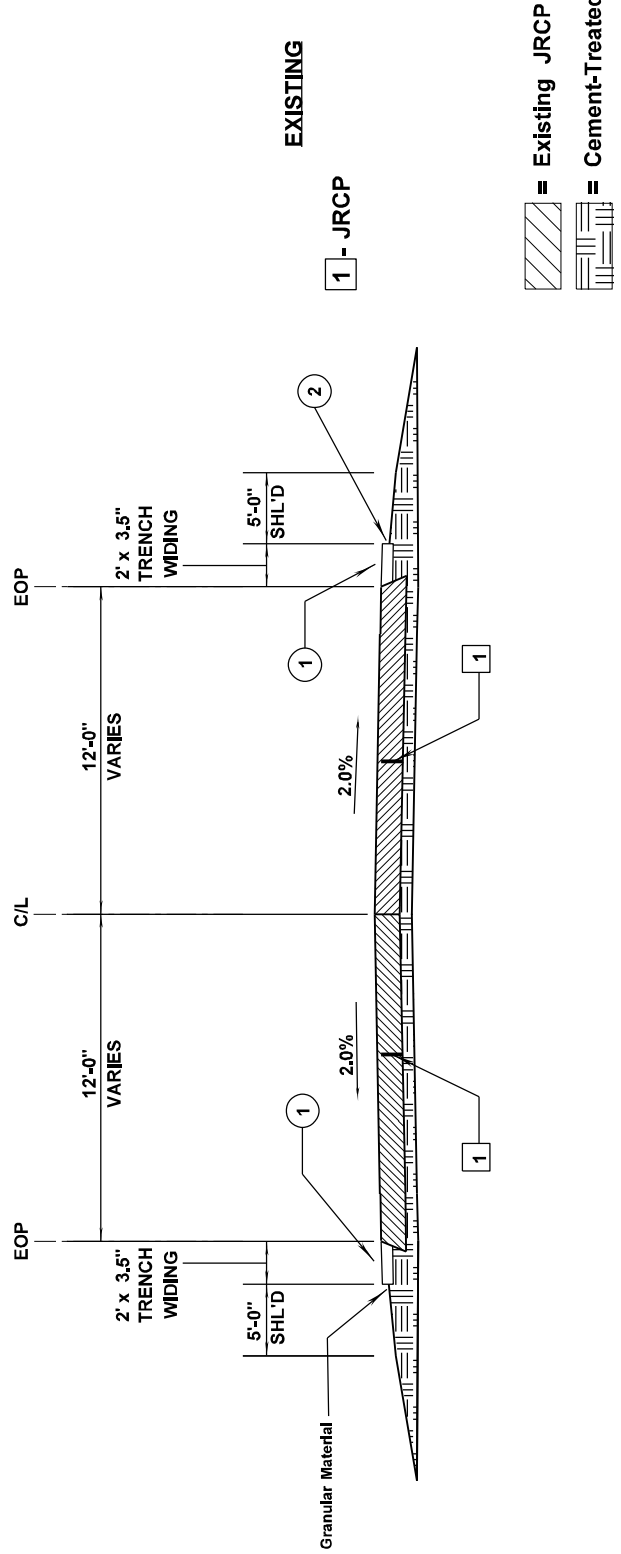


HINDS COUNTY
108233/301000
HIGHWAY 18

HWY 18 STATION NUMBERS

STA. 969+45 - STA. 1000+58

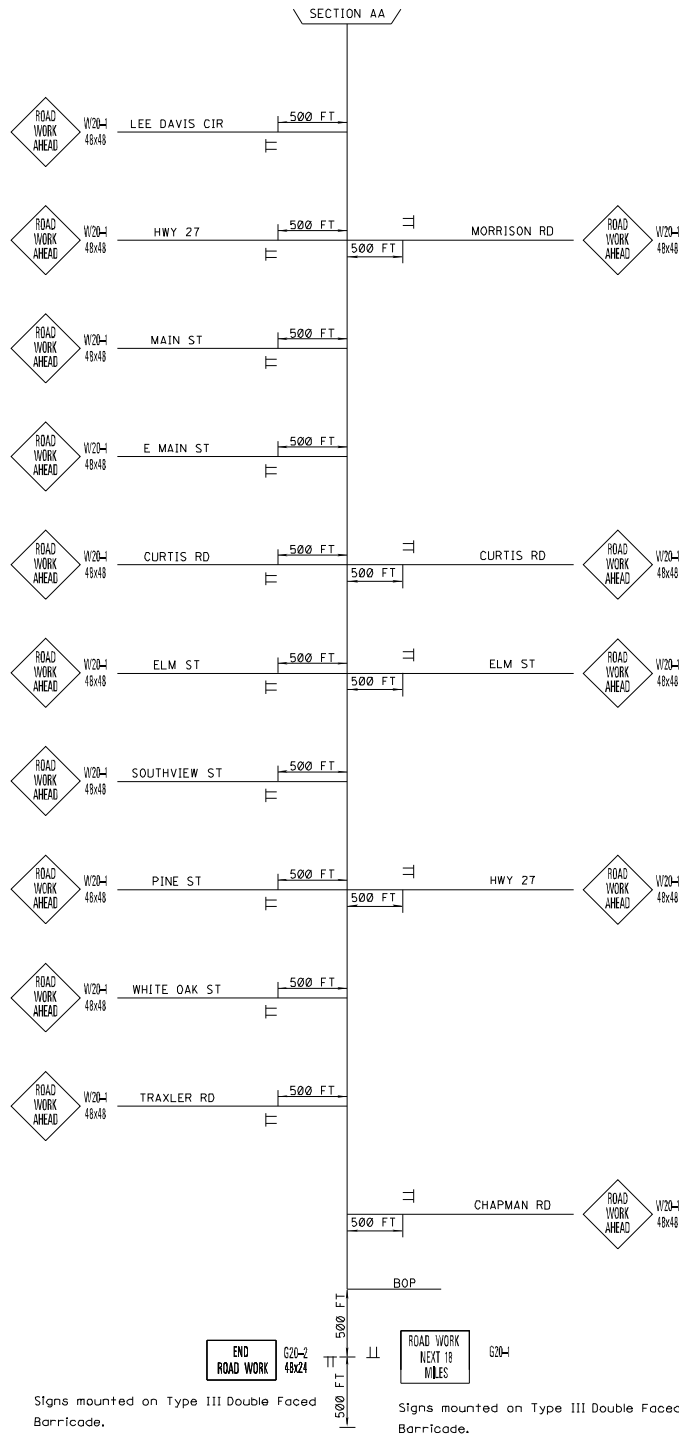
SEE OTHER TYPICALS FOR THE REST
OF THE STATION NUMBERS



PROPOSED

- ① 3.5" Trench widening per cross section and detail.
- ② Place 2" and Variable Granular Material on shoulder as directed by the Engineer

11- Notice To Bidders No. 4222 - Cont'd.
 CONSTRUCTION SIGNING DETAIL
 HWY 18
 HINDS COUNTY
 STA. 10+00 - 287+50



ESTIMATED

- Traffic Control Signs Required:
- 1 - G20-1 "ROAD WORK NEXT 18 MILES"
 - 1 - G20-2 "END ROAD WORK"
 - 15 - W20-1 "ROAD WORK AHEAD"
 - 45 - R4-1 "DO NOT PASS"
 - 9 - R4-2 "PASS WITH CARE"
 - 9 - W14-3 "NO PASSING ZONE"
 - 2 - TYPE III DBL. FACE BARRICADES

