

## SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO.	<u>1</u>	DATED	<u>7/25/2022</u>	ADDENDUM NO.	_____	DATED	_____
ADDENDUM NO.	_____	DATED	_____	ADDENDUM NO.	_____	DATED	_____
ADDENDUM NO.	_____	DATED	_____	ADDENDUM NO.	_____	DATED	_____

Number

Description

1 Revised Notice to Bidder No. 4373; Amendment EBSx  
Download Required.

TOTAL ADDENDA: 1

(Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE \_\_\_\_\_

Contractor

BY \_\_\_\_\_

Signature

TITLE \_\_\_\_\_

ADDRESS \_\_\_\_\_

CITY, STATE, ZIP \_\_\_\_\_

PHONE \_\_\_\_\_

FAX \_\_\_\_\_

E-MAIL \_\_\_\_\_

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of \_\_\_\_\_ and the names, titles and business addresses of the executives are as follows:

President

Address

Secretary

Address

Treasurer

Address

The following is my (our) itemized proposal.

SP-0848-00(022)/ 108945301000

Lafayette County(ies)

Revised 01/26/2016

# MISSISSIPPI DEPARTMENT OF TRANSPORTATION

**SECTION 904 - NOTICE TO BIDDERS NO. 4373**

**CODE: (SP)**

**DATE:** 7/25/2022

**SUBJECT:** Scope of Work

**PROJECT:** SP-0848-00(022) / 108945301 -- Lafayette County

The contract documents do not include an official set of construction plans but may, by reference; include some Standard Drawings when so specified in a Notice to Bidders entitled, "Standard Drawings".

The work to be accomplished using the pay items and corresponding specifications set forth in this contract is to mill and overlay University Avenue beginning at Pegues Road and going easterly for approximately 0.13 miles to Slack Road, and Highway 334 beginning at University Avenue and going easterly for approximately 6.3 miles to the west end of Bridge #6.7 over Pumpkin Creek in Lafayette County.

The interchange at Highway 6 and Highway 334 will be omitted from this project (Station 70+00 to Station 89+77).

Bidders are advised that cross-slopes for curve super elevations shall be constructed in accordance with information provided by the Department. To assist the Contractor in correctly placing the cross-slope transitions, the Department will provide the stationing and percent slope information at the preconstruction conference.

It shall be the responsibility of the Contractor to protect the roadway and all existing structures, such as bridges and curb, from damage occurring as a result of the Contractor's operations. Damages to existing features caused by the Contractor's operations shall be repaired or replaced at no cost to the Department.

At bridge ends and at the end of workday, a taper of one (1) vertical inch for each three (3) horizontal feet shall be provided.

The Contractor shall make a utility location request to 811 prior to any excavation, except for trench widening or pavement removal/repair.

In order to expedite the safe movement of traffic and to protect each phase of the work as it is performed, a firm sequence of operations is essential. The work shall be begun and continually prosecuted.

The work shall consist of the following:

1. Failed areas shall be repaired using the following:

- 202-B, Removal of Asphalt Pavement, All Depths – for pavement structure.
- 202-B, Removal of Concrete Pavement w/Variable Depth Overlay
- 203-G, Excess Excavation – for material below the pavement structure
- 304-F, Crushed Stone – to be used to replace unsuitable material below the 1-foot limit
- 403-A, 19mm, ST, Asphalt Pavement
- 503-C, Saw Cut, Full Depth

NOTE: Failed areas are estimated as one foot (1') of excavation and backfilled with one foot (1') (maximum 3½" lifts) of 19-mm, ST, asphalt. The asphalt shall be placed per the Project Engineer's instructions.

NOTE: Failed areas are to be backfilled the same day as excavation.

Station	Side		Width (feet)		Length (feet)	Asphalt Area (SY)	Concrete Area (SY)
	Rt.	Lt.	Asphalt	Concrete			
10+25		x	0	24	10	0.00	26.67
67+00		x	6	0	50	33.33	0.00
68+00	x		3	0	100	33.33	0.00
68+00		x	2	10	10	2.22	11.11
72+50		x	2	0	75	16.67	0.00
72+75		x	2	10	10	2.22	11.11
90+00		x	6	0	100	66.67	0.00
94+75		x	2	10	10	2.22	11.11
97+50	x		10	0	10	11.11	0.00
100+00		x	2	10	10	2.22	11.11
106+00		x	3	0	400	133.33	0.00
106+25	x		6	0	50	33.33	0.00
108+25		x	2	10	10	2.22	11.11
112+50		x	3	0	50	16.67	0.00
115+00	x		6	0	100	66.67	0.00
117+00		x	2	10	10	2.22	11.11
120+50		x	2	10	10	2.22	11.11
123+00		x	3	0	100	33.33	0.00
126+25	x		6	0	50	33.33	0.00
129+50		x	2	10	10	2.22	11.11
132+75		x	2	10	10	2.22	11.11
136+75		x	2	10	10	2.22	11.11
138+00		x	2	0	50	11.11	0.00
146+25		x	2	10	10	2.22	11.11
149+50		x	2	10	10	2.22	11.11
161+00		x	2	10	10	2.22	11.11
162+75		x	2	10	10	2.22	11.11

Station	Side		Width (feet)		Length (feet)	Asphalt Area (SY)	Concrete Area (SY)
	Rt.	Lt.	Asphalt	Concrete			
164+00		x	2	10	10	2.22	11.11
169+75		x	2	10	10	2.22	11.11
172+75		x	2	10	10	2.22	11.11
177+50		x	2	10	10	2.22	11.11
180+50		x	2	10	10	2.22	11.11
181+50		x	2	0	50	11.11	0.00
182+50	x		12	0	200	266.67	0.00
182+75		x	2	10	10	2.22	11.11
204+00	x		6	0	150	100.00	0.00
206+50		x	2	10	10	2.22	11.11
207+50	x		6	0	50	33.33	0.00
210+25		x	2	10	10	2.22	11.11
251+50		x	2	10	10	2.22	11.11
256+75		x	2	10	10	2.22	11.11
262+25	x		6	0	25	16.67	0.00
262+75		x	2	10	10	2.22	11.11
265+00		x	2	10	10	2.22	11.11
266+00		x	2	10	10	2.22	11.11
269+00	x		12	0	75	100.00	0.00
269+75		x	2	10	10	2.22	11.11
272+00	x		12	0	75	100.00	0.00
281+00		x	2	10	10	2.22	11.11
281+75	x		6	0	50	33.33	0.00
282+50		x	2	10	10	2.22	11.11
286+50		x	2	10	10	2.22	11.11
292+50		x	2	10	10	2.22	11.11
297+00		x	3	0	110	36.67	0.00
297+50	x		6	0	50	33.33	0.00
299+00	x		6	0	75	50.00	0.00
301+00	x		12	0	75	100.00	0.00
302+50	x		12	0	100	133.33	0.00
304+00	x		12	0	75	100.00	0.00
308+00		x	2	10	10	2.22	11.11
308+75	x		6	0	10	6.67	0.00
309+00	x		6	0	10	6.67	0.00
312+00		x	2	10	10	2.22	11.11
315+00	x		10	0	10	11.11	0.00
315+50		x	2	10	10	2.22	11.11
319+50	x		6	0	50	33.33	0.00
319+75		x	2	10	10	2.22	11.11
320+50	x		6	0	50	33.33	0.00
321+50		x	2	10	10	2.22	11.11
322+00	x		6	0	100	66.67	0.00
324+00	x		2	0	25	5.56	0.00

Station	Side		Width (feet)		Length (feet)	Asphalt Area (SY)	Concrete Area (SY)
	Rt.	Lt.	Asphalt	Concrete			
330+25		x	2	10	10	2.22	11.11
332+50	x		6	0	25	16.67	0.00
334+50		x	3	0	50	16.67	0.00
336+50		x	3	0	50	16.67	0.00
337+50		x	2	10	10	2.22	11.11
344+00		x	2	10	10	2.22	11.11
348+50	x		6	0	10	6.67	0.00
352+50		x	2	10	10	2.22	11.11
352+50	x		6	0	50	33.33	0.00
357+00		x	2	0	25	5.56	0.00
362+00		x	2	10	10	2.22	11.11
364+00	x		6	0	100	66.67	0.00
367+25		x	2	10	10	2.22	11.11
368+00	x		6	0	10	6.67	0.00
369+00	x		6	0	30	20.00	0.00
370+00	x		6	0	50	33.33	0.00
372+50	x		6	0	50	33.33	0.00
374+25		x	2	10	10	2.22	11.11
378+50		x	2	10	10	2.22	11.11
380+50		x	2	0	20	4.44	0.00
381+00		x	2	0	20	4.44	0.00
387+10		x	2	10	10	2.22	11.11
387+75	x		6	0	350	233.33	0.00
390+75		x	2	10	10	2.22	11.11
394+00		x	2	10	10	2.22	11.11
396+00		x	2	10	10	2.22	11.11
Total						2373.33	571.11

The above areas were noted during the field inspection. Additional areas may require repair during construction.

