SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO.	1	DATED	5/21/2024	ADDENDUM NO.		DATED	
ADDENDUM NO		DATED		ADDENDUM NO.		DATED	
ADDENDUM NO		DATED		ADDENDUM NO.		DATED	

Number	Description
1	Revised Table of Contents; Revised NTB Nos. 5813 & 5814; Added SP 907-618-12; Amendment EBSx Download Required.

TOTAL ADDENDA: 1
(Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

_____
Contractor

BY _____
Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

_____
President Address

_____
Secretary Address

_____
Treasurer Address

The following is my (our) itemized proposal.

STP-0008-03(063)/ 109444301000

Rankin County(ies)

Revised 01/26/2016

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
TABLE OF CONTENTS

PROJECT: STP-0008-03(063)/109444301 - Rankin

907-405-1	Stone Matrix Asphalt
907-413-2	Cleaning and Sealing Joints and Cracks
907-618-11	Work Zone Law Enforcement
907-618-12	Traffic Control Management
907-619-6	Temporary Portable Rumble Strips
907-625-3	Methyl Methacrylate Pavement Markings for Interstate Shields
907-700-1	Materials and Tests
907-701-3	Hydraulic Cement
907-702-4	Bituminous Materials
907-703-2	Gradation
907-705-1	Stone Riprap
907-707-3	Joint Materials
907-711-2	Plain Steel Wire
907-712-1	Fence and Guardrail
907-714-3	Miscellaneous Materials
907-718-1	Timber and Dimension Lumber
907-720-2	Acceptance Procedure for Glass Beads
907-721-4	Materials for Signing
907-808-1	Joint Repair
907-823-7	Preformed Joint Seal
907-824-2	Routine Bridge Repair

Section 905 - Proposal, Proposal Bid Items, Combination Bid Proposal
Certification of Performance - Prior Federal-Aid Contracts
Certification Regarding Non-Collusion, Debarment and Suspension
SAM.GOV Registration and Unique Entity ID
Section 902 - Contract Form
Section 903 - Contract Bond Forms
Form -- OCR-485

Progress Schedule

(REVISIONS TO THE ABOVE WILL BE INDICATED ON THE SECOND SHEET
OF SECTION 905 AS ADDENDA)

05/21/2024 12:00 PM

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 5813

DATE: 05/21/2024

SUBJECT: Specialty Items

PROJECT: STP-0008-03(063)/109444301 - RANKIN

Pursuant to the provisions of Section 108, the following work items are hereby designated as "Specialty Items" for this contract. Bidders are reminded that these items must be subcontracted in order to be considered as specialty items.

CATEGORY: DISPOSAL OF BUILDINGS, RIGHT OF WAY CLEARING & GRUBBING

Line No	Pay Item	Description
0050	202-B240	Removal of Traffic Stripe

CATEGORY: GUARDRAIL, GUIDERAIL

Line No	Pay Item	Description
0220	606-B001	Guard Rail, Class A, Type 1
0230	606-C003	Guard Rail, Cable Anchor, Type 1
0240	606-D005	Guard Rail, Bridge End Section, Type A
0250	606-D022	Guard Rail, Bridge End Section, Type I
0260	606-E005	Guard Rail, Terminal End Section, Flared
0270	606-E007	Guard Rail, Terminal End Section, Non-Flared
0280	606-G002	Special Sections, Guard Rail Bridge End Connector

CATEGORY: MISCELLANEOUS/ SPECIALTY WORK ITEMS

Line No	Pay Item	Description
0150	423-A001	Rumble Strips, Ground In

CATEGORY: PAVEMENT STRIPING AND MARKING

Line No	Pay Item	Description
0420	626-A001	6" Thermoplastic Double Drop Traffic Stripe, Skip White
0430	626-B002	6" Thermoplastic Double Drop Traffic Stripe, Continuous White
0440	626-E001	6" Thermoplastic Double Drop Traffic Stripe, Continuous Yellow
0450	626-G004	Thermoplastic Double Drop Detail Stripe, White
0460	626-G005	Thermoplastic Double Drop Detail Stripe, Yellow
0470	626-H001	Thermoplastic Double Drop Legend, White
0480	626-H002	Thermoplastic Double Drop Legend, White
0490	627-K001	Red-Clear Reflective High Performance Raised Markers
0500	627-L001	Two-Way Yellow Reflective High Performance Raised Markers
0580	907-625-G001	Methyl Methacrylate Pavement Marking Interstate Route Shield Marker

CATEGORY: TRAFFIC CONTROL - PERMANENT

Line No	Pay Item	Description

CATEGORY: TRAFFIC CONTROL - PERMANENT

Line No	Pay Item	Description
0510	630-F006	Delineators, Guard Rail, White
0520	630-F007	Delineators, Guard Rail, Yellow
0530	630-G005	Type 3 Object Markers, OM-3R or OM-3L, Post Mounted

CATEGORY: TRAFFIC CONTROL - TEMPORARY

Line No	Pay Item	Description
0300	619-A1001	Temporary Traffic Stripe, Continuous White
0310	619-A2001	Temporary Traffic Stripe, Continuous Yellow
0320	619-A3001	Temporary Traffic Stripe, Skip White
0330	619-A5001	Temporary Traffic Stripe, Detail
0340	619-A6001	Temporary Traffic Stripe, Legend
0350	619-A6002	Temporary Traffic Stripe, Legend
0360	619-D1001	Standard Roadside Construction Signs, Less than 10 Square Feet
0370	619-D2001	Standard Roadside Construction Signs, 10 Square Feet or More
0380	619-G4001	Barricades, Type III, Double Faced
0390	619-G4005	Barricades, Type III, Single Faced

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 5814 **CODE: (SP)**

DATE: 05/21/2024

SUBJECT: Scope of Work

PROJECT: STP-0008-03(063) / 109444301 – Rankin County

The contract documents do not include an official set of construction plans but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, "Standard Drawings".

A general description of the work required on the project is to mill/overlay and concrete rehabilitation of approximately 2 miles of US 49 from Richland Creek (BOP STA 965+00) to US 80 (EOP STA 1066+85) along with the adjacent Frontage Roads. Details of specific work are mentioned in the following sections.

u

US 49 STA 965+00 (BOP) to STA 1066+85 (EOP)

Work in this section shall consist of milling 1½" and variable on mainline and shoulders. After milling, the area shall be inlaid with 1½" of 9.5-mm SMA on the mainline and 9.5-mm, HT asphalt on the shoulders. Additional work shall include full depth repairs of the CRCP and 4-foot longitudinal joint repair. See attached tables for additional details. All guardrail not meeting current MDOT standards shall be replaced. Traffic will be allowed to run on the milled surfaces no more than five (5) consecutive days.

US 49 Frontage Roads

Work in this section shall consist of milling 1½" and variable and inlaying with 1½" and variable of 9.5-mm SMA. Failed areas listed in the provided table shall be repaired full depth using 12.5-mm, HT, Leveling asphalt. Paving limits for the Frontage Road will extend past the BOP to the construction joint approaching Richland Creek, approximately STA 951+00. All guardrail not meeting current MDOT standards shall be replaced. Traffic will be allowed to run on the milled surfaces no more than five (5) consecutive days.

GENERAL NOTES:

MILLING

Milling/paving shall not begin until an **approved** asphalt mix design has been received, nor until such time that, in the opinion of the Engineer, weather conditions have been consistently suitable enough to allow placement of the asphalt pavement after the milling operations.

The reclaimed asphalt pavement (RAP) material removed by the milling operation shall become the property of the Contractor.

Where milling is required, the Contractor shall provide outlets in the existing shoulders at sufficient intervals to prevent pooling or standing water on the milled surface; the cost of which shall be absorbed in other items bid.

Milling and paving operations shall be performed such that a -2% slope from centerline is provided in normal crown roadway sections. Super-Elevation through curves shall be maintained as it currently exists or improved as directed. Where slope correction is required correction will be made by milling, paving, or combination thereof as directed by the Engineer. Milling correction: Mill outside edge of pavement to a depth of 1½" on a 2% slope towards the centerline. Paving Correction: Mill to depth of 1½" on existing slope and 2 ¼" and variable on centerline and 1½" on outside edge. Combination Method: Combination of both methods as directed by the Engineer to achieve the desired slope. In super elevated areas where correct SE exist milling will transition to thickness through curves. Where correct SE does not exist milling will transition at curves to correct SE as directed by the Engineer.

