

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO. <u> 1 </u>	DATED <u> 4/20/2023 </u>	ADDENDUM NO. _____	DATED _____
ADDENDUM NO. _____	DATED _____	ADDENDUM NO. _____	DATED _____
ADDENDUM NO. _____	DATED _____	ADDENDUM NO. _____	DATED _____

Number	Description
1	Revised Notice to Bidder Nos. 4954 & 4955; Revised Bid Items; Amendment EBSx Download Required.

TOTAL ADDENDA: 1
(Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

Contractor

BY _____
Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

_____	President	Address
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_____	Secretary	Address
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_____	Treasurer	Address
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The following is my (our) itemized proposal.

NH-0026-01(082)/ 108176301000

Lauderdale County(ies)

Revised 01/26/2016

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 4954

DATE: April 20, 2023

SUBJECT: Specialty Items

PROJECT: NH-0026-01(082)/108176301 - LAUDERDALE

Pursuant to the provisions of Section 108, the following work items are hereby designated as "Specialty Items" for this contract. Bidders are reminded that these items must be subcontracted in order to be considered as specialty items.

CATEGORY: GUARDRAIL, GUIDERAIL

Line No	Pay Item	Description
0200	606-B003	Guard Rail, Class A, Type 1, 'W' Beam, Metal Post
0220	606-D020	Guard Rail, Bridge End Section, Type H, Metal Post
0230	606-E005	Guard Rail, Terminal End Section, Flared

CATEGORY: MISCELLANEOUS/ SPECIALTY WORK ITEMS

Line No	Pay Item	Description
0180	423-A001	Rumble Strips, Ground In

CATEGORY: PAVEMENT STRIPING AND MARKING

Line No	Pay Item	Description
0380	626-A001	6" Thermoplastic Double Drop Traffic Stripe, Skip White
0390	626-C002	6" Thermoplastic Double Drop Edge Stripe, Continuous White
0400	626-E001	6" Thermoplastic Double Drop Traffic Stripe, Continuous Yellow
0410	626-G004	Thermoplastic Double Drop Detail Stripe, White
0420	626-G005	Thermoplastic Double Drop Detail Stripe, Yellow
0430	626-H001	Thermoplastic Double Drop Legend, White
0440	626-H002	Thermoplastic Double Drop Legend, White
0450	627-J001	Two-Way Clear Reflective High Performance Raised Markers
0460	627-K001	Red-Clear Reflective High Performance Raised Markers
0470	627-L001	Two-Way Yellow Reflective High Performance Raised Markers

CATEGORY: TRAFFIC CONTROL - PERMANENT

Line No	Pay Item	Description
0480	630-A001	Standard Roadside Signs, Sheet Aluminum, 0.080" Thickness
0490	630-A003	Standard Roadside Signs, Sheet Aluminum, 0.125" Thickness
0510	630-F002	Delineators, Flexible Post Mounted, Crossover, Type I, Yellow
0520	630-F006	Delineators, Guard Rail, White
0530	630-F007	Delineators, Guard Rail, Yellow
0540	630-G003	Type 3 Object Markers, OM-3L, Post Mounted
0550	630-G007	Type 3 Object Markers, OM-3R, Post Mounted

CATEGORY: TRAFFIC CONTROL - TEMPORARY

Line No	Pay Item	Description
0280	619-A1001	Temporary Traffic Stripe, Continuous White
0290	619-A2001	Temporary Traffic Stripe, Continuous Yellow
0300	619-A3001	Temporary Traffic Stripe, Skip White
0310	619-A5001	Temporary Traffic Stripe, Detail
0320	619-A6001	Temporary Traffic Stripe, Legend
0330	619-A6002	Temporary Traffic Stripe, Legend
0340	619-D1001	Standard Roadside Construction Signs, Less than 10 Square Feet
0350	619-D2001	Standard Roadside Construction Signs, 10 Square Feet or More
0360	619-G4005	Barricades, Type III, Single Faced
0362	619-G5001	Free Standing Plastic Drums
0364	619-E1001	Flashing Arrow Panel, Type C

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 – NOTICE TO BIDDERS NO. 4955

CODE: (SP)

DATE: 04/20/2023

SUBJECT: Scope of Work

PROJECT: NH-0026-01(082) / 108176301 -- Lauderdale County

The contract documents do not include an official set of construction plans, but may, by reference, include some Standard Drawings or Special Design Drawings.

Work on the project shall consist of milling and overlaying approximately 5.0 miles of asphalt pavement on SR 19 beginning approximately 0.5 miles north of North Hills Street at bridge # 24.4 in Lauderdale County and ending at the paving joint approximately 0.2 miles north of SR 494.

From the BOP at Station 114+93 (NB) & 114+79 (SB) to the EOP at Station 354+90

Prior to the mill and overlay, the island and ramp from east bound SR 494 to south bound SR 19 and from west bound State Blvd Extension to north bound SR 19 shall be modified as shown in the drawings following the scope of work. The area to be removed at each site shall be saw cut using pay item 503-C: Saw Cut, Full Depth prior to removal to provide a neat removal area. Removal of the existing island and the curb and gutter around the island will be paid for using pay item 202-B: Removal of Island Pavement, All Types. Removal of curb and gutter on the side of the ramp across from the island will be paid for using pay item 202-B: Removal of Curb and Gutter, All Types. Removal of asphalt will be paid for under pay item 202-B: Removal of Asphalt Pavement, All Depths. After removal of pavement is complete, the area to be paved shall be excavated to approximately 14½” at SR 494 and 10½” at State Blvd below the surface of the existing road. This depth could vary depending on field conditions and approval of the Engineer. The excavated material shall be removed from the project and payment will be made using pay item 203-G: Excess Excavation, LVM, AH. At the State Blvd Extension site, curb and gutter shall be replaced on the side of the island that has been saw cut and new curb and gutter will be placed on the north side of the ramp. At the SR 494 site, curb and gutter shall be placed all the way around the new island and on the south side of the ramp as shown in the following drawings using pay item 609-D: Combination Curb and Gutter, Type 2. The subgrade shall be compacted to the satisfaction of the Engineer before placement of new asphalt. New asphalt shall be placed as shown in the attached typical section and will be placed in lift thicknesses not to exceed that specified in Subsection 401.02.4 of the Standard specifications. Asphalt will be paid for under pay items 403-A: 12.5-mm, MT, Asphalt Pavement, 403-B: 12.5-mm, MT, Asphalt Pavement, Leveling, and 403-A: 19-mm, MT, Asphalt Pavement. The shoulder shall be brought back to grade with crushed stone. This work will be paid for under pay item 304-D: Granular Material, Crushed Stone.

The existing 28 foot wide and variable asphalt pavement shall be milled 2” and inlay with 2” of 12.5-mm, MT, asphalt. Any drop offs or drainage issues caused during milling and paving operations shall be corrected by the Contractor. Drop offs shall be addressed as per Standard

Drawing TCP-SC. Traffic will be allowed to run on milled surfaces no more than seven (7) consecutive days. Any material bladed from the existing shoulder shall be used to raise the existing shoulder to match the new pavement edge elevation, and any surplus material shall be spread uniformly along the edge of the shoulders, slopes, or other adjacent areas daily as directed by the Engineer and will be an absorbed item. Shoulders have been inspected and it was determined only a small amount of crushed stone was needed. There is a quantity of crushed stone to be used in localized areas to eliminate excessive drop offs.

Intersecting roads and crossovers shall be milled 2” and inlayed with 2” of 12.5-mm, MT, asphalt. All guardrail pads shall be removed using appropriate pay items, and replaced with 12.5-mm, MT, asphalt.

Where extra depth milling is required (per attached table), areas shall be milled 5” and variable and refilled to the existing surface with 12.5-mm, MT, asphalt to eliminate drop offs.

At bridges listed in the “Bridge Locations and Estimated Quantities table”, joints shall be repaired as per the attached drawings. All bridges shall be swept and maintained subsequent to milling and paving operations and during construction of the project to remove any existing debris plus any debris accumulated from construction activities. The costs of sweeping and cleaning of bridges shall be absorbed in other items bid.

Guardrail Replacement

The length of guardrail required is based on a terminal section of 37.5’. For any other length terminal section, the length of normal guardrail will be adjusted.

All guardrail removed shall be replaced the same day and prior to reopening the adjacent lane of traffic. Voids created by the removal of posts, concrete anchors, footings, etc. shall be treated as shown on the attached drawing. This work shall consist of the following sequence of operations: removal of the existing guardrail and posts, removal of the entire guardrail pad, repaving the guardrail pad, and installation of the new guardrail. All guard rails, including rail, terminal end sections, bridge end sections, and metal posts will become property of MDOT and shall be delivered by the Contractor to the MDOT Newton Maintenance Facility located at 7759 Highway 80 West, Newton, MS. The Contractor shall coordinate the delivery of these items with MDOT in advance by contacting Jay Franklin at 601-946-7820. Concrete anchors, wooden posts, and wooden rails will become the property of the Contractor and shall be removed from the Project Site. The Contractor shall not damage or disturb the existing guardrail or posts during the grading or the paving operation. The guardrail pad shall be constructed using 12.5-mm, MT, asphalt and shall be 4” thick. The cost of the removal of the guardrail delineators and object marker signs shall be included in other items bid.

GENERAL NOTES

Milling

Milling shall not begin until an **approved** asphalt mix design has been received, nor until such time that, in the opinion of the Engineer, weather conditions have been consistently suitable enough to allow placement of the asphalt pavement after the milling operations.

The reclaimed asphalt pavement (RAP) material removed by the milling operation shall become the property of the Contractor.

Where milling is required, the Contractor shall provide outlets at sufficient intervals to prevent pooling or standing water on the milled surface, the cost of which shall be absorbed in other items bid.

Milling and paving operations shall be performed such that a -2% slope from centerline is provided in normal crown roadway sections. Superelevation through curves shall be maintained as it currently exists or improved as directed. Where slope correction is required, correction shall be made by milling, paving, or combination thereof as directed by the Engineer. Milling correction: Mill outside edge of pavement to a depth of 1½” on a 2% slope towards the centerline. Paving Correction: Mill to depth of 1½” on existing slope and 2¼” and variable on centerline and 1½” on outside edge. Combination Method: Combination of both methods as directed by the Engineer to achieve the desired slope. In super elevated areas where correct SE exist, milling shall transition to thickness through curves. Where correct SE does not exist, milling will transition at curves to correct SE as directed by the Engineer. Milling operations shall be performed in accordance with the Contract documents and the MDOT Standard Specifications. Variable width and length transitions may be required for ties at ramps, local roads, project limits. Milling of driveway pads shall be conducted in a manner to prevent gouging or otherwise affecting the roadway pavement structure and slope. Milling of driveway pads shall not be done in simultaneous path with main line milling.

Traffic will be allowed to travel on the milled surface for seven (7) days. Traffic will be allowed to run on all milled local roads and crossovers for 30 days unless otherwise stated. This allowance is for the Contractor’s convenience, and thus, the Contractor is responsible for any pavement failures or damage sustained during this period.

Paving

Per Subsection 401.02.3.2, the asphalt mix design shall be submitted to the Engineer at least 10 working days prior to its proposed use.

Work shall be conducted and coordinated in a manner to prevent a longitudinal joint of more than 2¼” where traffic is expected to cross. Adjacent lanes and shoulders shall be brought up to grade as required to prevent drop-offs and as specified in Subsection 618.03.3. Payment for milling, leveling, and granular shoulder work will be made using the appropriate pay items. Uneven Lanes signs shall be used as required and as shown on the Standard Drawings.

Publicly maintained roads and streets shall be paved to the existing right-of-way and in accordance with the attached drawings.

Privately owned entrances shall be paved to the shoulder line per the included typical drawing unless otherwise directed. Pad dimensions shall match the existing lengths and widths unless otherwise directed. Pads shall be shaped horizontally and vertically to prevent excessive drop-

offs. Any new driveway pads deemed necessary by the Engineer shall be placed according to specifications.

Granular Shoulder Material

Where applicable, the existing shoulders shall be raised to match the new pavement elevation by placing variable depth crushed stone. The shoulders shall be graded and pulled up on a daily basis to eliminate drop-offs in excess of 2¼". Placement of the crushed stone on the finished asphalt course shall not be permitted. The existing shoulder shall be scarified to allow incorporation of the new shoulder material. The material shall be bladed, rolled, and compacted to a finished slope of four percent (4%) in normal crown sections. Placement of this material shall be performed to provide a uniform and compacted shoulder with a minimum depth and width of material placed. Shoulders with adequate shoulder material in place shall be bladed to a slope of four percent (4%) in normal crown sections. The cost of blading will be an absorbed item and should be included in the price of other items bid. Crushed concrete will not be allowed.

Crushed stone shall be provided around driveway pads as directed to prevent shoulder drop-offs and shall be placed in a timely manner. Drop-offs exceeding 2¼" shall be corrected within two (2) calendar days of the placement of the pad. Crushed stone shall be used as directed by the Engineer.

Any excess material generated as a result of shoulder blading shall be used on the existing shoulder to match the new pavement elevation and any surplus material shall be spread along the edge of the shoulders, fore slopes, or other adjacent areas as directed by the Engineer and will be an absorbed item. Material which cannot be suitably placed in adjacent areas and deemed to be excess excavation by the Engineer shall be removed from the project site. Payment for removal of excess material will be made using pay item 203-G: Excess Excavation.

Temporary and Permanent Pavement Markings

Temporary traffic stripe shall be required immediately after the milling and/or required overlay and prior to opening area to traffic. Temporary stripe shall be placed in the same location and configuration as the permanent stripe except that it may be offset as required for milling and paving operations. If temporary stripe is offset, the Contractor shall conduct operations in a manner to ensure the final temporary stripe is placed at the required location of the permanent stripe. If removal of temporary offset stripe is required in order to achieve the correct location and alignment of permanent stripe, the cost of removal will be absorbed in other items bid. Placing double temporary centerline will not be allowed.

Temporary striping shall conform to finished stripe specifications for alignment, neatness, and straightness.

The use of short strips of traffic tape will not be allowed unless approved by the Engineer.

All permanent striping shall be double drop thermoplastic, 90-mil thickness unless otherwise specified in Subsection 626.03.1.2. Edge lines shall be placed to accommodate the lane widths shown on the attached applicable typical sections unless prevented by field conditions.

Rumble stripe shall be placed throughout the project limits in accordance with the attached details and Standard Drawings. Rumble stripe placement will also be required outside of project limits from Sta. 105+13 to 114+79 LT/LT, Sta. 109+00 to 114+79 RT/LT in the left lanes, and Sta. 105+53 to 114+93 LT/RT, Sta. 111+18 to 114+93 in the right lanes.

Payment for edge stripe on local roads shall be made under pay item 626-G: Thermoplastic Double Drop Detail Stripe, White when the length of said stripe is less than 150 feet when measured from the end of the radius. If the measured length is greater than 150 feet, then payment shall be made under Pay Item No. 626-C: 6" Thermoplastic Double Drop Edge Stripe, Continuous White.

Payment for centerline stripe on local roads shall be made under pay item 626-G: Thermoplastic Double Drop Detail Stripe, Yellow when the length of said stripe is less than 150 feet when measured from the stop bar. If the measured length is greater than 150 feet, then payment shall be made under pay item No. 626-E: 6" Thermoplastic Double Drop Traffic Stripe, Continuous Yellow. Centerline Stripe shall be omitted on local roads whose width is less than 20 feet.

The face of all existing undisturbed asphalt curbs shall be painted with at least two coats of white traffic paint with glass beads being required in the top coat. The cost associated with the painting of new or existing curb is to be included in other items bid.

Guardrail

Guardrails shall be replaced at the locations shown on the attached table. Removal of guardrail shall consist of removal of bridge end section, w-beam/thrie beam, terminal end section, posts, and all other appurtenances. All guardrail removed shall be replaced the same day and prior to reopening the adjacent lane of traffic. Voids created by the removal of posts, concrete anchors, footings, etc. shall be backfilled and compacted in accordance with Section 203 of the Standard Specifications.

The asphalt guardrail pad shall be removed or milled and repaved prior to the placement of the new guardrail. Removal of the guardrail pad shall be paid for using the milling pay item. Asphalt shall be extended under the guard rail and 2' behind guard rail post as per the attached detail. The area to be paved shall be bladed to accommodate 4" of 12.5-mm, MT asphalt. The elevation of the finished surface of the asphalt pavement shall provide for the required MASH guardrail height (see Standard Drawings).

Guardrail lengths are based on terminal end length of 37.5'. If terminal of length other than this is used, an adjustment in w-beam length is required.

All dimensions and spacings for bridge rail connectors shall be verified in the field by the Contractor prior to fabrication.

Traffic Control

The Contractor shall erect and maintain construction signing and provide all signs and traffic control devices necessary to safely maintain traffic around and through the work areas in accordance with the Traffic Control Plan and the MUTCD. The cost shall be included in the price bid for pay item 618-A: Maintenance of Traffic. Fluorescent orange sheeting shall be used on all

construction and traffic control signs except those designated in the plans to be black legend and border on white background.

Standard roadside construction signs, barricades, etc. shall be placed in accordance with the attached tables, drawings, and as directed by the Engineer. W20-1 signs shall be placed on all public road approaches as shown or as directed. Payment for standard roadside construction signs, barricades, etc. will be made using the appropriate pay items.

The Contractor shall on a daily basis, remove all debris from within the roadway and a 30-foot clear zone which, in the opinion of the Engineer, is a hazard to the traveling public. This activity shall begin with the beginning of work or the beginning of the contract time, whichever comes first. No direct payment will be made for the debris removal; the cost should be included in the prices of items bid. Failure of the Contractor to remove the debris as prescribed herein shall be just cause for withholding the monthly progress estimate payment or suspending active operations until the debris is satisfactorily removed by the Contractor.

Temporary asphalt joints (aka paper joints) shall be employed at all locations requiring traffic to traverse an uneven, transverse, pavement joint. Paper joints shall be a minimum of nine feet (9') in length and for the full width of the milled/paved surface. Paper joints shall be adequately maintained.

Potholes that may exist or occur in the existing pavement shall be patched in a timely manner as required. Patching of potholes shall be considered an absorbed item.

Permanent Signs

Permanent signs as listed on the attached tables shall be replaced. Unless otherwise listed in the attached tables, existing posts, anchors, angles/bars, and other components shall be reused. The Contractor shall use new bolts, screws, washers, nuts, etc. of the required sizes in the installation of signs. If required as part of the sign replacement activities, all post and I-beam lengths in these plans are estimated. Post lengths for all signs shall be verified in the field by the Contractor prior to fabrication. Installation dates shall be clearly written in bold black markings on the back bottom half of all signs with a permanent marking stick that is waterproof, fade resistant, and marks on wet or dry surfaces. If existing sign posts or footings are to be replaced, the existing posts and footings shall be removed and the area backfilled and compacted in accordance with Section 203 of the Standard Specifications. Removal of sign post, and footing and backfilling will be paid using the removal of sign pay item.

Object markers at bridge approaches and other locations shall be replaced as shown in the attached table. Removal of object markers shall be included in the cost of other items bid.

