

**SECTION 905 -- PROPOSAL (CONTINUED)**

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO. <u>  1  </u>	DATED <u>  4/19/2023  </u>	ADDENDUM NO. <u>          </u>	DATED <u>          </u>
ADDENDUM NO. <u>          </u>	DATED <u>          </u>	ADDENDUM NO. <u>          </u>	DATED <u>          </u>
ADDENDUM NO. <u>          </u>	DATED <u>          </u>	ADDENDUM NO. <u>          </u>	DATED <u>          </u>

Number	Description
1	Revised NTB No. 4879; Revised Bid Items; Amendment EBSx Download Required.

TOTAL ADDENDA:   1    
(Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE \_\_\_\_\_

\_\_\_\_\_  
Contractor

BY \_\_\_\_\_  
Signature

TITLE \_\_\_\_\_

ADDRESS \_\_\_\_\_

CITY, STATE, ZIP \_\_\_\_\_

PHONE \_\_\_\_\_

FAX \_\_\_\_\_

E-MAIL \_\_\_\_\_

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of \_\_\_\_\_ and the names, titles and business addresses of the executives are as follows:

_____ President	_____ Address
_____ Secretary	_____ Address
_____ Treasurer	_____ Address

The following is my (our) itemized proposal.

MP-3018-11(010)/ 308321301000 & MP-3018-15(004)/ 308321302000  
Claiborne & Copiah County(ies)

Revised 01/26/2016

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 – NOTICE TO BIDDERS NO. 4879**

**CODE: (SP)**

**DATE: 04/11/2023**

**SUBJECT: Scope of Work**

**PROJECT: MP-3018-11(010) / 308321301 & MP-3018-15(004) / 308321302 - Claiborne and Copeiah Counties**

The contract documents do not include an official set of construction plans, but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, “Standard Drawings.”

**MISSISSIPPI HIGHWAY 18  
MILL & OVERLAY FROM 0.4 MILES WEST OF LEMON ROAD  
TO 1.0 MILES WEST OF CARPENTER ROAD  
LOG MILE 18.986 TO 24.561 -- CLAIBORNE COUNTY  
LOG MILE 0.000 TO 2.301 -- COPIAH COUNTY  
OMIT SECTION 0.880-2.046 COPIAH COUNTY**

In general, the work to be accomplished using the pay items and corresponding specifications set forth in this contract is to mill & overlay approximately 6.7 miles of SR 18 in Claiborne & Copeiah Counties from 0.4 miles west of Lemon Road to 1.0 miles west of Carpenter Road. (There is an omitted section between log mile 0.880 and 2.046 in Copeiah County.)

Work on the project shall consist of the following:

1. The Contractor shall erect and maintain construction signing, provide all signs, and traffic handling devices in accordance with the Traffic Control Plan. The costs for this work are to be included in the price bid for pay item number 618-A, Maintenance of Traffic. All traffic control devices on this project should comply with the latest version of the MUTCD. Fluorescent orange sheeting shall be used on all construction and traffic control signs except for those designated in the plans to be black legend and border on white background. The Contractor will be required to use 42-inch channelizer cones with 6-inch wide reflective tape and 16 pound vertical panel bases for each cone.
2. The Contractor shall remove any failed areas on the main facility as directed by the Project Engineer using the following construction sequence:

**NOTE:** Repair of failed areas shall be done before the milling of lanes occurs.

- a) Saw cut full depth through the asphalt pavement.
- b) Remove failed asphalt pavement.

- c) Remove any unsuitable material in the subgrade as directed by the engineer. The removal of this material will be paid for as excess excavation.
- d) Backfill and stabilize failed area with 19-mm, ST, Asphalt Pavement, Leveling. The final grade of asphalt shall match the existing grade of the pavement. No lift of 19-mm, ST, Asphalt Pavement, Leveling shall be greater than 3" or as designated by the Project Engineer. Densities will not be required on this asphalt. Contractor shall roll each lift to refusal or as directed by Project Engineer.
- e) All repairs must be complete by the end of the workday and the lane closures must be removed from the roadway so that all lanes of travel are open thereafter.

NOTE: See the included Pavement Repair Sheet.

- 3. The Contractor shall underseal the roadway and shoulders adjacent to each end of Bridges #13.1 and #17.4. The undersealing shall extend from the bridge end out to 25 feet. This work will be paid under the pay item 907-420-A: Underseal. (See Special Provision 907-420).
- 4. The Contractor shall fine mill SR 18 to a depth of 1½". The main roadway shall be milled on a 2% cross slope or the appropriate super-elevation in each direction from the center line. Milling shall be performed at bridge ends, local roads, aprons, and other areas as designated by the Project Engineer to ensure smooth transitions of new overlay with existing grade. It is the Contractor's responsibility to ensure the drainage of surface water from the milled areas including the use of shoulder cuts.