Milling operations shall be performed in accordance with the Contract documents and the Standard Specifications. Variable width and length transitions may be required for tie-ins at ramps, local roads, project limits.

Milling of driveway pads shall be conducted in a manner to prevent gouging or otherwise affecting the roadway pavement structure and slope. Milling of driveway pads shall not be done in simultaneous path with mainline milling.

Traffic will be allowed to travel on the mainline milled surface for five (5) days and the Contractor will be assessed a penalty of **$5,000.00** per calendar day afterwards until the mainline milled surface is covered with the next lift of asphalt, which also includes Frontage Roads. Additionally, traffic will be allowed to run on all milled surfaces other than the mainline for 30 days unless otherwise stated, and the Contractor will be assessed a penalty of **$1,000** per calendar day afterwards until the non-mainline milled surface is covered with the next lift of asphalt. The additional allowance for the non-mainline milled surface is for the Contractor's convenience, and thus, the Contractor is responsible for any pavement failures or damage sustained during this period. Milling and paving of paved shoulders shall conform to Subsection 406.03.2 of the Standard Specifications.

PAVING

Per Subsection 401.02.3.2, the asphalt mix design shall be submitted to the Engineer at least 10 working days prior to its proposed use.

Prior to mainline milling and paving operations, failed areas in the existing concrete pavement shall be removed and repaired. CRCP concrete failed areas shall be repaired as per the attached details and be paid under 8" and Variable Continuously Reinforced Concrete Pavement, Broom Finish. The concrete pavement failures shall be removed by saw cutting and excavating the failed material. Any failures in the cement treated base shall be removed and replaced with Class "C" concrete and payment will be made under pay item 503-D: Concrete for Base Repair. A list of the failed areas is shown in the attached tables. Pavement repairs shall be completed as a continuous operation in order to minimize traffic impacts. 12.5-mm HT, Leveling asphalt shall

be placed to grade over the concrete repair, prior to opening traffic. Payment will be made under 403-B. Lane closures shall remain in place until the failed area has been completely repaired and lane closures shall not be left unattended.

A table showing locations of underlying problems with the concrete pavement has been included. The following sequence of operations will be used to correct the underlying concrete pavement problems: Mill and replace the asphalt over the joint to a maximum depth of six inches (6") at four feet (4') in width. After milling and prior to replacement, if there is any remaining depth of asphalt over the joint, any failed asphalt shall be repaired by removing all loose/broken pieces. Replacement of milled area and any repair areas shall be made with 12.5-mm HT, Leveling asphalt back to existing finish grade.

Failed areas in the existing pavement on the Frontage Road shall be removed and backfilled with 12.5-mm HT, Leveling asphalt as per the attached typical sections and details. Asphalt shall be placed in multiple lifts with a maximum lift thickness of 3". Any granular/chemically treated/stone/etc. base or subgrade material deemed unsuitable by the Engineer shall be removed as directed and backfilled with 12.5-mm HT, Leveling asphalt. Payment for the excavation of the granular base and subgrade will be made using pay item 203-G: Excess Excavation. A list of the failed areas is shown in the attached tables. Pavement repairs shall be completed as a continuous operation in order to minimize traffic impacts. Lane closures shall remain in place until the failed area has been completely repaired. Lane closures may not be left unattended.

Prior to mainline paving operations and subsequent to the repair of failed areas, spot milling shall be performed in the areas listed in the attached tables and at other areas as directed by the Engineer. Spot milling shall be performed at a depth of 5" and variable and overlayed at a depth of 5" and variable in the areas to remove cracked/oxidized asphalt. Payment for milling and paving will be made using the appropriate pay items. "Uneven Lanes" signs shall be used as required and as shown on the Standard Drawings. See attached table for more details.

The surface lift for failed area repairs or concrete punchout repairs shall have a maximum deviation of 3/8" as determined by a 10-foot straight edge. Any location that deviates more than this tolerance, as determined by the Engineer, shall be corrected at no additional cost to the State.

Publicly maintained roads and streets shall be paved to the existing right-of-way and in accordance with the attached drawings.

Privately owned entrances shall be paved to the shoulder line per the included typical drawing unless otherwise directed. Pad dimensions shall match the existing lengths and widths unless otherwise directed. Pads shall be shaped horizontally and vertically to prevent excessive drop-offs. Any new driveway pads deemed necessary by the Engineer shall be placed according to specifications.

If traditional excavation methods are used, the removal area shall first be saw cut full depth including concrete, where applicable, to create a neat line and prevent damage to the adjacent pavement structure. Payment for saw cuts will be made using the appropriate items. If milling techniques are used, the area will not require saw cuts but care should be exercised to create a

neat removal line and to prevent damage to the adjacent pavement structure. If saw cuts are used in conjunction with milling, payment will be made using the appropriate pay items. Payment will not be made for saw cuts that are not performed.

GRANULAR SHOULDER MATERIAL

Where applicable, the existing shoulders shall be raised to match the new pavement elevation by placing variable depth granular material. The shoulders shall be graded and pulled up on a daily basis to eliminate drop-offs in excess of 2¼". Placement of the granular material on the finished asphalt course shall not be permitted. The existing shoulder shall be scarified to allow incorporation of the new shoulder material. The material shall be bladed, rolled, and compacted to a finished slope of four percent (4%) in normal crown sections. Placement of this material shall be performed to provide a uniform and compacted shoulder with a minimum depth and width of material placed. Shoulders with adequate shoulder material in place shall be bladed to a slope of four percent (4%) in normal crown sections. The cost of blading will be an absorbed item and is to be included in the price of other items bid. Crushed concrete will not be allowed.

Granular material (crushed stone) shall be provided around driveway pads as directed to prevent shoulder drop-offs and shall be placed in a timely manner. Drop-offs exceeding 2¼" shall be corrected within two (2) calendar days of the placement of the pad.

Any material excavated from the existing shoulder during pavement widening operations or as a result of shoulder blading shall be used on the existing shoulder to match the new pavement elevation and any surplus material shall be spread along the edge of the shoulders, fore slopes, or other adjacent areas as directed by the Engineer and will be an absorbed item. Material which cannot be suitably placed in adjacent areas and deemed to be excess excavation by the Engineer shall be removed from the project site. Payment for removal of excess material will be made using pay item 203-G: Excess Excavation. Asphalt placed as fill material on the shoulders shall be removed; the cost of which shall be absorbed in other items bid.

TEMPORARY AND PERMANENT PAVEMENT MARKINGS

Temporary traffic stripe will be required immediately after the milling and/or required overlay and prior to opening area to traffic. Temporary stripe shall be placed in the same location and configuration as the permanent stripe except that it may be offset as required for milling and paving operations. If temporary stripe is offset, the Contractor shall conduct operations in a manner to insure the final temporary stripe is placed at the required location of the permanent stripe. If removal of temporary offset stripe is required in order to achieve the correct location and alignment of permanent stripe. The cost of removal will be absorbed in other items bid. Placing double temporary centerline will not be allowed.

Temporary striping shall conform to finished stripe specifications for alignment, neatness, and straightness.

The use of short strips of traffic tape will not be allowed unless approved by the Engineer.

All permanent striping will be double drop thermoplastic, 90-mil thickness unless otherwise specified in Subsection 626.03.1.2. Edge lines will be placed to accommodate the lane widths shown on the attached applicable typical sections unless prevented by field conditions.

Per Subsection 626.03.1.2, a binder-sealer shall be applied to the concrete pavement or bridge surface prior to the placement of the thermoplastic material and shall be absorbed under the thermoplastic pay items. The type and amount of binder-sealer used shall adhere to the thermoplastic manufacturer's recommendations.

Rumble strip shall be placed throughout the project limits in accordance with the attached details and Standard Drawings.

Permanent raised pavement markers shall be installed on mainline and local public roads after completion of all paving operations.