Random Clearing

Random clearing shall be required on this project as shown in the Random Clearing Table. When random clearing is required in areas such as safety/clear zones, cutting shall be approximately flush with the tree trunk. This work will also include the removal and proper disposal of the cut material off of the right of way, or it may be chipped or shredded by mechanical means and mulched on the right of way to the satisfaction of the Engineer. Payment for random clearing will

be per station (see table). Clearing limits shall be 50' from edge of the widest point of the pavement or to R.O.W. whichever comes first. Widths shown on the attached table don't extend from the edge of pavement, they only show the width of the actual trees/brush to be cleared and are for estimation purposes only.

Tree Removal

Tree removal will be required on this project as shown in the attached table. Only trees shown on this table will be paid for under the tree removal pay item. Any trees that fall in the random clearing areas will be paid for under the random clearing item.

Miscellaneous Notes

It shall be the responsibility of the Contractor to protect existing structures such as pipes, inlets, aprons, bridges, etc. from damage which might occur during construction. The Contractor shall replace or repair, as directed by the engineer, any structures damaged by the Contractor during the life of the contract. No payment will be made for replacement or repair of damaged items.

Any signs that are in conflict with construction of this project shall be removed and relocated by the Contractor as directed by the Engineer; the cost of which is to be absorbed in other items bid.

Removal of existing raised pavement markers shall be included in the prices for other items bid.

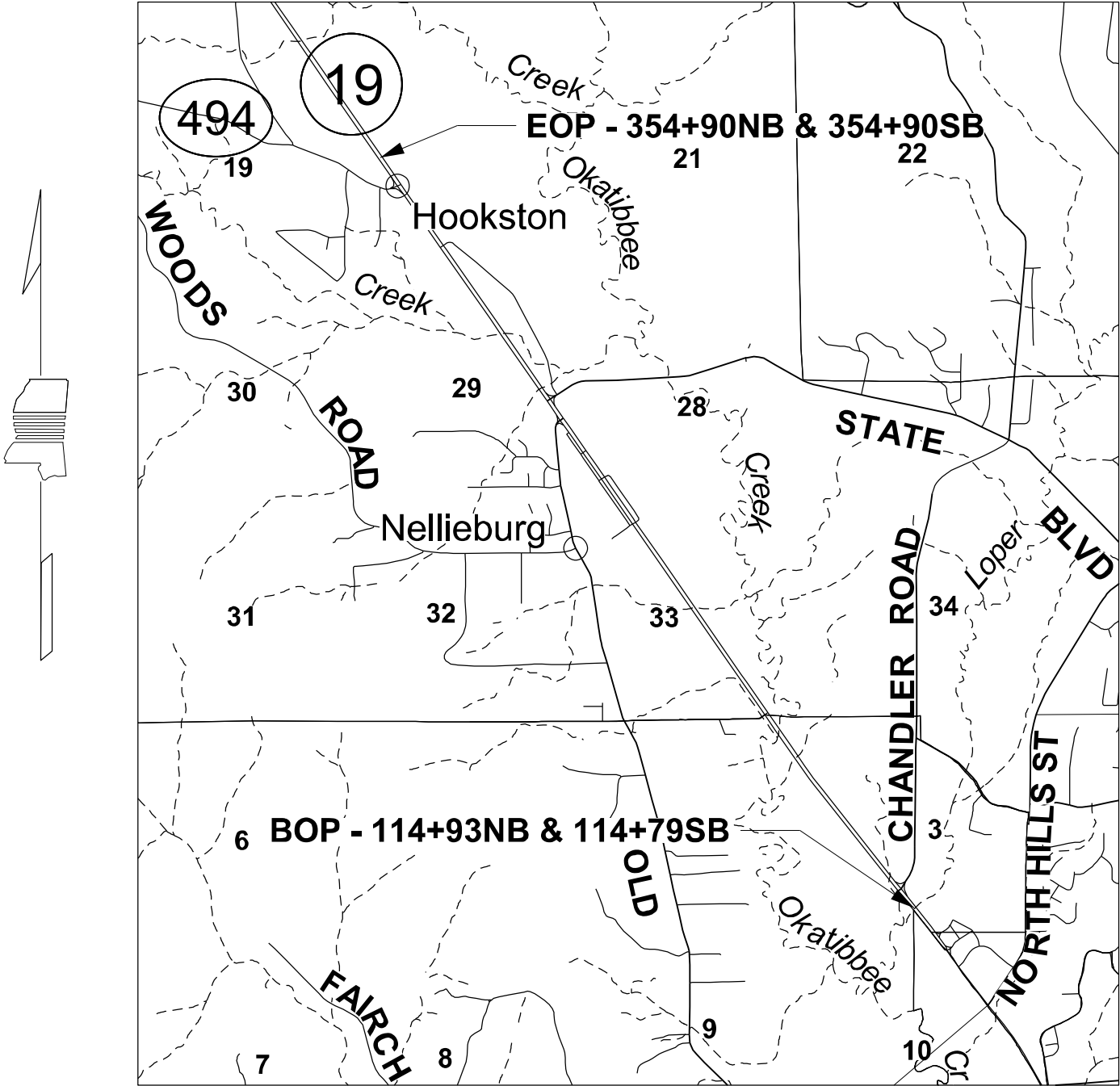
Incidental work such as removing vegetation, shaping and compacting shoulders, removing and resetting signs and/or mailboxes, removing excess asphalt material, project clean-up, and other items of incidental work necessary to complete the project will not be measured for separate payment and will be considered included in the prices of items bid.

Prior to the final inspection, bridges, islands, and areas with curb shall be swept/cleaned. Care should be taken to prevent milled asphalt, asphalt debris, vegetative/granular debris, etc. from entering drainage structures or clogging other drainage ways. Disposal of material will not be measured for separate payments. The Contractor is responsible for contacting 811 about any conflicting utilities during construction.

Box culverts listed in the attached table shall have the existing debris and sediment removed by the Contractor, and shall be paid for using pay item 202-B: Removal of Debris and Sand From Box Culvert, 10-Foot and Greater Width, 202-B: Removal of Debris and Sand From Box Culvert, 6-Foot to Less Than 10-Foot Width, and 202-B: Removal of Debris from Drainage Channel. The applicable pay item shall be measured along the length of the box culvert or in the case of multiple barrels along the length of each barrel of the box culvert. The depth of sediment listed for each box culvert is for estimation purposes only, and the actual depth of the sediment shall be field verified by the Contractor prior to bidding the Project. The disposal of this material will not be measured for separate payment.

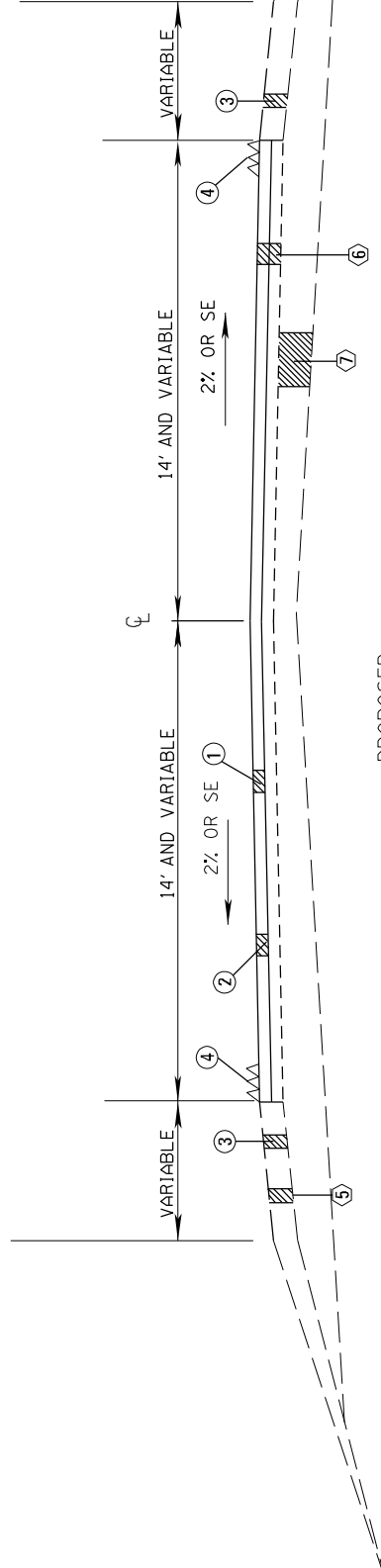
The Contractor shall adhere to Special Design Sheets SDTCP-D and SDTCP-E when performing lane closures associated with modifications to the SR 494 and State Boulevard intersections.

NH-0026-01(082)/108176301000
HWY 19 FROM BRIDGE NUMBER 24.4 TO SR 494
BOP - 114+93NB & 114+79SB
EOP - 354+90



NH-0026-01(082) / 108176301000 TYPICAL SECTION

Station 114+93 NB & 114+79 SB (BOP) TO
354+90 NB&SB (EOP)
RT & LT LANES
MAINLINE

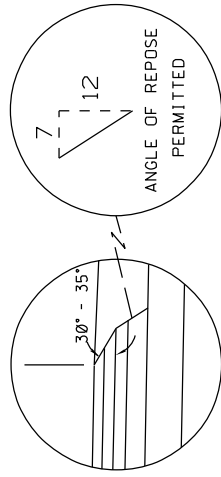


PROPOSED

- ① 2" & VAR. DEPTH FINE MILLING REQ'D
- ② 2" DEPTH ASPHALT REQ'D, 12.5 mm MIX, MT
- ③ VARIABLE DEPTH CRUSHED STONE AS NEEDED
- ④ GROUND IN RUMBLE STRIPE

EXISTING

- ⑤ 4" & VARIABLE GRANULAR MATERIAL IN PLACE
- ⑥ 13" & VARIABLE HMA
- ⑦ NB: 10" UNTREATED GRANULAR MATERIAL
SB: 3/4" DBST OVER 4" CLAY GRAVEL

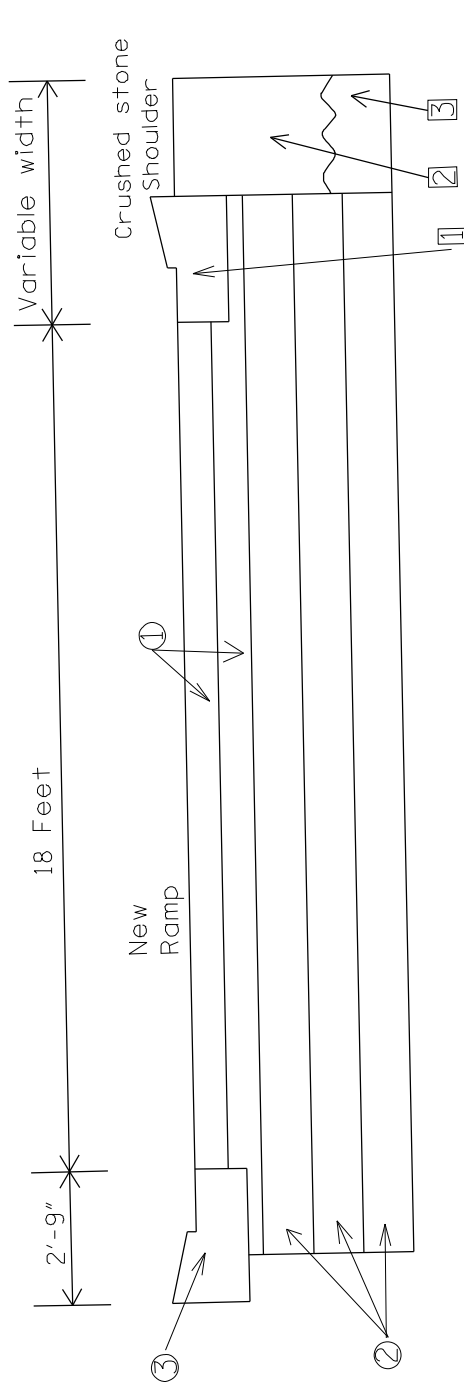


SAFETY EDGE REQ'D TOP 2 LIFTS ONLY
(NOT A PAY ITEM)

NH-0026-01(082)/108176-301

Lauderdale County

State BLVD Intersection Typical Section



① 4 1/2" of 12.5mm MT, -leveling Asphalt Pavement, 1 @ 2", 1 @ 2 1/2", as per scope of work

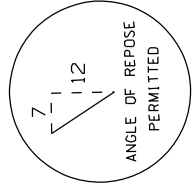
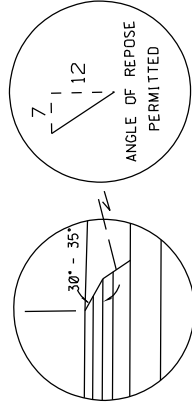
② 6" of 19mm MT, Asphalt Pavement, 2 @ 3"

③ Combination Curb & Gutter, Type 2 As per Standard Drawing SD-1

① Combination Curb & Gutter Type 2A as per standard drawing SD.1

② Build shoulder back up to grade with crushed stone

③ any material deemed unsuitable by the engineer will be removed using pay item 203-G002, Excess Excavation, LVM, AH

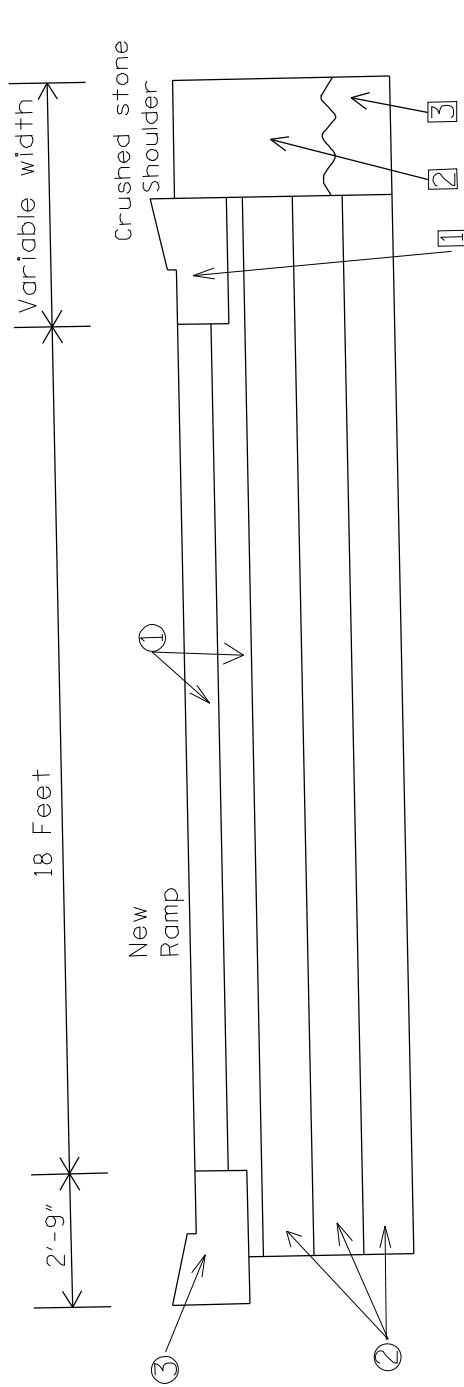


SAFETY EDGE REQ'D TOP 2 LIFTS ONLY (NOT A PAY ITEM)

NH-0026-01(082)/108176-301

Lauderdale County

Hwy 494 Intersection Typical Section

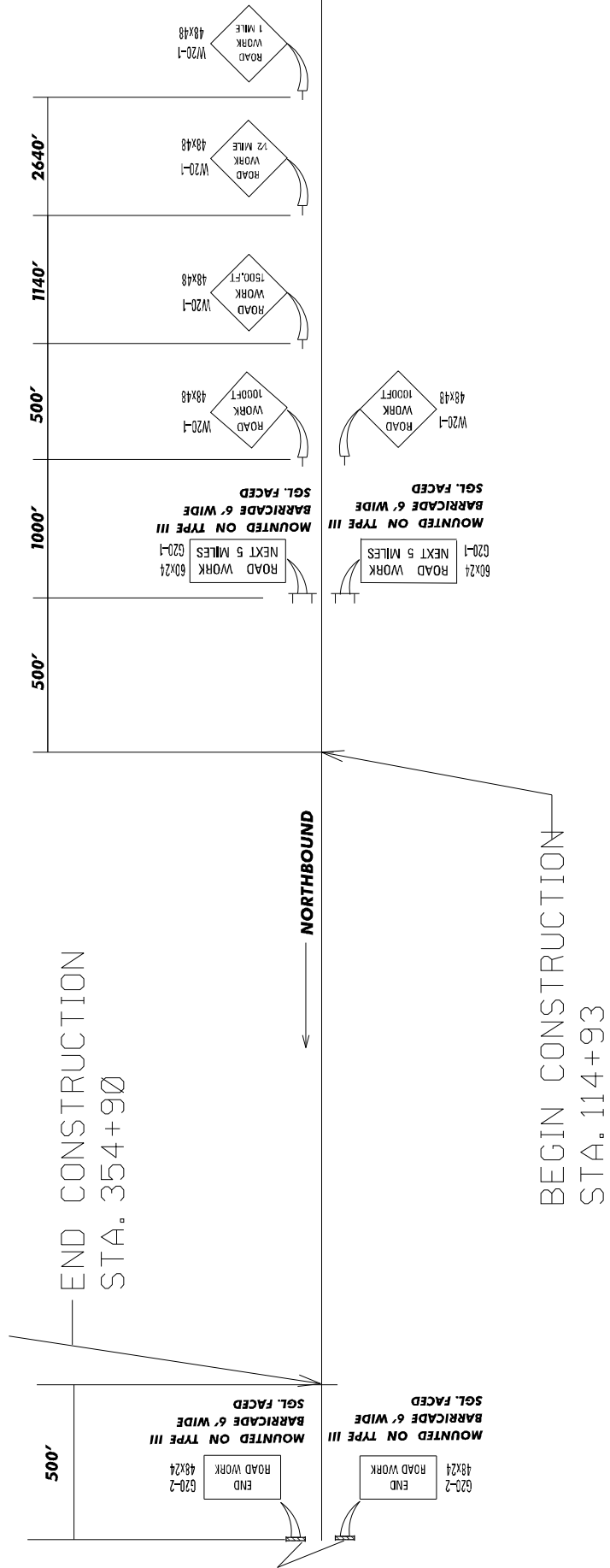


- ① 4 1/2" of 12.5mm MT, leveling Asphalt Pavement, 1 @ 2", 1 @ 2 1/2", as per scope of work
- ② 10" of 19mm MT, Asphalt Pavement, 1 @ 3", 2 @ 3 1/2"
- ③ Combination Curb & Gutter, Type 2 As per Standard Drawing SD-1