- The existing asphalt pavement shall be fine milled to a depth of two inches (2"). Milling operations shall be on the mainline, local roads and driveway pads. The Mississippi Department of Transportation will retain fifty (50%) percent or up to 5,000 tons of the milling material. The Contractor shall deliver the milling material to the MDOT Lafayette County Maintenance Shop located at 495 Highway 7 South in Oxford. The Contractor shall provide all necessary equipment and qualified personnel to push material into a suitable stockpile.

Area	SY
Mainline	82,150
Local Roads	5,500
Pads	5,000
Total	92,650

Payment for fine milling of pavement will be made under pay item 406-D, and shall include all cost associated with the milling operation.

NOTE: Milled surfaces shall be covered with surface asphalt within seven (7) calendar days of removal. The Contractor will be charged a fee of \$5,000.00 for each full or partial day in which the milled surface is left uncovered after the seven (7) calendar days.

NOTE: During this operation and prior to placement of the asphalt, due care shall be required to keep surface water from ponding on the roadway surface; continuous monitoring of the project may be required.

NOTE: During this operation and prior to placement of the asphalt, Contractor shall repair and maintain all potholes.

3. 1” and variable of 9.5-mm, ST, Leveling asphalt shall be placed for the leveling of the mainline to correct cross slopes. This work does not include leveling across the previous leveled horizontal curves.

9.5-mm, ST, Leveling Asphalt		
Length (ft)	Width (ft)	Tons
10,650	24	1,575

4. 9.5-mm, ST, Leveling asphalt shall be placed for the leveling of existing horizontal curves to correct cross slope on the previous milled surface. A maximum lift of three inches (3”) shall be maintained for curve leveling. Granular material shall be placed on the shoulder prior to leveling of curves to maintain the legal drop-off requirements.

9.5-mm, ST, Leveling Asphalt for Curve Correction		
# Curves	Length (ft)	Tons
23	20,200	6,000

5. 2” of 12.5-mm, ST asphalt shall be placed as a surface course on the previous leveled surface.

12.5-mm, ST Asphalt	
Area	Total
Mainline (SR 334)	9,075
University Avenue	175
Local Roads	575
Pads	575
Total	10,400

NOTE: Contractor shall saw and seal the transverse joint on the mainline surface course.

6. Granular material shall be placed on the shoulders as directed to raise the existing shoulders to the new surface course grade.

Granular Material, Class 3, Group D			
Centerline Length (feet)	Width Left & Right (feet)	Depth (inches)	Tons
33,430	3	3	3,150

NOTE: Shoulders shall be bladed, shaped, and compacted throughout the length of the project regardless of whether granular material is required.

NOTE: Granular material not required for the final shape of the shoulders may require removal under the pay item for excess excavation and may include small amounts of asphalt.

NOTE: Due care shall be taken during this operation to blade material to the roadway and away from the ditch line. Material inadvertently bladed to the roadway vegetation shall be removed at no cost to the Department.

7. Temporary traffic stripe shall be placed daily as per Section 618 of the Standard Specifications.
8. Guardrails shall be removed and replaced at the following locations:

BR #	Station	Guardrail Removal	Guardrail Installation	Terminal Section	Bridge Section Type H	Delineators, Guard Rail White
2.8	194+93	426	245	4	4	26
3.4	228+53	488	245	4	4	26
3.5	233+69	488	245	4	4	26
3.6	239+49	439	245	4	4	26
6.7	399+25	490	245	4	4	26
		2,331	1,225	20	20	130

9. Permanent pavement markings (thermoplastic striping, two-way clear reflective high performance raised markers, red-clear reflective high performance raised markers and two-way yellow reflective high performance raised markers) shall be placed as required.

Existing traffic stripe on bridges shall be removed and replaced. The length of bridges is 648 feet.

10. All existing post mounted standard roadside signs estimated in the attached table shall be replaced. The Contractor shall deliver the removed signs to the MDOT Lafayette County Maintenance Shop located at 495 Highway 7 South in Oxford. All signs and hardware shall be removed from post prior to delivery. The Contractor is required to verify the sign quantity prior to ordering materials. All hardware and footings required for the erection of new signs and post shall be absorbed in other items of work.

Sign Quantity			
Pay Item No.	Description	Unit	Quantity
202-B	Removal of Sign, Including Post and Footing	EA	128
630-A	Standard Roadside Signs, Sheet Aluminum, .08" Thickness	SF	148
630-A	Standard Roadside Signs, Sheet Aluminum, .125" Thickness	SF	537
630-A	Standard Roadside Signs, Sheet Aluminum, .1" Thickness	SF	272
630-C	Square Tube Post, 2.0 lb/ft	LF	1,884
630-C	Square Tube Post, 4.0 lb/ft	LF	75
630-G	Type 3 Object Marker, OM-3R or OM-3L	EA	50

The Contractor shall provide all signs and traffic handling devices necessary to safely maintain traffic around or through the work areas.

Incidental work such as removing vegetation, shaping and compaction of shoulder, necessary and incidental grading of roadway ditches and other incidental work that is necessary to complete the work will not be measured for separate payment and the cost will be included in the bid items provided.

The Engineer may direct the use of additional cones at County roads or intersections within lane closures and will be absorbed in pay item 618-A: Maintenance of Traffic.



Station #	Lane	Sign Description	Sign Code	Removal U-Channel	Notes	Type 3 OM	0.08"	0.1"	0.125"	Square Tube Post 2 lb/ft	Square Tube Post 4 lb/ft
65+00	R	Do Not Enter	R5-1	1				9			15
65+00	R	Yield	R1-2		same post as do not enter		5.1				
65+00	L	Do Not Enter	R5-1					9			
65+00	L	Yield	R1-2	1	same post as do not enter		5.1				15
67+00	R	Chevron	W1-8L	1					5	15	
67+00	R	Chevron	W1-8R		same post as chevron				5		
67+25	R	Chevron	W1-8L	1					5	15	
67+25	R	Chevron	W1-8R		same post as chevron				5		
68+50	R	No Parking Any Time	R7-1	1			1.5			15	
68+50	R	No Parking Any Time	R7-1		same post as no parking any time		1.5				
68+50	L	No Parking Any Time	R7-1	1			1.5			15	
68+50	L	No Parking Any Time	R7-1		same post as no parking any time		1.5				
69+00	L	No Parking Any Time	R7-1	1			1.5			15	
69+00	L	No Parking Any Time	R7-1		same post as no parking any time		1.5				
70+75	L	Signal Ahead Symbol	W3-3	1					9	15	
71+10	R	Side Road Right	W2-2R	1					9	15	
73+50	L	Curves Ahead	W1-4L	1					9	15	
73+50	L	Cautionary 20 mph	W13-1		same post as curves ahead		4				
75+00	R	Bridge May Ice	W8-13	1					9	15	
75+10	R	JCT.	M2-1	1			2.15			15	
75+10	R	SR6	M1-6		same post as Jct.		4				
75+10	L	Speed Limit 45	R2-1	1			5			15	
75+50	L	36" Stop	R1-1	1				7.46		15	
75+50	R	36" Stop	R1-1	1				7.46		15	
75+50	L	Stop Ahead Symbol	W3-1a	1					9	15	
75+50	R	Stop Ahead Symbol	W3-1a	1					9	15	
90+25	L	Type 3 OM's	OM-3L	1		1				15	
90+25	L	Type 3 OM's	OM-3R		same post as OM-3L	1					
90+50	L	36" Stop	R1-1	1	Sheila Rd.			7.46		15	
90+50	L	Stop Ahead Symbol	W3-1a	1					9	15	
91+25	L	Speed Limit 45	R2-1	1			5			15	
91+75	R	Side Road Right	W2-2R	1					9	15	
93+00	R	School Crossing	S1-1	1					6.25	15	
93+50	R	36" Stop	R1-1	1				7.46		15	
95+50	R	School Crossing	S1-1	1					6.25	15	
95+75	L	Side Road Right	W2-2R	1					9	15	
96+00	L	36" Stop	R1-1	1				7.46		15	
96+00	L	Stop Ahead Symbol	W3-1a	1					9	15	
96+75	R	Right Lane Must Turn Right	R3-7	1			6.25			15	
98+00	L	No Parking Any Time	R7-1	1			1.5			15	
98+00	L	No Parking Any Time	R7-1		on same post as no parking any time		1.5				
99+50	R	Right Lane Must Turn Right	R3-7	1			6.25			15	
99+75	L	Side Road Right	W2-2R	1					9	15	
101+00	L	Do Not Enter	R5-1	1				9		15	
101+00	L	Yield	R1-2		on same post as do not enter		5.1				
101+00	R	Do Not Enter	R5-1	1				9			15
101+00	R	Yield	R1-2		on same post as do not enter		5.1				
102+50	L	Do Not Enter	R5-1	1				9			15
102+50	L	Yield	R1-2		on same post as do not enter		5.1				
114+00	R	Left Curve Ahead	W1-2L	1					9		15
114+00	R	45 mph cautionary	W13-1		same post as left curve ahead		4				
119+25	R	Speed Limit 45	R2-1	1			5			15	
121+50	L	Speed Limit 45	R2-1	1			5			15	
127+00	L	Right Curve Ahead	W1-2R	1					9	15	
127+00	L	45 mph cautionary	W13-1		same post as right curve ahead		4				
129+25	R	Speed Limit 55	R2-1	1			5			15	
129+50	L	Speed Limit 45	R2-1	1			5			15	
130+50	R	Side Road Right	W2-2R	1					9	15	
136+00	R	36" Stop	R1-1	1				7.46		15	
136+00	R	Stop Ahead Symbol	W3-1a	1					9	15	
136+00	L	Left Right Arrow	W1-7		missing			8		15	