NOTE: The deck of Bridge #13.2 is overlaid with asphalt pavement. Based on recommendations from Bridge Division, the following will pertain to the work on Bridge #13.2, and this work will only be modified by the approval of the Engineer.

- a) The Contractor shall core the existing asphalt overlay located on these bridge decks to determine its thickness in multiple locations on both the centerline and edge of the travel lanes. The results shall be reported to the Engineer.
- b) A milling machine with a milling head no larger than four (4) feet wide may be used to remove the asphalt overlay to a depth such that the milling head does not come in contact with the bridge deck; however, the asphalt thickness remaining on the Bridge after the milling operation shall not be less than ½ ". Once this depth is reached, the Engineer shall examine the remaining asphalt to determine whether it requires removal. If the remaining asphalt appears to have a sufficient bond the bridge deck, then the ½" lift may be left in place upon approval of the Engineer.
- c) If the remaining asphalt is not sufficiently bonded to the bridge deck, the Contractor must use another approach, to be approved by the Engineer, to remove the remainder of the asphalt in a way that does not harm the existing structure. (Some bridge joints may have steel armor in place.)

- d) The Contractor shall place a new lift of asphalt on the bridge deck. The new asphalt lift shall be 1½" of 9.5-mm, ST, asphalt. After paving, the Contractor will saw and seal the joints.

NOTE: The use of dynamic rollers is prohibited for compaction of the asphalt lift on the bridge deck. Only static compaction shall be allowed.

NOTE: The asphalt removal from the bridge decks will be paid for under the milling pay item in the proposal.

NOTE: Any damage that occurs to the existing structure during the construction process shall be repaired as directed by the Engineer at no additional cost to the State.

NOTE: Temporary wedges of full lane width asphalt pavement shall be placed by the Contractor immediately after the milling process to allow the safe transition of traffic. The length of the wedges will be three feet (3') for every ½ inch in height. These wedges shall be maintained in a satisfactory condition by the Contractor until the permanent asphalt pavement is placed. All costs for placing and maintaining these wedges shall be absorbed in other pay items. Cold mix will not be allowed.

NOTE: All bridges within the project limits (inclusive of the omitted section) shall be swept clean of debris. All costs for sweeping bridges shall be absorbed in other pay items.

5. The Contractor shall place 1½" of 9.5-mm, ST, asphalt on the milled surface of SR 18 having a two percent (2%) cross slope or the appropriate super elevation rate in each direction from the centerline. Any work to control the laydown equipment for proper placement of the asphalt in the superelevated curves shall be absorbed by the Contractor at no additional cost to the State. The asphalt shall be placed in 12-foot wide passes on the main roadway and in widths as necessary at intersections and other areas where the pavement width varies.

Local roads shall be paved to the right of way or as directed by the Project Engineer. Aprons shall be constructed at existing ramps that do not have paved aprons by placing 3" of 9.5-mm, ST asphalt in widths and lengths as directed by the Project Engineer. Existing paved aprons shall be paved to match final main line grades. Any site grading at local roads or drives will not be measured for separate payment but will be considered an absorbed item.

6. Temporary striping shall conform to finished stripe specifications for alignment, reflectivity, straightness, and neatness. Temporary stripe shall be placed as needed for safe movement of traffic. All permanent pavement markings are to be hot thermoplastic. Edge lines will be placed to maintain a 12-foot lane width.

NOTE: On bridge #17.4, the old traffic stripe shall be removed and replaced with thermoplastic stripe as per the specifications in the Standard Specifications. The removal of stripe pay item is for stripe removal on concrete surfaces.

7. Existing shoulders shall be raised to match the new pavement by grading existing material and/or placing any needed granular material; all to be bladed and dressed to a finished slope of 4%.

Note: Any existing low shoulders or at any time there is a differential more than 2", the Contractor shall raise the shoulder grade up to the current asphalt grade. The Contractor may pull up existing shoulder material if possible or place new granular material. Incidental work such as removing vegetation, shaping, and compacting shoulders including the base for paved aprons, and other incidental work that is necessary to complete the work will not be measured for separate payment and the cost will be included in the items bid.

8. New guard rail shall be installed using the pay items provided in the proposal. New Type 3 object markers shall be installed at the guardrail locations.

NOTE: See the included Guardrail sheet.