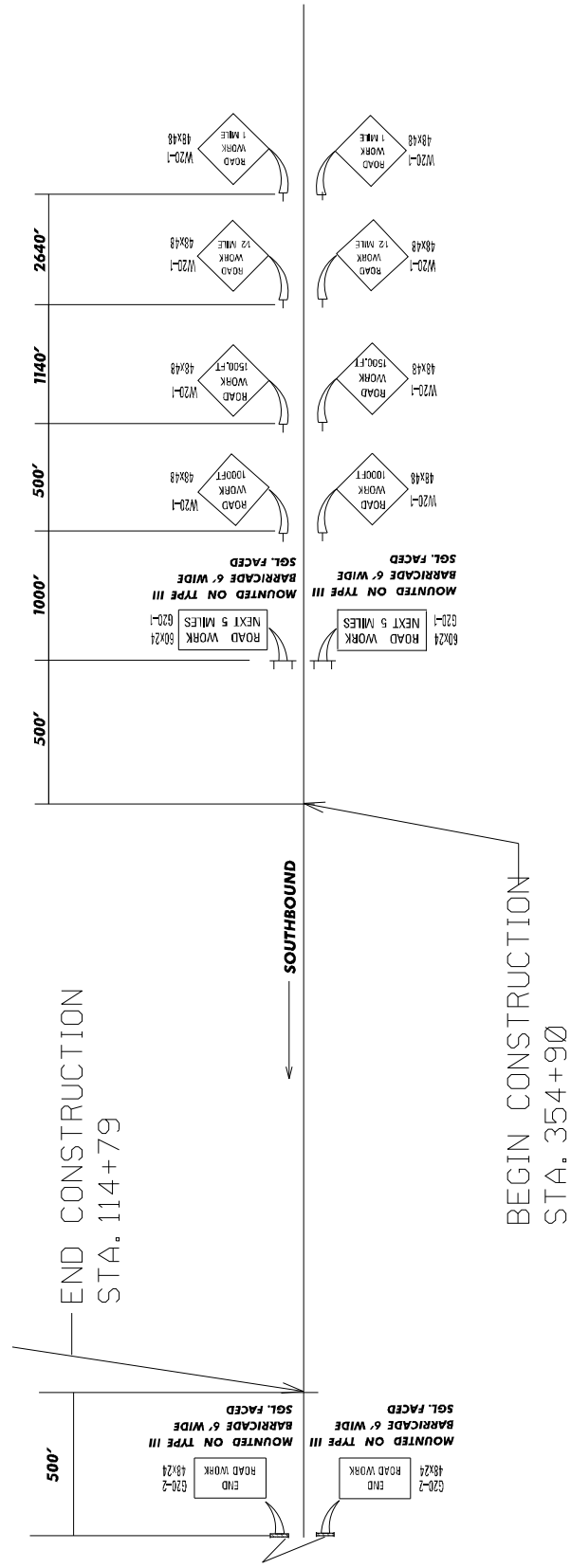
- ① Combination Curb & Gutter Type 2A as per standard drawing SD.1
- ② Build shoulder back up to grade with crushed stone
- ③ any material deemed unsuitable by the engineer will be removed using pay item 203-G002, Excess Excavation, LVM, AH

SAFETY EDGE REQ'D TOP 2 LIFTS ONLY (NOT A PAY ITEM)

DETAIL OF CONSTRUCTION SIGNING NORTH BOUND NH-0026-01(082) / 108176301000



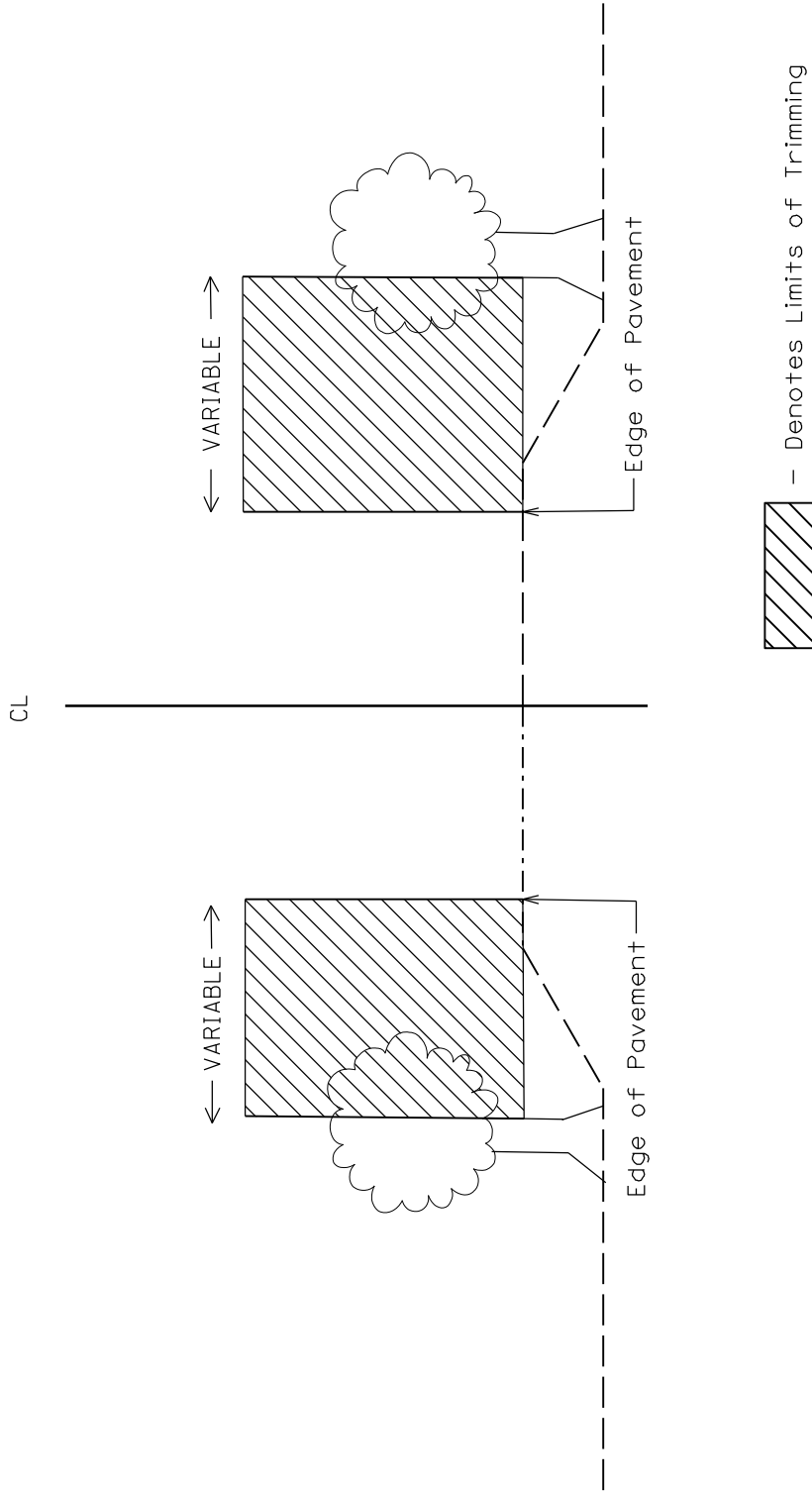
**DETAIL OF CONSTRUCTION SIGNING
SOUTH BOUND
NH-0026-01(082) / 108176301000**



NH-0026-01(082)/108176301000						
Construction Signs						
Sign	Dimensions	SF	Description	Quantity	Total SF	10 SF or >
G20-1	60x24	10	ROAD WORK NEXT 5 MILES	4	40	40
G20-2	48x24	8	END ROAD WORK	4	32	0
W20-1	48x48	16	ROAD WORK NEXT 1000 FT	4	64	64
W20-1	48x48	16	ROAD WORK NEXT 1500 FT	3	48	48
W20-1	48x48	16	ROAD WORK 1/2 MILE	3	48	48
W20-1	48x48	16	ROAD WORK 1 MILE	3	48	48
W20-1	48x48	16	ROAD WORK AHEAD	10	160	160
Location of W20-1 "ROAD WORK AHEAD" Signs						
Chandler Rd			Candy Cane Ln.			
Whippoorwill Rd			Otis Seal Dr			
Cricket Rd North			Hwy 19 Frontage Rd			
Cricket Rd South			Old 8th Street Rd			
Hwy 494			State Blvd Ext			

DETAIL OF LIMB TRIMMING

SR 19 From Bridge #24.4 To Paving Joint 0.2 Miles North Of SR 494



Notes:

1. Tree limbs shall be cut flush to tree trunk.
2. All debris associated with trimming is to be removed from project.
3. Limb trimming will be paid using Pay Item 201-D001 Random Clearing.

Clearing SR19 from Walmart to SR 494 108176									
Location	Sta	TO	Sta	Width	Area SF	Area ACRE	Pay in STA	Approximate # of trees	
NB	243+60		247+75	15	6225	0.143	4.150	15	
SB	348+60		340+50	31	6851	0.157	8.100	52	
SB	315+60		305+50	30	30300	0.696	10.100	126	
SB	282+50		274+50	17	13600	0.312	8.000	21	
SB	214+40		211+00	25	8500	0.195	3.400	6	

Tree Removal			
Station	Location	Description	Total
310+00	North Bound	2 Large Oaks in front of Brick House	2
321+00	South Bound	Pine in front of Palm Reader Building	1
317+50	South Bound	Large Oak just South of Palm Reader	1
304+00	South Bound	Large Pine between 2 Driveways	1
296+50	South Bound	2 Large Pines just across the ditch	2
289+00	South Bound	Hardwood at the corner of wooden fence	1
Total			8

Drainage Structure Cleanout Quantities							
Station	Box Width	Box Length	Depth of Sediment	Length of Channels	Total Length for pay	Pay Item	Notes
195+00	Arch Pipe	X	1'	125'	125'	202-B276 Removal of Debris from Drainage Channel	The arch pipe will not require cleaning, the drainage ditches out to R.O.W. will
203+60	14.5'	230'	18"	109'	230'	202-B096 Removal of Debris and Sand from box culvert 10' and greater	
229+00	14.5'	185'	1'	20'	185'	202-B096 Removal of Debris and Sand from box culvert 10' and greater	
268+00	21'	205'	2'	146'	205'	202-B096 Removal of Debris and Sand from box culvert 10' and greater	
276+00	6'	210'	1'	35'	210'	202-B273 Removal of Debris and Sand from box culvert 6' - 10'	

Extra Depth Milling										
Lane	Begin	End	LF	SF	Fine Milling (SY)	Asphalt For Tack Coat (GAL)	12.5mm MT			
RT/RT	127+65	128+45	80	1120	124.4	12.44	35.00			
RT/RT	162+00	165+23	323	4522	502.4	50.24	141.31			
LT/LT	174+00	176+00	200	2800	311.1	31.11	87.50			
LT & RT/LT	173+00	171+05	195	5460	606.7	60.67	170.63			
LT/LT	165+76	164+00	176	2464	273.8	27.38	77.00			
RT/LT	128+13	126+43	170	2380	264.4	26.44	74.38			
RT & LT/LT	124+70	124+00	70	1960	217.8	21.78	61.25			
Total =			1214	20706	2300.6	230.06	647.06			

Bridge Locations and Estimated Quantities

County	Route	Mile Point	Existing Joint Type	Joint Repair	Joint Repair Without Epoxy	Saw Cut			Preformed Joint Seal		
				LF	LF	Type I (LF)	Type II (LF)	>2.5" to 3.5" (LF)	Type I (LF)	Type II (LF)	> 2.5" to 3.5" (LF)
Lauderdale	SR 19	24.4B	PREFORMED	3	165	168	0	0	84	0	0
Lauderdale	SR 19	24.4B	OPEN			0	0	168	0	0	84
Lauderdale	SR 19	24.6B	PREFORMED	7	181	188	0	0	94	0	0
Lauderdale	SR 19	24.6B	OPEN			0	188	0	0	94	0
Lauderdale	SR 19	25.4B	SILICONE	9	317	282	0	184	141	0	92
Lauderdale	SR 19	25.4B	OPEN			0	188	0	0	94	0
Lauderdale	SR 19	27.6B	PREFORMED		188	188	0	0	94	0	0
Lauderdale	SR 19	27.6B	OPEN			94	94	0	47	47	0
Lauderdale	SR 19	24.4A	PREFORMED		168	168	0	0	84	0	0
Lauderdale	SR 19	24.4A	OPEN			168	0	0	84	0	0
Lauderdale	SR 19	24.6A	OPEN	5	235	240	0	0	120	0	0
Lauderdale	SR 19	24.6A	PREFORMED			240	0	0	120	0	0
Lauderdale	SR 19	25.4A	SILICONE	16	266	282	0	92	141	0	47
Lauderdale	SR 19	25.4A	OPEN			94	94	0	47	47	0
Lauderdale	SR 19	27.6A	PREFORMED	12	180	192	0	0	96	0	0
Lauderdale	SR 19	27.6A	OPEN			192	0	0	96	0	0
Total =				52	1700	2496	564	444	1248	282	223

LAUDERDALE COUNTY-108176/301000

Guardrail Quantities

STATION	GUARDRAIL		FLARED TERMINAL END SECT	TANGENT TERMINAL END SECT	Cable Anchor TYPE I (EA)	BRIDGE END SECTION			DELINEATORS		Type 3 Object Markers (EA)	GUARDRAIL REMOVAL (LF)	REMARKS
	W-BEAM (LF)	TRANS. SECT. (LF)				THIRIE BEAM (LF)	TYPE "H" (EA)	TYPE "C" (EA)	TYPE "F" (EA)	SPEC. DESIGN BR END CONN. (EA)			
125+00	RT/RT	12.5	1	1	1	1			4	5	1 (OM-3R)	87.5	24.6B
125+00	LT/RT	37.5	1	1	1						1 (OM-3L)	112.5	24.6B
165+00	RT/RT	137.5	1	1	1				7	7	1 (OM-3R)	212.5	25.4B
165+00	LT/RT	137.5	1	1	1						1 (OM-3L)	212.5	25.4B
282+00	RT/RT	137.5	1	1	1				7	7	1 (OM-3R)	212.5	27.6B
282+00	LT/RT	137.5	1	1	1						1 (OM-3L)	212.5	27.6B
118+00	RT/LT	137.5	1	1	1					7	1 (OM-3L)	212.5	24.4A
118+00	LT/LT	137.5	1	1	1				7	7	1 (OM-3R)	212.5	24.4A
126+00	RT/LT	137.5	1	1	1						1 (OM-3L)	212.5	24.6A
126+00	LT/LT	137.5	1	1	1				7	7	1 (OM-3R)	212.5	24.6A
170+00	RT/LT	137.5	1	1	1						1 (OM-3L)	212.5	25.4A
170+00	LT/LT	137.5	1	1	1				7	7	1 (OM-3R)	212.5	25.4A
288+27	LT/LT	188.75	1	1	1				7	7	1 (OM-3R)	225	27.3
284+00	RT/LT	137.5	1	1	1					7	1 (OM-3L)	212.5	27.6A
284+00	LT/LT	137.5	1	1	1				7	7	1 (OM-3R)	212.5	27.6A
TOTAL =		1868.75	14	1	1	14	0	0	53	47	15	2975	
		LF.	EA.	EA.	EA.	EA.	EA.	EA.	EA.	EA.	EA.	LF.	

* REMOVAL OF ALL GUARDRAIL (BRIDGE END SECTIONS, W-BEAM, TYPE I CABLE ANCHORAGE, TERMINAL END SECTIONS, ETC.) WILL BE PAID UNDER PAY ITEM 202-B REMOVAL OF GUARD RAIL.
 * REMOVAL OF GUARDRAIL (DELINEATORS ARE CONSIDERED INCIDENTAL TO THE REMOVAL OF GUARDRAIL AND WILL NOT BE MEASURED AS A SEPARATE PAY ITEM).
 * ALL GUARDRAIL (METAL RAIL AND METAL POSTS ONLY), WOODEN POSTS, ALL BLOCKOUTS, CONCRETE ANCHORS, ETC. WILL BE THE PROPERTY OF THE CONTRACTOR.
 ** TOTAL GUARDRAIL LENGTH IS BASED ON A TERMINAL END SECTION 37.5' LONG. IF A TERMINAL END SECTION OF A DIFFERENT LENGTH IS USED, THE LENGTH OF THE W-BEAM MAY HAVE TO BE ADJUSTED.

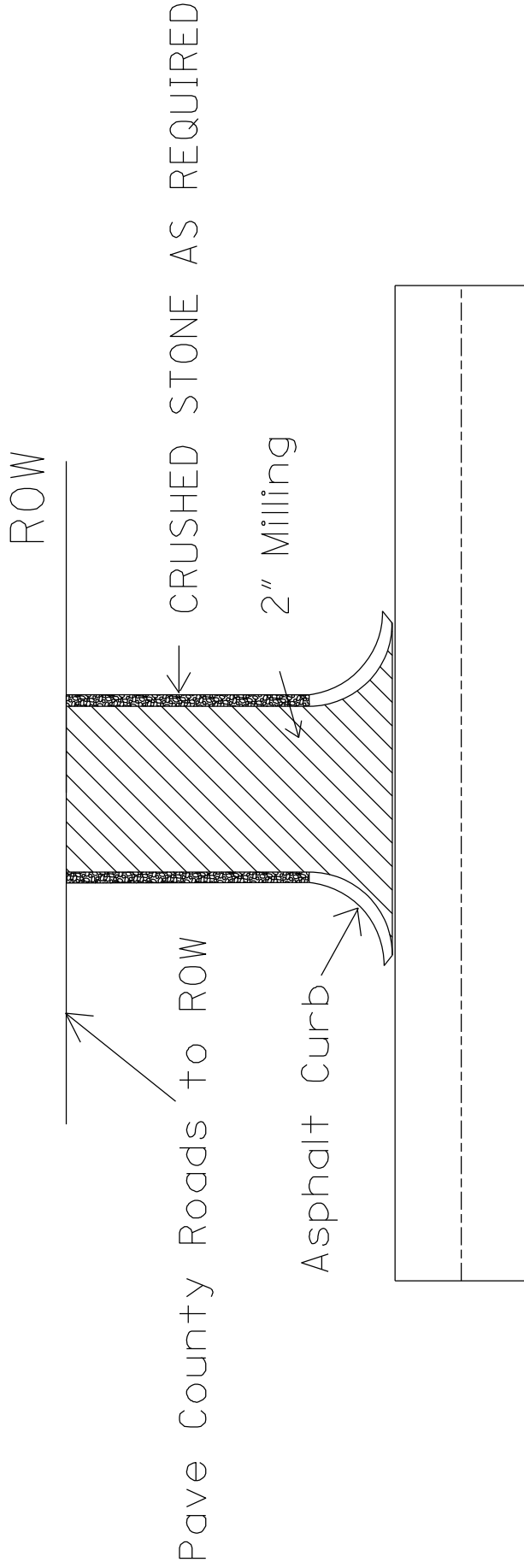
PROJECT NO. NH-0026-01(082) / 108176301
 COUNTY: Lauderdale

STANDARD ROADSIDE SIGNS - 0.080" THICKNESS										
STATION	SIGN NUMBER	SIZE (in. x in.)	AREA (sf)	SQUARE POST (sf)			(7/16" x 2-1/2") BARS	Class "B" Conc (cy)	REMARKS	
				2 lb/ft	4 lb/ft	3.72 lbs/lf				
204+00	R2-1	24x30	5.00						LT/LT/LN	
204+00	R2-1	24x30	5.00						RT/LT/LN	
Total this sheet =			10.00	0.00	0.00			0.00		