Payment for edge stripe on local roads shall be made under pay item 626-G004: Thermoplastic Double Drop Detail Stripe, White when the length of said stripe is less than 150 feet when measured from the end of the radius. If the measured length is greater than 150 feet, then payment shall be made under pay item 626-B002: 6" Thermoplastic Double Drop Traffic Stripe, Continuous White.

Payment for centerline stripe on local roads shall be made under pay item 626-G005: Thermoplastic Double Drop Detail Stripe, Yellow when the length of said stripe is less than 150 feet when measured from the stop bar. If the measured length is greater than 150 feet, then payment shall be made under pay item 626-E001: 6" Thermoplastic Double Drop Traffic Stripe, Continuous Yellow. Centerline Stripe shall be omitted on local roads whose width is less than 20 feet.

The face of all existing undisturbed curbs shall be painted with at least two (2) coats of white traffic paint with glass beads being required in the top coat. The cost associated with the painting of new or existing curb is to be included in other items bid.

Pavement section marking tape on this project shall be located prior to overlaying and placed back in the same location after paving operations have ceased. The section marking shall be 8-inch high performance cold plastic detail stripe and shall be four feet (4') in length. The marking shall be centered across the centerline stripe. The cost of this item shall be absorbed in other items bid.

Interstate route shields shall be installed as per the attached details.

GUARDRAIL

Guardrails shall be replaced at the locations shown on the attached table. Removal of guardrail shall consist of removal of bridge end section, w-beam/thrie beam, terminal end section, posts, and all other appurtenances. All guardrail removed shall be replaced the same day and prior to reopening the adjacent lane of traffic. Voids created by the removal of posts, concrete anchors,

footings, etc. shall be backfilled and compacted in accordance with Section 203 of the Standard Specifications.

The asphalt guardrail pad shall be milled and paved up to the face of the guardrail. The remaining asphalt guardrail pad behind the face of the guardrail shall be removed and shall be paid for using the milling pay item. The guardrail pad shall be reconstructed using crushed stone granular material and shall be a minimum of 4" in depth. If blading is required in order to meet the minimum depth, then said blading shall be an absorbed item and the excavated material shall be retained and used to raise the existing shoulder to match the new pavement elevation. Material which cannot be placed and blended in adjacent areas and deemed to be excess excavation by the Engineer shall be removed under pay item 203-G: Excess Excavation. Prior to the placement of the crushed stone, a soil sterilant shall be applied as per Subsection 616.03.2 and Geotextile Stabilization, Type V, Non-Woven installed underneath the limits of the crushed stone. The installed guardrail shall meet all requirements in order to be MASH compliant.

Guardrail lengths are based on terminal end length of 37.5'. If terminal of length other than this is used, an adjustment in w-beam length is required.

All dimensions and spacings for bridge rail connectors shall be verified in the field by the Contractor prior to fabrication.

Object markers at bridge approaches and other locations shall be replaced as shown in the attached table. Removal of object markers shall be absorbed in the cost of other items bid.

TRAFFIC CONTROL

The Contractor shall erect and maintain construction signing and provide all signs and traffic control devices necessary to safely maintain traffic around and through the work areas in accordance with the Traffic Control Plan and the MUTCD. The cost shall be included in the price bid for pay item 618-A: Maintenance of Traffic. Fluorescent orange sheeting shall be used on all construction and traffic control signs except those designated in the plans to be black legend and border on white background.

Standard roadside construction signs, barricades, etc. shall be placed in accordance with the attached tables, drawings, and as directed by the Engineer. W20-1 signs shall be placed on all public road approaches as shown or as directed. Payment for standard roadside construction signs, barricades, etc. will be made using the appropriate pay items.

The Contractor shall on a daily basis, remove all debris from within the roadway and a 30-foot clear zone which, in the opinion of the Engineer, is a hazard to the traveling public. This activity shall begin with the beginning of work or the beginning of the contract time, whichever comes first. No direct payment will be made for the debris removal; the cost is to be included in the prices of items bid. Failure of the Contractor to remove the debris as prescribed herein shall be just cause for withholding the monthly progress estimate payment or suspending active operations until the debris is satisfactorily removed by the Contractor.

Temporary asphalt joints (aka paper joints) shall be employed at all locations requiring traffic to traverse an uneven, transverse, pavement joint. Paper joints shall be a minimum of nine feet (9') in length and for the full width of the milled/paved surface. Paper joints shall be adequately maintained.

Potholes that may exist or occur in the existing pavement shall be patched in a timely manner as required. Patching of potholes shall be considered an absorbed item.

Temporary portable rumble strips, as described in Special Provision No. 907-619, shall be used in advance of each lane closure on the Frontage Roads. Direct payment will not be made for this item and shall be considered absorbed under pay item 618-A: Maintenance of Traffic.

GENERAL EPOXY REPAIR

All epoxy repairs shall be performed in accordance with the attached details shown on the epoxy repair drawings and in accordance with the notes herein. Repair concrete spalled areas on the bridge as directed by the Project Engineer and at the locations listed in the attached tables. The Contractor shall determine the depth of reinforcement prior to any saw cutting. Spalled areas where pack rust has developed around or on reinforcement shall be blasted clean prior to repairing the spalled location. All areas of the bridge repaired with epoxy mortar shall be restored to the original dimensions as shown in the information plans, unless noted otherwise.

Materials:

1. Epoxy Resin: Resin shall be selected from the MDOT Approved Products List.
2. Silica Sand: the materials shall be bagged general purpose cleaning sand.
3. Epoxy Mortar Mix: the epoxy mortar mix shall consist of part liquid epoxy and part clean dry sand mixed in the ratio recommended by the Manufacturer.

Applications:

1. A Representative of the Epoxy Manufacture must be present for sufficient time to ensure that the Contractor is properly schooled in the use of the epoxy material.
2. Prior to placement of the mortar mix, the prepared surface shall be lightly primed with neat epoxy.
3. Acetone alcohol may be used to clean and lubricate trowels.
4. Curing time shall be in accordance with the Manufacturer's recommendations.

All items of work related to epoxy repair shall be paid for under pay item 907-824-A003: General Epoxy Repair. Epoxy repair under this pay item is for general concrete spall repairs, and shall be bid such that the item may be increased, decreased, or eliminated as directed by the Project Engineer.

MISCELLANEOUS NOTES

It shall be the responsibility of the Contractor to protect existing structures such as pipes, inlets, aprons, bridges, etc. from damage which might occur during construction. This includes manholes and other utilities in the roadway. The Contractor shall replace or repair, as directed by the Engineer, any structures damaged by the Contractor during the life of the contract. No payment will be made for replacement or repair of damaged items.

Any signs that are in conflict with construction of this project shall be removed and relocated by the Contractor as directed by the Engineer. The cost of which shall be absorbed in other items bid.

Removal of existing raised pavement markers shall be included in the prices for other items bid.

Incidental work such as removing vegetation, shaping and compacting shoulders, removing and resetting signs and/or mailboxes, removing excess asphalt material, project clean-up, and other items of incidental work necessary to complete the project will not be measured for separate payment and will be considered included in the prices of items bid.

Prior to the final inspection, bridges, islands, and areas with curb shall be swept/cleaned. Care should be taken to prevent milled asphalt, asphalt debris, vegetative/granular debris, etc. from entering drainage structures or clogging other drainage ways. Disposal of material will not be measured for separate payments.