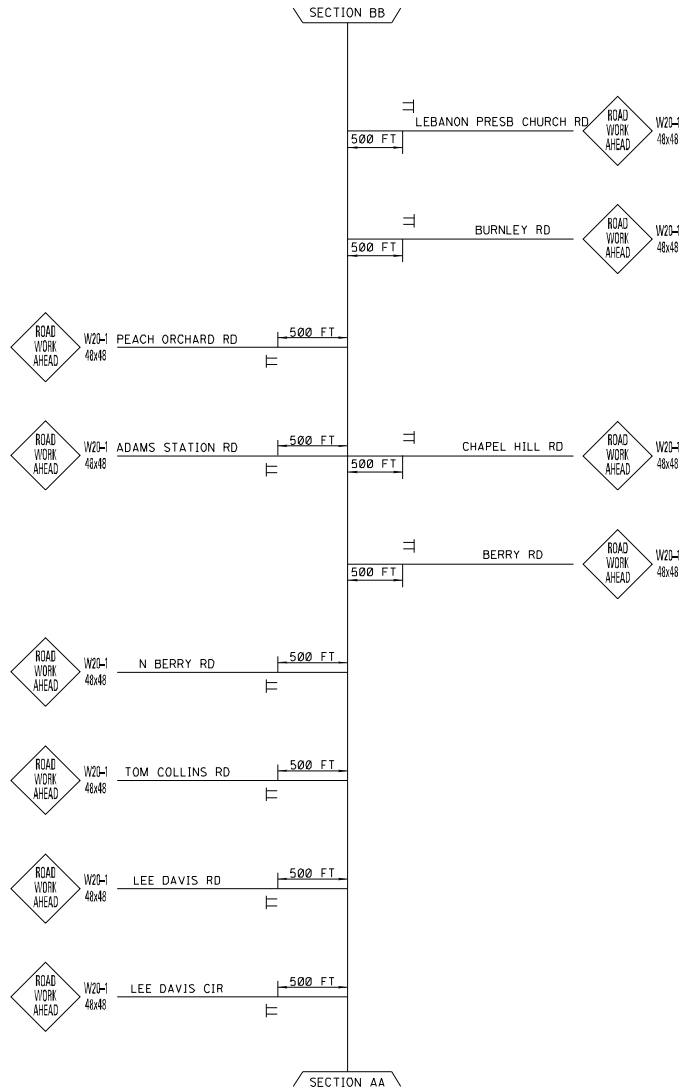
NOTES: One (1) W20-1 "ROAD WORK AHEAD" Sign Is Required at each Local Road, Street or Highway Entering the Project.
 R4-1 "DO NOT PASS", R4-2 "PASS WITH CARE" AND W14-3 "NO PASSING ZONE" signs are required in accordance with Subsection 618.03.3 and as specified in the MUTCD. If No Passing zones are 1000 ft or more, install additional "DO NOT PASS" signs on maximum spacing of 750 ft. Payment for these signs will be under the appropriate pay item numbers in the summary of quantities

CONSTRUCTION SIGNING DETAIL

HWY 18

HINDS COUNTY

STA. 287+50 - 572+50



ESTIMATED

Traffic Control Signs Required:

- 10 - W20-1 "ROAD WORK AHEAD"
- 61 - R4-1 "DO NOT PASS"
- 7 - R4-2 "PASS WITH CARE"
- 8 - W14-3 "NO PASSING ZONE"

NOTES: One (1) W20-1 "ROAD WORK AHEAD" Sign is Required at each Local Road,

Street or Highway Entering the Project.

R4-1 "DO NOT PASS", R4-2 "PASS WITH CARE"

AND W14-3 "NO PASSING ZONE" signs

are required in accordance with Subsection

618.03.3 and as specified

in the MUTCD. If No Passing zones are 1000

ft or more, install additional

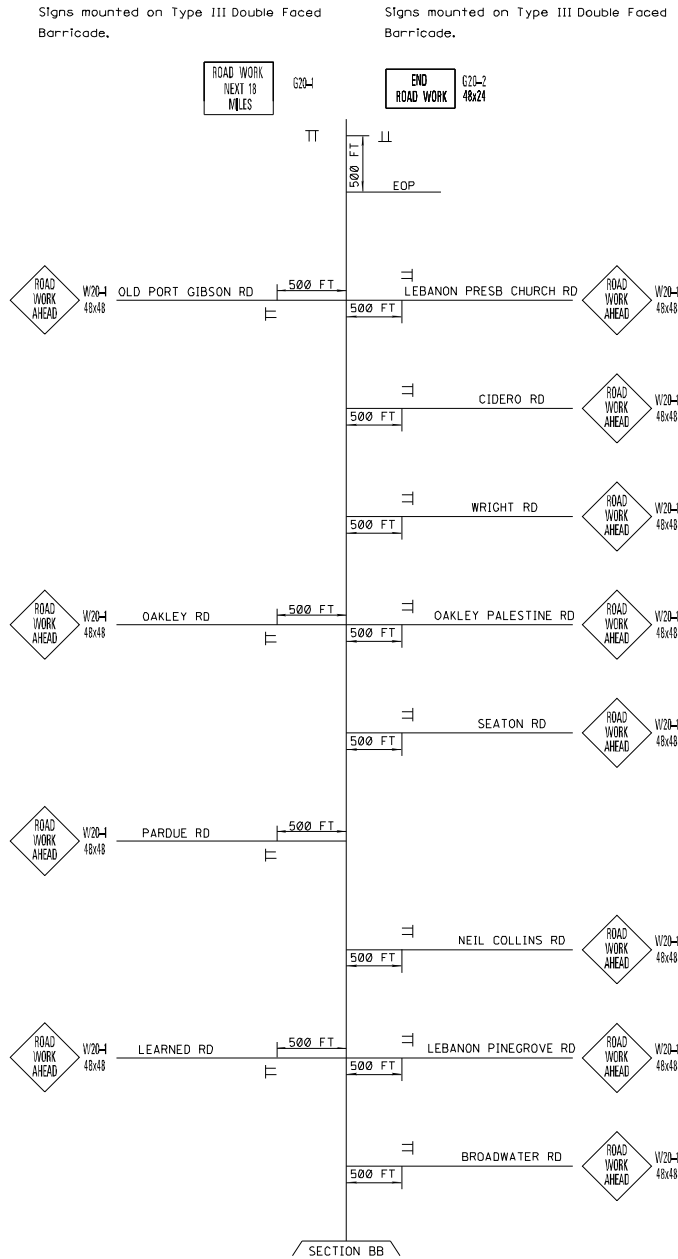
"DO NOT PASS" signs on maximum spacing of 750 ft.