PROJECT NO. NH-0026-01(082) / 108176301
 COUNTY: Lauderdale

STANDARD ROADSIDE SIGNS - 0.125" THICKNESS										
STATION	SIGN NUMBER	SIZE (in. x in.)	AREA (sf)	SQUARE POST (lf)		(7/16" x 2-1/2") BARS 3.72 lbs/lf	Class "B" Conc (cy)	REMARKS		
				2 lb/ft	4 lb/ft					
122+00	W8-13	48x48	16.00		14.00			LT/LT/LN (Missing)		
122+00	W8-13	48x48	16.00		14.00			RT/LT/LN		
134+00	W8-13	48x48	16.00		14.00			RT/LT/LN		
134+00	W8-13	48x48	16.00		14.00			LT/LT/LN		
221+00	R1-1	48" Octagon	16.00					SAM RD (LT/LT/LN)		
260+00	R1-1	48" Octagon	16.00					Old 8th Street Rd (LT/LT/LN)		
Total this sheet =			96.00	0.00	56.00				0.00	


Milling and Paving Detail County Roads



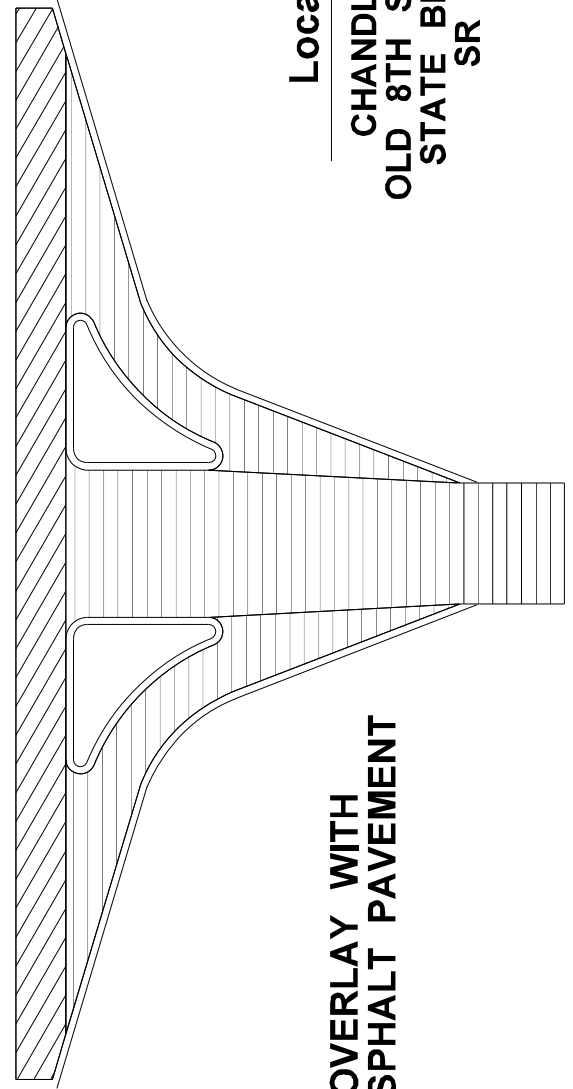
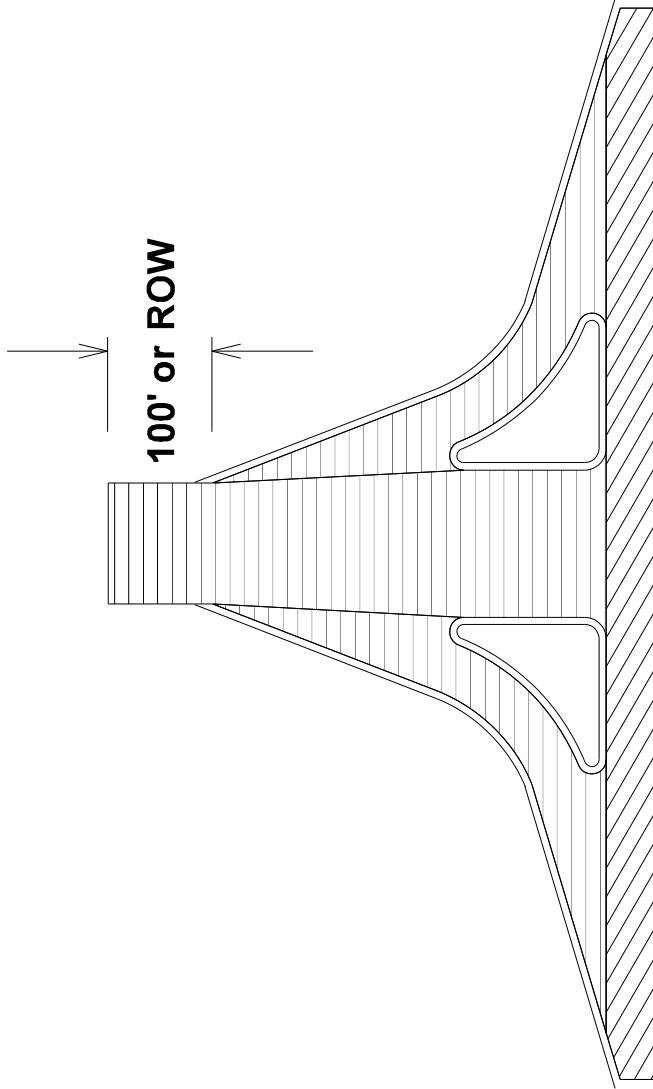
COUNTY ROADS:

- WHIPPOORWILL RD.
- CRICKET RD. (NORTH)
- CRICKET RD. (SOUTH)
- CANDY CANE LANE
- OTIS SEAL DR.
- SR 19 FRONTAGE ROAD

Notes:

- Mill limits of county/local roads at a depth of 2".
- Place 2" of 12.5mm, MT, Mixture to tie to mainline overlay.
- Milling/Paving area = 

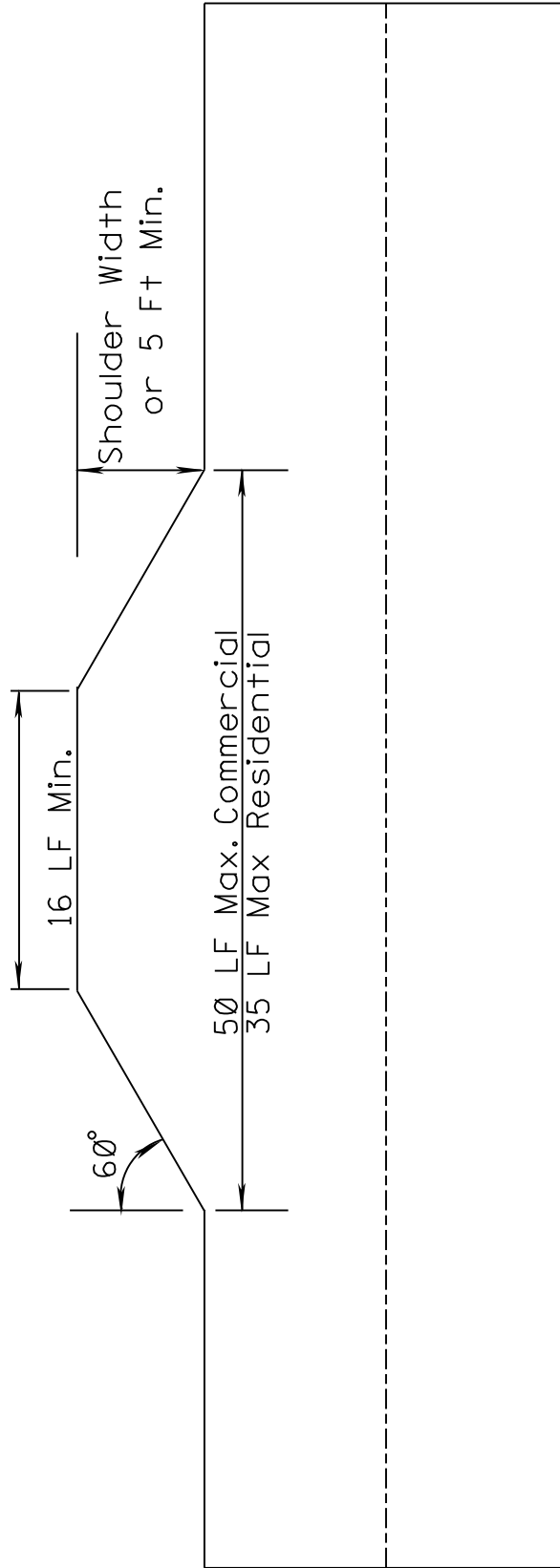
**LAUDERDALE COUNTY- SR. 19
TYPICAL MILLING PLAN FOR
CHANNELIZED INTERSECTIONS**