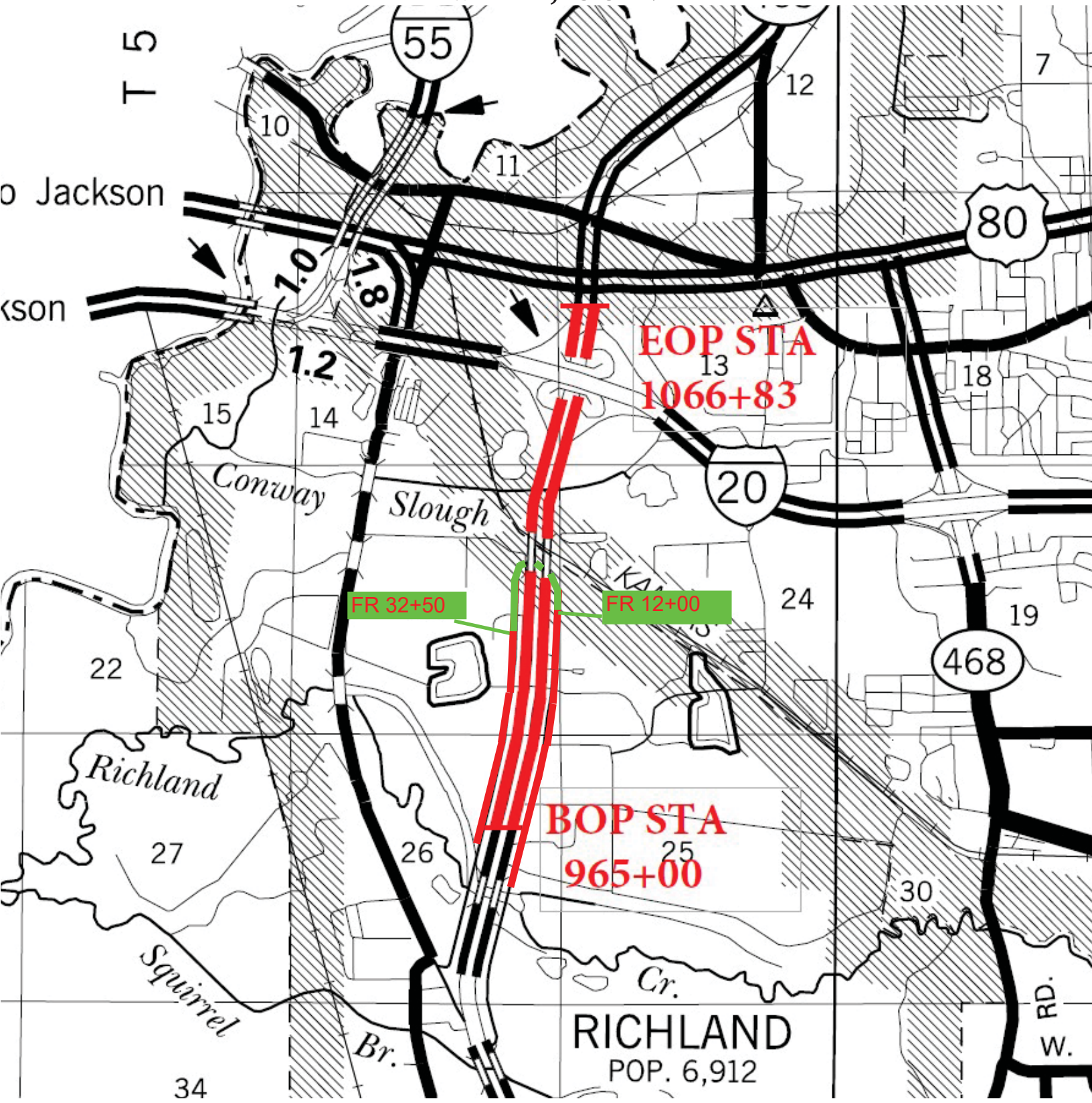
Following the overlaying operation the transverse joints in the pavement shall be sawed and sealed within seven (7) days. The details for sawing and sealing transverse joints for this section are in the Standard Specifications. The width of the sawing and sealing operation will be in the attached table. It is the responsibility of the Contractor to locate and mark all existing joints that are to be sawed and sealed prior to the milling operation. The Contractor shall notify the Department when this is to take place so that they can oversee the work and determine the width that each joint will be sawed and sealed.

Removal of sign, post, and footing and backfilling will be paid using the removal of sign pay item.

Ramp closures will not be allowed without written approval from the Engineer.

STP-0008-03(063) 109444 / 301000.

Rankin, US 49

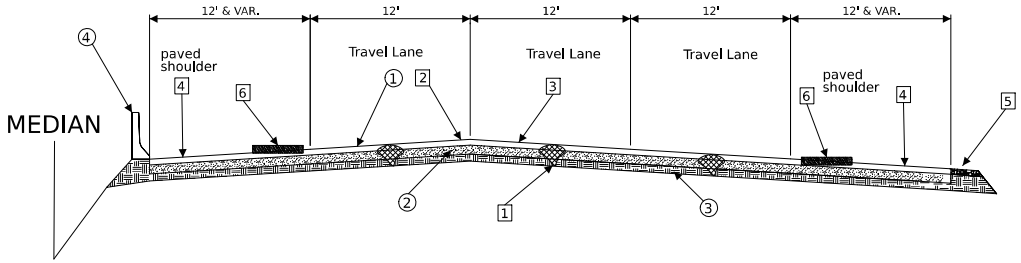


STATE	PROJECT NO.
MISS.	STP-0008-03(063)

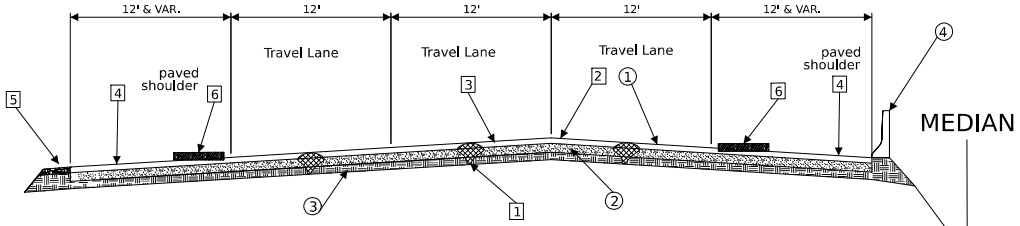
RANKIN COUNTY
STP-0008-03(063)
109444/301000
US 49

US 49 SIX LANE TYPICAL SECTION

US 49 NORTHBOUND
STATION: BOP TO 983+00



US 49 SOUTHBOUND
STATION: BOP TO 990+00



Existing:

1. 5" and variable of existing asphalt
2. 8" & variable of Continuously Reinforced Concrete Pavement (CRCP)
3. 6" of cement treated base
4. 42" concrete median barrier, Type IV

Proposed:

1. Repair failed area full depth per concrete punchout typical
2. Mill 1 1/2" and variable of existing asphalt pavement
3. Overlay mainline with 1 1/2" SMA 9.5-mm Mixture
4. Overlay shoulder 1 1/2" 9.5-mm, HT, Asphalt Pavement
5. Place variable depth Crushed Stone to bring shoulders to grade
6. Place rumble strip on shoulder

LEGEND

- HMA
- CRCP
- Cement treated base
- Crushed stone
- Rumble stripe

- 10 -

Notice to Bidders No. 5814 -- Cont'd.

DATE	REVISION	BY
6/19/03	JW/JWDS	GM

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
DETAIL OF
US 49 MAINLINE

PROJECT: STP-0008-03(063)
COUNTY: RANKIN
FILENAME: TS-49 NORTH TYPICAL.DGN
DESIGN TEAM DISTRICT 5 CHECKED DATE

WORKING NUMBER TS-1
SHEET NUMBER 1

PLAN
RANKIN COUNTY DIVISION
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

MOODY 00.00ANPW TS-49 NORTH TYPICAL ZH.DGN

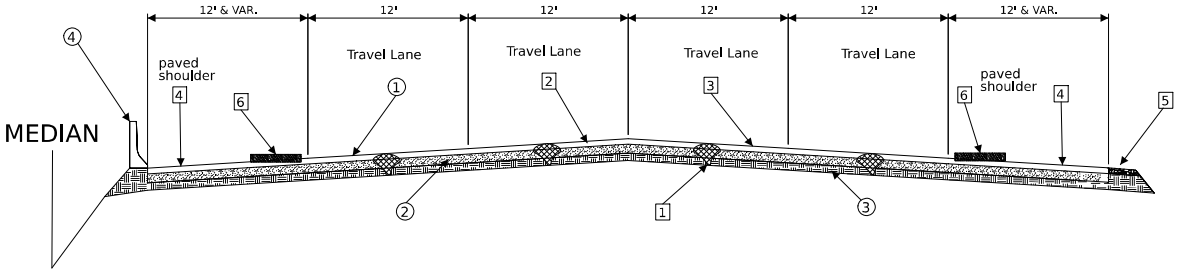
STATE	PROJECT NO.
MISS.	STP-0008-03(063)