Payment for these signs will be under the

appropriate pay item numbers in the summary of

quantities

-13- Notice To Bidders No. 4222 - Cont'd.
 CONSTRUCTION SIGNING DETAIL
 HWY 18
 HINDS COUNTY
 STA. 572+50 - 1000+58



ESTIMATED

- Traffic Control Signs Required:
- 1 - G20-1 "ROAD WORK NEXT 18 MILES"
 - 1 - G20-2 "END ROAD WORK"
 - 12 - W20-1 "ROAD WORK AHEAD"
 - 69 - R4-1 "DO NOT PASS"
 - 18 - R4-2 "PASS WITH CARE"
 - 17 - W14-3 "NO PASSING ZONE"
 - 2 - TYPE III DBL. FACE BARRICADES

NOTES: One (1) W20-1 "ROAD WORK AHEAD" Sign Is Required at each Local Road, Street or Highway Entering the Project.
 R4-1 "DO NOT PASS", R4-2 "PASS WITH CARE" AND W14-3 "NO PASSING ZONE" signs are required in accordance with Subsection 618.03.3 and as specified
 In the MUTCD, If No Passing zones are 1000 ft or more, Install additional "DO NOT PASS" signs on maximum spacing of 750 ft. Payment for these signs will be under the appropriate pay item numbers in the summary of quantities

SP-0039-02(054) 108233/301000
Hinds County

Cross Drain Repair, Removal Quantities											
Beginning	Ending	Lane	Sawcut Length	Sawcut Width	Saw Cut Total (LF)	Pavement Removal (SY)	Estimated Excess (CY)	RCP Removal (LF)	FES Removal (EA)	Remarks	
121+60	121+75	BOTH	0	30	60	16.66	80	56	2	Remove	
354+47	354+53	LT	4	10	14	2.22	2.22	0	0	Reset	
354+47	354+53	RT	4	10	14	2.22	2.22	0	0	Reset	
384+00	384+04	RT	4	10	14	2.22	2.22	0	0	Reset	
395+90	15' offset	RT	0	0	0	0	1.78	0	0	Reset	
443+00	443+04	RT	4	10	14	2.22	1.78	0	0	Reset	
443+00	27' offset	RT	0	0	0	0	1.78	0	0	Reset	
554+60	554+64	RT	4	10	14	2.22	1.78	0	0	Reset	
					Total	130	28	94	56	2	

Note: Failed areas may be adjusted at the discretion of the Engineer.

Cross Drain Repair, Reset Quantities											
Beginning	Ending	Lane	RCP (LF)	FES (EA)	Collar (Class "B")	Crushed Stone (TON)	Asphalt (TON)	Riprap (TON)	Remarks		
121+60	121+75	BOTH	56*	2	0	157.5	9.16	44	Replace		
354+47	354+53	LT	0	0	0.41	3.00	0.98	0	Reset		
354+47	354+53	RT	0	0	0.41	3.00	0.98	0	Reset		
384+00	384+04	RT	0	0	0.41	3.00	0.98	0	Reset		
396+40	15' offset	RT	0	0	0.41	0.00	0.00	0	Reset		
443+00	443+04	RT	0	0	0.32	3.00	0.98	0	Reset		
443+00	27' offset	RT	0	0	0.32	0.00	0.00	0	Reset		
554+60	554+64	RT	0	0	0.41	3.00	0.98	0	Reset		
			Total	56	2	173	14	44			

* 24" Geotextile at joints absorbed in other bid items

SP-0039-02(054) 108233/301000

Hinds County

Full Depth Repair (Page 1 of 2)										
STA	to	STA	Lane	Length	Width	Saw Cuts (ft)	Pavement Removal (SY)	Estimated Excess (CY)	Estimated Asphalt Req. (Tons)	Remarks
258+95		259+45	CENTER	50	14	128	77.78	12.96	77.00	
263+34		263+60	LT	26	16	58	46.22	7.70	45.76	
280+10		280+16	LT	6	14	34	9.33	1.56	9.24	
283+10		283+16	LT	6	14	34	9.33	1.56	9.24	
291+05		291+11	BOTH	6	28	56	18.67	3.11	18.48	
312+00		312+08	LT	8	14	36	12.44	2.07	12.32	
314+45		314+51	BOTH	6	28	56	18.67	3.11	18.48	
330+40		330+46	BOTH	6	28	56	18.67	3.11	18.48	
331+30		331+36	LT	6	14	34	9.33	1.56	9.24	
334+90		334+90	RT	6	14	34	9.33	1.56	9.24	
339+00		339+06	RT	6	14	34	9.33	1.56	9.24	
341+10		341+16	RT	6	14	34	9.33	1.56	9.24	
344+00		345+25	LT	125	14	153	194.44	32.41	192.50	
353+60		353+66	LT	6	14	34	9.33	1.56	9.24	
354+50		354+60	LT	10	14	38	15.56	2.59	15.40	
355+45		355+51	RT	6	14	34	9.33	1.56	9.24	
357+95		361+10	BOTH	160	28	56	497.78	82.96	492.80	
358+80		361+10	LT	30	14	58	46.67	7.78	46.20	
365+25		365+60	RT	35	14	63	54.44	9.07	53.90	
365+60		365+95	LT	35	14	63	54.44	9.07	53.90	
366+00		366+06	LT	6	14	34	9.33	1.56	9.24	
366+90		366+96	LT	6	14	34	9.33	1.56	9.24	
371+10		371+16	RT	6	14	34	9.33	1.56	9.24	
372+00		374+10	LT	210	14	238	326.67	54.44	323.40	
373+00		373+10	RT	10	14	38	15.56	2.59	15.4	
381+75		381+81	BOTH	6	28	56	18.67	3.11	18.48	
387+95		388+01	LT	6	14	34	9.33	1.56	9.24	
389+50		389+56	LT	6	14	34	9.33	1.56	9.24	
395+90		396+02	BOTH	12	28	56	37.33	6.22	36.96	
396+05		396+11	LT	6	14	34	9.33	1.56	9.24	
396+10		396+30	RT	20	14	48	31.11	5.19	30.80	
396+95		397+01	LT	6	14	34	9.33	1.56	9.24	
401+00		401+06	LT	6	14	34	9.33	1.56	9.24	
403+20		403+26	RT	6	14	34	9.33	1.56	9.24	
403+90		403+96	LT	6	14	34	9.33	1.56	9.24	
408+50		408+56	BOTH	6	28	56	18.67	3.11	18.48	
408+70		408+76	LT	6	14	34	9.33	1.56	9.24	
411+75		411+81	LT	6	14	34	9.33	1.56	9.24	
414+60		414+66	BOTH	6	28	56	18.67	3.11	18.48	
427+35		427+41	RT	6	14	34	9.33	1.56	9.24	
430+70		430+76	LT	6	14	34	9.33	1.56	9.24	
430+80		430+86	LT	6	14	34	9.33	1.56	9.24	
436+90		436+96	LT	6	14	34	9.33	1.56	9.24	
440+50		440+56	BOTH	6	28	56	18.67	3.11	18.48	
442+90		442+70	BOTH	20	28	56	62.22	10.37	61.60	
445+80		445+86	BOTH	6	28	62	18.67	3.11	18.48	
446+60		446+85	BOTH	25	28	56	77.78	12.96	77.00	
448+95		449+01	LT	6	14	34	9.33	1.56	9.24	
454+65		454+95	LT	30	14	58	46.67	7.78	46.2	
454+65		454+71	RT	6	14	34	9.33	1.56	9.24	
463+50		463+56	RT	6	14	34	9.33	1.56	9.24	
488+25		488+31	BOTH	6	28	56	18.67	3.11	18.48	
				Total		2631	2017	336	1997	

Note: Failed areas may be adjusted at the discretion of the Engineer.