**MIII 2" AND OVERLAY WITH
2" 12.5MM, MT, ASPHALT PAVEMENT**

Locations
**CHANDLER RD
OLD 8TH STREET RD
STATE BLVD. EXT.
SR 494**

TYPICAL RAMP/PAD DETAIL

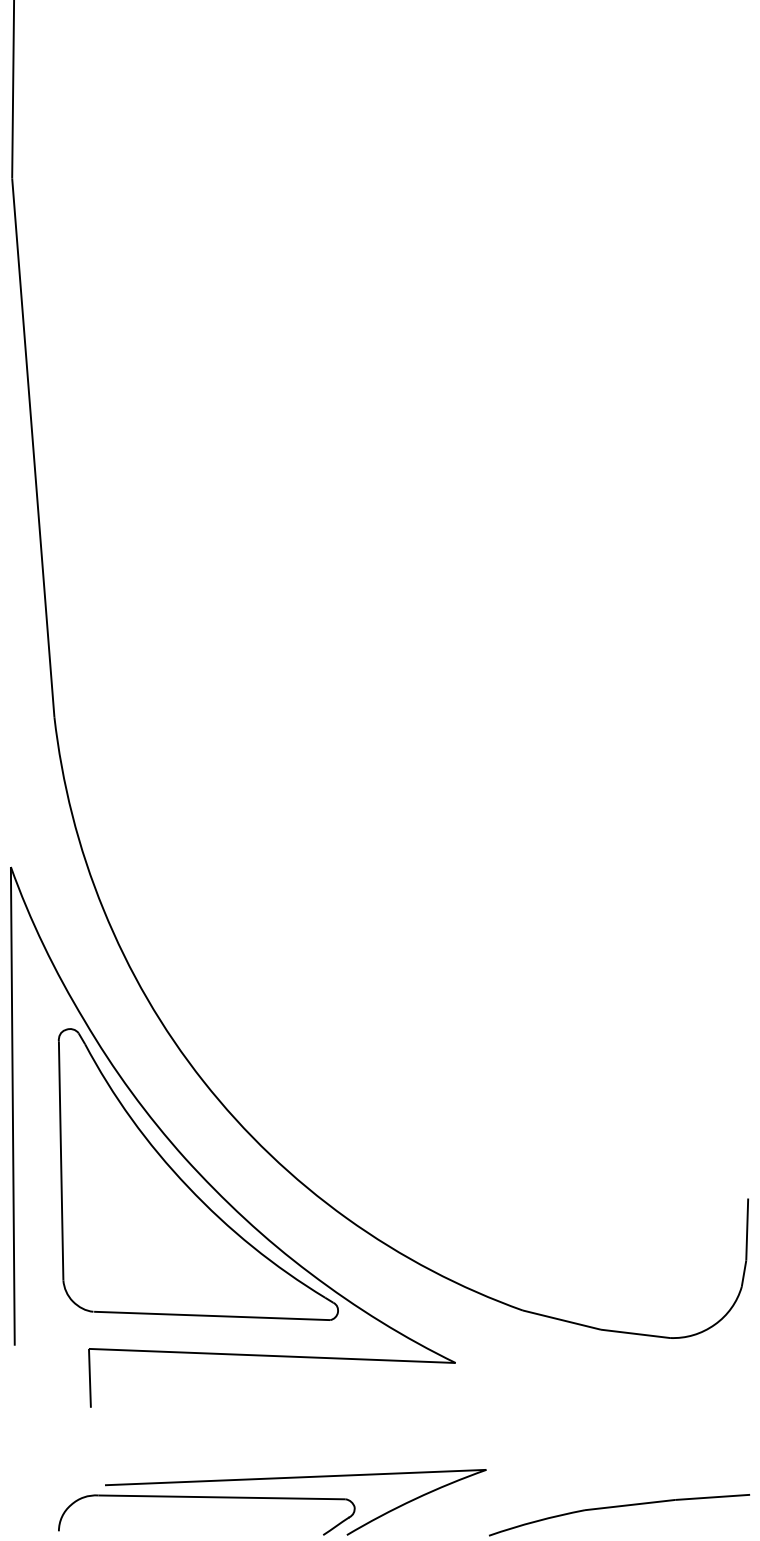


NH-0026-01(082)/108176-301

Lauderdale County

State BLVD Intersection
Modification

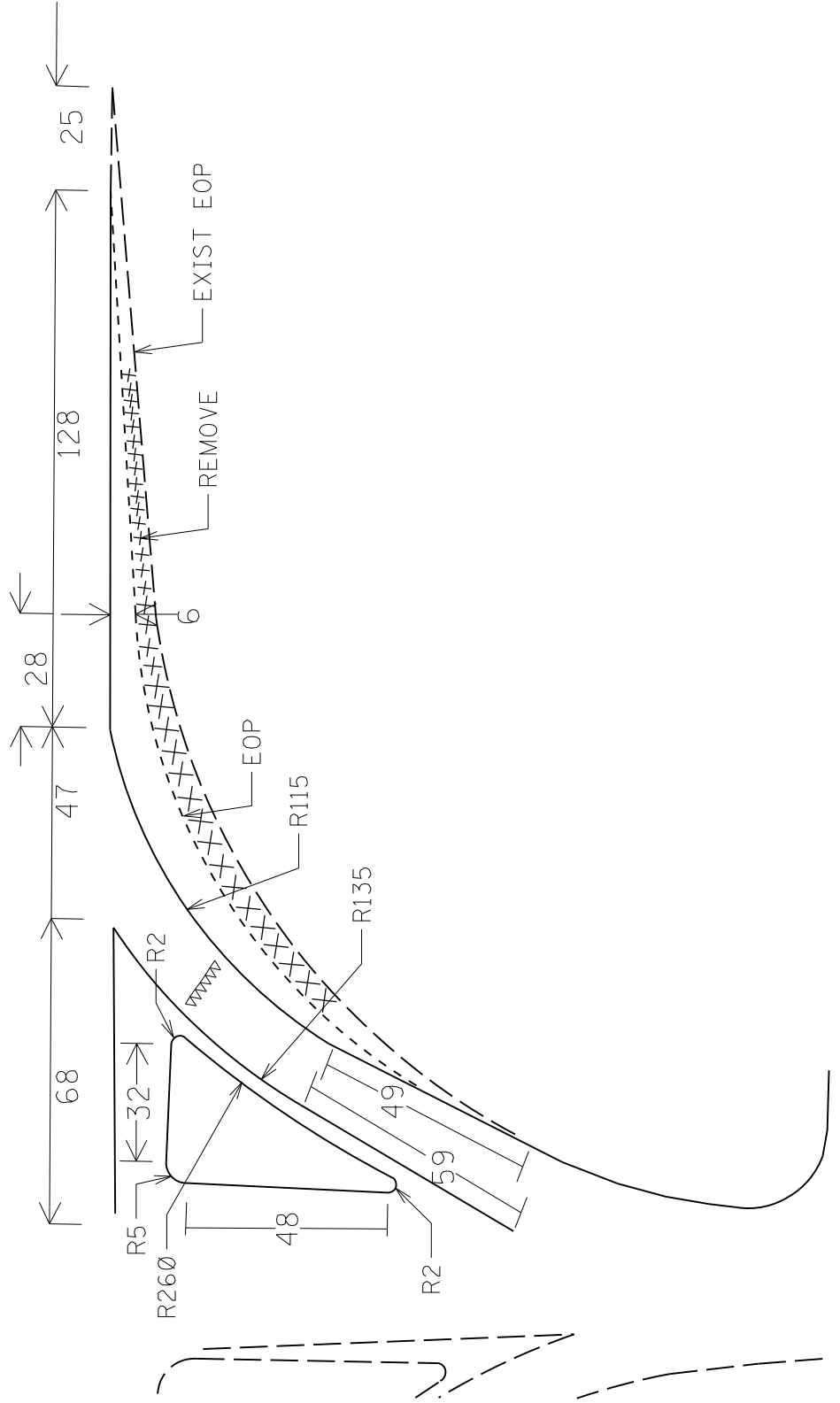
Existing



NH-0026-01(082)/108176-301 Lauderdale County

State BLVD Intersection Modification

Proposed

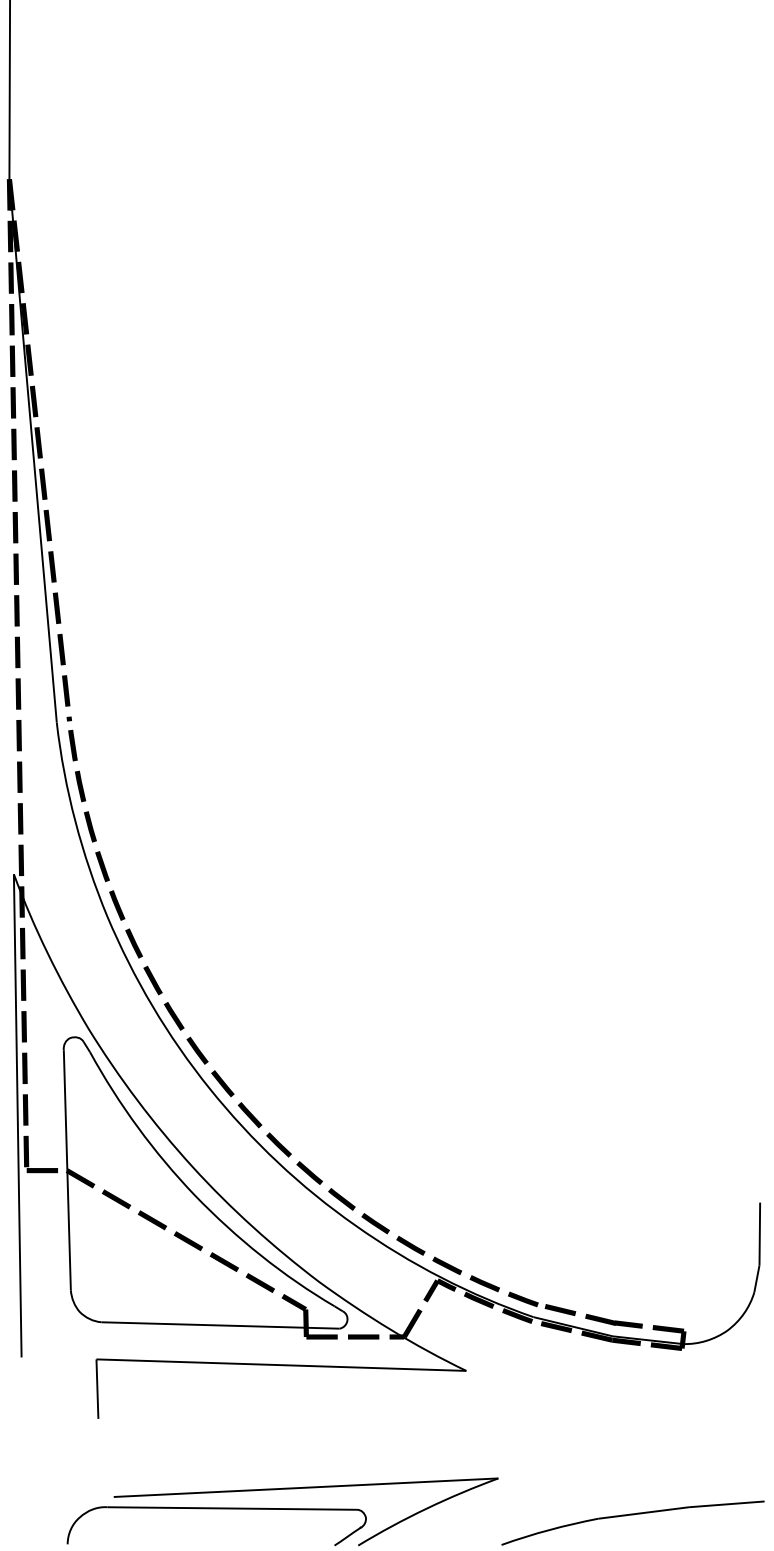


NH-0026-01(082)/108176-301

Lauderdale County

State BLVD Intersection
Modification

Removal Limits

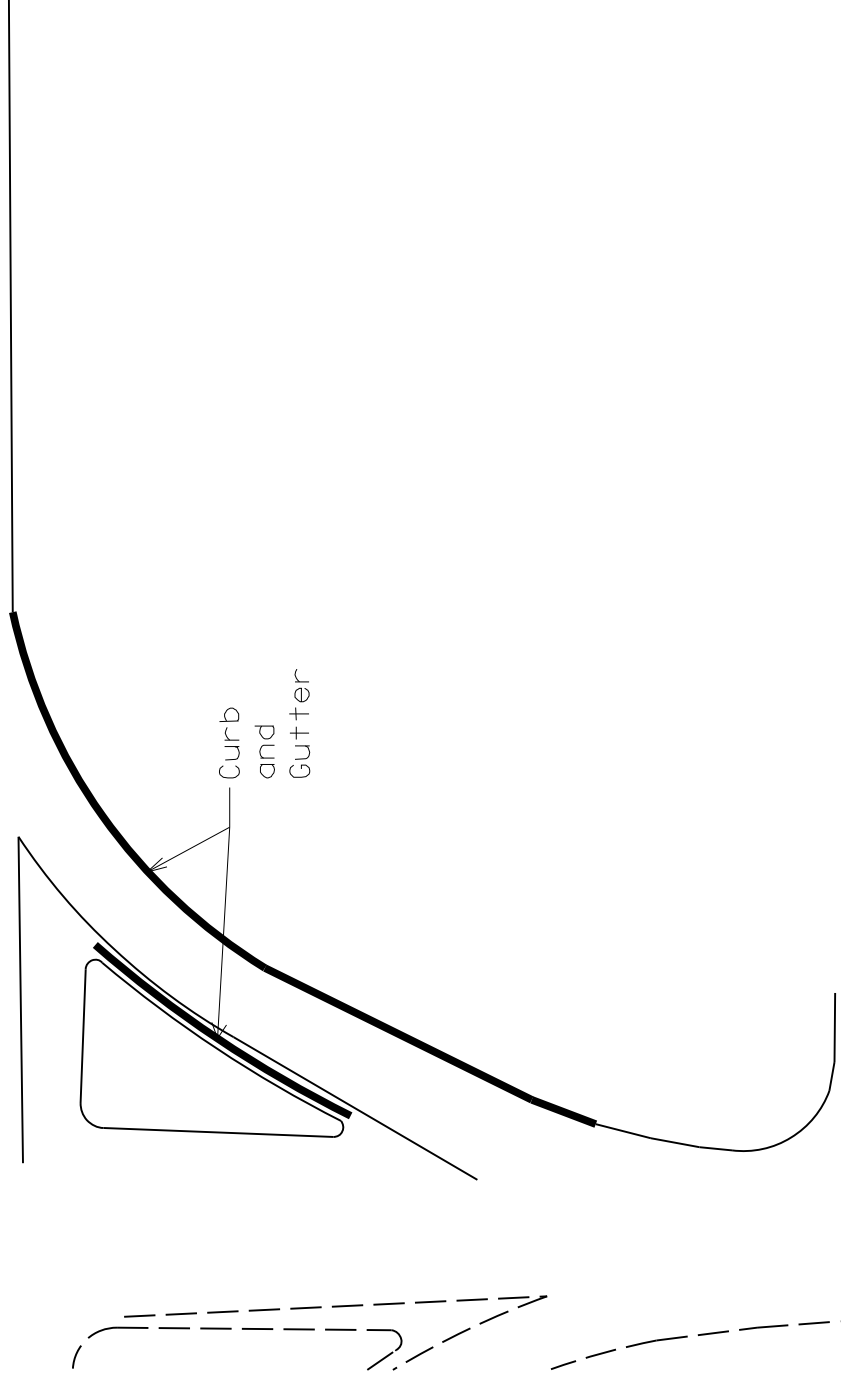


NH-0026-01(082)/108176-301

Lauderdale County

State BLVD Intersection Modification

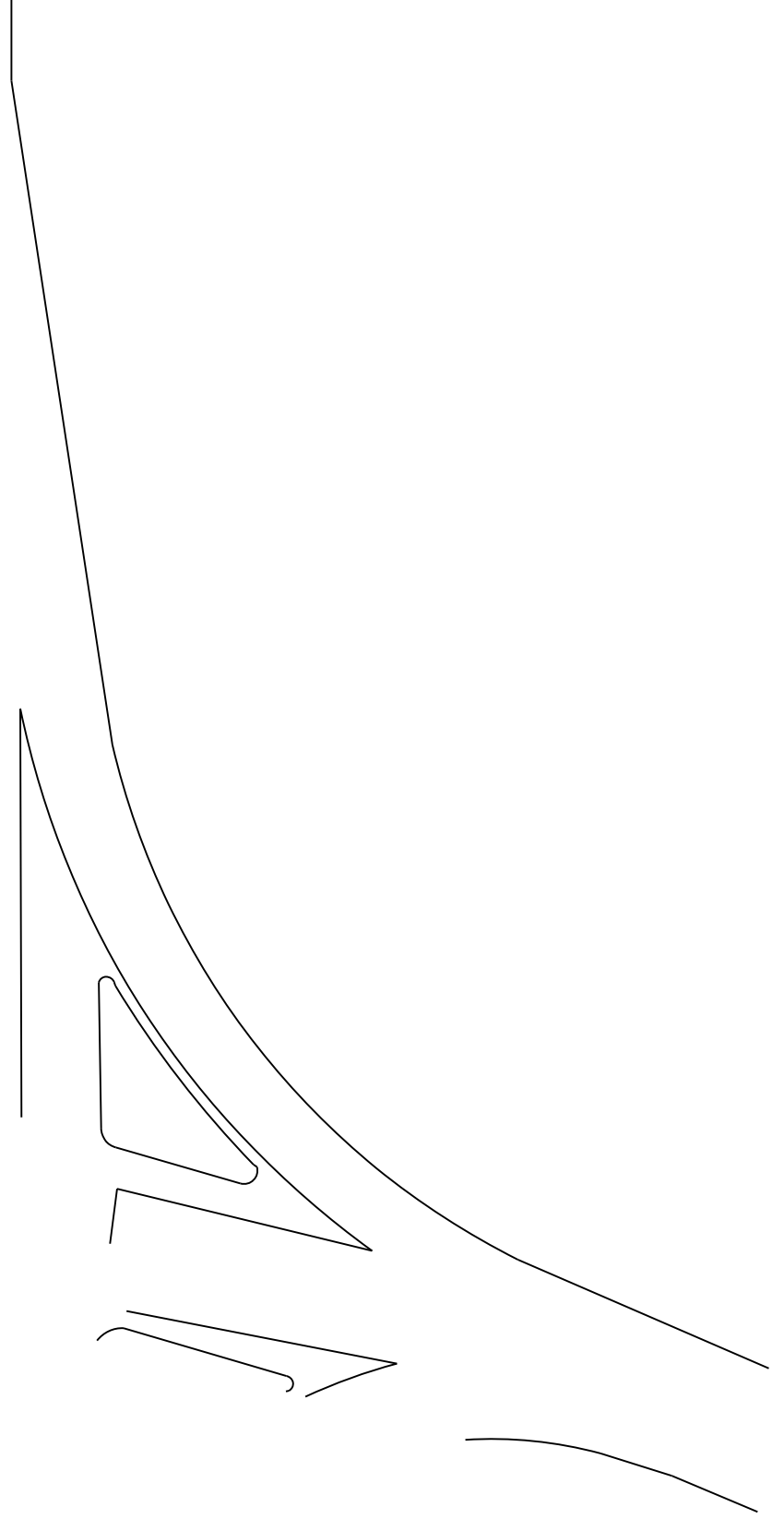
Location of Curb and Gutter



NH-0026-01(082)/108176-301
Lauderdale County

Hwy 494 Intersection
Modification

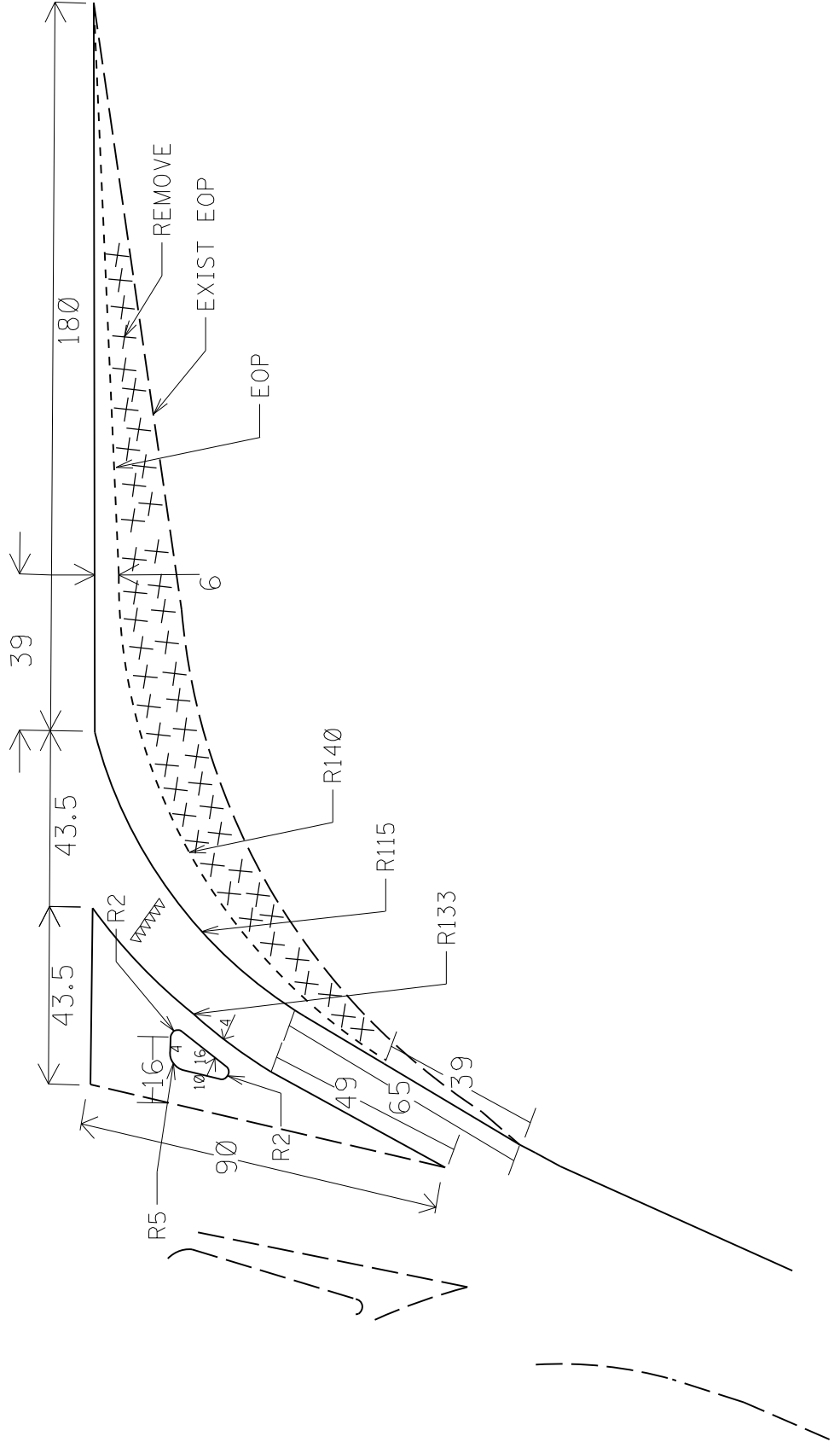
Existing



NH-0026-01(082)/108176-301 Lauderdale County

Hwy 494 Intersection Modification

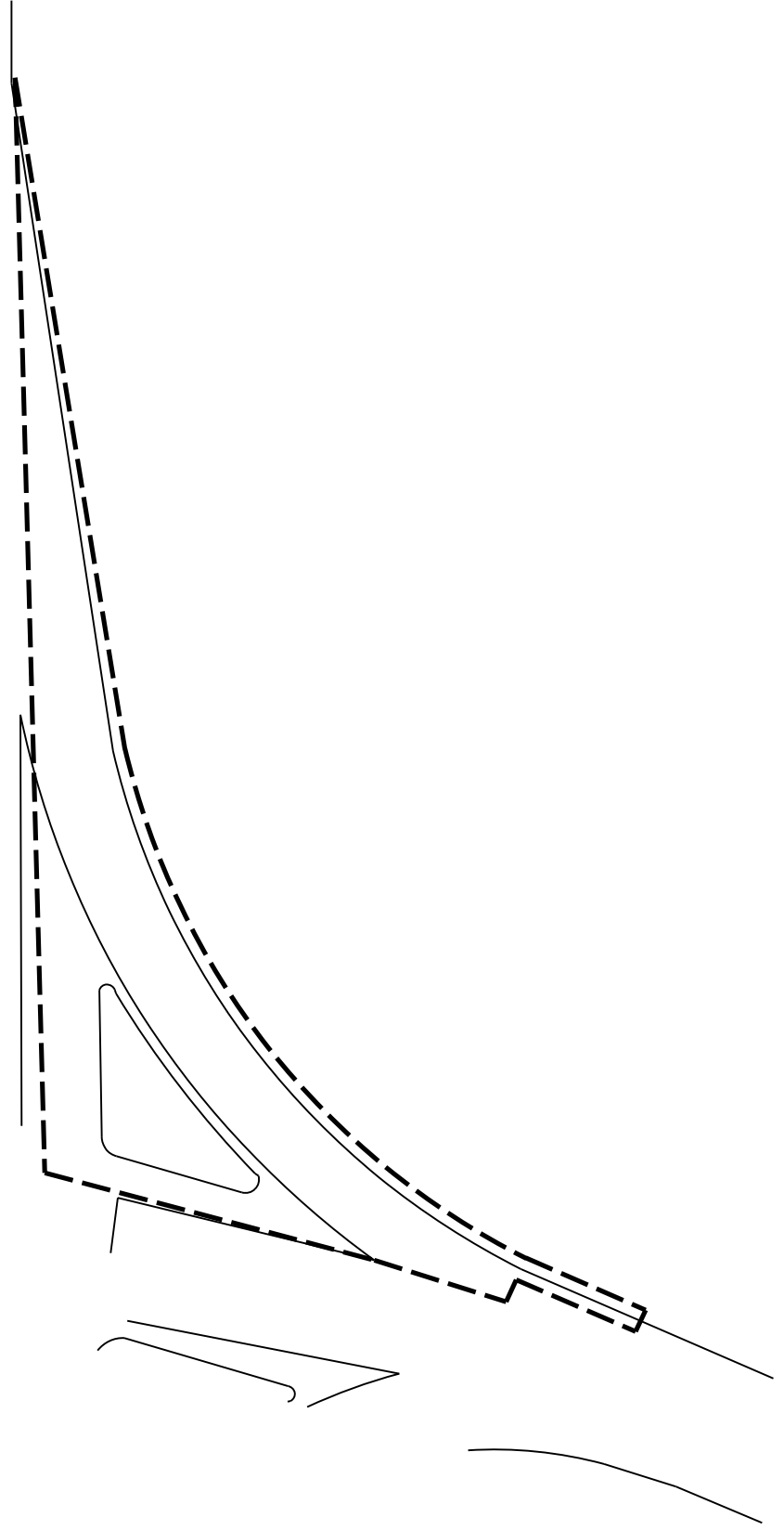
Proposed



NH-0026-01(082)/108176-301
Lauderdale County

Hwy 494 Intersection
Modification

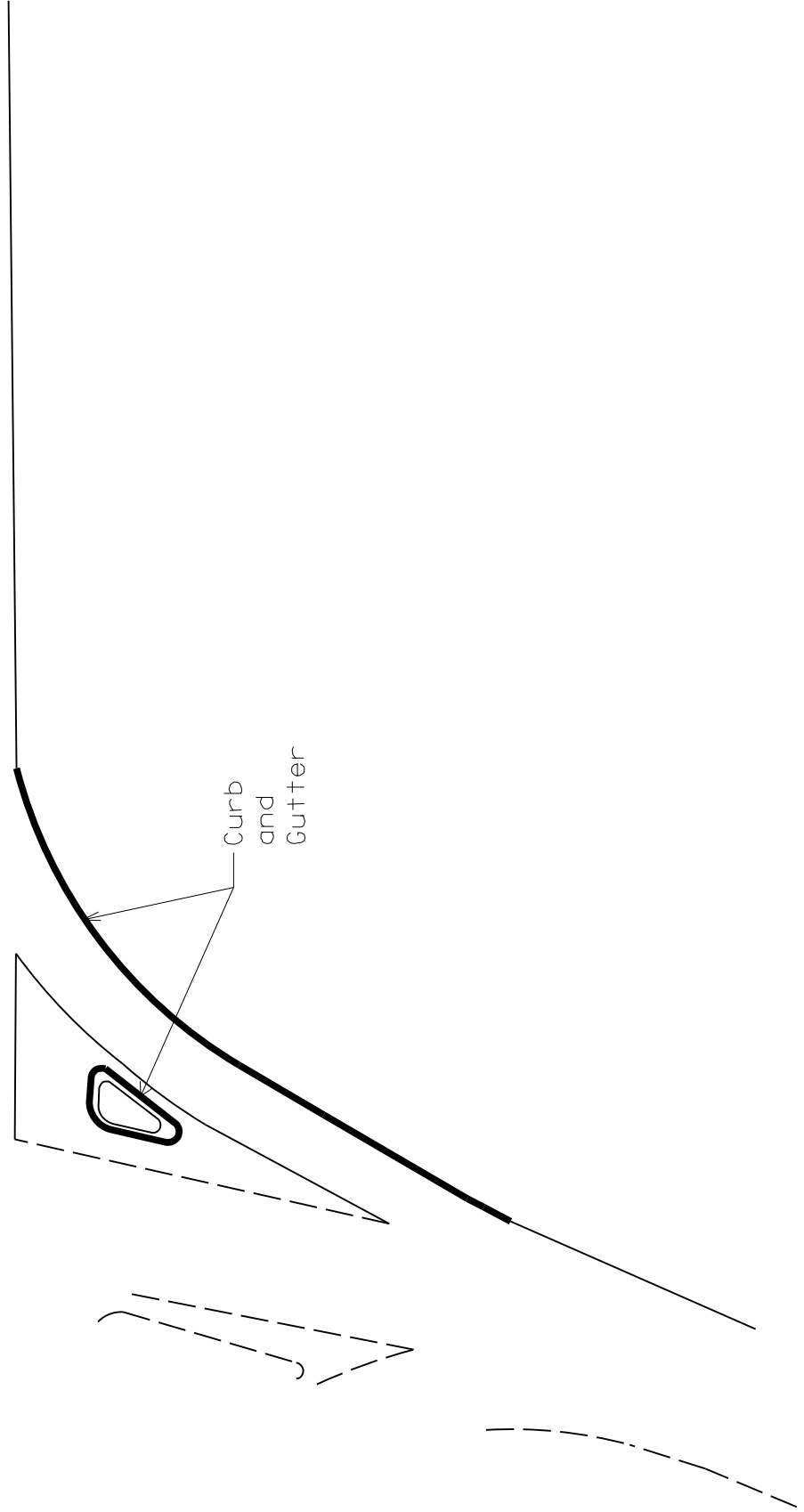
Removal Limits



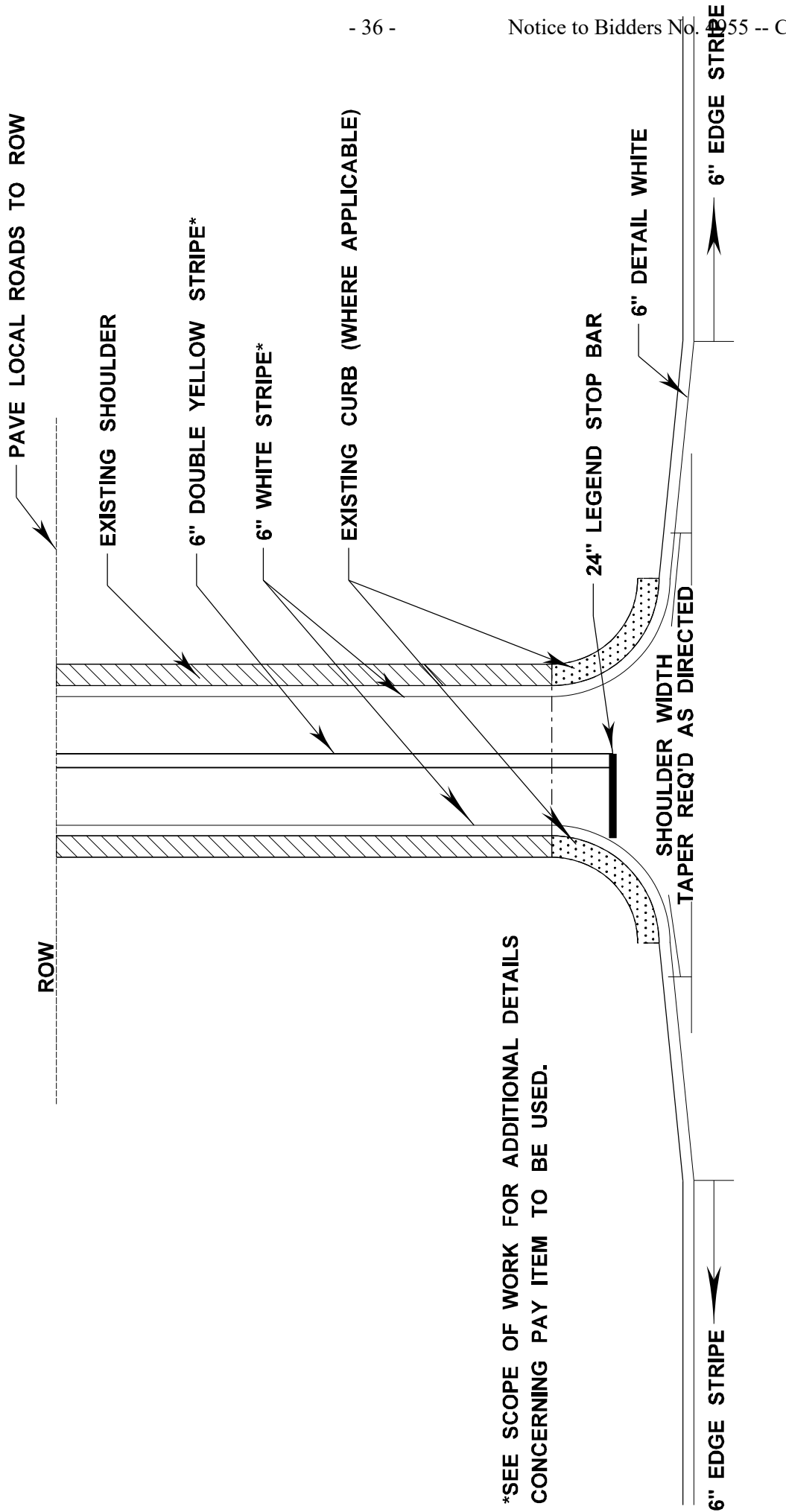
NH-0026-01(082)/108176-301 Lauderdale County

Hwy 494 Intersection Modification

Location of Curb and Gutter



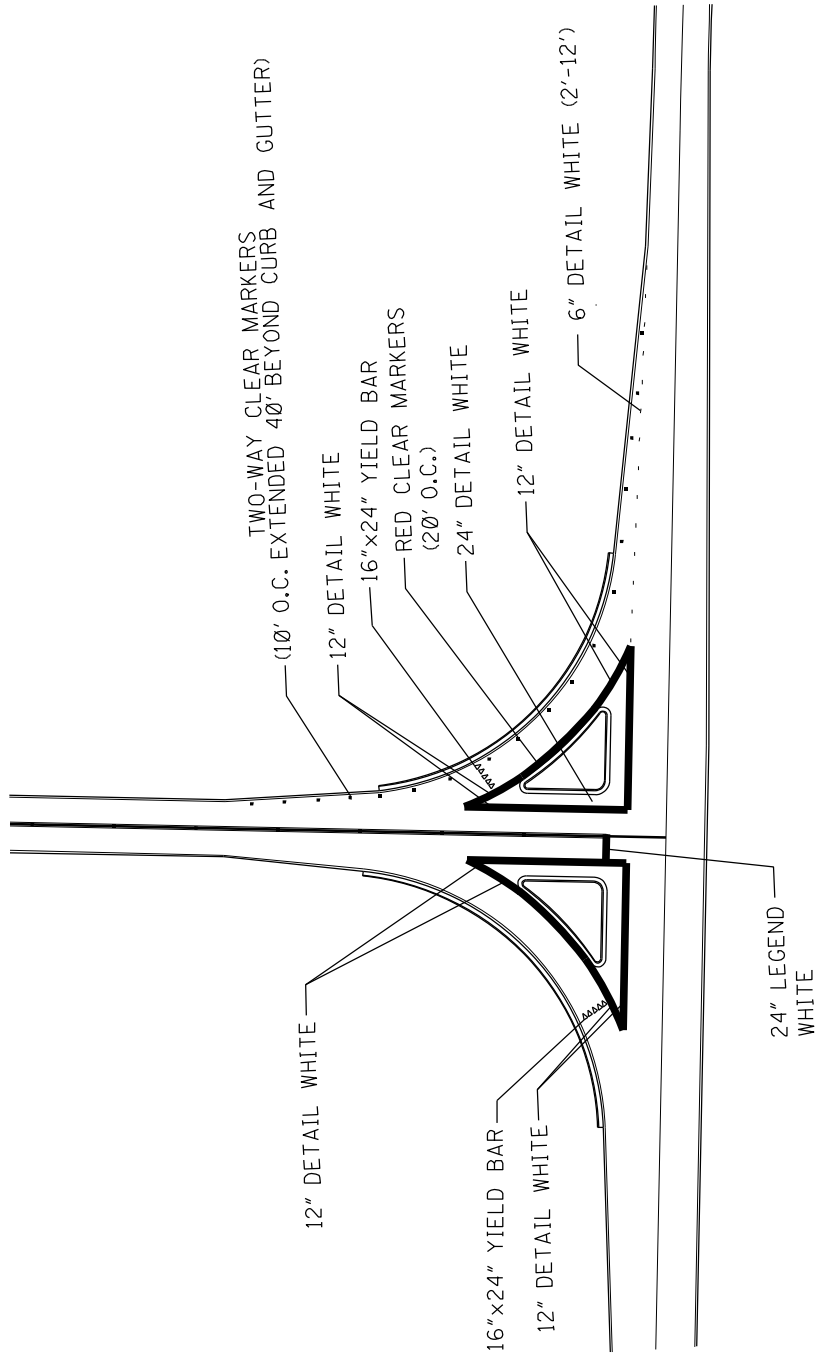
STRIPE DETAIL - LOCAL ROADS



*SEE SCOPE OF WORK FOR ADDITIONAL DETAILS CONCERNING PAY ITEM TO BE USED.

NOTE: CENTERLINE STRIPE SHALL BE OMITTED ON LOCAL ROADS WHOSE WIDTH IS LESS THAN 20 FEET.

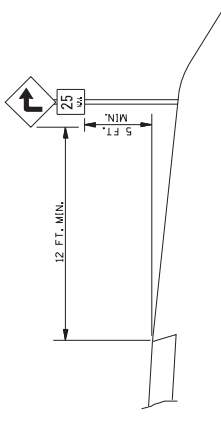
PAVEMENT MARKINGS FOR CHANNELIZED INTERSECTION



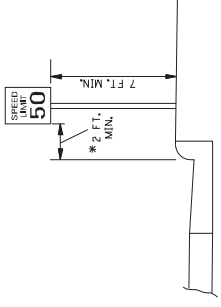
Locations

Chandler Rd
Old 8th Street Rd
State Blvd. Ext.
SR 494

WARNING SIGN WITH ADVISORY SPEED PLAQUE IN RURAL AREA

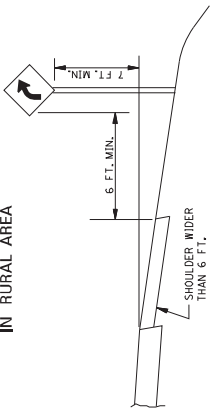


ROADSIDE SIGN IN BUSINESS, COMMERCIAL, OR RESIDENTIAL AREA

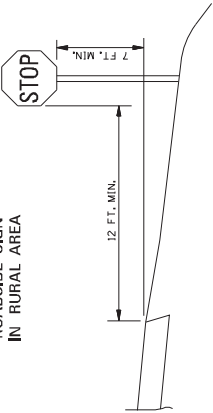


* THE 2 FT. MINIMUM OFFSET APPLIES ONLY TO STANDARD SIGNS MOUNTED ON U-POSTS. ALL STANDARD SIGNS MOUNTED ON PIPE WILL BE OFFSET A MINIMUM OF 4 FT. RAMP DESTINATION SIGNS WILL BE OFFSET 4 FT. FROM THE SHOULDERS.