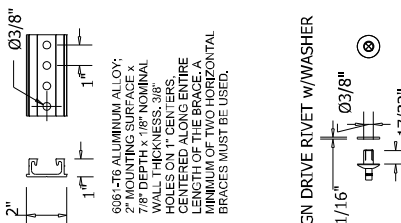
Station #	Lane	Sign Description	Sign Code	Removal U-Channel	Notes	Type 3 OM	0.08"	0.1"	0.125"	Square Tube Post 2 lb/ft	Square Tube Post 4 lb/ft
136+50	L	Speed Zone Ahead	W3-5	1					9	15	
138+00	R	Curves Ahead	W1-5	1					9	15	
138+00	R	40 mph cautionary	W13-1		on same post as curves ahead		4				
138+75	R	Side Road Right	W2-2R	1					9	15	
142+00	L	Side Road Left	W2-2L	1					9	15	
147+00	R	36" Stop	R1-1	1				7.46		15	
184+00	L	Curves Ahead	W1-5		missing				9	15	
184+00	L	40 mph cautionary	W13-1		on same post as curves ahead		4				
185+00	R	Side Road Right	W2-2R		missing				9	15	
186+00	Both	Type 3 OM's	OM-3L & R	4		4				36	
187+75	R	Bridge May Ice	W8-13	1					9	15	
189+00	R	36" Stop	R1-1	1				7.46		15	
189+00	R	Stop Ahead Symbol	W3-1a	1					9	15	
189+00	L	Left Right Arrow	W1-7		missing			8		15	
192+00	L	Side Road Left	W2-2L		missing				9	15	
194+75	Both	Type 3 OM's	OM-3L & R	2	mounted on same post	4				18	
196+50	Both	Type 3 OM's	OM-3L & R	2	mounted on same post	4				18	
204+25	L	Bridge May Ice	W8-13	1					9	15	
205+00	R	Side Road Left	W2-2L		missing				9	15	
207+00	R	Curves Ahead	W1-4R	1					9	15	
207+00	R	35 mph cautionary	W13-1		on same post as curves ahead		4			15	
212+50	L	36" Stop	R1-1	1				7.46		15	
212+50	L	Stop Ahead Symbol	W3-1a	1					9	15	
212+50	R	Left Right Arrow	W1-7		missing			8		15	
219+25	L	Side Road Right	W2-2R	1					9	15	
224+00	R	Bridge May Ice	W8-13	1					9	15	
226+00	L	Curves Ahead	W1-4L		missing				9	15	
226+00	L	35 mph cautionary	W13-1		on same post as curves ahead		4				
228+50	Both	Type 3 OM's	OM-3L & R	2		2				18	
230+25	Both	Type 3 OM's	OM-3L & R	2		2				18	
233+50	Both	Type 3 OM's	OM-3L & R	2		2				18	
234+25	Both	Type 3 OM's	OM-3L & R	2		2				18	
236+00	R	Side Road Left	W2-2L	1					9	15	
239+50	Both	Type 3 OM's	OM-3L & R	2		2				18	
240+10	Both	Type 3 OM's	OM-3L & R	2		2				18	
241+50	L	36" Stop	R1-1	1				7.46		15	
241+50	L	Stop Ahead Symbol	W3-1a	1					9	15	
241+50	R	Left Right Arrow	W1-7		missing			8		15	
243+00	R	Curves Ahead	W1-5		missing				9	15	
243+00	R	35 mph cautionary	W13-1		on same post as curves ahead			4			
272+50	R	36" Stop	R1-1	1				7.46		15	
272+50	R	Stop Ahead Symbol	W3-1a	1					9	15	
272+50	L	Left Right Arrow	W1-7	1				8		15	
278+90	L	Curves Ahead	W1-5	1					9	15	
278+90	L	35 mph cautionary	W13-1		on same post as curves ahead		4				
279+75	R	Curves Ahead	W1-5	1					9	15	
279+75	R	35 mph cautionary	W13-1		on same post as curves ahead		4				
280+00	L	Side Road Left	W2-2L	1					9	15	
306+50	L	Curves Ahead	W1-5	1					9	15	
306+50	L	35 mph cautionary	W13-1		on same post as curves ahead		4				
307+00	R	Curves Ahead	W1-5		missing				9	15	
307+00	R	40 mph cautionary	W13-1		on same post as curves ahead		4				
308+50	Both	Type 3 OM's	OM-3L & R	4		4				36	

Station #	Lane	Sign Description	Sign Code	Removal U-Channel	Notes	Type 3 OM	0.08"	0.1"	0.125"	Square Tube Post 2 lb/ft	Square Tube Post 4 lb/ft
327+50	R	Side Road Left	W2-2L	1					9	15	
333+00	Both	Type 3 OM's	OM-3L & R	2	mounted on same post	4				18	
333+25	L	36" Stop	R1-1	1				7.46		15	
333+25	L	Stop Ahead Symbol	W3-1a	1					9	15	
333+25	R	Left Right Arrow	W1-7		missing			8		15	
337+75	Both	Type 3 OM's	OM-3L & R	2	mounted on same post	4				18	
345+75	R	Side Road Right	W2-2R	1					9	15	
350+00	L	Curves Ahead	W1-5		missing				9	15	
350+00	L	40 mph cautionary	W13-1		on same post as curves ahead		4				
350+50	R	Left Curve Ahead	W1-2L	1					9	15	
350+50	R	35 mph cautionary	W13-1		on same post as left curve ahead		4				
353+25	L	Left Right Arrow	W1-7		reuse existing post			8			
353+25	R	36" Stop	R1-1	1				7.46		15	
353+25	R	Stop Ahead Symbol	W3-1a	1					9	15	
353+25	L	Left Right Arrow	W1-7		missing			8		15	
354+50	R	Double Side Road Right	W2-8R	1					9	15	
357+00	R	36" Stop	R1-1	1				7.46		15	
357+00	R	Stop Ahead Symbol	W3-1a	1					9	15	
357+00	L	Left Right Arrow	W1-7		missing			8		15	
358+00	L	Side Road Left	W2-2L	1					9	15	
359+50	R	36" Stop	R1-1	1				7.46		15	
359+50	L	Left Right Arrow	W1-7		missing			8		15	
360+25	R	36" Stop	R1-1	1				7.46		15	
360+25	R	Stop Ahead Symbol	W3-1a	1					9	15	
360+25	L	Left Right Arrow	W1-7		missing			8		15	
360+25	L	Side Road Left	W2-2L	1	remove only						
360+50	Both	Type 3 OM's	OM-3L & R	2	mounted on same post	4				18	
365+10	L	Double Side Road Left	W2-8L	1					9	15	
368+10	Both	Type 3 OM's	OM-3L & R	2	mounted on same post	4				18	
371+50	L	Right Curve Ahead	W1-2R	1					9	15	
371+50	L	35 mph cautionary	W13-1		on same post as right curve ahead		4				
379+00	R	Side Road Left	W2-2L	1					9	15	
387+00	L	36" Stop	R1-1	1				7.46		15	
387+00	L	Stop Ahead Symbol	W3-1a	1					9	15	
387+00	R	Left Right Arrow	W1-7	1				8		15	
393+75	R	Bridge May Ice	W8-13	1					9	15	
394+00	L	Side Road Right	W2-2R	1					9	15	
399+25	Both	Type 3 OM's	OM-3L & R	2		2				18	
401+00	Both	Type 3 OM's	OM-3L & R	2		2				18	
EOP	L	Bridge May Ice	W8-13	1					9	15	
Total				128		50	142.15	271.82	545.5	1884	75

## **Permanent Signing Plan General Notes**

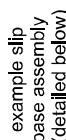
1. All permanent signs shall conform to the latest edition of the Manual on Uniform Traffic Control Devices.
2. All sign locations shall be approved by the Project Engineer prior to installation.
3. All post, pipe, and I-beam lengths in these plans are estimates. Post lengths for all signs shall be verified in the field by the Contractor prior to fabrication.
4. The retroreflective sign sheeting on permanent ground-mounted signs shall be as follows: brown background sheeting on guide signs shall be minimum Type VIII; green and blue background sheeting on guide signs shall be minimum Type IX; all white, yellow, fluorescent yellow and fluorescent yellow/green sheeting shall be Type XI. All sign sheeting on overhead signs shall be Type XI.
5. The retroreflective sign sheeting on rigid, temporary traffic control (orange) signs shall be minimum Type IX.
6. Direct-applied legend, border, and/or shields are to be used on all guide signs. Digitally produced signs including copy, shields, legend, symbols, or images will not be allowed without written approval from MDOT's Project Engineer.
7. Installation dates shall be clearly written in bold black markings on the back bottom half of all signs with a permanent marking stick that is waterproof, fade resistant and marks on wet or dry surfaces.
8. All existing signs which are to be removed as a part of this project that are not in conflict with construction shall remain in place until new signs are installed unless noted or directed otherwise by the Project Engineer. Roadway signs that are in conflict with construction shall be removed and relocated or covered by the contractor as directed by the project engineer the cost of which shall be absorbed in other bid items.
9. All side road, stop sign mounted street name signs to be salvaged and stored at the direction of the project engineer for delivery to the City (not a separate pay item).