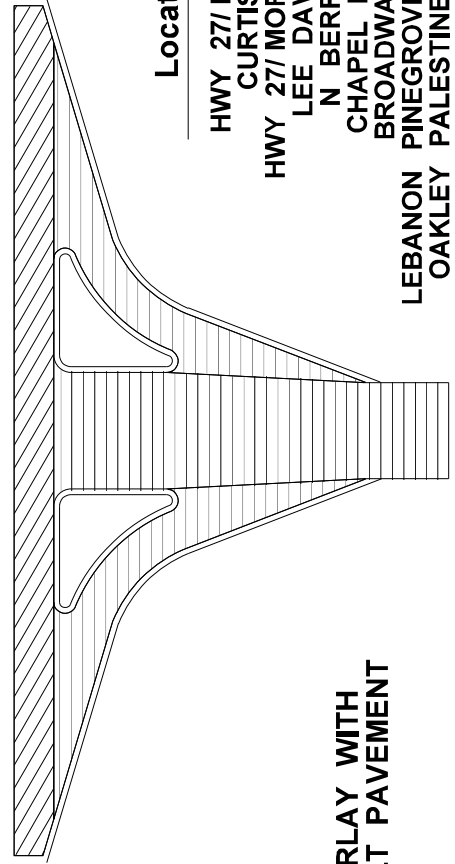
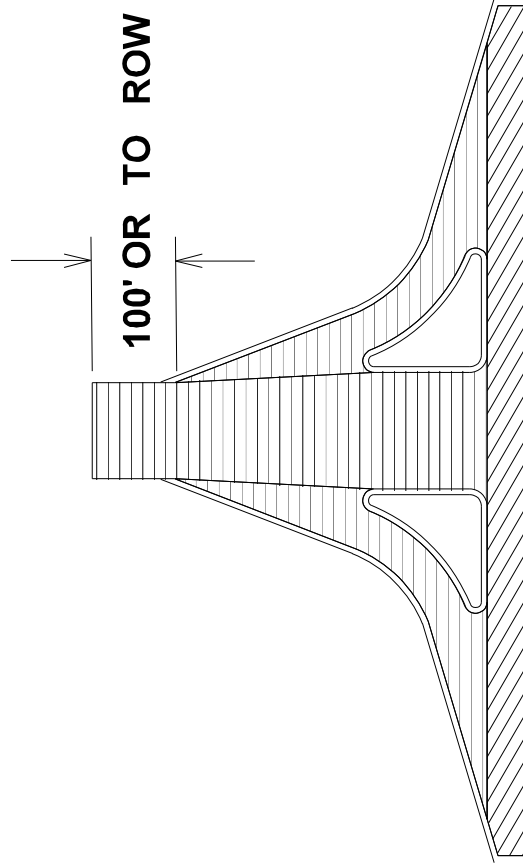
SP-0039-02(054) 108233/301000

Hinds County

Full Depth Repair (Page 2 of 2)										
STA	to	STA	Lane	Length	Width	Saw Cuts (ft)	Pavemet Removal (SY)	Estimated Excess (CY)	Estimated Asphalt Req. (Tons)	Remarks
494+05		494+11	LT	6	14	34	9.33	1.56	9.24	
494+15		494+21	RT	6	14	34	9.33	1.56	9.24	
494+75		494+81	LT	6	14	28	9.33	1.56	9.24	
497+50		497+56	RT	6	14	34	9.33	1.56	9.24	
500+50		500+56	BOTH	6	28	56	18.67	3.11	18.48	
505+00		505+06	RT	6	14	34	9.33	1.56	9.24	
515+85		515+91	LT	6	14	34	9.33	1.56	9.24	
516+30		516+36	RT	6	14	34	9.33	1.56	9.24	
520+75		520+81	LT	6	14	34	9.33	1.56	9.24	
528+20		528+26	RT	6	14	34	9.33	1.56	9.24	
580+70		580+76	BOTH	6	28	56	18.67	3.11	18.48	
592+78		593+18	RAMP	25	11	47	30.56	5.09	30.25	
619+50		619+56	LT	6	14	34	9.33	1.56	9.24	
626+70		626+76	LT	6	14	34	9.33	1.56	9.24	
627+70		627+76	RT	6	14	34	9.33	1.56	9.24	
656+00		656+06	LT	6	14	34	9.33	1.56	9.24	
656+60		656+80	RT	15	14	43	23.33	3.89	23.10	
702+50		702+56	RT	6	14	34	9.33	1.56	9.24	
710+00		710+06	RT	6	14	34	9.33	1.56	9.24	
735+65		735+71	LT	6	14	34	9.33	1.56	9.24	
736+70		736+76	RT	6	14	34	9.33	1.56	9.24	
747+50		747+56	RT	6	14	34	9.33	1.56	9.24	
752+20		752+26	LT	6	14	34	9.33	1.56	9.24	
752+80		752+86	RT	6	14	34	9.33	1.56	9.24	
756+20		756+26	BOTH	6	28	56	18.67	3.11	18.48	
776+00		776+06	LT	6	14	34	9.33	1.56	9.24	
781+85		781+91	LT	6	14	34	9.33	1.56	9.24	
785+00		785+06	LT	6	14	34	9.33	1.56	9.24	
785+20		785+26	LT	6	14	34	9.33	1.56	9.24	
787+75		787+81	LT	6	14	34	9.33	1.56	9.24	
797+00		797+06	LT	6	14	34	9.33	1.56	9.24	
798+25		798+31	LT	6	14	34	9.33	1.56	9.24	
802+00		802+06	BOTH	6	28	56	18.67	3.11	18.48	
802+25		802+37	BOTH	12	28	68	37.33	6.22	36.96	
813+00		813+15	RT	15	14	43	23.33	3.89	23.10	
813+80		813+86	LT	6	14	34	9.33	1.56	9.24	
813+85		813+97	LT	12	14	40	18.67	3.11	18.48	
817+05		817+11	LT	6	14	34	9.33	1.56	9.24	
827+60		827+66	RT	6	14	34	9.33	1.56	9.24	
837+70		837+76	LT	6	14	34	9.33	1.56	9.24	
849+35		849+41	RT	15	14	43	23.33	3.89	23.1	
851+30		851+36	LT	6	14	34	9.33	1.56	9.24	
858+50		858+56	LT	6	14	34	9.33	1.56	9.24	
861+05		861+11	LT	6	14	34	9.33	1.56	9.24	
866+30		866+50	LT	20	14	48	31.11	5.19	30.80	
887+50		887+56	RT	6	14	34	9.33	1.56	9.24	
927+75		927+81	BOTH	6	28	56	18.67	3.11	18.48	
961+50		961+56	BOTH	6	28	56	18.67	3.11	18.48	
967+43		967+55	RT	14	14	42	21.78	3.63	21.56	
967+27		967+55	LT	28	14	56	43.56	7.26	43.12	
995+28		995+48	RAMP	20	10	60	22.22	3.70	22.00	
				Total	2010	714	119	707		

Note: Failed areas may be adjusted at the discretion of the Engineer.

