

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO. <u> 1 </u>	DATED <u> 5/22/2024 </u>	ADDENDUM NO. _____	DATED _____
ADDENDUM NO. _____	DATED _____	ADDENDUM NO. _____	DATED _____
ADDENDUM NO. _____	DATED _____	ADDENDUM NO. _____	DATED _____

Number	Description
1	Revised Table of Contents; S.P 907-202-5 replaced S.P 907-202-4; Added S.P 907-618-12; Revised or Added Plan Sheet Nos. 8002 & 8005; Amendment EBSx Download Required.

TOTAL ADDENDA: 1
(Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

Contractor

BY _____
Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

_____ President	_____ Address
_____ Secretary	_____ Address
_____ Treasurer	_____ Address

The following is my (our) itemized proposal.
 NHPP-0055-02(253)/ 107899301000 & NHPP-0055-02(260)/ 108384301000
 Hinds & Rankin County(ies)

Revised 01/26/2016

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION
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NHPP-0055-02(260)/108384301 - Rankin**

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**PROJECT: NHPP-0055-02(253)/107899301 - Hinds
NHPP-0055-02(260)/108384301 - Rankin**

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(REVISIONS TO THE ABOVE WILL BE INDICATED ON THE SECOND SHEET
OF SECTION 905 AS ADDENDA)

05/22/2024 10:57 AM

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-202-5

CODE: (SP)

DATE: 02/05/2024

SUBJECT: Removal of Bridge Deck With Rotomilling

Section 202, Removal of Structures and Obstructions, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction as amended by this special provision is applicable to hydrodemolition of bridge decks only.

907-202.01--Description. This work shall consist of the removal of bridge deck concrete using hydrodemolition equipment as preparation for bridge deck repairs or overlay. All work shall be performed in accordance with the details shown on the plans or as directed by the Engineer.

907-202.02--Materials and Equipment. Cold milling or mechanically scarifying equipment shall be self-propelled with sufficient power, traction, and stability and capable of uniformly removing the surface to the depths required in a satisfactory manner. The maximum cutting width of equipment allowed is 4 feet with a maximum weight of 25 tons.

The hydrodemolition equipment shall be a self-propelled machine that utilizes a high pressure water jet stream capable of removing concrete to the depths shown on the plans or as directed by the Engineer and be capable of removing rust and concrete particles from reinforcing steel. Hand-held wands or pneumatic hammers, 30-pound class maximum, shall be used to remove unsound concrete in areas that are inaccessible or inconvenient to the self-propelled machine, such as areas under reinforcing steel or around expansion joints. Pneumatic hammers and chipping tools exceeding a 15-pound class shall not be operated at an angle exceeding 45° relative to the surface of the bridge deck. Such tools may be started in the vertical position but must be immediately tilted to a 45° operation angle.

907-202.03--Construction Requirements. Prior to milling the contractor shall field verify the depth of the reinforcing steel. If the use of milling results in exposing, snagging, or dislodging reinforcing steel, the milling depth shall be reduced as necessary immediately. If exposing, snagging, or dislodging of reinforcing steel cannot be avoided, milling shall be stopped immediately and the remaining removal shall be accomplished using the hydrodemolition equipment. One-quarter inch (1/4") to one-half inch (1/2") cover above the reinforcing steel shall remain after milling. All damage to the reinforcing steel resulting from the Contractor's operation shall be repaired or replaced at the Contractor's expense as directed by the Engineer.

Prior to the commencement of the removal operation, the hydrodemolition equipment shall be calibrated on an area of sound concrete approximately 2 feet x 5 feet as directed by the Engineer. The cost of the calibration procedure shall be included in the unit price bid for hydrodemolition. The Engineer shall verify the following settings:

1. Water pressure (minimum 13,000 psi; maximum 20,000 psi)
2. Machine staging control (step)
3. Nozzle size

4. Nozzle speed (travel)

During the calibration, any or all of the above settings may be adjusted in order to achieve removal in accordance with the requirements of the plans. When the designated depth of removal is attained, the settings shall be recorded and maintained throughout the removal -operation unless otherwise directed by the Engineer. The depth of removal shall be verified periodically and, if necessary, the equipment re-calibrated to ensure the plan depth of removal is obtained.