FLAT PANEL SIGN BRACE



**MATERIALS: 1.4" WIDE x 11ga.  
TYPE 304, #2B FINISHED  
STAINLESS STEEL BRACKET.  
INCLUDES STAINLESS STEEL  
3/8" x 16 x 2" CARRIAGE BOLT  
AND SERRATED FLANGED  
NUT, ONE CLAMP  
REQUIRED FOR EACH  
BRACE TO POST JUNCTION.**

MADE OF 16GA PREGALVANIZED STEEL WITH AN ID CAPABLE OF FITTING INTO THE INSIDE OF A 2-1/2" SQUARE STEEL POST WITHOUT ANY FASTENERS. A CAST ALUMINUM FRICTION FIT CAP MAY ALSO BE USED.

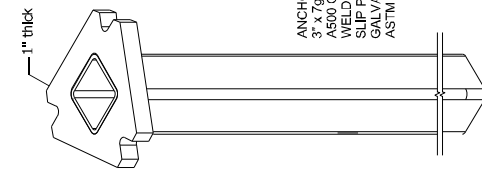


ANCHOR STUB

A. CONCRETE FOOTINGS FOR EXTRUDED PANEL SIGNS SHALL BE 18" DIAMETER X 48" DEEP. ALL OTHER CONCRETE FOOTINGS SHALL BE MINIMUM 12" DIAMETER X 36" DEEP UNLESS OTHERWISE SPECIFIED IN THE PLANS. IF BAGGED CONCRETE IS USED, THE COST SHALL BE ABSORBED.

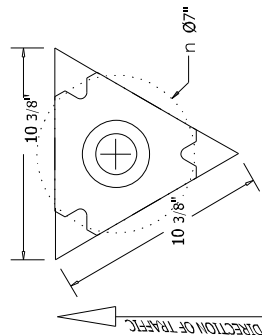
B. BRASS SHIMS MAY BE USED BETWEEN SLIP PLATES TO LEVEL THE UPPER SLIP PLANE.

C. SIGN BRACE CLAMPS ALLOW SIGN BRACE TO BE ADJUSTED UP, DOWN, LEFT OR RIGHT IN ORDER TO ACHIEVE PERFECT POSITION OF SIGN PANEL.



## SURFACE MOUNT BASE

RECEIVER TO BE CAST  
FROM DUCTILE IRON  
ASTM A536 CLASS 65-45-12.  
GALVANIZED PER ASTM A153.







A572 SLIP PLATE  
WELDED TO A500  
GRADE 4350  
WELDED TO A572  
MOUNTING PLATE  
GALVANIZED  
PER ASTM A153

10"

4"

SLIP

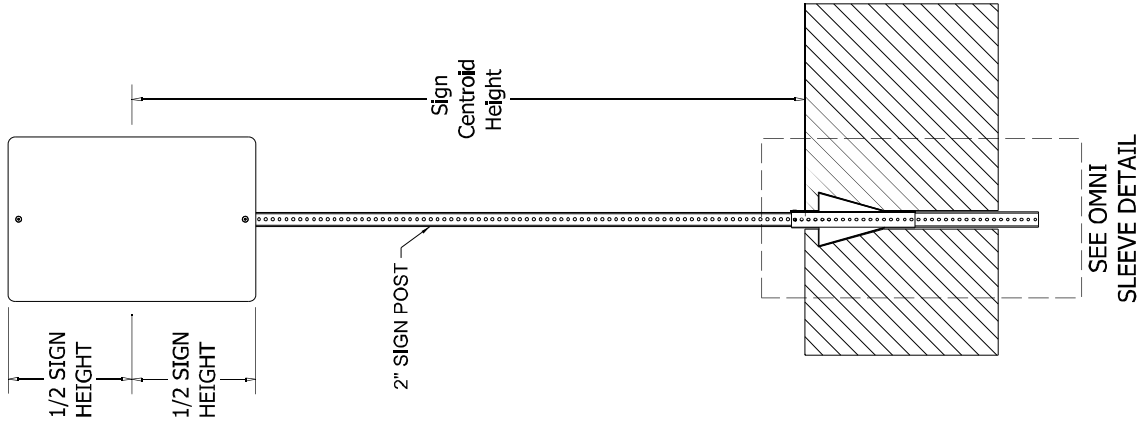
ANCHOR  
3" x 7ga x 36" (OR 3" x 7ga x 48")  
A500 GRADE B TUBE  
WELDED TO A572  
SLIP PLATE.  
GALVANIZED PER  
ASTM A153.

 <p>1/2"-13 x 3" GRADE 8 DOUBLE HEX BOLT, (EACH PER KIT) GALVANIZE PER ASTM B695</p>	 <p>1/2" U.S. SAE FLAT WASHER ANSI B 18.22, (EACH PER KIT) GALVANIZE PER ASTM B695</p>	 <p>1/2" U.S. SAE 3/16" THICK FLAT COATED HARDENED SLIP WASHER, (EACH PER KIT)</p>	 <p>1/2"-13 GRADE 8 LARGE FLANGE NUT (EACH PER KIT) GALVANIZE PER ASTM B695</p>
<h2>Notice to Bid</h2>			
<h3>SLIP BASE MATCH PLATE HARDWARE KIT</h3>			

Notice to Bidders No. RTA-4373 -- Co

18	2025/04/01	MISSISSIPPI DEPARTMENT OF TRANSPORTATION SIGN SUPPORT HARDWARE	2-5" SQUARE POST	PROJ. NO.: ----- COUNTY: -----	WORKING NUMBER: 155-1 SHEET NUMBER: 155-1	DATE: 9/20/2012 PERSON: TFD OFFICE: TFD
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SOIL INSTALLATION

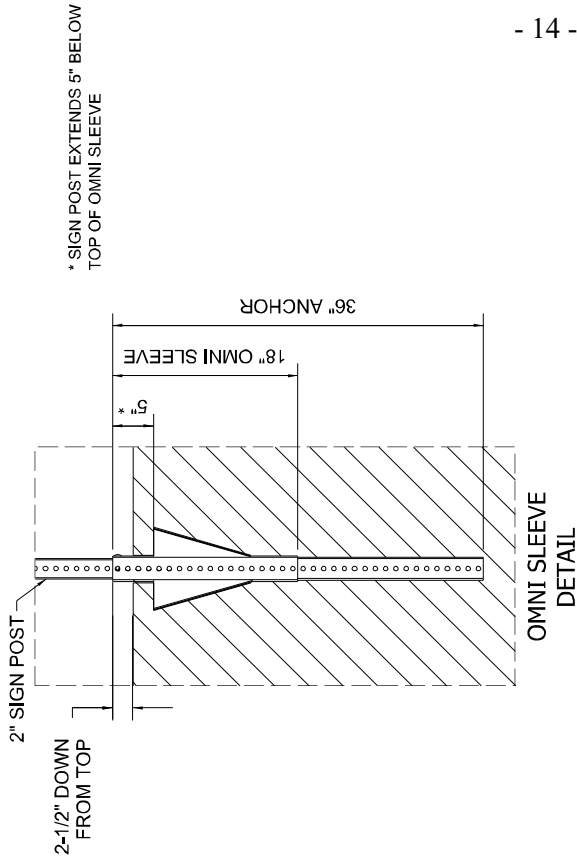


**MATERIALS:**  
PERFORATED SQUARE STEEL TUBE POST AND ANCHOR (PSST): Must be ASTM A1011, Grade 50 steel with an average minimum yield strength after cold-forming must be 60,000 psi. It must be corner welded, scarfed after welding, then zinc coated after scarfing. It must be coated with a chromate conversion coating and a clear organic polymer topcoat. Its interior and exterior will be galvanized. It must have 7/16" holes spaced 1" apart along the centerline of each of its four sides.

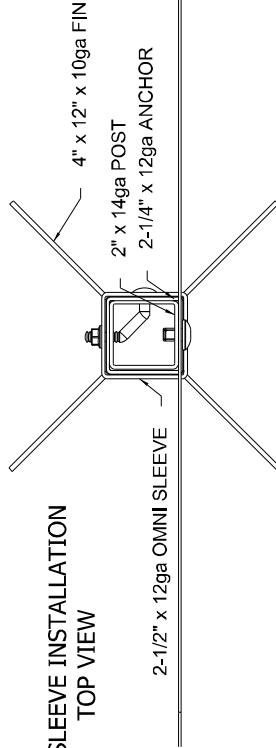
POST must be 2" x 14ga x appropriate length.