**HINDS COUNTY - HWY 18
TYPICAL MILLING PLAN FOR
CHANNELIZED INTERSECTIONS**

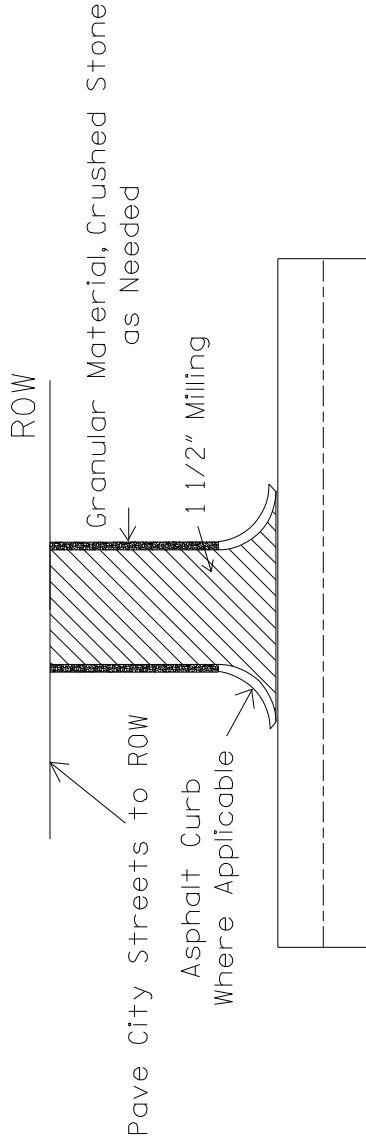


Locations

- HWY 27/PINE ST.
- CURTIS RD.
- HWY 27/MORRISON RD.
- LEE DAVIS RD.
- N BERRY RD.
- CHAPEL HILL RD.
- BROADWATER RD.
- LEBANON PINEGROVE RD./LEARNED RD.
- OAKLEY PALESTINE RD./OAKLEY RD.
- OLD PORT GIBSON RD.

**MILL 1 1/2" AND OVERLAY WITH
2" 12.5MM, ST, ASPHALT PAVEMENT**

S.R. 18 - Hinds County Milling and Paving Detail City Streets



Notes:

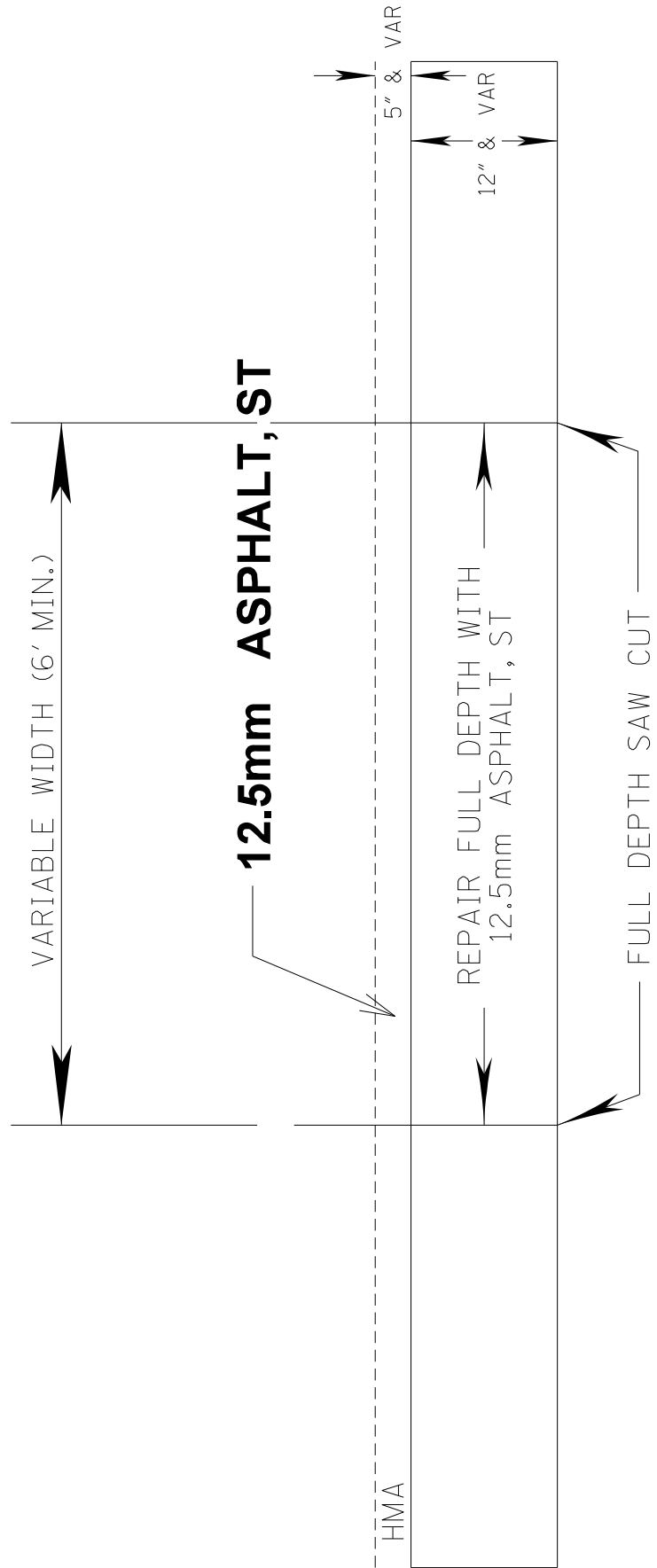
- Mill limits of county/local roads at a depth of 1 1/2".
- Place 2" of 12.5mm, ST, Mixture to tie to mainline overlay.
- Milling/Paving area = ▨

County Roads:

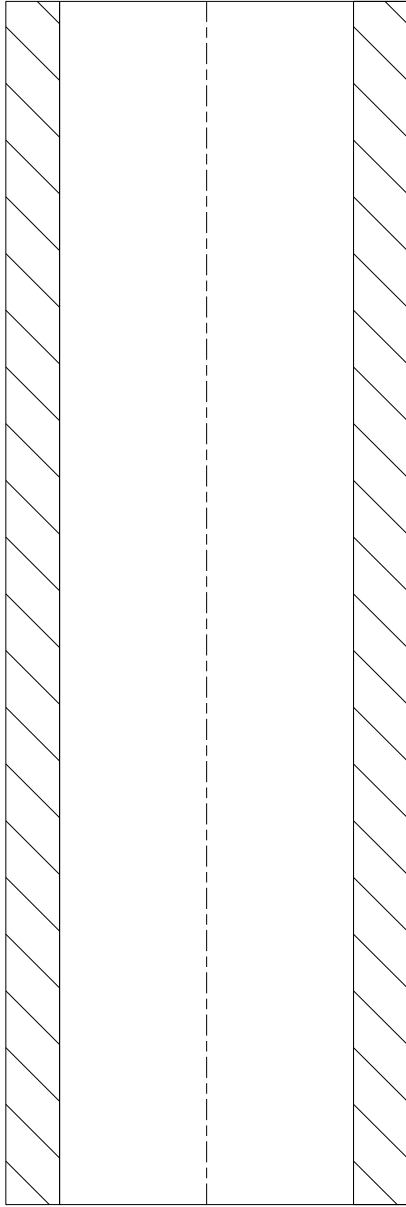
- Broadwater Rd.
- Neil Collins Rd.
- Seaton Rd.
- Wright Rd.
- Cidero Rd.
- Chapman Rd.
- Traxler Rd.
- White Oak Rd.
- White Oak St.
- Southview St.
- Elm St.
- E. Main St.
- Main St.
- Morrison Rd.
- Lee Davis Cir.
- Tom Collins Rd.
- Berry Rd.
- Burnley Rd.
- Lebanon Presb Church Rd.