RANKIN COUNTY
STP-0008-03(063)
109444/301000
US 49

US 49 EIGHT LANE TYPICAL SECTION

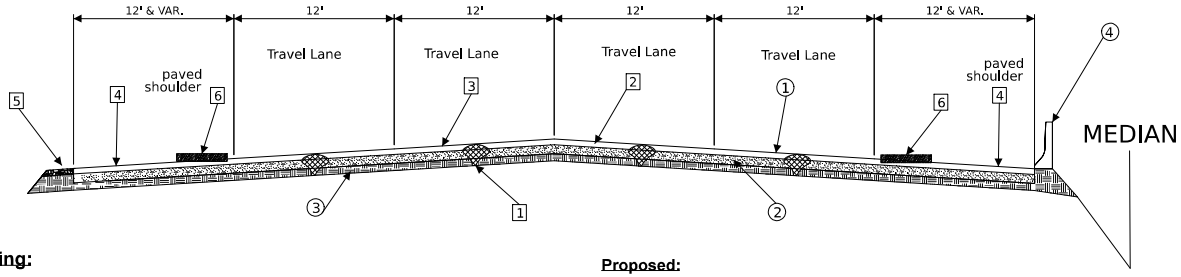
US 49 NORTHBOUND

STATION: 983+00 TO 1029+00



US 49 SOUTHBOUND

STATION: 990+00 TO 1033+50



LEGEND

- = HMA
- = CRCP
- = Cement treated base
- = Crushed stone
- = Rumble stripe

Existing:

1. 5" and variable of existing asphalt
2. 8" & variable of Continuously Reinforced Concrete Pavement (CRCP)
3. 6" of cement treated base
4. 42" concrete median barrier, Type IV

Proposed:

1. Repair failed area full depth per concrete punchout typical
2. Mill 1 1/2" and variable of existing asphalt pavement
3. Overlay mainline with 1 1/2" SMA 9.5-mm Mixture
4. Overlay shoulder 1 1/2" 9.5-mm, HT, Asphalt Pavement
5. Place variable depth Crushed Stone to bring shoulders to grade
6. Place rumble strip on shoulder

Notice to Bidders No. 5814 -- Cont'd.

DATE	REVISION	BY
6/19/23	ON PLANS	CM

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
DETAIL OF
US 49 MAINLINE

PROJECT: STP-0008-03(063)
COUNTY: RANKIN
FILENAME: TS-49 NORTH TYPICAL.DGN
DESIGN TEAM DISTRICT 5 CHECKED DATE

WORKING NUMBER TS-2
SHEET NUMBER: 2

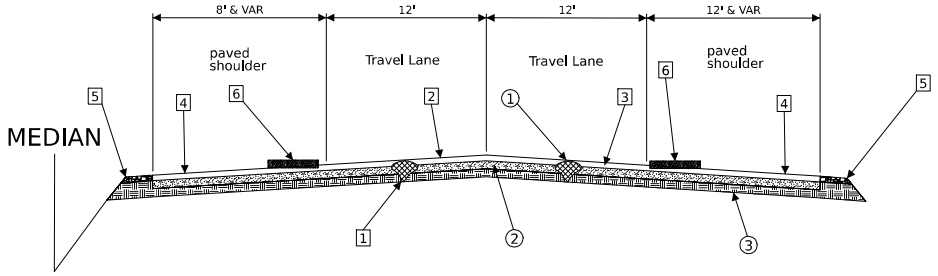
MMDDYY 001.00 ANPM TS-49 NORTH TYPICAL ZH.DGN

STATE	PROJECT NO.
MISS.	STP-0008-03(063)

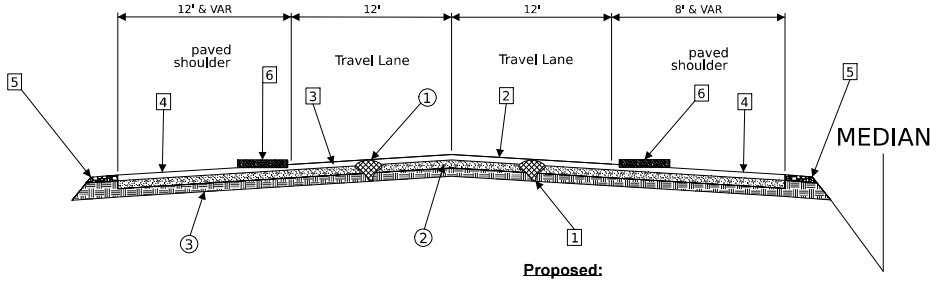
RANKIN COUNTY
STP-0008-03(063)
109444/301000
US 49

US 49 FOUR LANE TYPICAL SECTION

US 49 NORTHBOUND
STATION: 1029+00 TO EOP



US 49 SOUTHBOUND
STATION: 1033+50 TO EOP



Existing:

1. 5" and variable of existing asphalt
2. 8" & variable of Continuously Reinforced Concrete Pavement (CRCP)
3. 6" of cement treated base

Proposed:

1. Repair failed area full depth per concrete punchout typical
2. Mill 1 1/2" and variable of existing asphalt pavement
3. Overlay mainline with 1 1/2" SMA 9.5-mm Mixture
4. Overlay shoulder 1 1/2" 9.5-mm, HT, Asphalt Pavement
5. Place variable depth Crushed Stone to bring shoulders to grade
6. Place rumble strip on shoulder

LEGEND

- = HMA
- = CRCP
- = Cement treated base
- = Crushed stone
- = Rumble stripe

Notice to Bidders No. 5814 -- Contd.

BY	DATE	REVISION
CM	6/19/23	JOHNNIES

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
DETAIL OF
US 49 MAINLINE

PROJECT: STP-0008-03(063)
COUNTY: RANKIN
FILENAME: TS-49 NORTH TYPICAL.DGN
DESIGN TEAM: DISTRICT 5 CHECKED: DATE:

WORK UNIT NUMBER: TS-3
SHEET NUMBER: 3

MOODY 001.00ANPH TS-49 NORTH TYPICAL ZHL.DGN

PLAN
BRIDGE DIVISION
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

STATE	PROJECT NO.
MISS.	STP-0008-03(063)

RANKIN COUNTY
STP-0008-03(063)
109444/301000
US 49

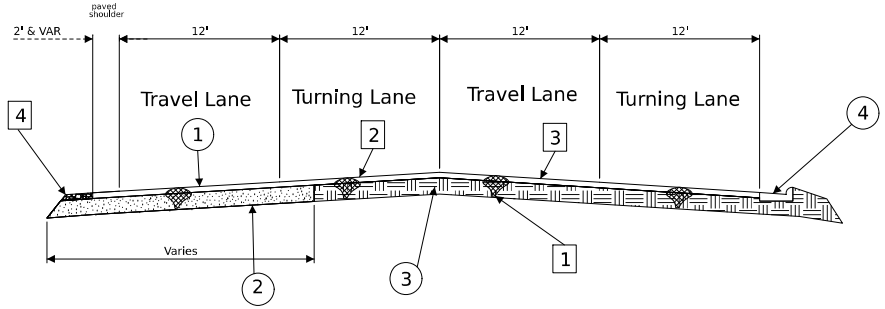
FRONTAGE ROAD FOUR LANE TYPICAL SECTION

EAST FRONTAGE ROAD

STATION: 975+00 TO 1010+00
12+00 TO 16+58

WEST FRONTAGE ROAD

STATION: 985+00 TO 1018+00
32+50 TO 23+32



Existing:

1. 11 1/2 to 16" and variable of existing asphalt
2. 12" of untreated topping
3. 6" and variable untreated granular material
4. Combination Curb & Gutter (Type 3A Mod.)