The concrete bridge deck shall be removed as detailed in the plans or directed by the Engineer. At a minimum, the last one-half inch (1/2") of removal shall be accomplished with hydrodemolition equipment. After the hydrodemolition is completed, the deck shall be inspected (by sounding) to insure that all partial depth deteriorated concrete has been removed. Should deteriorated concrete be found, the Contractor shall remove the areas of deteriorated concrete by additional passes of the hydrodemolition equipment or jackhammers.

No removal of concrete by conventional (mechanical impact) methods will be allowed within a bridge unit (expansion joint to expansion joint) following concrete placement within the same unit until 48 hours of curing has elapsed, unless otherwise approved by the Engineer.

The Contractor shall provide shielding, as necessary, to insure containment of all dislodged concrete within the removal area in order to protect the traveling public from flying debris both on and under the work site.

Waste water from the hydrodemolition process shall be controlled and filtered to produce a visibly clear water prior to releasing it to the surrounding environment. Sediment basins at the end of or outside of the structure shall be used if further filtration is required to produce visibly clear water. Bridge deck drains shall be plugged during the hydrodemolition process. The release of wastewater and solids generated by full depth hydrodemolition shall be minimized.

Cleaning of the bridge deck shall be performed with a vacuum system capable of removing wet debris and water. The deck shall then be blown dry with air to remove excess water and residual debris. Cleaning shall be done before debris and water are allowed to dry on the deck surface. All exposed reinforcing steel which is left unsupported by the hydrodemolition process shall be adequately supported and protected from bending by vacuum trucks or any other equipment. All reinforcing steel damaged or dislodged by these operations shall be replaced with epoxy coated bars of the same size in accordance with the plans or approved by the Engineer, at no additional costs to the State.

When full depth repair is specified on plans, only those areas marked in the field by the Engineer as full depth repair will be paid for as full depth repair. Other areas where hydrodemolition equipment blows through the deck shall be the responsibility of the Contractor and will not be paid for as full depth repair.

907-202.04--Method of Measurement. Removal of Bridge Deck, Hydrodemolition shall be measured by the square yard of the total deck area regardless of depth. Measurements shall be made to the nearest 0.1 square yard.

907-202.05--Basis of Payment. The accepted quantity of Removal of Bridge Deck, Hydrodemolition will be paid for at the contract unit price per square yard, which price will be full compensation for all materials, equipment and labor necessary to remove and dispose of all concrete and other debris to the depth shown on the plans or as directed by the Engineer. This item shall also include vacuuming, shielding, containment and filtration of waste water, additional jackhammering and all other aspects of work necessary to remove bridge deck concrete by hydrodemolition.

Payment will be made under:

907-202-B: Removal of Bridge Deck, Hydrodemolition - per square yard

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-618-12

CODE: (SP)

DATE: 05/03/2024

SUBJECT: Traffic Control Management

Section 618, Maintenance of Traffic and Traffic Control Plan, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-618.01--Description.

907-618.01.2--Traffic Control Management. Delete subparagraph (g) of Subsection 618.01.2 on page 441, and substitute the following.

- g) Perform a minimum of once-a-week inspections from the Notice to Proceed until a Partial or Final Maintenance Release is obtained. Once work begins, daily daytime inspections and weekly nighttime inspections are required on projects with predominantly daytime work, and daily nighttime inspections and weekly daytime inspections are required on projects with predominantly nighttime work. Weekly inspections will be allowed for periods outside of active construction. When lane closures are present or any non-fixed signs or traffic handling devices such as cones or barrels are in place, inspections shall be performed daily whether work is being performed or not.