HINDS COUNTY
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SR 18

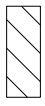
FULL DEPTH REPAIR DETAIL



S.R. 18 - Hinds County Trench Widening Detail



Notes:

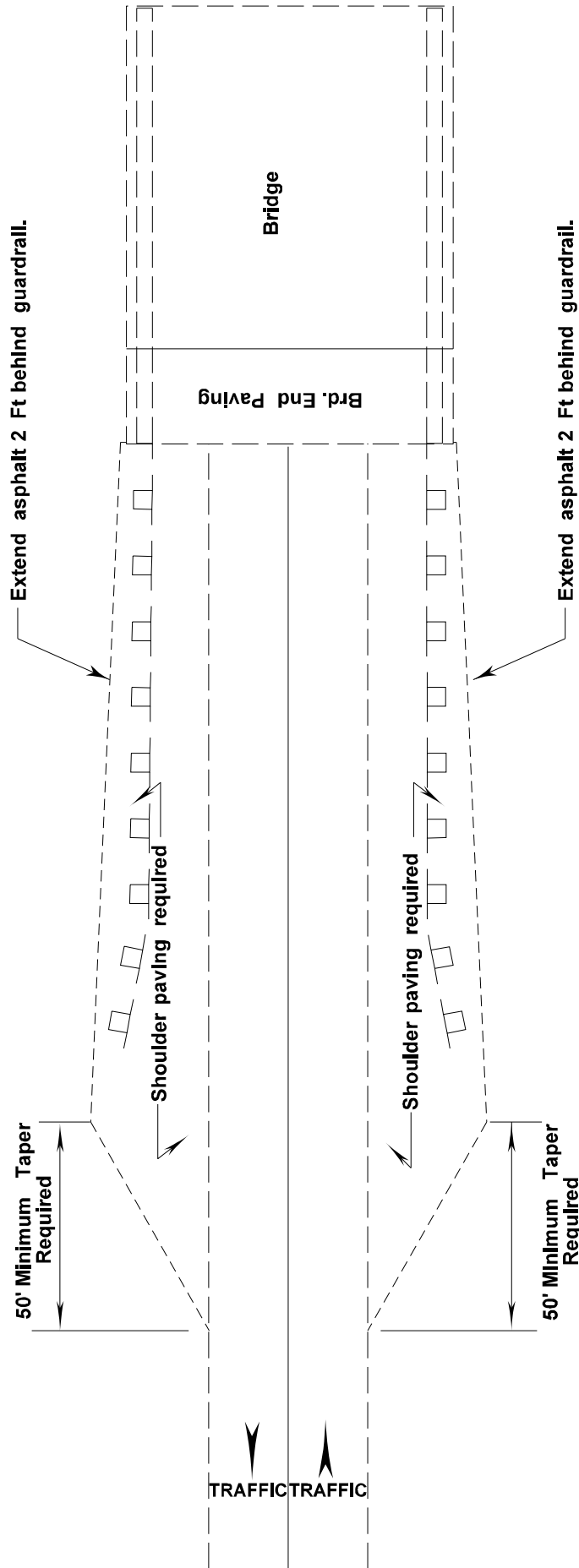
-Trench Widening area = 

-Trench Widening to be 2' width by 3.5" depth of 19mm, ST, Asphalt pavement, trench widening

-Widening to be placed at the discretion of the engineer

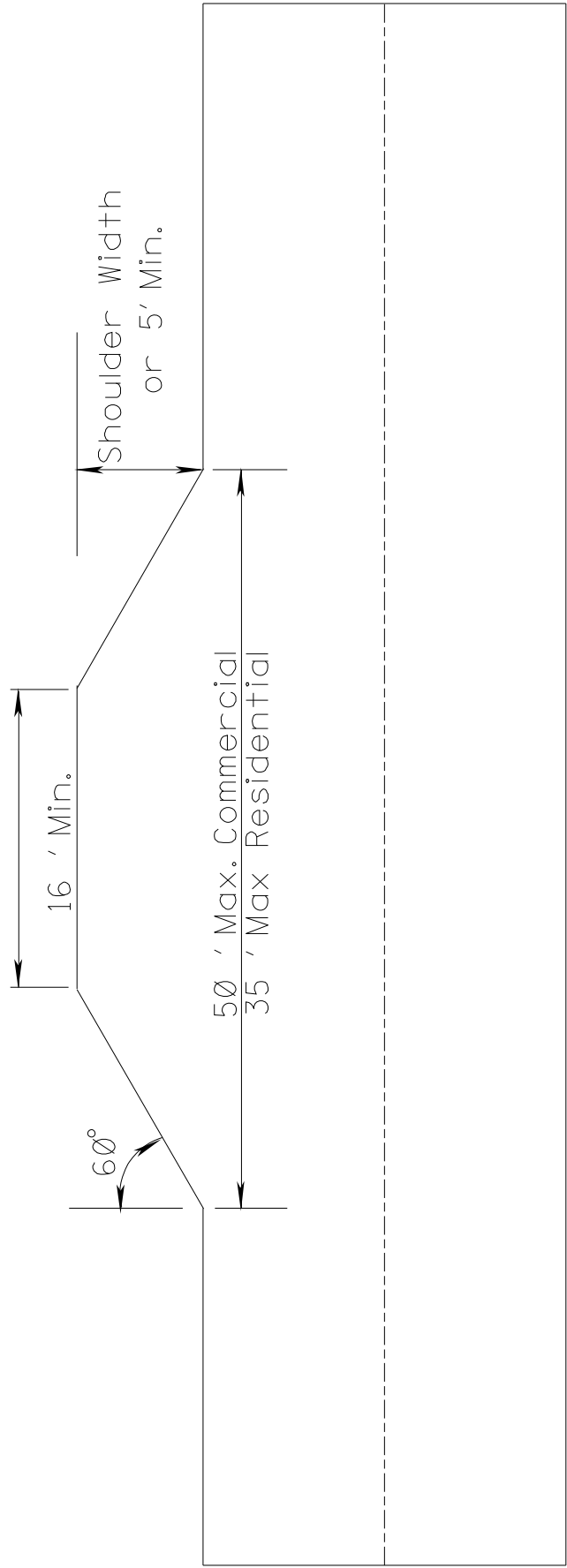
**HINDS COUNTY
SP-0039-02(054) 108233/301000**