Proposed:

1. Repair failed area per asphalt punchout typical
2. Mill 1 1/2" and variable of existing asphalt pavement
3. Overlay mainline with 1 1/2" SMA 9.5-mm Mixture
4. Place variable depth Crushed Stone to bring shoulders to grade

LEGEND

- = HMA
- = Untreated granular material
- = Crushed stone
- = Untreated Topping

DATE	REVISION	BY	CM
6/19/23	No. 1		

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
DETAIL OF
FRONTAGE ROADS

PROJECT: STP-0008-03(063)
COUNTY: RANKIN
FILENAME: TS-49 NORTH TYPICAL.DGN
DESIGN TEAM DISTRICT 5 CHECKED DATE

WORKING NUMBER
TS-4
SHEET NUMBER 4

Notice to Bidders No. 5814 -- (cont'd.)

STATE	PROJECT NO.
MISS.	STP-0008-03(063)

RANKIN COUNTY
STP-0008-03(063)
109444/301000
US 49

FRONTAGE ROAD TWO LANE TYPICAL SECTION

EAST FRONTAGE ROAD

STATION: 951+00 to 975+00

WEST FRONTAGE ROAD

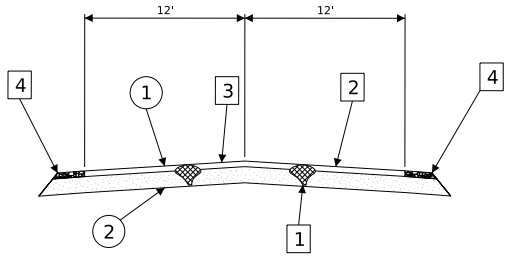
STATION: 957+00 to 985+00

LEGEND

= HMA

= Crushed stone

= Untreated Topping



Existing:

(1) – 11 1/2 to 16" and variable of existing asphalt

(2) – 12" of untreated topping

Proposed:

1 – Repair failed area per asphalt punchout typical

2 – Mill 1 1/2" and variable of existing asphalt pavement

3 – Overlay mainline with 1 1/2" SMA 9.5-mm Mixture

4 – Place variable depth Crushed Stone to bring shoulders to grade

BY	REVISION	DATE
GM	ADJ NOTES	6/19/23

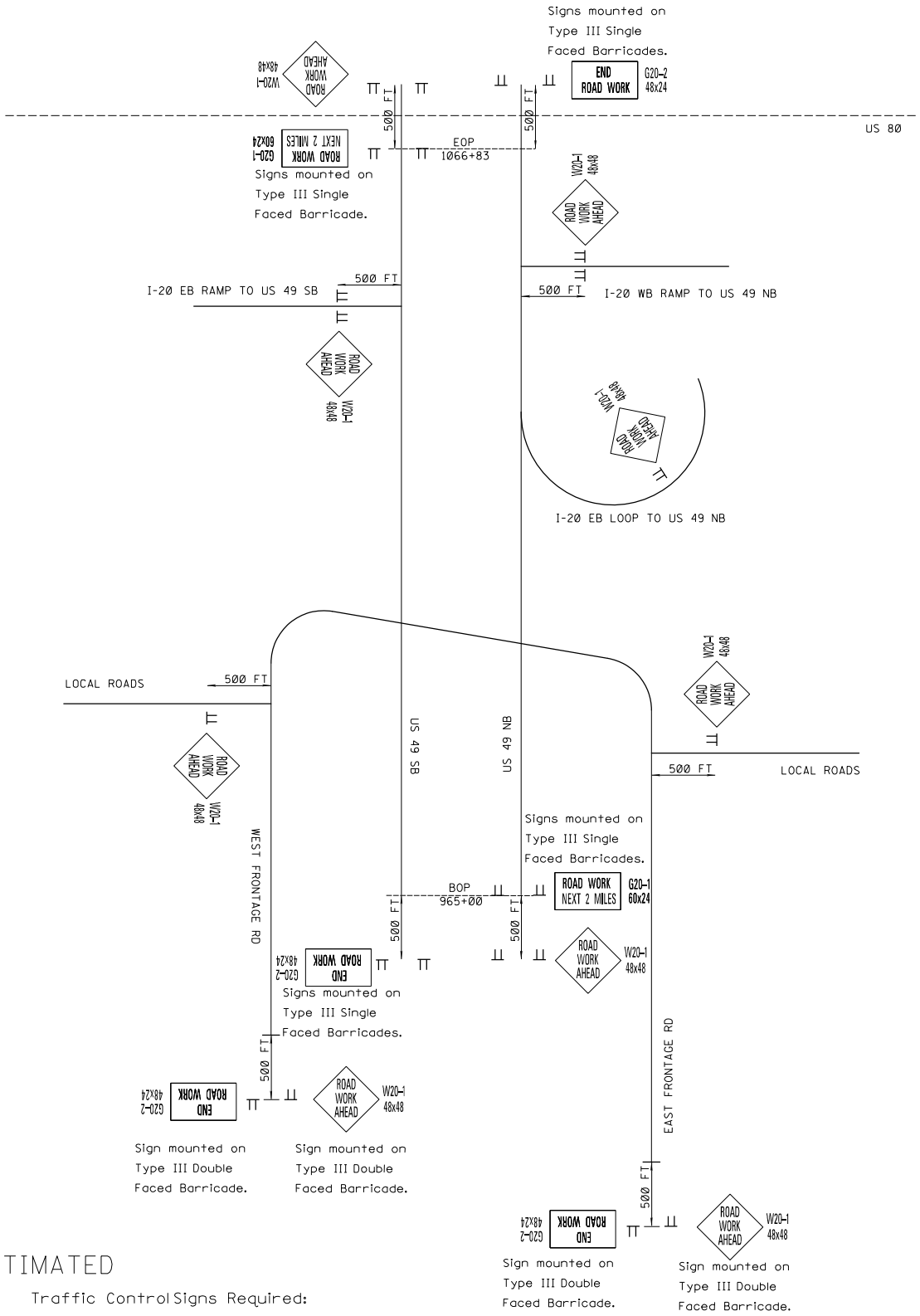
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
DETAIL OF
FRONTAGE ROADS

PROJECT: STP-0008-03(063)
COUNTY: RANKIN
FILENAME: TS-49 NORTH TYPICAL.DGN
DESIGN TEAM DISTRICT 5 CHECKED DATE

WORKING NUMBER TS-5
SHEET NUMBER 5

Notice to Bidders No. 5814 -- Cont'd.

CONSTRUCTION SIGNING DETAIL
US 49 RANKIN COUNTY



ESTIMATED

Traffic Control Signs Required:

4 - G20-1 "ROAD WORK NEXT 2 MILES"

6 - G20-2 "END ROAD WORK"

14 - W20-1 "ROAD WORK AHEAD"

4 - TYPE III DBL. FACE BARRICADES (6LF)

8 - TYPE III SGL. FACE BARRICADES (6LF)

NOTES: One (1) W20-1 "ROAD WORK AHEAD" Sign is Required at each Local Road, Street or Highway Entering the Project. See Standard Roadside Construction Sign Table for Locations.

619-D2001 Standard Roadside Construction Signs, 10 Square Feet or More					
Station	**Location**	**Description**	**Quantity**	**Unit**	**Remarks**
957+50	East Frontage Rd	W20-1 (Road Work Ahead)	16	SF	Kroger Dr
968+85	East Frontage Rd	W20-1 (Road Work Ahead)	16	SF	FedEx South Entrance
975+60	East Frontage Rd	W20-1 (Road Work Ahead)	16	SF	Davis Johnson Dr
988+30	East Frontage Rd	W20-1 (Road Work Ahead)	16	SF	Interstate Dr
1003+15	East Frontage Rd	W20-1 (Road Work Ahead)	16	SF	Distribution Dr
23+00	West Frontage Rd	W20-1 (Road Work Ahead)	16	SF	Carrier Blvd
993+40	West Frontage Rd	W20-1 (Road Work Ahead)	16	SF	Lake Dr
702+00	LT & RT	W20-1 (Road Work Ahead)	32	SF	I-20 EB Ramp to US 49 SB
160+00	LT	W20-1 (Road Work Ahead)	16	SF	I-20 EB Loop to US 49 NB
55+60	LT & RT	W20-1 (Road Work Ahead)	32	SF	I-20 WB Ramp to US 49 NB
East Frontage Rd	RT	W20-1 (Road Work Ahead)	16	SF	South of BOP
West Frontage Rd	LT	W20-1 (Road Work Ahead)	16	SF	South of BOP
US 49 NB	LRL & RRL	G20-1 (Road Work Next 2 Miles)	20	SF	South of BOP
US 49 SB	LLL & RLL	G20-1 (Road Work Next 2 Miles)	20	SF	North of EOP
TOTAL			**264**	**SF**	