ANCHOR must be 2-1/4" x 12ga x 36".

OMNI SLEEVE (to be absorbed) must be:  
2-1/2" x 12ga x 18" and contain four 4" x 12" x 10ga ASTM A569 steel flns welded to each of the four corner edges of the square sleeve with 4" flat end of each fln positioned 5" down from one end, and must be galvanized. A medium corner bolt must be used to secure the assembly.



OMNI SLEEVE INSTALLATION  
TOP VIEW



FMS CON:	PROJECT NO.
STATE	MISS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
SIGN SUPPORT HARDWARE	
BY	DATE
REVISION	DATE
2.0" SQUARE POST	
PROJ. NO.:	---
COUNTY:	---
MOD. NUMBER	PS-2
SHEET NUMBER	---
FILE NAME: ISS-2.DGN	CHECKED: DATE: 9/2/2021

[illegible]

STATE: MISS. PROJECT NO.: MISS.

SHEET NUMBER: 51-2  
 SHEET TOTAL: 61-07

**GENERAL NOTES:**

1. ALL MISSISSIPPI SHIELDS DO NOT HAVE AN OUTSIDE BORDER.
2. INTERSTATE, U.S., AND MISSISSIPPI SHIELDS ARE TO BE USED ONLY ON GUIDE SIGNS. SEE OTHER DRAWINGS FOR SHIELDS TO BE USED INDEFINITELY AND TRAIL MARKERS.
3. ON INTERSTATE SHIELDS, THE BACKGROUND SHALL BE REFLECTIVE.
4. ON U.S. AND MISSISSIPPI SHIELDS, THE BACKGROUND SHALL BE REFLECTIVE.
5. IN SOME CASES, NUMERALS CANNOT BE ACCOMMODATED WITHIN THE SHIELD. IN THESE CASES, THE NUMERALS MAY BE REDUCED TO SERIES "A" NUMERALS OR AS SMALLER NUMERALS AS NECESSARY TO FIT THE SHIELD. AVAILABLE.

M1-1

M1-1

M1-1

DIMENSIONS (INCHES)	
NUMERAL	SHIELD
1	100
2	100
3	100
4	100
5	100
6	100
7	100
8	100
9	100
10	100
11	100
12	100
13	100
14	100
15	100
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100	100

DIMENSIONS (INCHES)	
NUMERAL	SHIELD
1	100
2	100
3	100
4	100
5	100
6	100
7	100
8	100
9	100
10	100
11	100
12	100
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89	100
90	100
91	100
92	100
93	100
94	100
95	100
96	100
97	100
98	100
99	100
100	100

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**  
**ROADWAY DESIGN DIVISION**  
**STANDARD PLAN**

**ROUTE SHIELDS AND "EXIT ONLY" PANELS**

ISSUE DATE: MAY 8, 1971

**GENERAL NOTES:**

1. THESE "EXIT ONLY" SIGNS FABRICATED ON 0.063" THICK ALUMINUM OR 0.063" THICK STEEL. THESE SIGNS WILL NOT BE PAINT "ON" AS SEPARATE SIGNS BUT SHALL BE CONSIDERED AS PART OF THE MAJOR SIGNS TO WHICH THEY ARE AFFIXED.
2. LETTER SIZE: 2" SERIES "D".
- COLOR: LEGEND = BLACK; BACKGROUND = HIGH-INTENSITY YELLOW.