9. Raised pavement markers shall be placed at 80-foot intervals in tangents and 40-foot intervals in curves and urban areas along the centerline or roadway. Existing raised pavement markers shall be removed prior to the placement of asphalt and shall be considered an absorbed item of work.

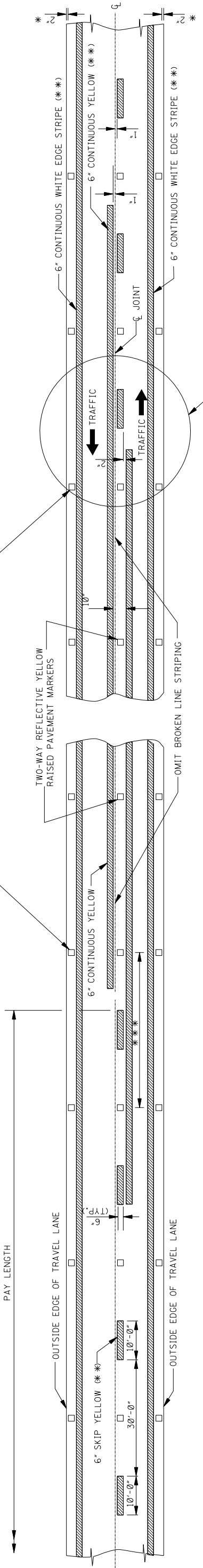
Edge line raised pavement markers will also be placed on this project. The markers for this work will be paid under the 627-J: Two-Way Clear marker pay item in the proposal. (See the included sheet RPM-1, Edge Line Raised Pavement Marking Placement).

**PAVEMENT REPAIR**

<b>No.</b>	<b>Lane</b>	<b>Log Mile</b>	<b>Lat.</b>	<b>Long</b>	<b>Length (LF)</b>	<b>Width (LF)</b>	<b>Area (SY)</b>	<b>Excess Excav. (CY)</b>	<b>Sawcut (LF)</b>	<b>19mm Asp. (Ton)</b>
1	CL	19.664	31.99626	-90.79662	15	6	10	3	42	7
2	EB	20.143	31.99639	-90.78858	15	6	10	3	27	7
							<b>20</b>	<b>6</b>	<b>69</b>	<b>14</b>

Log Mile	GUARD RAIL															
	Lane		Removal		w-beam		Terminals		Br. End Sect.		Br. End Sect.		Delinator		Type 3	
	Location		LF		LF	EA	Non-flared	EA	Type A Modified	EA	Type Mod	EA	White	EA	Obj. Marks.	EA
19.200	Box Culvert	EB	RT	163		87.5	2						3		2	
19.200	Box Culvert	WB	LT	163		87.5	2						3		2	
20.090	Br. #13.1	EB	SW	219		137.5	1	1					6		1	
20.090	Br. #13.1	WB	NW	119		37.5	1	1					3		1	
20.090	Br. #13.1	EB	SE	119		37.5	1	1					3		1	
20.090	Br. #13.1	WB	NE	219		137.5	1	1					6		1	
24.384	Br. #17.4	EB	SW	213		150.0	1			1			5		1	
24.384	Br. #17.4	WB	NW	213		150.0	1			1			5		1	
24.384	Br. #17.4	EB	SE	213		150.0	1			1			5		1	
24.384	Br. #17.4	WB	NE	213		150.0	1			1			5		1	
				<b>1854</b>		<b>1125.0</b>	<b>12</b>	<b>4</b>		<b>4</b>			<b>44</b>		<b>12</b>	

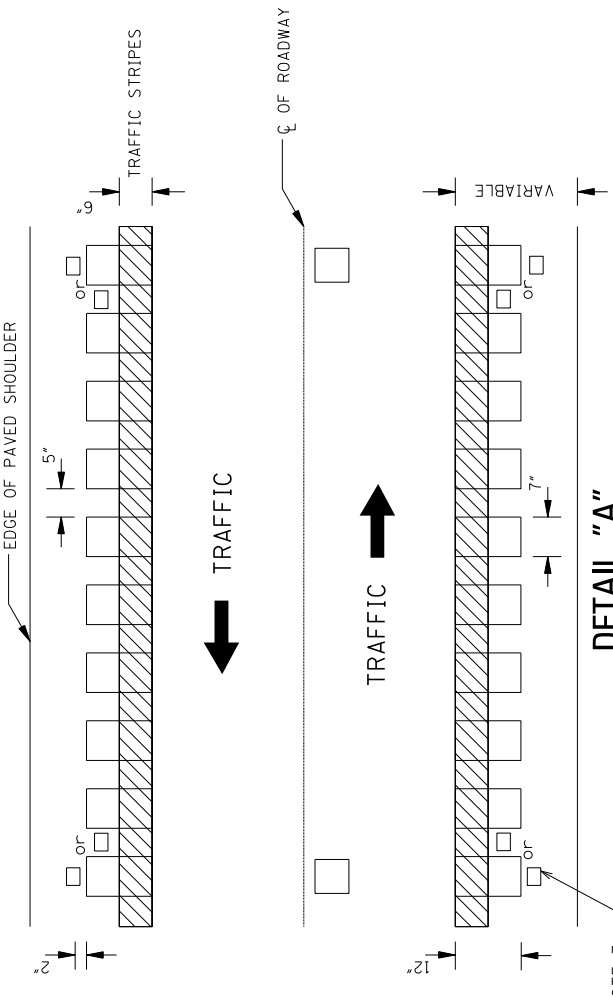
Note: Log Mile 7.645 is the intersection of SR 18 with US 61 in Port Gibson