**TYPICAL DETAIL OF ADDITIONAL SHOULDER PAVING
REQUIRED AT GUARDRAIL LOCATIONS**



*Asphalt Thickness
See scope of work for additional details

TYPICAL RAMP/PAD DETAIL





WORKING NUMBER
 SPSR-
 SHEET NUMBER
 \$ P C \$

NOT TO SCALE
 MISSISSIPPI DEPARTMENT OF TRANSPORTATION

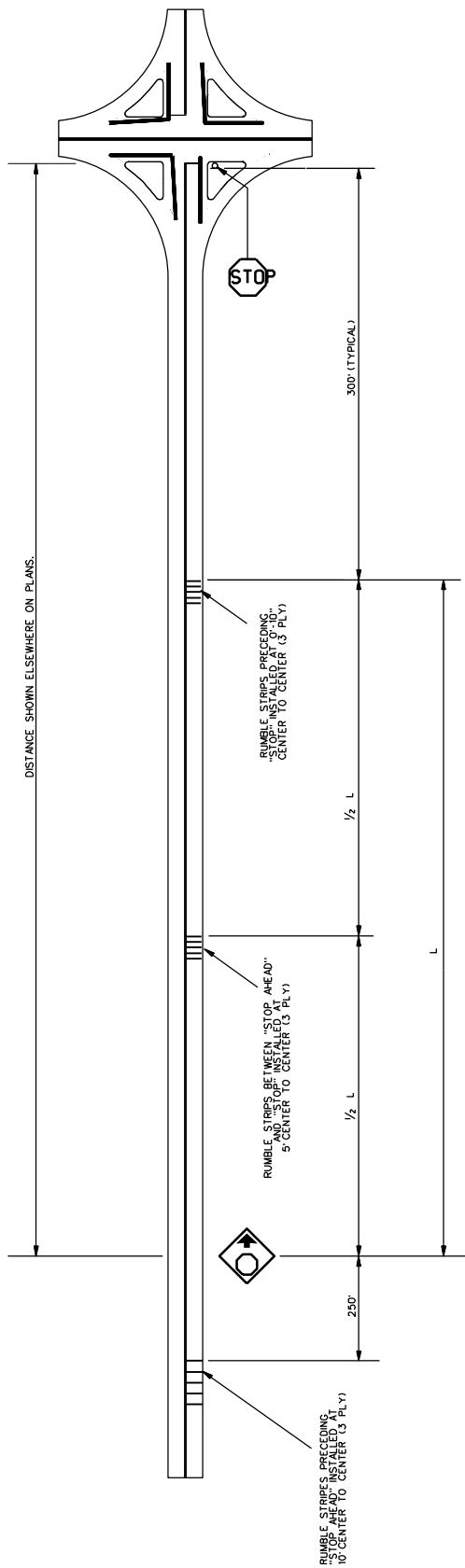
STOP SIGN
 RUMBLE

PROJ. NO.:
 COUNTY:

FILE NAME:
 DESIGN TEAM

DATE: 8-7-17
 CHECKED:

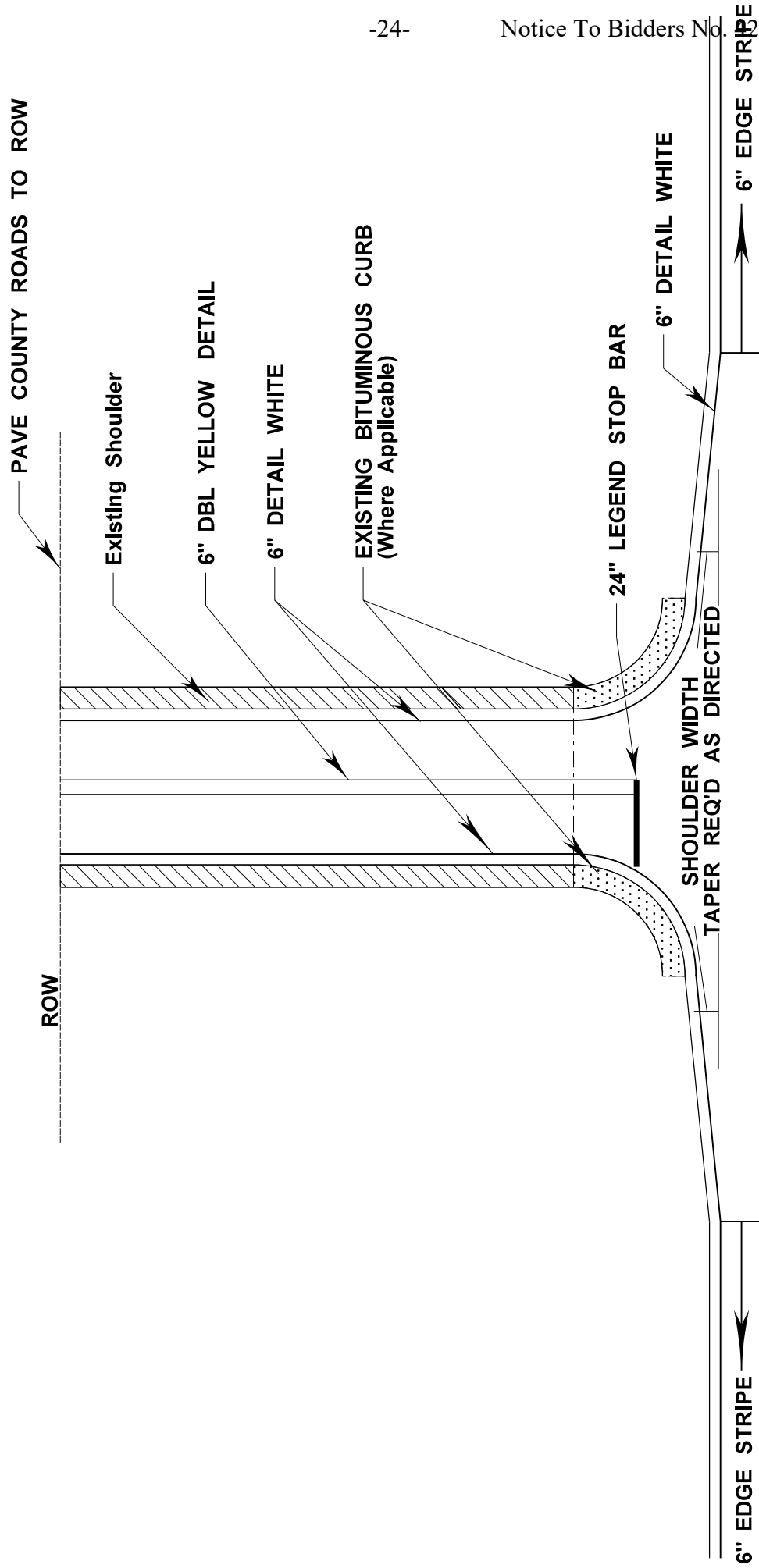
DATE	REVISION	BY



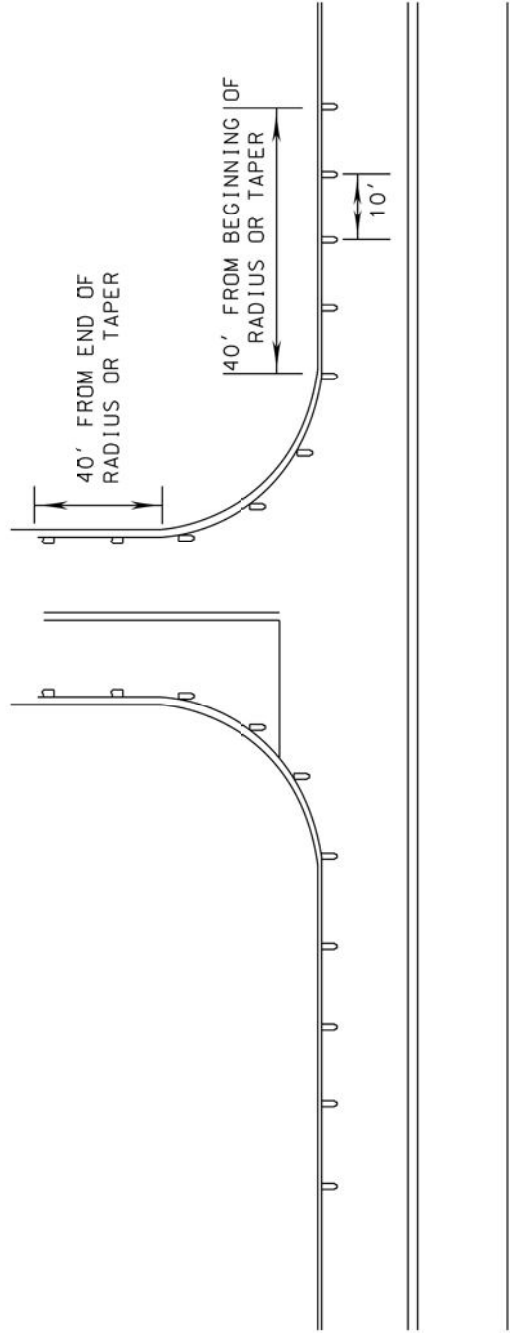
- NOTES:
 INSTALL RUMBLE STRIPS AS SHOWN:
1. ONE SET OF RUMBLE STRIPS APPROXIMATELY 250 FT. FROM "STOP AHEAD".
 2. ONE SET OF RUMBLE STRIPS APPROXIMATELY 300 FT. FROM "STOP".
 3. ONE SET OF RUMBLE STRIPS APPROXIMATELY HALF WAY BETWEEN FIRST AND LAST SET.
 4. RUMBLE STRIPS TO BE 6" THERMOPLASTIC (120 MIL/EACH, 360 MIL TOTAL).
 5. FIVE RUMBLE STRIPS PER SET MINIMUM.
 6. INSTALLATION MAY VARY DUE TO TERRAIN.
 7. SIGN DESIGNATIONS AND LOCATIONS ARE FOR REFERENCE ONLY UNLESS INCLUDED AS A PAY ITEM
 8. REMOVAL OF EXISTING STRIPS AND/OR PLACEMENT OF NEW RUMBLE STRIPS WILL BE ABSORBED IN OTHER ITEMS BID.