907-618.05--Basis of Payment. Delete pay item 618-A on page 449 and substitute the following.

907-618-A: Maintenance of Traffic

- lump sum



DESIGNED BY: ALEX HAWKINS
 DETAILED BY: ALEX HAWKINS
 CHECKED BY: JACOB FRIESS
 DATE: 2024-02-07

FMS CON: 107899/301000
 PROJECT NO.: NHP-0055-02(253)
 COUNTY: HINDS

BRIDGE AT STA. 58+72.655 LT. LN.
BRIDGE AT STA. 58+99.295 RT. LN.
PLAN REVISIONS
 DIR OF STRUCTURES, STATE BRIDGE ENGINEER - SCOTT WESTERFIELD, P.E.
 DEP. DIR OF STRUCTURES, ASST. STATE BRIDGE ENGINEER - MICAH DEW, P.E.

WK. NO. **PRS-1**
 SHEET NO. **8002**

BRIDGE DIVISION			
REVISIONS			
DATE	SHEET NO.	DESCRIPTION	BY
4/22/24	8005	Revised dimension	AWH

ADDENDUM



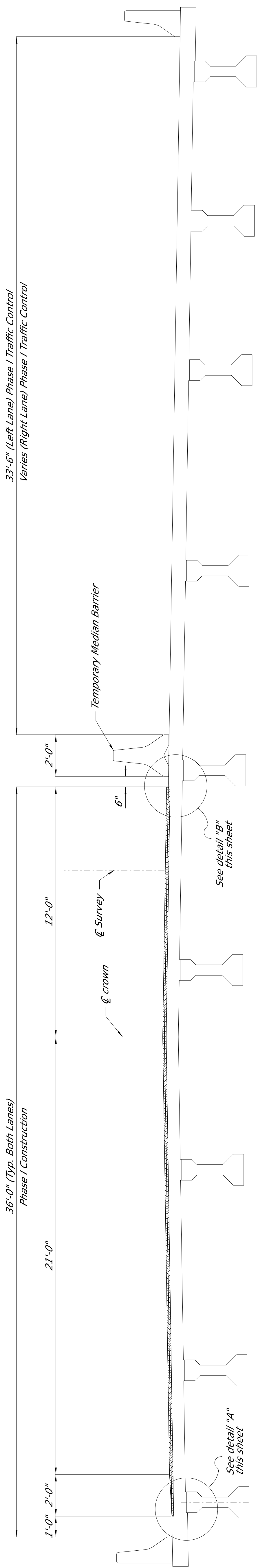
MDOT
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

DESIGNED BY: ALEX HAWKINS
 DETAILED BY: ALEX HAWKINS
 CHECKED BY: JACOB FRIESS
 DATE: 2024-02-07

FMS CON: 107899/301000
 PROJECT NO.: NHP-0055-02(253)
 COUNTY: HINDS

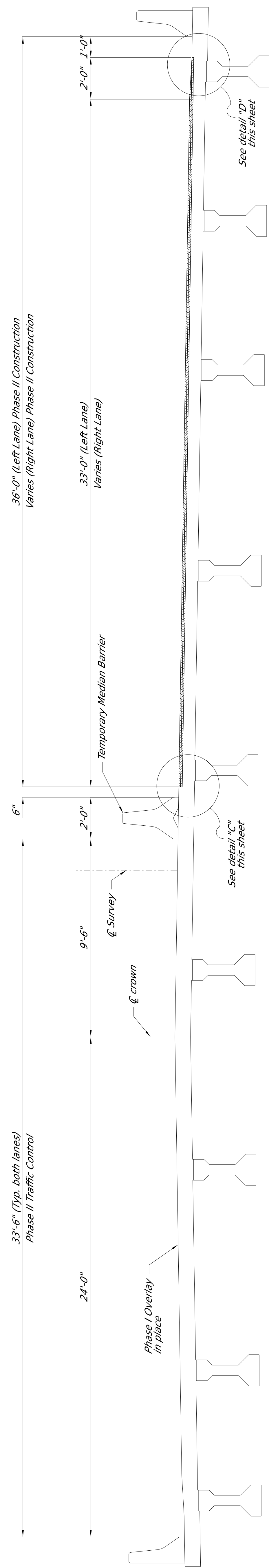
HYDRODEMOLITION DETAILS
 BRIDGE AT STA. 58+72.655 LT. LN.
 BRIDGE AT STA. 58+99.295 RT. LN.
 DIR OF STRUCTURES, STATE BRIDGE ENGINEER - SCOTT WESTERFIELD, P.E.
 DEP. DIR OF STRUCTURES, ASST. STATE BRIDGE ENGINEER - MICAH DEW, P.E.