**TWO-WAY TRAFFIC (ASPHALT OR CONCRETE PAVEMENT)**

- \* TWO-WAY TRAFFIC TWO-WAY CLEAR OR ONE-WAY CLEAR RAISED PAVEMENT MARKERS (AS SPECIFIED ELSEWHERE)

- \* TWO-WAY TRAFFIC TWO-WAY CLEAR OR ONE-WAY CLEAR RAISED PAVEMENT MARKERS (AS SPECIFIED ELSEWHERE)



**DETAIL "A" PLACEMENT OF RPMs ON PAVEMENTS WITH RUMBLE STRIPS**

**DETAIL "A" PLACEMENT OF RPMs ON PAVEMENTS WITHOUT RUMBLE STRIPS**

- GENERAL NOTES:
- \* 1. 2" UNLESS SHOWN ELSEWHERE ON THE PLANS, FOR STRIPING ON RUMBLE STRIP SECTIONS REFER TO WK. SHEETS RS-1, RS-2, AND RS-3.
  - \*\* 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR PLASTIC AS INDICATED IN PAY ITEMS).
  - \*\*\* 3. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

TANGENT SECTIONS	URBAN AREA (ft-in)	RURAL AREA (ft-in)
HORIZONTAL CURVES	40'-0"	80'-0"
INTERCHANGE LIMITS	40'-0"	+ 40'-0"

+ NOTE: ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINES THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.

- 4. EDGE LINE RAISED PAVEMENT MARKERS ARE TO BE PLACED AT THE SAME SPACING AS THE CENTERLINE MARKERS. EDGE LINE MARKERS WILL BE OMITTED WHERE CONFLICTS OCCUR WITHIN INTERSECTIONS, CROSSOVERS, ETC. EDGE LINE RPMs WILL NOT BE PLACED IN URBAN AREAS UNLESS OTHERWISE SPECIFIED
- 5. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MDT "APPROVED SOURCES OF MATERIALS."
- 6. THE CRITERIA FOR NO-PASSING ZONES CAN BE FOUND IN THE MDT ROADWAY DESIGN MANUAL, SECTION 11-1.01.
- 7. WHERE RUMBLE STRIPS ARE PRESENT, THE EDGE LINE MARKERS WILL BE PLACED OUTSIDE THE RUMBLE STRIPS OR BETWEEN THE RUMBLE STRIPS AS DIRECTED BY THE ENGINEER.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN

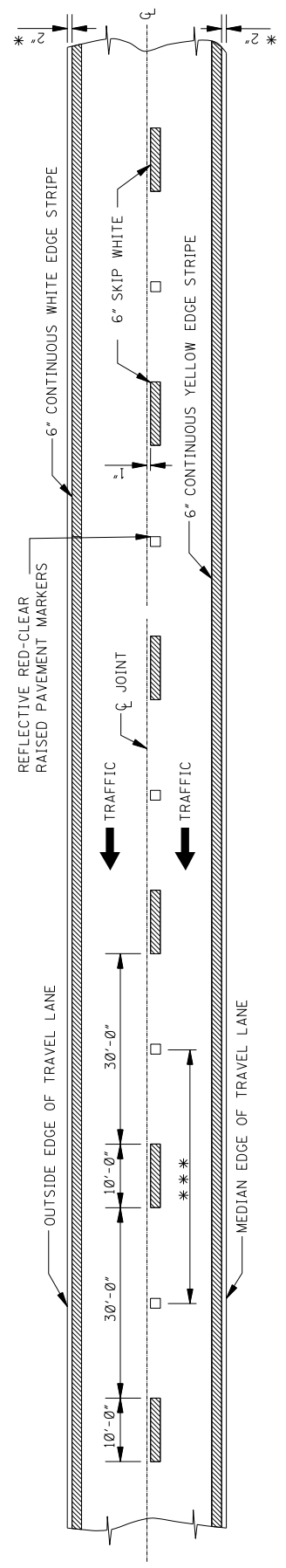
EDGE LINE RAISED PAVEMENT MARKING PLACEMENT 2-LANE ROADWAYS

PROJ. NO.: COUNTY:

WORKING NUMBER RPM-1 SHEET NUMBER

DATE: FILE NAME: RPM-1.dgn DESIGN TEAM: CHECKED: DATE:

**4-LANE WITH ONE-WAY TRAFFIC**





Mill & Overlay approximately 7 miles of SR 18 from 0.4 miles west of Lemon Road to 1 mile west of Carpenter Road, known as State Project Nos. MP-3018-11(010) / 308321301 & MP-3018-15(004) / 308321302 in Claiborne & Copiah Counties.

Line No.	Item Code	Adj Code	Quantity	Units	Description [Fixed Unit Price]
<b>Roadway Items</b>					
0010	202-B007		20	Square Yard	Removal of Asphalt Pavement, All Depths
0020	202-B136		1,854	Linear Feet	Removal of Guard Rail
0030	202-B240		500	Linear Feet	Removal of Traffic Stripe
0040	203-G001	(E)	6	Cubic Yard	Excess Excavation, FM, AH
0050	304-B004	(GT)	1,500	Ton	Granular Material, Class 5, Group D
0060	403-A015	(BA1)	8,400	Ton	9.5-mm, ST, Asphalt Pavement
0070	403-B006	(BA1)	14	Ton	19-mm, ST, Asphalt Pavement, Leveling
0080	406-D003		7,900	Ton	Fine Milling of Bituminous Pavement, All Depths
0090	407-A001	(A2)	6,400	Gallon	Asphalt for Tack Coat
0092	413-E001		150	Linear Feet	Sawing and Sealing Transverse Joints in Asphalt Pavement
0100	503-C010		69	Linear Feet	Saw Cut, Full Depth
0110	606-B001		1,125	Linear Feet	Guard Rail, Class A, Type 1
0120	606-D006		4	Each	Guard Rail, Bridge End Section, Type A Modified
0130	606-D025		4	Each	Guard Rail, Bridge End Section, Type Modified
0140	606-E007		12	Each	Guard Rail, Terminal End Section, Non-Flared
0150	618-A001		1	Lump Sum	Maintenance of Traffic
0160	618-B001		2	Square Feet	Additional Construction Signs [\$10.00]
0170	619-A1001		27	Mile	Temporary Traffic Stripe, Continuous White
0180	619-A2001		13	Mile	Temporary Traffic Stripe, Continuous Yellow
0190	619-A4002		7	Mile	Temporary Traffic Stripe, Skip Yellow
0200	619-A5001		1,300	Linear Feet	Temporary Traffic Stripe, Detail
0210	619-A6002		800	Linear Feet	Temporary Traffic Stripe, Legend
0220	620-A001		1	Lump Sum	Mobilization
0230	626-B002		14	Mile	6" Thermoplastic Double Drop Traffic Stripe, Continuous White
0240	626-D001		4	Mile	6" Thermoplastic Double Drop Traffic Stripe, Skip Yellow
0250	626-E001		6	Mile	6" Thermoplastic Double Drop Traffic Stripe, Continuous Yellow
0260	626-G004		1,600	Linear Feet	Thermoplastic Double Drop Detail Stripe, White
0270	626-G005		1,050	Linear Feet	Thermoplastic Double Drop Detail Stripe, Yellow
0280	626-H002		1,050	Linear Feet	Thermoplastic Double Drop Legend, White
0290	627-J001		1,500	Each	Two-Way Clear Reflective High Performance Raised Markers
0300	627-L001		678	Each	Two-Way Yellow Reflective High Performance Raised Markers
0310	630-F006		44	Each	Delineators, Guard Rail, White

<b>Line No.</b>	<b>Item Code</b>	<b>Adj Code</b>	<b>Quantity</b>	<b>Units</b>	<b>Description [Fixed Unit Price]</b>
0320	630-G005		12	Each	Type 3 Object Markers, OM-3R or OM-3L, Post Mounted
0330	907-420-A001		20,000	Pounds	Undersealing
0340	907-606-PP005		4	Each	Guard Rail, Bridge Connector, Per Plans
0350	907-619-B001		66	Linear Feet	Temporary Portable Rumble Strips