E11-1a

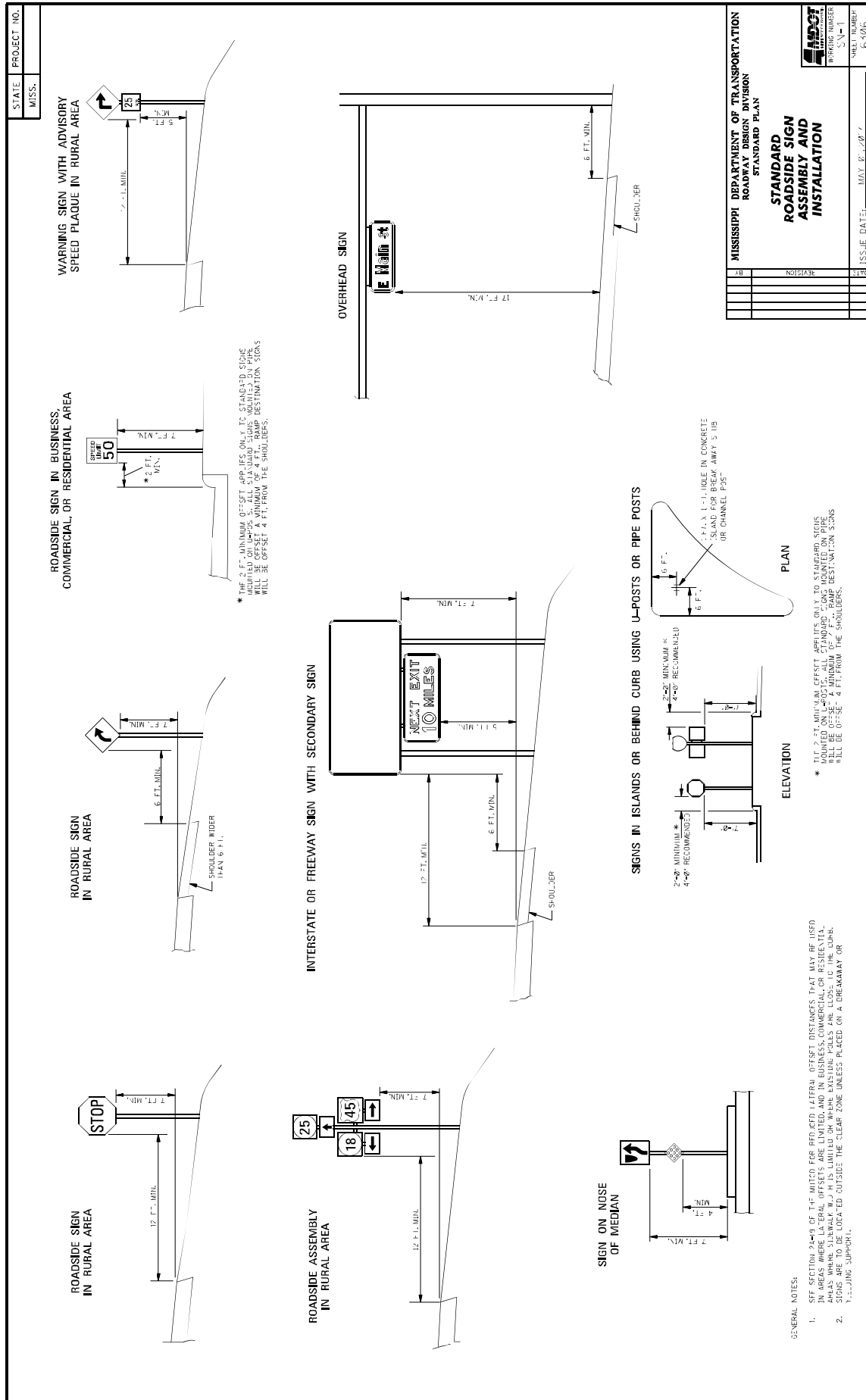
E11-1b

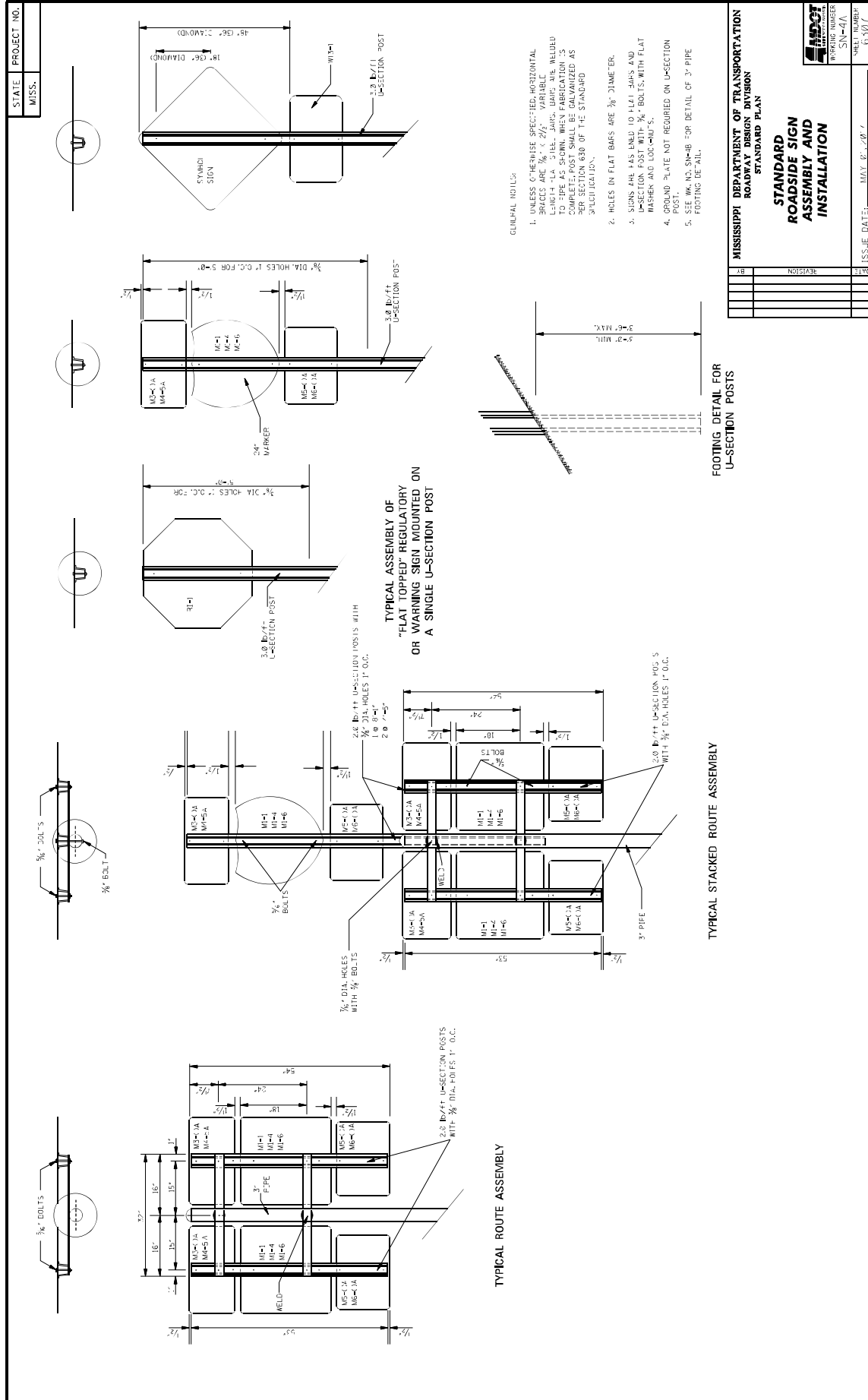
E11-1c

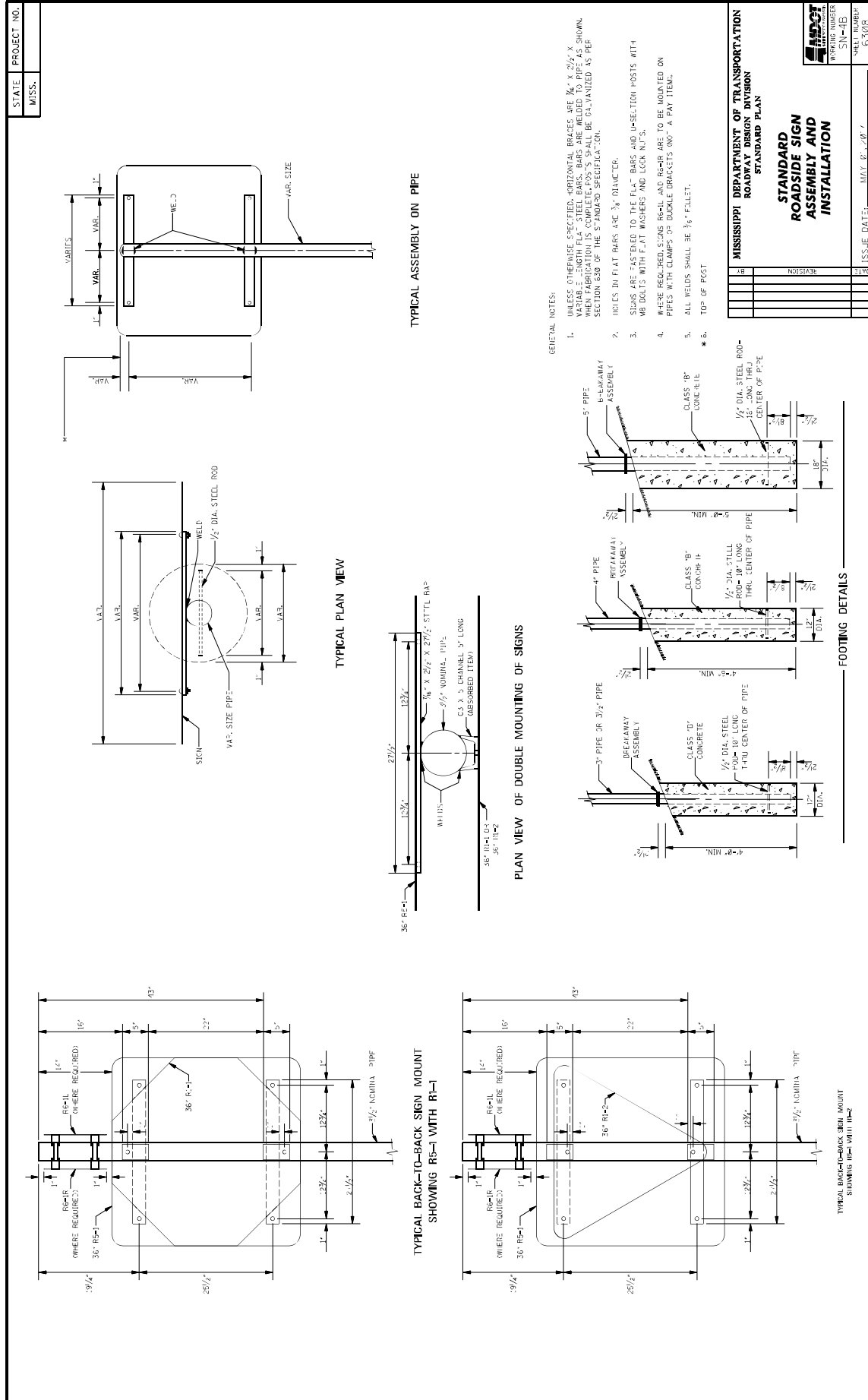


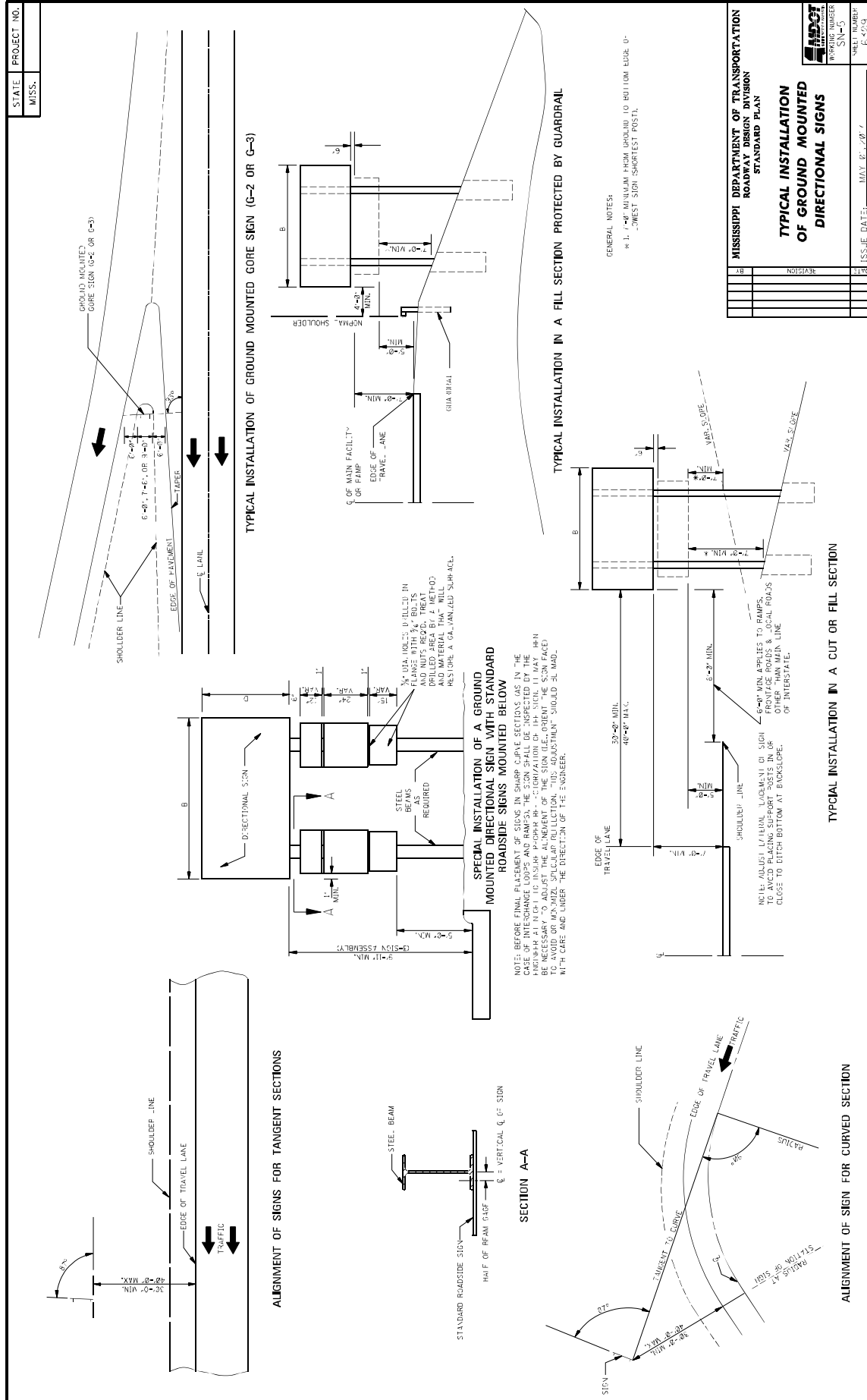
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**TYPICAL 2-POST INSTALLATION  
WITHOUT EXIT SIGN**