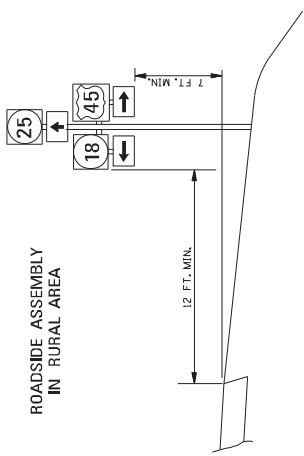
ROADSIDE SIGN IN RURAL AREA



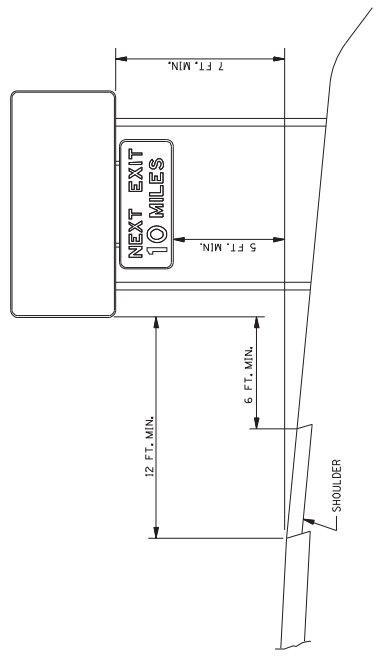
ROADSIDE SIGN IN RURAL AREA



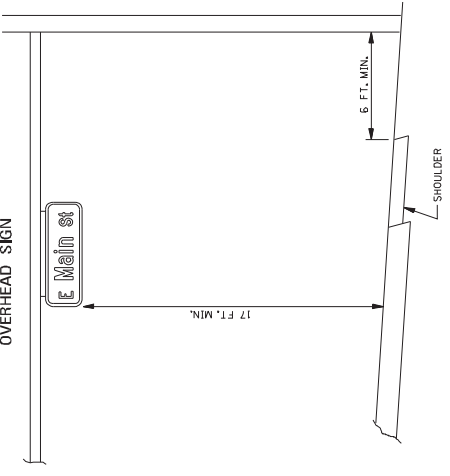
ROADSIDE ASSEMBLY IN RURAL AREA



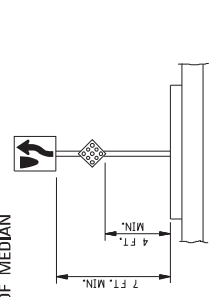
INTERSTATE OR FREEWAY SIGN WITH SECONDARY SIGN



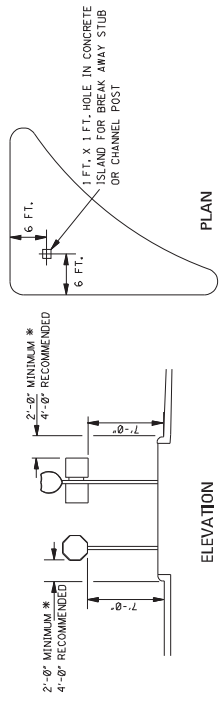
OVERHEAD SIGN



SIGN ON NOSE OF MEDIAN



SIGNS IN ISLANDS OR BEHIND CURB USING U-POSTS OR PIPE POSTS



PLAN

ELEVATION

* THE 2 FT. MINIMUM OFFSET APPLIES ONLY TO STANDARD SIGNS MOUNTED ON U-POSTS. ALL STANDARD SIGNS MOUNTED ON PIPE WILL BE OFFSET 4 FT. FROM THE SHOULDERS.

- GENERAL NOTES:
1. SEE SECTION 26-19 OF THE MUTCD FOR REDUCED LATERAL OFFSET DISTANCES THAT MAY BE USED ON INTERSTATE OR FREEWAY SIGNS. ON ALL OTHER SIGNS, INCLUDING RAMP DESTINATION SIGNS IN BUSINESS, COMMERCIAL, OR RESIDENTIAL AREAS WHERE SIDEWALK WIDTH IS LIMITED OR WHERE EXISTING POLES ARE CLOSE TO THE CURB, SIGNS SHALL BE LOCATED OUTSIDE THE CLEAR ZONE UNLESS PLACED ON A BREAKAWAY OR YIELDING SUPPORT.

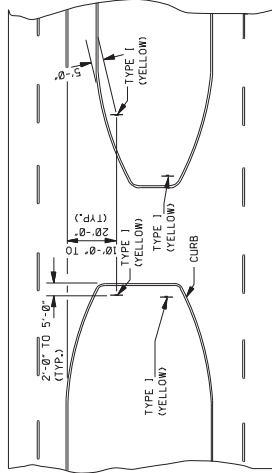
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

STANDARD SIGN ASSEMBLY AND INSTALLATION

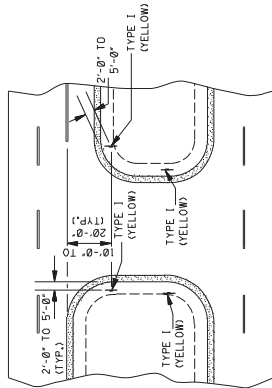
BY _____ REVISION _____

ISSUE DATE: AUGUST 01, 2017

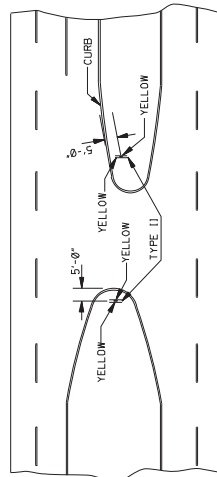
PROJECT NUMBER: 6306



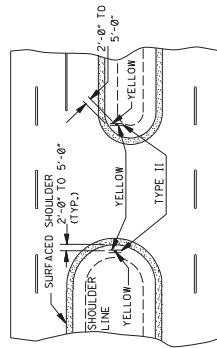
TYPICAL DELINEATION AT A CURBED CROSSOVER WITH A MEDIAN WIDTH OVER 42'-0"



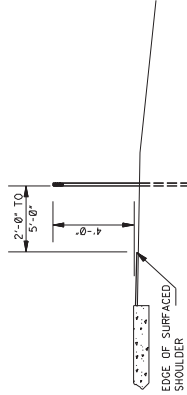
TYPICAL DELINEATION AT A CROSSOVER WITH USABLE SHOULDERS AND A MEDIAN WIDTH OVER 42'-0"



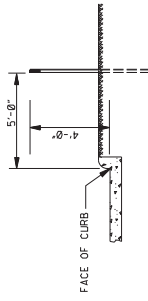
TYPICAL DELINEATION AT A CURBED CROSSOVER WITH A MEDIAN WIDTH OF 42'-0" OR LESS



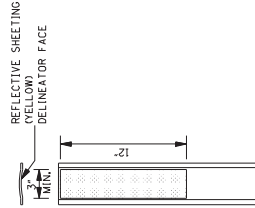
TYPICAL DELINEATION AT A CROSSOVER WITH USABLE SHOULDERS AND A MEDIAN WIDTH OF 42'-0" OR LESS



DELINEATOR MOUNTING ON CROSSOVER WITH USABLE SHOULDER

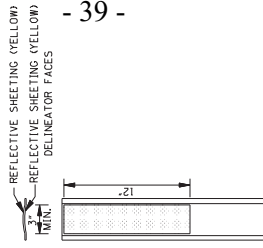


DELINEATOR MOUNTING ON CURBED CROSSOVER



DETAIL OF TYPE I FLEXIBLE POST DELINEATOR

NOTE: CARBONITE'S CURV-FLEX DELINEATOR POSTS ARE SHOWN. OTHER FLEXIBLE POSTS THAT HAVE BEEN APPROVED FOR LISTING IN THE DEPARTMENT'S "APPROVED SOURCE OF MATERIALS" MAY BE FURNISHED.