619-D1001 Standard Roadside Construction Signs, Less than 10 Square Feet					
Station	**Location**	**Description**	**Quantity**	**Unit**	**Remarks**
US 49 NB	LRL & RRL	G20-2 (End Road Work)	16	SF	North of EOP
US 49 SB	LLL & RLL	G20-2 (End Road Work)	16	SF	South of BOP
East Frontage Rd	LT	G20-2 (End Road Work)	8	SF	South of BOP
West Frontage Rd	RT	G20-2 (End Road Work)	8	SF	South of BOP
TOTAL			**48**	**SF**	

619-G4005 Barricades, Type III, Single Faced					
Station	**Location**	**Description**	**Quantity**	**Unit**	**Remarks**
US 49 NB	LRL & RRL	Mounted on G20-1	12	LF	
US 49 SB	LLL & RLL	Mounted on G20-1	12	LF	
US 49 NB	LRL & RRL	Mounted on G20-2	12	LF	
US 49 SB	LLL & RLL	Mounted on G20-2	12	LF	
TOTAL			**48**	**LF**	

619-G4001 Barricades, Type III, Double Faced					
Station	**Location**	**Description**	**Quantity**	**Unit**	**Remarks**
East Frontage Rd	RT	Mounted on W20-1	6	LF	
West Frontage Rd	LT	Mounted on W20-1	6	LF	
East Frontage Rd	LT	Mounted on G20-2	6	LF	
West Frontage Rd	RT	Mounted on G20-2	6	LF	
TOTAL			**24**	**LF**	

EXTRA DEPTH REPAIR, 109444/301000							
Station	Station	Location	Width (ft)	Length (ft)	406-D001 Fine Milling of Bituminous Pavement, All Depths, SY	403-B001 12.5-mm, HT, Asphalt Pavement, Leveling, Ton	407-A001 Asphalt for Tack Coat, Gal
West Frontage Rd							
23+32	25+45	SB RT TURN LN	14	213	331	91	33
23+32	25+45	SB LN	12	213	284	78	28
23+32	25+45	NB LT TURN LN	12	213	284	78	28
23+32	25+45	NB LN	12	213	284	78	28
TOTAL					1183	325	117

NOTE: AREA LISTED IN TABLE SHALL BE CORRECTED PRIOR TO PAVING 9.5 MM SMA ASPHALT.
12.5 MM HT LEVELING TO BE PLACED IN TWO LIFTS AT 2.5 INCHES, OR AS DIRECTED BY THE ENGINEER

FULL DEPTH PUNCHOUT REPAIRS, 109444/301000

Station	Station	Location	Width (ft)	Length (ft)	503-C010 Saw Cut, Full Depth, LF	503-C004 Saw Cut, 3-inch, LF	503-B001 Saw Cut Longitudinal Joint, LF	202-B009 Removal of Asphalt Pavement, Failed Areas, SY	202-B069 Removal of Conc. Pvmt w/ Var. Depth Overlay, SY	503-D001 Concrete for Base Repair, CY	503-E002 Tie Bars, NO.5 Deformed, Drilled And Epoxied or Grouted, EA	503-A001 8" and Variable Reinforced Concrete Pavement, Broom Finish, SY	403-B001 12.5-mm, HT, Asphalt Pavement, Leveling, Ton	407-A001 Asphalt for Tack Coat, Gal
East Frontage Rd														
986+80	988+88	SB LN	14	208	236			323.556					196	32.356
990+35	991+00	SB LN	14	65	93			101.111					61	10.111
991+70	994+50	SB LN	14	280	308			435.556					264	43.556
1006+10	1007+06	SB LN	14	96	124			149.333					90	14.933
16+40	16+60	SB LN	18	20	76			40.000					18	4.000
West Frontage Rd														
976+51	976+59	Ramp	24	8	48	48			21.333	4.000		21.333	6	2.133
985+30	986+66	NB RT	12.5	136	161			188.889					114	18.889
985+55	985+85	NB LT	11	30	82			36.667					22	3.667
987+75	989+11	NB RT	12.5	136	161			188.889					114	18.889
993+50	994+06	RRL	12.5	56	81			77.778					47	7.778
999+25	1000+30	SB LN	12	105	234			140.000					85	14.000
1006+48	1007+38	NB LN	12.5	90	205			125.000					76	12.500
1006+75	1008+30	SB LN	12.5	155	335			215.278					130	21.528
27+36	28+19	SB LN	12.5	83	191			115.278					51	11.528
US 49 North Bound														
988+19	988+27	RRL	12.5	8	25	25	16		11.111	2.000	8	11.111	3	1.111
989+90	990+07	RRL	12.5	17	25	25	34		23.611	4.000	17	23.611	6	2.361
1063+46	1063+54	RT CL	12	8	32	24	8		10.667	2.000	4	10.667	3	1.067
US 49 South Bound														
979+00	979+93	Ramp	30	211	60	60	422		703.333	117.000	211	703.333	193	70.333
983+79	984+47	Ramp	17	68	102	34	68		128.444	21.000	34	128.444	35	12.844
986+37	987+00	Ramp	12	63	87	24	63		84.000	14.000	32	84.000	23	8.400
TOTAL					2666	240	611	2137.333	982.500	164.000	306	982.500	1537	311.983

Note: CRCP repairs were estimated using the PR-1B Typical CRC Pavement Repair Standard. If the Contractor elects to use PR-1A Optoinal Welding Method, then the pay item quantities will be adjusted accordingly.

GUARD RAIL QUANTITIES 109444/301000

GUARDRAIL					TERMINAL END			BRIDGE END			DELINEATORS				
STATION	STATION	LOCATION (LT/RT)	THRIE BEAM	W-BEAM	FLARED END SECTION	NON FLARED END SECTION	ANCHOR TYPE 1	SPECIAL SECTIONS, GUARD RAIL BRIDGE END CONNECTOR	TYPE "A"	TYPE "I"	WHITE	YELLOW	TYPE 3 OBJECT MARKERS (EA)	GUARDRAIL REMOVAL	REMARKS
16+67	17+23	RT/RT	18.75	0	1					1	3		1	56.25	NO 'W' BEAM REQUIRED
945+28	946+20	RT LN		37.5		1		1	1		4		1		WEST FRONTAGE RD
945+28	946+20	RT LN		37.5		1		1	1		4		1		EAST FRONTAGE RD. RELOCATION OF MAILBOXES MAY BE REQ.
949+63	950+56	LT LN		37.5		1		1	1		4		1		WEST FRONTAGE RD
949+63	950+56	RT LN		0				1					1	0	EXISTING GUARDRAIL TO REMAIN. SPECIAL SECTION REQUIRED
949+63	950+56	LT LN		37.5		1		1	1		4		1		EAST FRONTAGE RD
1005+00	1015+06	RT/RT	18.75	950	1					1	20		1	1006.25	
1005+46	1015+77	LT/LT	18.75	975			1			1	21		1	1031.25	
1020+20	1030+26	LT/LT	18.75	950	1					1	20		1	1006.25	
1037+00	1041+06	RT/LT	18.75	350	1					1	10		1	406	
1048+76	1051+89	RT/RT		268.75	1		1				8		2	312.5	WOODEN POSTS ANCHORED IN CONCRETE.
1049+95	1052+95	LT/LT		256.25	1		1				8		2	300	
1053+07	1056+63	LT/LT		406.25	1		1				10		2	450	POSTS IN CONCRETE PAVEMENT. 12" BLOCKOUTS REQUIRED
1062+00	1063+87	RT/LT		143.75	1		1					5	1	187	
TOTAL =			93.75	4450	8	4	5	5	4	5	116	5	17	4755.5	
			LF.	LF.	EA.	EA.	EA.	EA.	EA.	EA.	EA.	EA.	EA.	LF.	