S.R. 18 - HINDS COUNTY
COUNTY ROAD PAVING/STRIPING
DETAIL

STRIPE DETAIL



TYPICAL FOR RAISED PAVEMENT MARKERS
PLACED ON SIDE ROAD RADIUS



NOTE 1. MARKERS SHALL BE PLACED EVERY 10 FEET.

NOTE 2. MARKERS SHALL BE VISIBLE FROM THE TRAVELING MOTORIST ON STATE DESIGNATED HIGHWAYS.

NOTE 3. MARKERS SHALL BE HIGH PERFORMANCE TWO WAY CLEAR.

NOTE 4. FIVE (5) MARKERS SHALL BE PLACED ALONG MAINLINE EDGE STRIPE.

NOTE 5. MARKERS FOR COUNTY ROADS SHALL CONTINUE DOWN THE EDGE STRIPE A DISTANCE OF 40 FEET.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 4225

CODE: (SP)

DATE: 5/6/2022

SUBJECT: Underground Utilities

PROJECT: SP-0039-02(054) / 108233301 – Hinds County

Bidders are hereby advised that utility lines owned and maintained by MDOT may be present within the project limits. These utilities are not located by Mississippi 811. It shall be the Contractor's responsibility to coordinate with MDOT to have the utility lines located and marked prior to beginning work. The Contractor shall give a minimum of three (3) working days of advance notice for locate requests.

Additionally, it shall be the Contractor's responsibility to maintain the utility markings or have the ability to survey the marked utilities and re-establish said utility markings as needed. The Department shall only be responsible for locating and marking the utilities once per Contract.

The contacts for MDOT utility lines are as follows:

Underground Power Lines:

Michael Lee – 601-683-3341 – mlee@mdot.ms.gov

Billy Coward – 601-683-3341 – bcoward@mdot.ms.gov

Underground Communication Lines:

Kerby McFarland – 601-359-7450 – kmcfarland@mdot.ms.gov

Steven Newell – 601-359-7450 – snewell@mdot.ms.gov

Henry Lewis – 601-359-1454 – hlewis@mdot.ms.gov

Underground Signal Lines:

Amrik Singh – 601-359-1454 – asingh@mdot.ms.gov

Kenneth Welch – 601-359-1454 – kwelch@mdot.ms.gov

Mill & Overlay approximately 18 miles of SR 18 from the Copiah County Line to the Beginning of the 4-Lane Section at Raymond, known as State Project No. SP-0039-02(054) / 108233301 in Hinds County.

Line No.	Item Code	Adj Code	Quantity	Units	Description [Fixed Unit Price]
Roadway Items					
0010	202-B129		2	Each	Removal of Flared End Section, All Sizes
0020	202-B188		2,759	Square Yard	Removal of Pavement, All Types and Depths
0030	202-B191		56	Linear Feet	Removal of Pipe, 8" And Above
0040	203-G001	(E)	1,904	Cubic Yard	Excess Excavation, FM, AH
0050	304-D002	(GT)	3,535	Ton	Granular Material, Crushed Stone
0060	304-F002	(GT)	208	Ton	Size 610 Crushed Stone Base
0070	403-A003	(BA1)	35,238	Ton	12.5-mm, ST, Asphalt Pavement
0080	403-B003	(BA1)	2,718	Ton	12.5-mm, ST, Asphalt Pavement, Leveling
0090	403-C003	(BA1)	250	Ton	19-mm, ST, Asphalt Pavement, Trench Widening
0100	406-D001		308,585	Square Yard	Fine Milling of Bituminous Pavement, All Depths
0110	407-A001	(A2)	31,350	Gallon	Asphalt for Tack Coat
0120	423-A001		35	Mile	Rumble Strips, Ground In
0130	503-C010		4,771	Linear Feet	Saw Cut, Full Depth
0140	601-B001	(S)	3	Cubic Yard	Class "B" Structural Concrete, Minor Structures
0150	603-CA055	(S)	56	Linear Feet	36" Reinforced Concrete Pipe, Class III
0160	603-CB006	(S)	2	Each	36" Reinforced Concrete End Section
0170	618-A001		1	Lump Sum	Maintenance of Traffic
0180	619-A1001		76	Mile	Temporary Traffic Stripe, Continuous White
0190	619-A2001		48	Mile	Temporary Traffic Stripe, Continuous Yellow
0200	619-A4002		22	Mile	Temporary Traffic Stripe, Skip Yellow
0210	619-A5001		72,720	Linear Feet	Temporary Traffic Stripe, Detail
0220	619-A6001		538	Square Feet	Temporary Traffic Stripe, Legend
0230	619-A6002		7,768	Linear Feet	Temporary Traffic Stripe, Legend
0240	619-D1001		1,196	Square Feet	Standard Roadside Construction Signs, Less than 10 Square Feet
0250	619-D2001		612	Square Feet	Standard Roadside Construction Signs, 10 Square Feet or More
0260	619-G4001		24	Linear Feet	Barricades, Type III, Double Faced
0270	620-A001		1	Lump Sum	Mobilization
0280	626-C002		38	Mile	6" Thermoplastic Double Drop Edge Stripe, Continuous White
0290	626-D001		11	Mile	6" Thermoplastic Double Drop Traffic Stripe, Skip Yellow
0300	626-E001		24	Mile	6" Thermoplastic Double Drop Traffic Stripe, Continuous Yellow
0310	626-G004		36,264	Linear Feet	Thermoplastic Double Drop Detail Stripe, White
0320	626-G005		96	Linear Feet	Thermoplastic Double Drop Detail Stripe, Yellow

Line No.	Item Code	Adj Code	Quantity	Units	Description [Fixed Unit Price]
0330	626-H001		269	Square Feet	Thermoplastic Double Drop Legend, White
0340	626-H002		3,884	Linear Feet	Thermoplastic Double Drop Legend, White
0350	627-J001		5,933	Each	Two-Way Clear Reflective High Performance Raised Markers
0360	627-K001		1,870	Each	Red-Clear Reflective High Performance Raised Markers
0370	627-L001		2,930	Each	Two-Way Yellow Reflective High Performance Raised Markers
0380	815-A007	(S)	44	Ton	Loose Riprap, Size 300
0390	907-619-B001		66	Linear Feet	Temporary Portable Rumble Strips