\* NOTE: PLACE POST BRACING ANGLE A" BOTTOM OF PRIMARY OR SUPPLEMENTARY SIGN WHICHEVER IS LOWER.  
(SEE "END ELEVATION OF POST AND FOOTING" ON SN-6A.)

SIGN POST SPACING TABLE									
2 POST					3 POST				
"B"	"C"	"D"	"E"	"F"	"B"	"C"	"D"	"E"	"F"
2'-0"	2'-0"	1'-0"	14'-6"	24'-0"	14'-6"	8'-0"	2'-0"	20'-0"	14'-6"
2'-6"	2'-6"	1'-6"	13'-6"	23'-0"	13'-6"	7'-6"	1'-6"	19'-0"	13'-6"
3'-0"	3'-0"	2'-0"	12'-6"	21'-0"	12'-6"	7'-0"	3'-0"	17'-0"	12'-6"
3'-6"	3'-6"	2'-6"	11'-6"	19'-0"	11'-6"	6'-6"	3'-6"	15'-0"	11'-6"
4'-0"	4'-0"	3'-0"	10'-6"	17'-0"	10'-6"	6'-0"	4'-0"	13'-6"	10'-6"
4'-6"	4'-6"	3'-6"	9'-6"	15'-0"	9'-6"	5'-6"	4'-6"	11'-6"	9'-6"
5'-0"	5'-0"	4'-0"	8'-6"	13'-0"	8'-6"	5'-0"	5'-0"	9'-6"	8'-6"
5'-6"	5'-6"	4'-6"	7'-6"	11'-6"	7'-6"	4'-6"	5'-6"	7'-6"	7'-6"
6'-0"	6'-0"	5'-0"	6'-6"	10'-0"	6'-6"	4'-0"	6'-0"	5'-6"	6'-0"
6'-6"	6'-6"	5'-6"	5'-6"	8'-6"	5'-6"	3'-6"	6'-6"	4'-6"	5'-6"
7'-0"	7'-0"	6'-0"	4'-6"	7'-0"	4'-6"	3'-0"	7'-0"	3'-6"	4'-6"
7'-6"	7'-6"	6'-6"	3'-6"	5'-6"	3'-6"	2'-6"	7'-6"	2'-6"	3'-6"
8'-0"	8'-0"	7'-0"	2'-6"	4'-0"	2'-6"	2'-0"	8'-0"	1'-6"	2'-0"
8'-6"	8'-6"	7'-6"	1'-6"	3'-0"	1'-6"	1'-0"	8'-6"	0'-6"	1'-0"
9'-0"	9'-0"	8'-0"	0'-6"	1'-0"	0'-6"	0'-0"	9'-0"	0'-0"	0'-0"

DESIGN PCS1 SPACING TABLE

[illegible]

- ① ALL STEEL SHALL BE GALVANIZED AFTER FABRICATION EXCEPT 45 NOTED ON TIE PLATES.
- ② PIPES MAY BE WELDED OR SEAMLESS.
- ③ BOLT WASHERS AND NUTS USED FOR FASTENING ALUMINUM STEEL SHIFTS AND PANELS SHALL BE ALUMINUM AS PER FOLLOWING TABLE.

DESCRIPTION	ASTM DESIGNATION	ALLOY
BO-TS AND WASHERS	H 205	2024-T4
PANEL BOLT NUTS	B 211	6061-T6
STOP NUTS	B 211	2017-T3

MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
ROADWAY DESIGN DIVISION  
STANDARD PLAN

## BREAKAWAY SIGN SUPPORTS

DATE	REVISION	BY

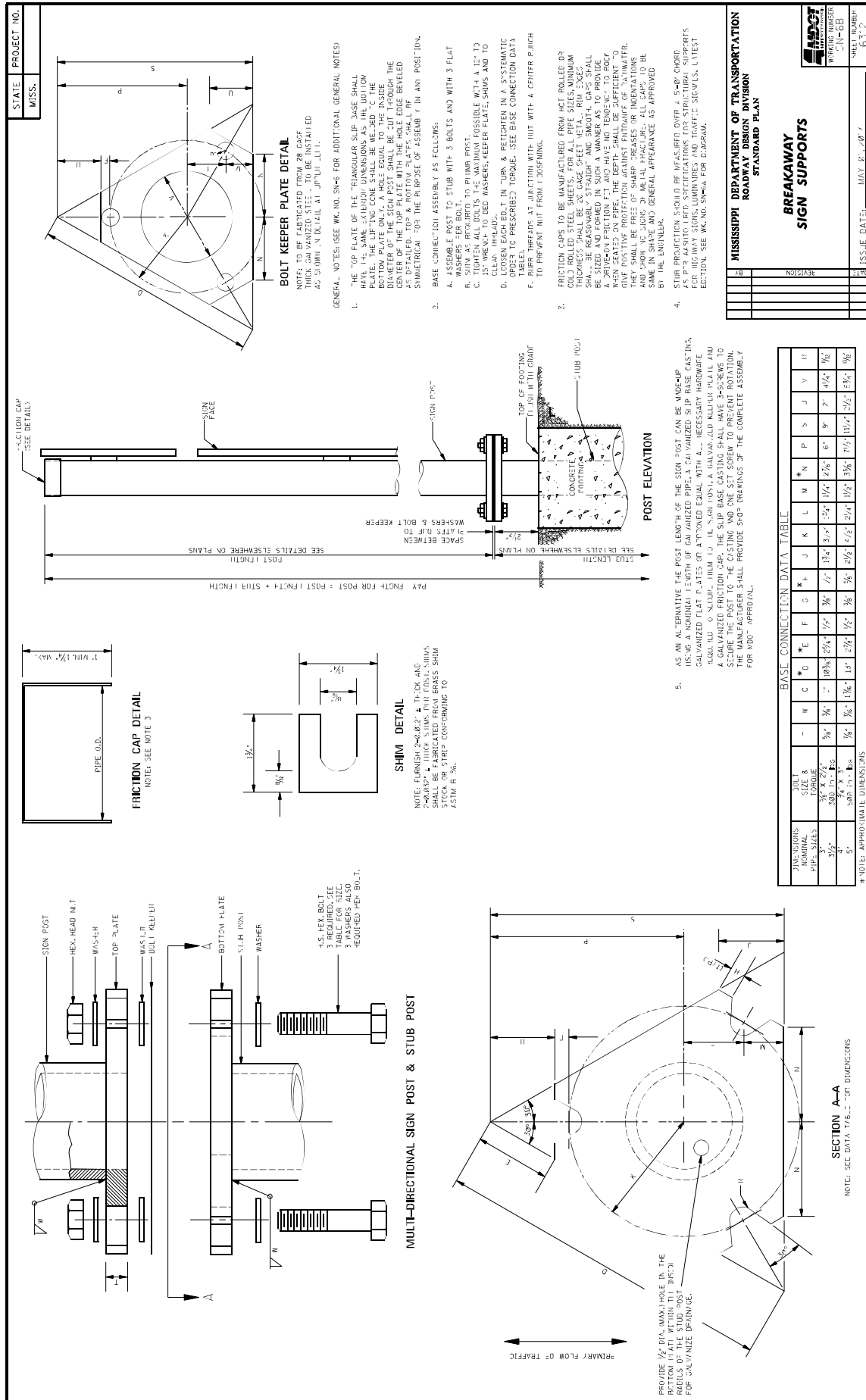
ISSUE DATE: MAY 02, 2001

STATE	PROJECT NO.
MISS.	