WK. NO.
3 OF 9
 SHEET NO.
8005



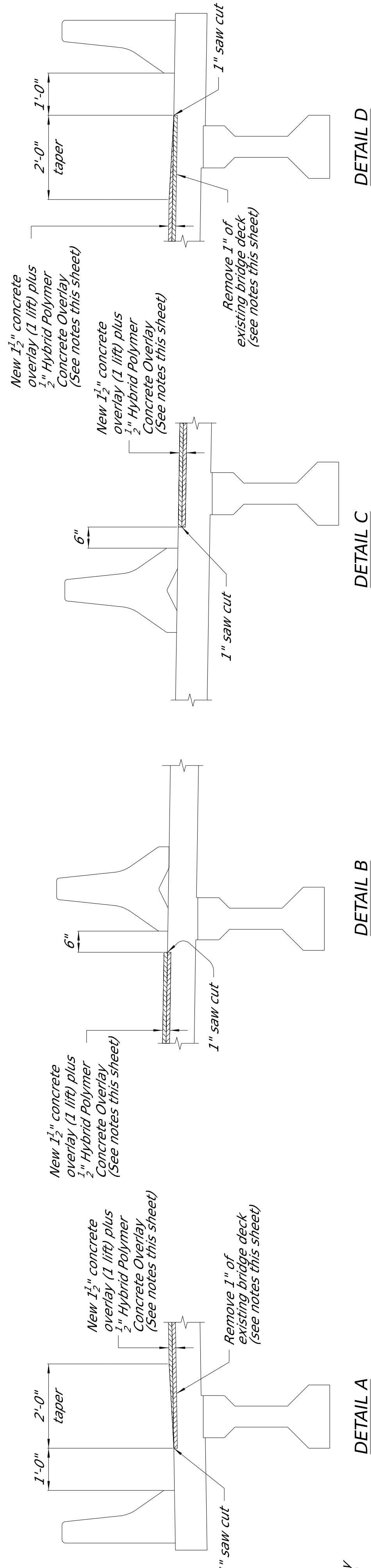
ELEVATION OF PHASE I CONSTRUCTION, BOTH BRIDGES

Looking in the direction of traffic flow.



ELEVATION OF PHASE II CONSTRUCTION, BOTH BRIDGES

Looking in the direction of traffic flow.



- CONCRETE OVERLAY NOTES:**
1. Remove a minimum 1" of existing bridge deck or to sound concrete.
 2. If sound concrete is reached at a depth less than 1", additional removal to achieve a 1" depth is not required.
 3. The new concrete overlay shall be placed in 1 lift. The new concrete overlay shall have a minimum thickness of 1 1/2".
 4. The Hybrid Polymer Concrete Overlay will add 1/2" to the thickness as shown on the details on this sheet.

- 1" SAWCUT & REINFORCEMENT NOTES:**
1. All 1" sawcuts shall be considered an absorbed item of work. The Contractor shall verify depth of reinforcing steel before making any sawcuts. The depth of the sawcut shall be no more than the depth of the reinforcing steel.
 2. All existing reinforcement shall remain in place. Any damage to reinforcing steel shall be repaired to the satisfaction of the Engineer at no cost to the State.