DETAIL OF TYPE II FLEXIBLE POST DELINEATOR

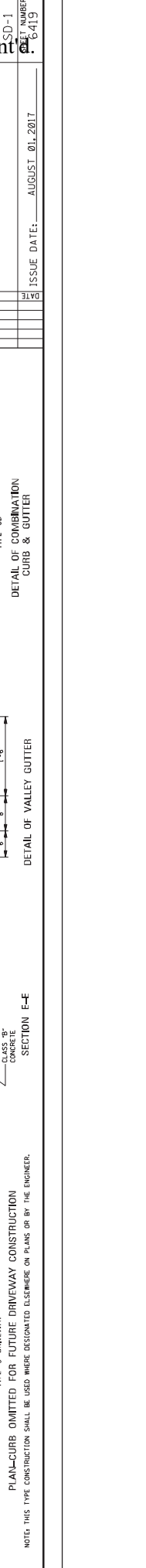
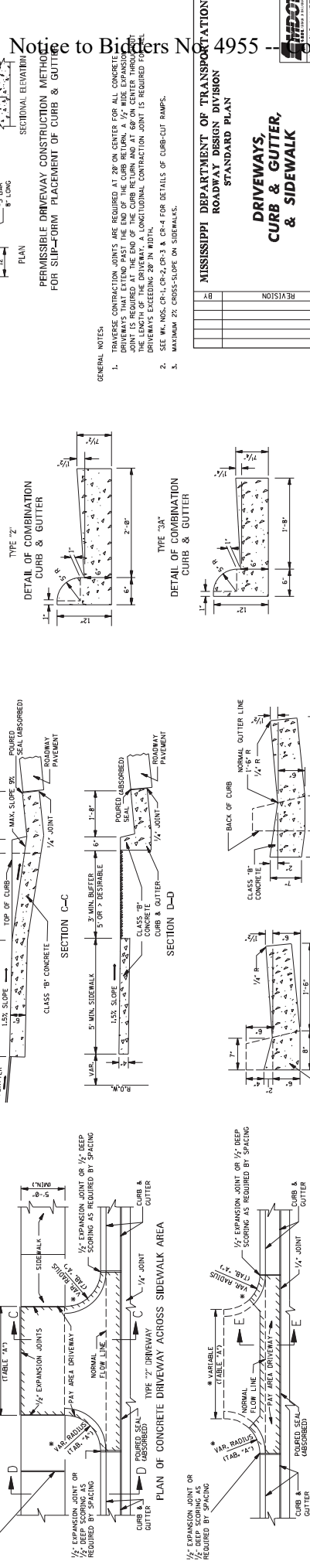
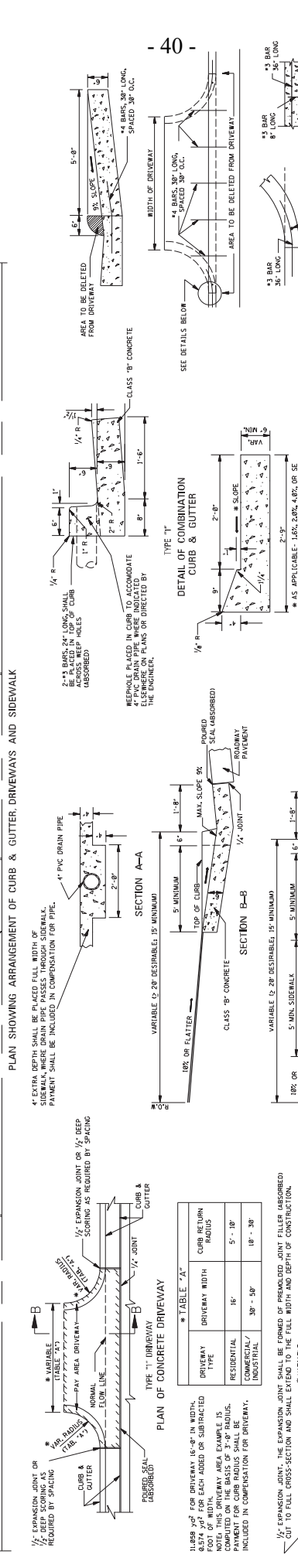
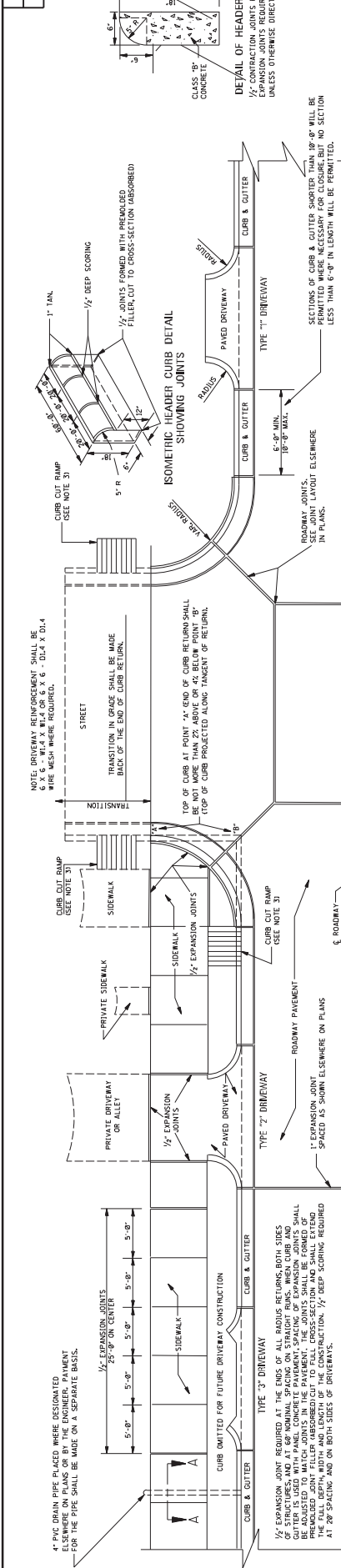
NOTE: PLACE DELINEATORS NO MORE THAN 28'-0" FROM EDGE OF TRAVEL LANES EDGES.

GENERAL NOTES:

1. THE UNIT PRICE OF DELINEATOR INCLUDES COST(S) OF DELINEATOR FACE(S), POST, HARDWARE AND INSTALLATION.
2. DELINEATOR FACE WILL BE ENCAPSULATED LENS REFLECTIVE SHEETING.
3. POST REQUIRING THE INSTALLATION OF A BASE SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATION.
4. THE COLOR OF DELINEATORS SHALL BE THE COLOR OF THE ADJACENT EDGELINE PER MUTCD SECTION 3F.03.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN		
BY	REVISION	DATE

**TYPICAL CROSSOVER
DELINEATION**



PERMISSIBLE DRIVEWAY CONSTRUCTION METHODS FOR SLIP-OFF™ PLACEMENT OF CURB & GUTTER

GENERAL NOTES:

1. TRANSVERSE CONTRACTION JOINTS ARE REQUIRED AT 20' ON CENTER FOR ALL CONCRETE DRIVEWAYS THAT EXTEND PAST THE END OF THE CURB RETURN. A 1/2" WIDE EXPANSION JOINT IS REQUIRED AT THE END OF THE CURB RETURN AND AT 60' ON CENTER THROUGH DRIVEWAYS EXCEEDING 20' IN WIDTH.
2. SEE W. NOS. CR-1, CR-2, CR-3 & CR-4 FOR DETAILS OF CURB-CUT RAMPS.
3. MAXIMUM 2% CROSS-SLOPE ON SIDEWALKS.

REVISION	DATE	ISSUE DATE:
		AUGUST 01, 2017

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**DRIVEWAYS,
CURB & GUTTER,
& SIDEWALK**

PROJECT NUMBER
SD-1
DRAWING NUMBER
6419

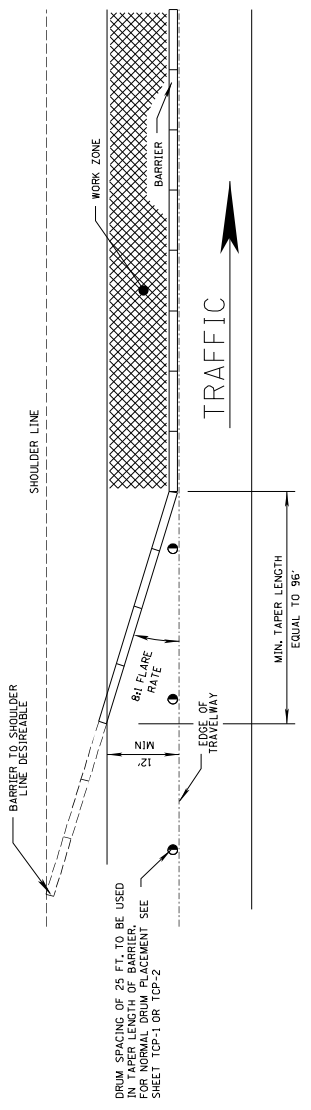
NOTE: THIS TYPE CONSTRUCTION SHALL BE USED WHERE DESIGNATED ELSEWHERE ON PLANS OR BY THE ENGINEER.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
LANE CLOSURE DETAILS FOR GREATER THAN 3 INCH DROPOFF

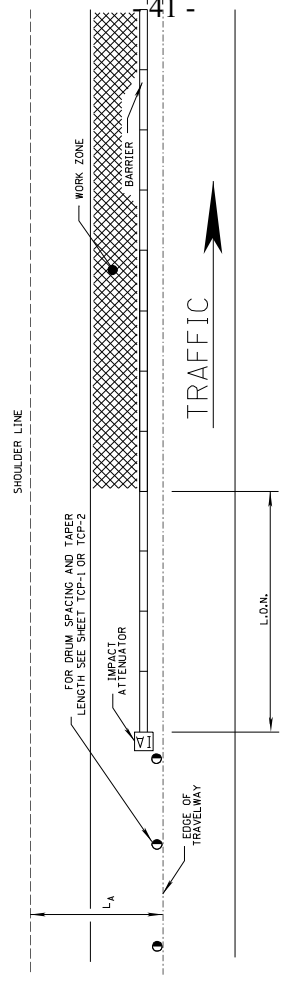
PROJ. NO.: _____
COUNTY: _____

FILE NAME: SDTCPC-ADDGN
DESIGN TEAM: _____
UPDATE DATE: _____
CHECKED DATE: _____

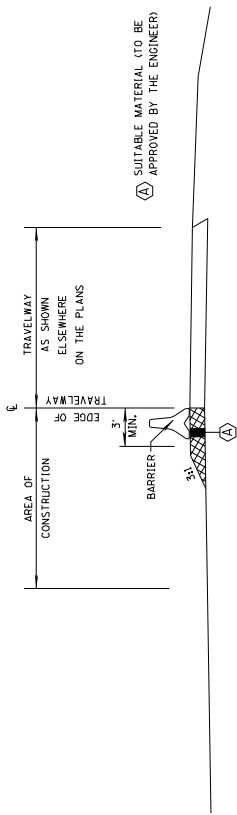
DATE _____
BY _____
REVISION _____



DETAIL OF POSITIVE BARRIER WITH TAPER

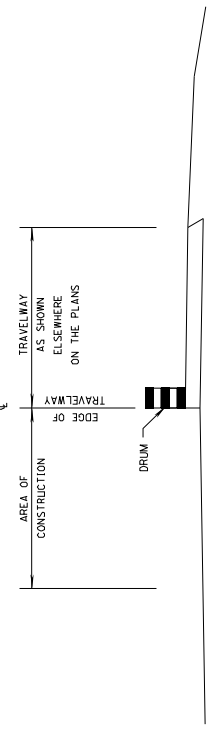


DETAIL OF POSITIVE BARRIER WITH IMPACT ATTENUATOR



ELEVATION VIEW FOR POSITIVE BARRIER

- NOTES:
1. POSITIVE BARRIER IS REQUIRED IN THE AREA OF OPEN PUNCH CUTS THAT ARE WITHIN SIX (6) FEET OF THE TRAVELWAY WHENEVER ACTUAL REPAIR WORK IS NOT BEING PERFORMED WITHIN THE LANE CLOSURE.
 2. MATERIAL USED TO SUPPORT POSITIVE BARRIER MUST BE AT SAME ELEVATION AS PAVEMENT IN ADJACENT TRAVELWAY.
 3. DELINEATORS REQUIRED ON ALL NON-REFLECTIVE BARRIER, AS SHOWN ON WORKING NO. OMB-3.



ELEVATION VIEW FOR DRUM

- NOTES:
1. WHILE WORK IS BEING PERFORMED WITHIN THE LANE CLOSURE DROPOFFS MUST BE PROTECTED WITH DRUMS, ETC. IN EMERGENCIES EXCAVATED SECTION MAY BE BACKFILLED WITH GRANULAR MATERIAL TO AVOID OVERNIGHT DROPOFFS.
 2. LANE CLOSURES WITH OPEN PUNCH OUT AREAS MAY NOT BE LEFT UNATTENDED WHEN DRUMS ARE BEING USED FOR LANE CLOSURE

GENERAL NOTES:

1. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER OTHER BID ITEMS.
2. FOR DETAILS OF DRUM PLACEMENT SEE OTHER TRAFFIC CONTROL PLANS.

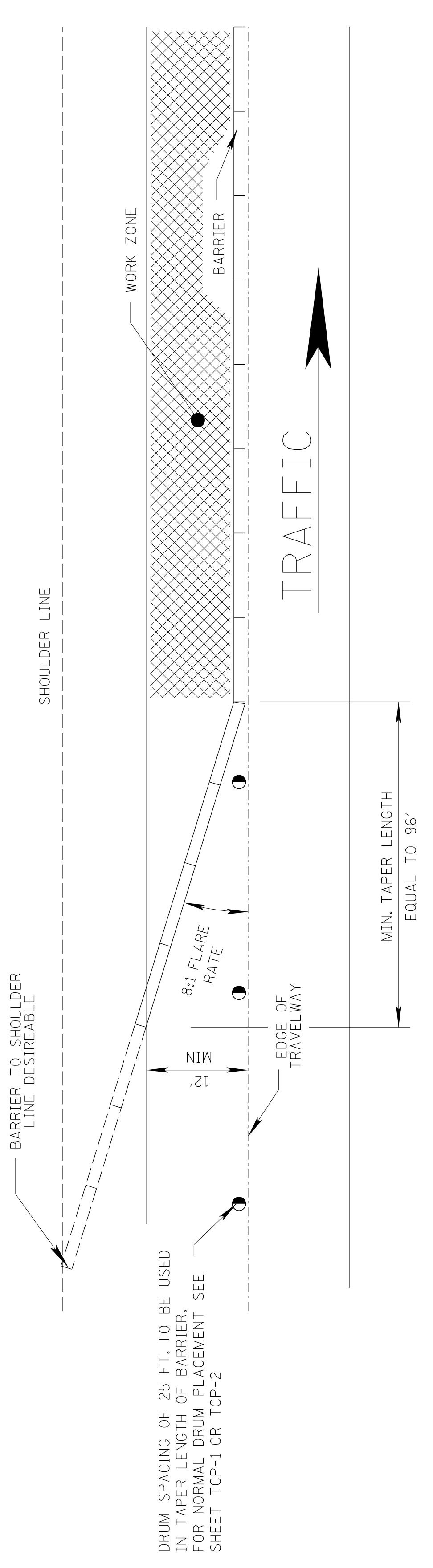
NOTES:

1. LENGTH OF NEED, L.O.N. = $\frac{L_1 \cdot V_{L_1}^2 + L_2^2}{L_1}$

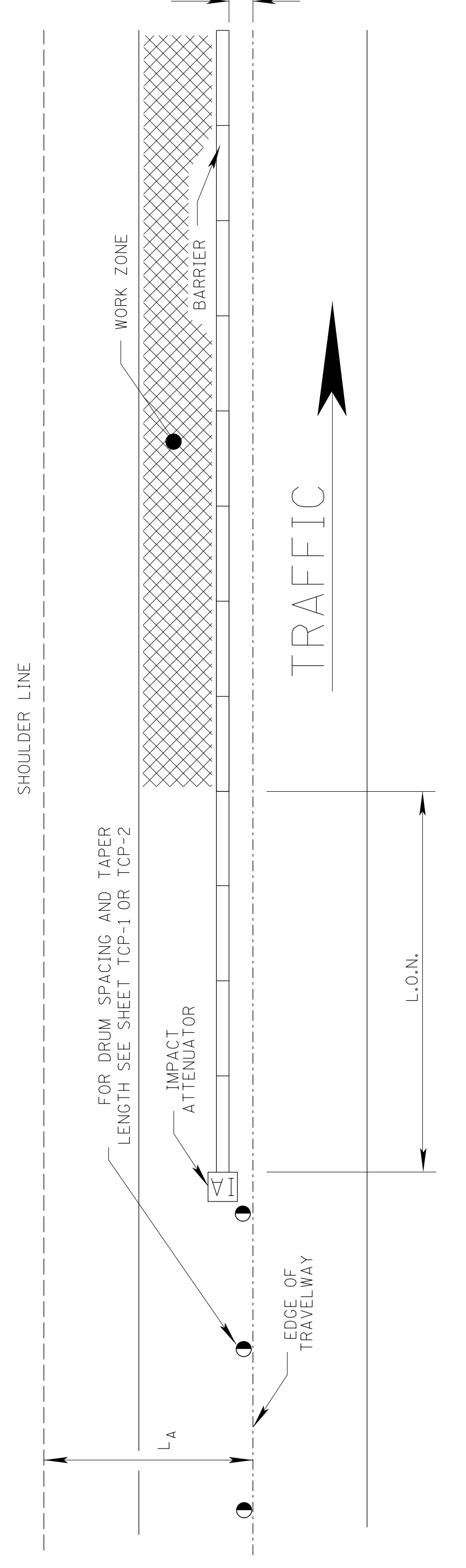
WHERE: L_1 = LATERAL EXTENT OF THE AREA OF CONCERN
 L_2 = RUNOUT LENGTH
 L_2 = LATERAL OFFSET FROM EDGE OF TRAVELED WAY TO BARRIER.