GENERAL NOTES FOR WORKING SHEETS SN-6, SM-6A AND SA-6B:

1. COULDS  
ALL FOOTINGS SHALL BE CLASS "B" CONCRETE. POST STEELS SHALL BE 3E". IN CONCRETE FOOTING A REINFORCING BAR SHALL BE PLACED AT THE TOP OF THE COLUMN. THE REINFORCING BAR SHALL BE PLACED AT THE TOP OF THE COLUMN.
2. BASE CONNECTION PROCEDURE  
BASE CONNECTION SHALL BE MADE WITH A FLAT WASHER ON EACH BOLT WITH A FLAT WASHER ON EACH BOLT. THE BOLT SHALL BE PLACED AT THE TOP OF THE COLUMN. THE BOLT SHALL BE PLACED AT THE TOP OF THE COLUMN.
3. POST LENGTH  
POST LENGTH SHALL BE DETERMINED BY THE DESIGNER. THE POST SHALL BE PLACED AT THE TOP OF THE COLUMN. THE POST SHALL BE PLACED AT THE TOP OF THE COLUMN.
4. WELDING  
WELDING SHALL BE PERFORMED IN SHOP BY ELECTRIC ARC PROCESS.
5. WELDING  
WELDING SHALL BE PERFORMED IN SHOP BY ELECTRIC ARC PROCESS.
6. WELDING  
WELDING SHALL BE PERFORMED IN SHOP BY ELECTRIC ARC PROCESS.
7. WELDING  
WELDING SHALL BE PERFORMED IN SHOP BY ELECTRIC ARC PROCESS.
8. WELDING  
WELDING SHALL BE PERFORMED IN SHOP BY ELECTRIC ARC PROCESS.
9. WELDING  
WELDING SHALL BE PERFORMED IN SHOP BY ELECTRIC ARC PROCESS.
10. WELDING  
WELDING SHALL BE PERFORMED IN SHOP BY ELECTRIC ARC PROCESS.

	WORKING NUMBER SN-6A	SHEET NUMBER 6311
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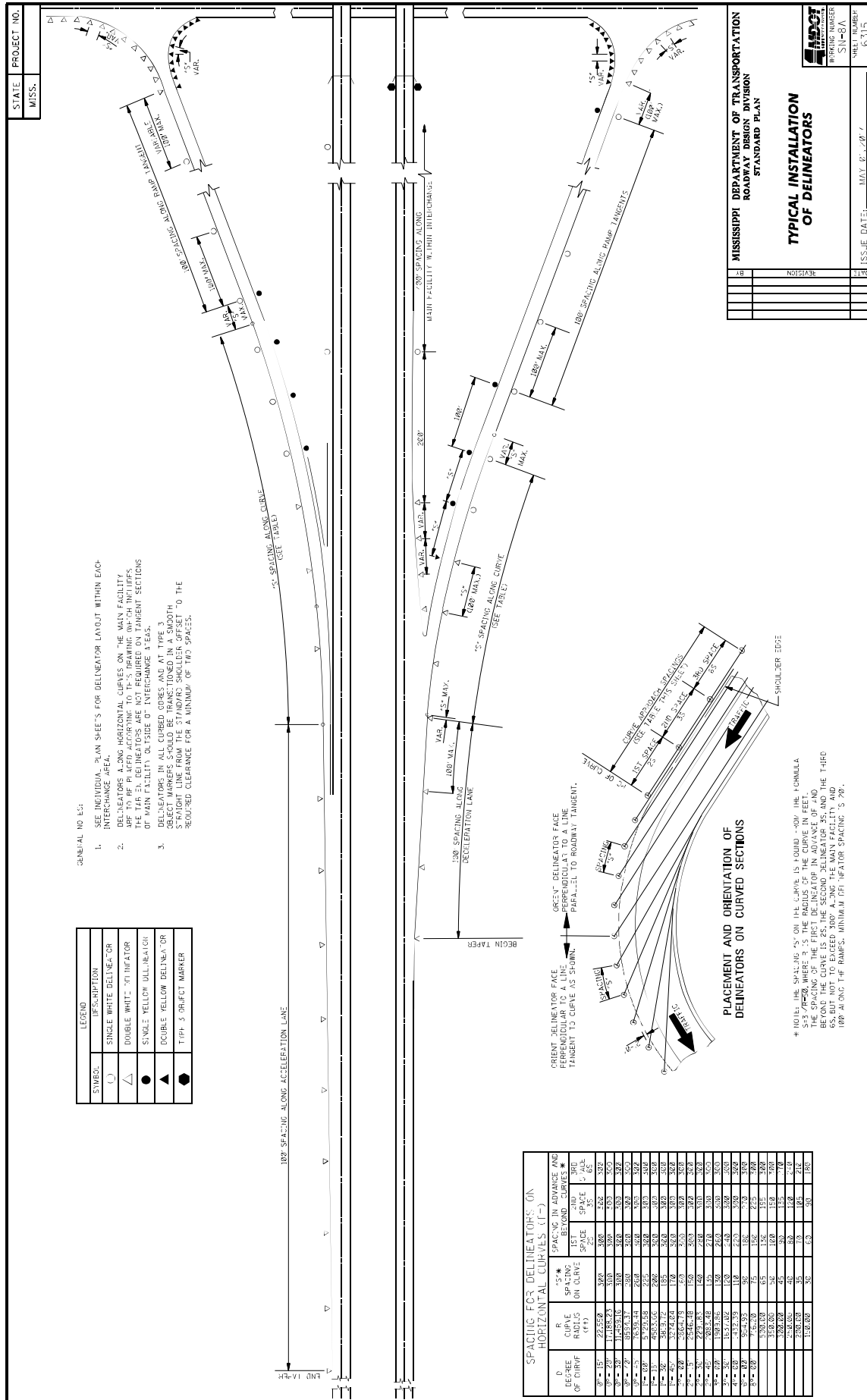
- GENERAL NOTES (SEE MK. NO. SN-6 FOR ADDITIONAL GENERAL NOTES)
1. THE TOP PLATE OF THE TRIANGULAR SLIP JASE SHALL HAVE THE SAME EXTERIOR DIMENSIONS AS THE BOTTOM PLATE. THE LIFTING CONE SHALL BE WELDED TO THE CENTER OF THE TOP PLATE WITH THE HOLE EDGE BEVELED AT DETAILED TOP & BOTTOM PLATES SHALL BE SYMMETRICAL FOR THE PURPOSE OF ASSEMBLY IN ANY POSITION.
  2. BASE CONNECTION ASSEMBLY AS FOLLOWS:
    - A. ASSEMBLE POST TO STUB WITH 3 BOLTS AND WITH 3 FLAT WASHERS PER BOLT.
    - B. SHIM AS REQUIRED TO PLUMB POST POSSIBLE WITH A 1/2" TO 1" SHIM.
    - C. 15" MINIMUM TO DEED WASHERS, KEEPER PLATE, SHIMS AND TO CLEAN HEADS.
    - D. LOOSEN EACH BOLT IN TURN & RETIGHTEN IN A SYSTEMATIC MANNER TO PRESCRIBED TORQUE. SEE BASE CONNECTION DATA TABLE.
    - E. RIBB THREAPE AT JUNCTION WITH NUT WITH A CENTER PUNCH TO PREVENT NUT FROM LOSING.
  3. FRICTION CAPS TO BE MANUFACTURED FROM HOT ROLLED OR COLD ROLLED STEEL SHEETS. FOR ALL PIPE SIZES, MINIMUM THICKNESS SHALL BE 20 GAGE SHEET METAL. RIM EDGES SHALL BE REASONABLY STRAIGHT AND SMOOTH. CAPS SHALL BE SEATED ON PIPE WITH A MINIMUM OF 1/4" TO 1/2" TO ROOF WHEN SEATED ON PIPE. THE DEPTH SHALL BE SUFFICIENT TO GIVE POSITIVE PROTECTION AGAINST ENTRAPMENT OF WATER. CAPS SHALL BE WELDED TO THE POST WITH A MINIMUM OF 1/4" TO 1/2" TO ROOF AND SHOW NO SIGN OF WEAR OR CRACKING. ALL CAPS TO BE SAME IN SHAPE AND GENERAL APPEARANCE AS APPROVED BY THE ENGINEER.
  4. STUB PROJECTION SHOULD BE MEASURED OVER A 5-8" CHORD AS PER ASTM D 1591 SPECIFICATIONS FOR STRUCTURAL SUPPORTS. SEE MK. NO. SN-6 FOR DIMENSIONS, LATEST EDITION. SEE MK. NO. SN-6 FOR DIMENSIONS.

5. AS AN ALTERNATIVE THE POST LENGTH OF THE SIGN POST CAN BE MADE UP USING A NOMINAL LENGTH OF GALVANIZED PIPE, A GALVANIZED SLIP BASE CASTING, GALVANIZED FLAT PLATES ATTACHED EQUAL WITH ALL NECESSARY HARDWARE. GALVANIZED FLAT PLATES ATTACHED EQUAL WITH ALL NECESSARY HARDWARE. GALVANIZED FLAT PLATES ATTACHED EQUAL WITH ALL NECESSARY HARDWARE. GALVANIZED FLAT PLATES ATTACHED EQUAL WITH ALL NECESSARY HARDWARE.

PROVIDE 1/2" DIA. MAX. HOLE IN THE RADIUS OF THE STUB POST FOR GALVANIZE DRAINAGE.

[illegible]







[illegible]