2. RUNOUT LENGTH (L_2) IS TO BE DETERMINED USING THE FOLLOWING TABLE:

DESIGN SPEED (mph)	RUNOUT LENGTH (L_2) GIVEN TRAFFIC VOLUME (ADT) (vfp)	
	OVER 10,000 vfp/day	UNDER 10,000 vfp/day
70	360	330
60	300	250
50	230	190
40	160	130
30	110	90
		80
		70



DETAIL OF POSITIVE BARRIER WITH TAPER



DETAIL OF POSITIVE BARRIER WITH IMPACT ATTENUATOR

NOTES:
1. LENGTH OF NEED, L.O.N. = $\frac{L_R(L_A - L_2)}{L_A}$

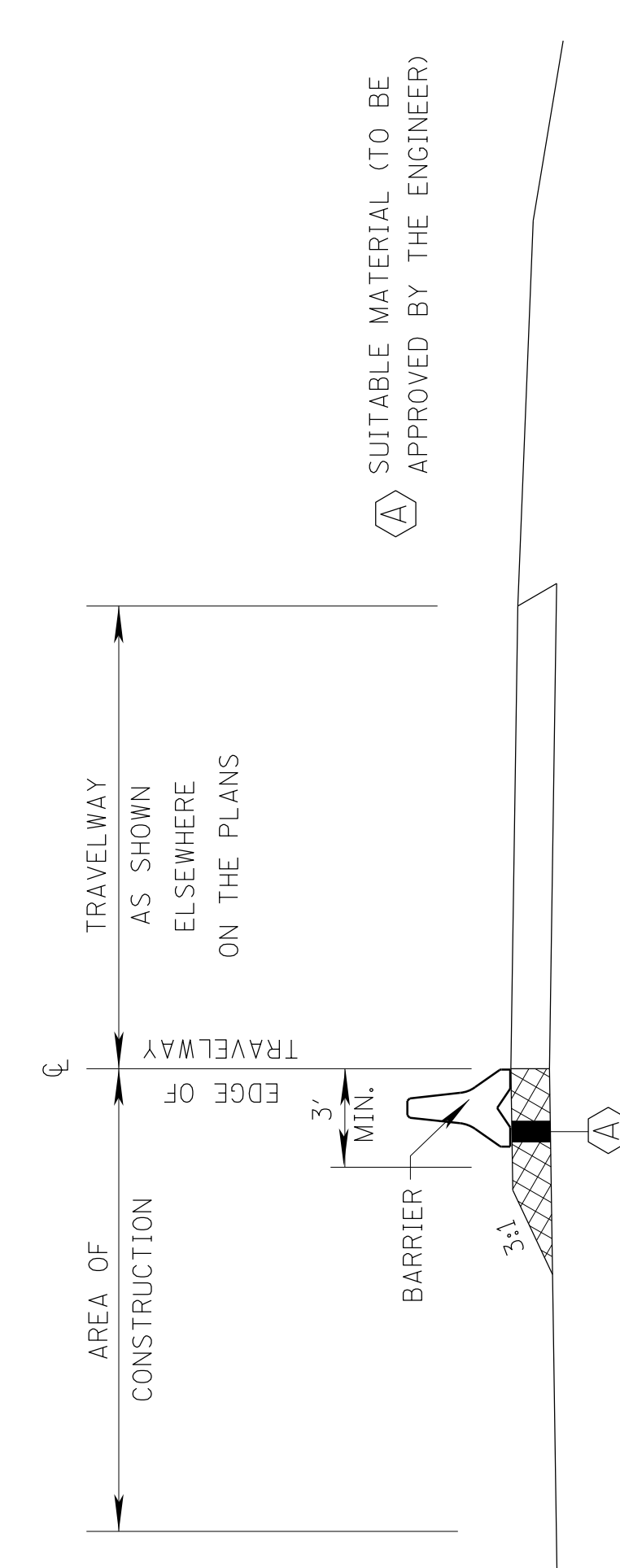
WHERE: L_A = LATERAL EXTENT OF THE AREA OF CONCERN
 L_R = RUNOUT LENGTH
 L_2 = LATERAL OFFSET FROM EDGE OF TRAVELED WAY TO BARRIER.

GENERAL NOTES:

1. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER OTHER BID ITEMS.
2. FOR DETAILS OF DRUM PLACEMENT SEE OTHER TRAFFIC CONTROL PLANS.

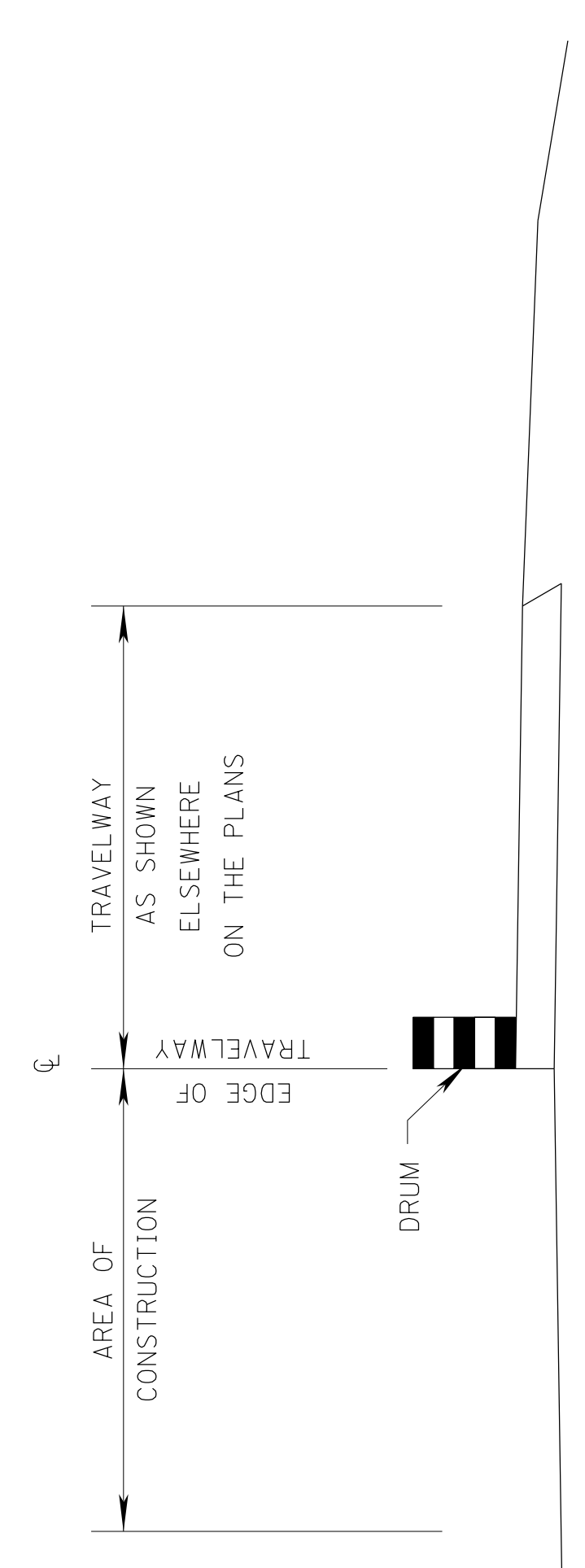
2. RUNOUT LENGTH (L_R) IS TO BE DETERMINED USING THE FOLLOWING TABLE:

DESIGN SPEED (mph)	RUNOUT LENGTH (L_R) GIVEN TRAFFIC VOLUME (ADT) (FT)			
	OVER 10,000 veh/day	5,000-10,000 veh/day	1,000-5,000 veh/day	UNDER 1,000 veh/day
70	360	330	290	250
60	300	250	210	200
50	230	190	160	150
40	160	130	110	100
30	110	90	80	70



ELEVATION VIEW FOR POSITIVE BARRIER

- NOTES:
1. POSITIVE BARRIER IS REQ'D IN THE AREA OF OPEN PUNCH OUTS THAT ARE WITHIN SIX (6) FEET OF THE TRAVELWAY WHENEVER ACTUAL REPAIR WORK IS NOT BEING PERFORMED WITHIN THE LANE CLOSURE.
 2. MATERIAL USED TO SUPPORT POSITIVE BARRIER MUST BE AT SAME ELEVATION AS PAVEMENT IN ADJACENT TRAVELWAY.
 3. DELINEATORS REQUIRED ON ALL NON-REFLECTIVE BARRIER, AS SHOWN ON WORKING NO. CMB-3.



ELEVATION VIEW FOR DRUM

- NOTES:
1. WHILE WORK IS BEING PERFORMED WITHIN THE LANE CLOSURE, DROP-OFFS MUST BE PROTECTED WITH DRUMS, ETC. IN EMERGENCIES, EXCAVATED SECTION MAY BE BACKFILLED WITH GRANULAR MATERIAL, STONE OR OTHER APPROVED MATERIAL TO AVOID OVERNIGHT DROP-OFFS.
 2. LANE CLOSURES WITH OPEN PUNCH OUT AREAS MAY NOT BE LEFT UNATTENDED WHEN DRUMS ARE BEING USED FOR LANE CLOSURE.

Notice to Bidders No. 4955

LANE CLOSURE DETAILS FOR GREATER THAN 3 INCH DROPOFF

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

PROJ. NO.: UPDATE
COUNTY: UPDATE

PRELIMINARY NOT FOR CONSTRUCTION
WORKING NUMBER SDTCP-03
SHEET NUMBER

FILENAME: SDTCP-C.DGN
DESIGN TEAM UPDATE CHECKED UPDATE DATE UPDATE

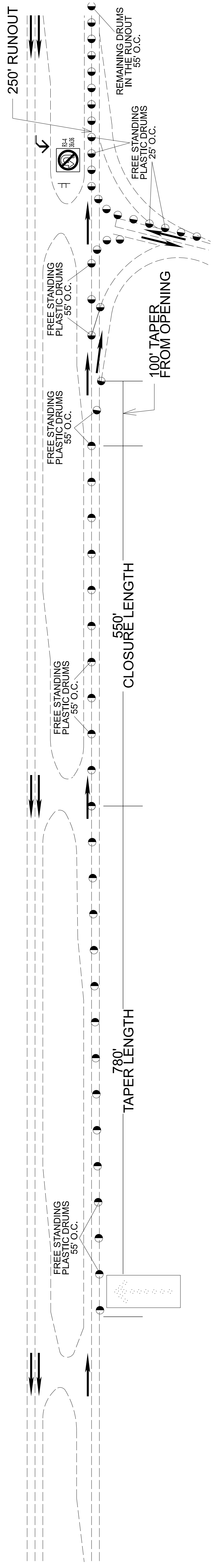
\$PG\$



DESIGNED BY:	
DETAILED BY:	
CHECKED BY:	
DATE:	
FMS CON: /	
PROJECT NO.:	
COUNTRY:	

OUTSIDE LANE CLOSURE

WK. NO. SDTCP-D
SHEET NO. 0



- NO MODIFICATIONS SHALL BE ALLOWED EXCEPT AS DIRECTED BY THE ENGINEER.
- ADJACENT CROSS-OVERS SHALL BE CLOSED AS DIRECTED BY THE ENGINEER AND SHALL ADHERE TO SPECIAL DESIGN SHEET SDTCP-E.
- THE WORK AREA TRAFFIC CONTROL SHALL ADHERE TO SPECIAL DESIGN SHEET SDTCP-C.
- ALL ITEMS WILL BE PAID FOR USING THE APPROPRIATE PAY ITEMS.

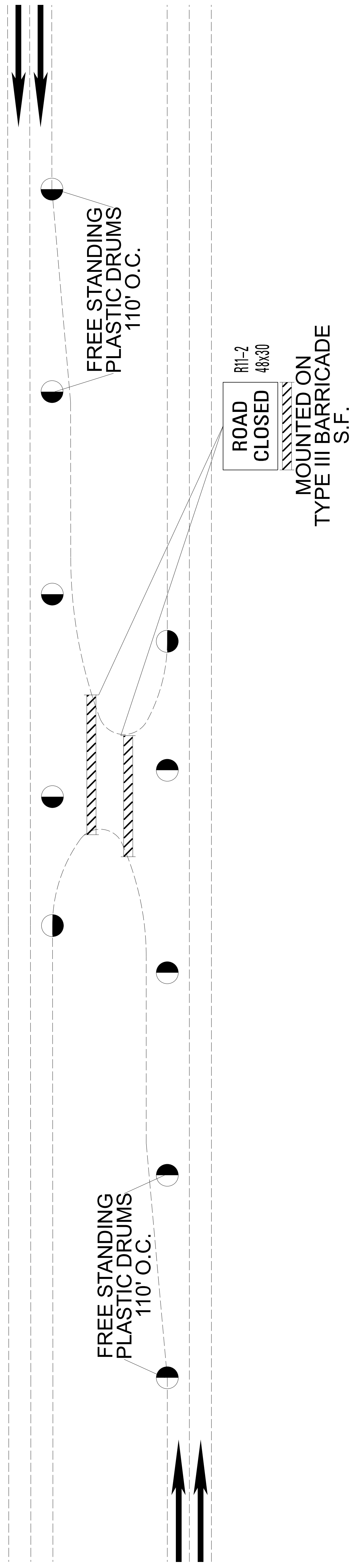


DESIGNED BY: _____
 DETAILED BY: _____
 CHECKED BY: _____
 DATE: _____

FMS CON: / _____
 PROJECT NO.: _____
 COUNTY: _____

CROSS-OVER CLOSURE

WK. NO. **SDTCP-E**
 SHEET NO. **0**



- NO MODIFICATIONS SHALL BE ALLOWED EXCEPT AS DIRECTED BY THE ENGINEER.
- ALL ITEMS WILL BE PAID FOR USING THE APPROPRIATE PAY ITEMS.

Mill & Overlay approximately 5 miles of SR 19 beginning approximately 0.5 miles north of North Hills Street to 0.2 miles north of SR 494, known as Federal Aid Project No. NH-0026-01(082) / 108176301 in Lauderdale County.

Line No.	Item Code	Adj Code	Quantity	Units	Description [Fixed Unit Price]
Roadway Items					
0010	201-D001		34	Station	Random Clearing
0020	202-B007		1,259	Square Yard	Removal of Asphalt Pavement, All Depths
0030	202-B088		342	Linear Feet	Removal of Curb & Gutter, All Types
0040	202-B096		620	Linear Feet	Removal of Debris and Sand From Box Culvert, 10-foot and Greater Width
0050	202-B158		2,975	Linear Feet	Removal of Guard Rail, Including Rails, Posts and Terminal Ends
0060	202-B168		246	Square Yard	Removal of Island Pavement, All Types
0070	202-B240		5,964	Linear Feet	Removal of Traffic Stripe
0080	202-B264		8	Each	Removal of Trees Greater Than 10"
0090	202-B273		210	Linear Feet	Removal of Debris and Sand From Box Culvert, 6-foot to Less than 10-foot Width
0100	202-B276		125	Linear Feet	Removal of Debris from Drainage Channel
0110	203-G002	(E)	506	Cubic Yard	Excess Excavation, LVM, AH
0120	304-D002	(GT)	100	Ton	Granular Material, Crushed Stone
0130	403-A002	(BA1)	21,205	Ton	12.5-mm, MT, Asphalt Pavement
0140	403-A005	(BA1)	600	Ton	19-mm, MT, Asphalt Pavement
0150	403-B002	(BA1)	320	Ton	12.5-mm, MT, Asphalt Pavement, Leveling
0160	406-D001		186,955	Square Yard	Fine Milling of Bituminous Pavement, All Depths
0170	407-A001	(A2)	18,695	Gallon	Asphalt for Tack Coat
0180	423-A001		15	Mile	Rumble Strips, Ground In
0190	503-C010		731	Linear Feet	Saw Cut, Full Depth
0200	606-B003		1,869	Linear Feet	Guard Rail, Class A, Type 1, 'W' Beam, Metal Post
0210	606-C003		1	Each	Guard Rail, Cable Anchor, Type 1
0220	606-D020		14	Each	Guard Rail, Bridge End Section, Type H, Metal Post
0230	606-E005		14	Each	Guard Rail, Terminal End Section, Flared
0240	606-E007		1	Each	Guard Rail, Terminal End Section, Non-Flared
0250	609-D003	(S)	500	Linear Feet	Combination Concrete Curb and Gutter Type 2
0260	616-A001	(S)	17	Square Yard	Concrete Median and/or Island Pavement, 10-inch
0270	618-A001		1	Lump Sum	Maintenance of Traffic
0280	619-A1001		18	Mile	Temporary Traffic Stripe, Continuous White
0290	619-A2001		16	Mile	Temporary Traffic Stripe, Continuous Yellow
0300	619-A3001		30	Mile	Temporary Traffic Stripe, Skip White
0310	619-A5001		64,260	Linear Feet	Temporary Traffic Stripe, Detail
0320	619-A6001		262	Square Feet	Temporary Traffic Stripe, Legend

Line No.	Item Code	Adj Code	Quantity	Units	Description [Fixed Unit Price]
0330	619-A6002		2,592	Linear Feet	Temporary Traffic Stripe, Legend
0340	619-D1001		50	Square Feet	Standard Roadside Construction Signs, Less than 10 Square Feet
0350	619-D2001		438	Square Feet	Standard Roadside Construction Signs, 10 Square Feet or More
0360	619-G4005		228	Linear Feet	Barricades, Type III, Single Faced
0362	619-G5001		250	Each	Free Standing Plastic Drums
0364	619-E1001		2	Each	Flashing Arrow Panel, Type C
0370	620-A001		1	Lump Sum	Mobilization
0380	626-A001		10	Mile	6" Thermoplastic Double Drop Traffic Stripe, Skip White
0390	626-C002		9	Mile	6" Thermoplastic Double Drop Edge Stripe, Continuous White
0400	626-E001		8	Mile	6" Thermoplastic Double Drop Traffic Stripe, Continuous Yellow
0410	626-G004		18,061	Linear Feet	Thermoplastic Double Drop Detail Stripe, White
0420	626-G005		14,069	Linear Feet	Thermoplastic Double Drop Detail Stripe, Yellow
0430	626-H001		131	Square Feet	Thermoplastic Double Drop Legend, White
0440	626-H002		1,296	Linear Feet	Thermoplastic Double Drop Legend, White
0450	627-J001		946	Each	Two-Way Clear Reflective High Performance Raised Markers
0460	627-K001		1,000	Each	Red-Clear Reflective High Performance Raised Markers
0470	627-L001		70	Each	Two-Way Yellow Reflective High Performance Raised Markers
0480	630-A001		10	Square Feet	Standard Roadside Signs, Sheet Aluminum, 0.080" Thickness
0490	630-A003		96	Square Feet	Standard Roadside Signs, Sheet Aluminum, 0.125" Thickness
0500	630-C001		56	Linear Feet	Square Tube Posts, 4.0 lb/ft
0510	630-F002		36	Each	Delineators, Flexible Post Mounted, Crossover, Type I, Yellow
0520	630-F006		53	Each	Delineators, Guard Rail, White
0530	630-F007		47	Each	Delineators, Guard Rail, Yellow
0540	630-G003		7	Each	Type 3 Object Markers, OM-3L, Post Mounted
0550	630-G007		8	Each	Type 3 Object Markers, OM-3R, Post Mounted
Bridge Items					
0560	907-808-A002	(S)	52	Linear Feet	Joint Repair
0570	907-808-A003	(S)	1,700	Linear Feet	Joint Repair Without Epoxy
0580	907-823-A001		1,248	Linear Feet	Preformed Joint Seal, Type I
0590	907-823-A002		505	Linear Feet	Preformed Joint Seal, Type II
0600	907-823-B001		2,496	Linear Feet	Saw Cut, Type I
0610	907-823-B002		1,010	Linear Feet	Saw Cut, Type II