REMOVAL OF ALL GUARDRAIL (BRIDGE END SECTIONS, W-BEAM, TYPE-I CABLE ANCHORAGE, TERMINAL END SECTIONS, ETC.) WILL BE PAID UNDER PAY ITEM 202-B REMOVAL OF GUARD RAIL.

REMOVAL OF GUARDRAIL DELINEATORS ARE CONSIDERED INCIDENTAL TO THE REMOVAL OF GUARDRAIL AND WILL NOT BE MEASURED AS A SEPARATE PAY ITEM.

ALL GUARDRAIL (METAL RAIL AND METAL POSTS ONLY) WILL BE RETAINED BY THE CONTRACTOR. WOODEN POSTS, ALL BLOCKOUTS, CONCRETE ANCHORS, ETC. WILL BE THE PROPERTY OF THE CONTRACTOR.

TOTAL GUARDRAIL LENGTH IS BASED ON A TERMINAL END SECTION 37.5' LONG. IF A TERMINAL END SECTION OF A DIFFERENT LENGTH IS USED, THE LENGTH OF THE W-BEAM MAY HAVE TO BE ADJUSTED.

REMOVAL OF OBJECT MARKERS WILL NOT BE MEASURED AS A SEPARATE PAY ITEM AND SHALL BE ABSORBED IN OTHER ITEMS

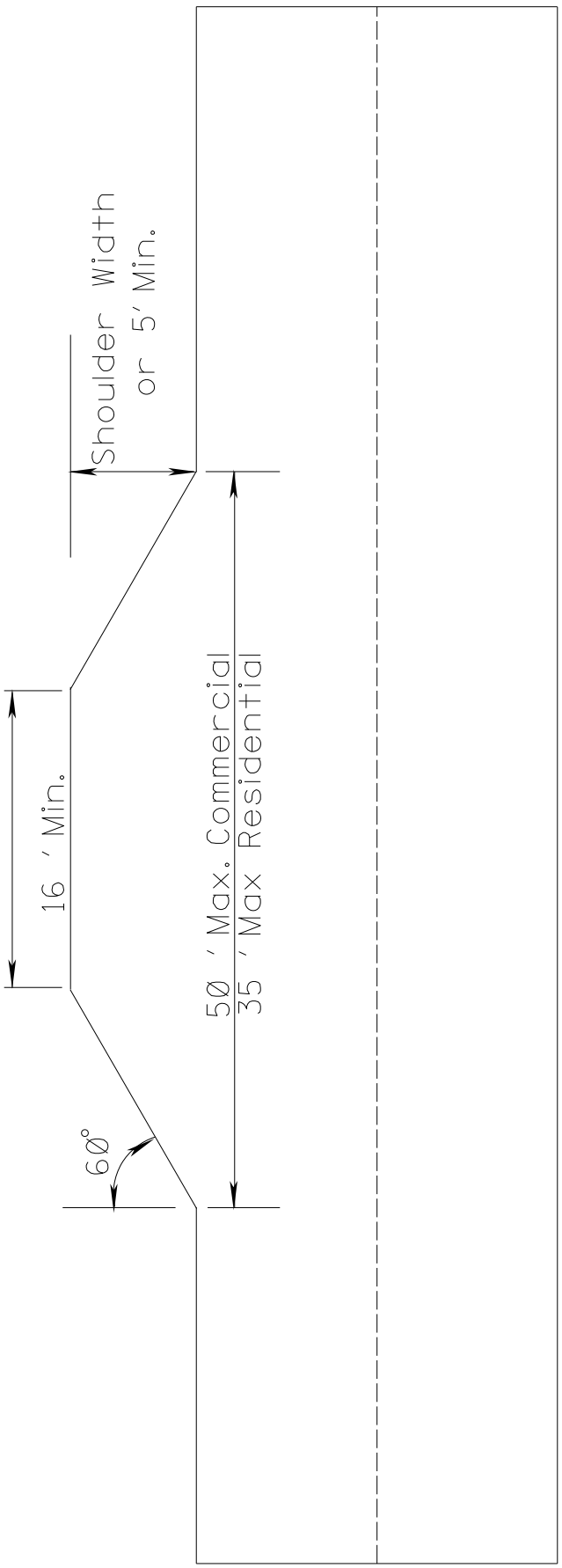
INCIDENTAL WORK SUCH AS REMOVING AND RESETTING MAILBOXES WILL NOT BE MEASURED FOR SEPARATE PAYMENT AND WILL BE CONSIDERED ABSORBED IN OTHER ITEMS

ROADWAY & BRIDGE JOINT REPAIRS 109444/301000								
Roadway				Bridge #16913				
Station	Location	907-413-D001 Cleaning and Filling Joints and Cracks, LF	907-413-E001 Sawing and Sealing Transverse Joints in Asphalt Pavement. LF	907-808-A002 Joint Repair, LF	907-823-A001 Preformed Joint Seal, Type I, LF	907-823-B001 Saw Cut, Type I, LF	907-824-A003 General Epoxy Repairs, SF	Remarks
US 49 North Bound								
1015+11	LT CL	12	12					
1015+11	RT CL	12	12					
1015+51	LT CL	12	12					
1015+51	RT CL	12	12					
US 49 South Bound								
1015+62	LT CL	12	12					
1015+62	RT CL	12	12					
1016+56	LT CL			20	10	20	2	End Wall
1019+26	LT CL			20	10	20	3	End Wall & Rail
1019+80	LT CL	12	12					
1019+80	RT CL	12	12					
1020+20	LT CL	12	12					
1020+20	RT CL	12	12					
TOTAL		120	120	40	20	40	5	

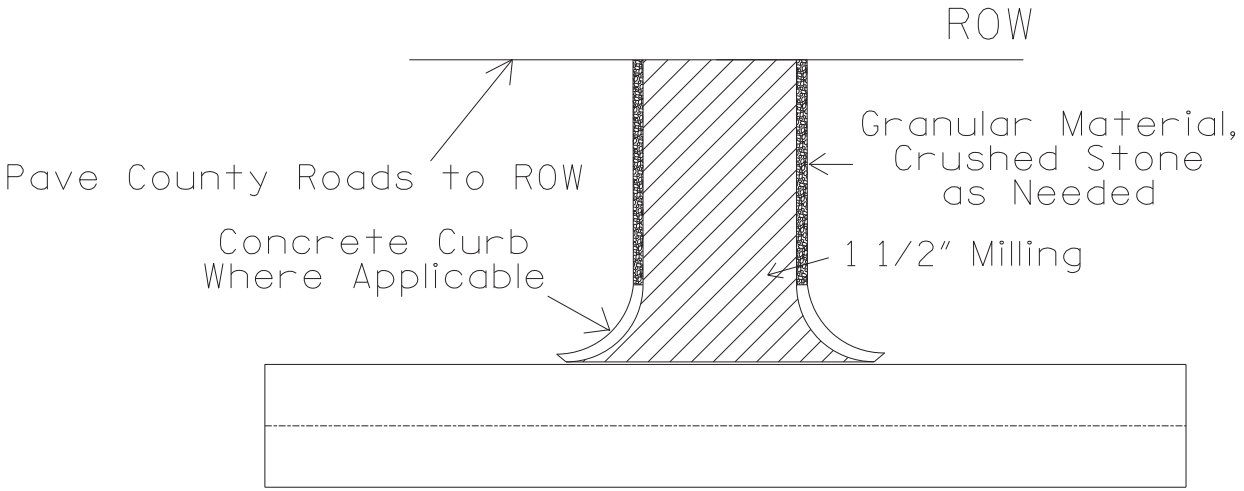
LONGITUDINAL JOINT REPAIRS, 109444/301000							
Station	Station	Location	Length	Width Avg.	406-A002 Cold Milling of Bituminous Pavement, All Depths, SY	403-B001 12.5-mm, HT, Asphalt Pavement, Leveling, Ton	407-A001 Asphalt for Tack Coat, Gal
US 49 Northbound							
965+66	969+58	RT LN	392	4	174	50	17
986+64	989+00	RT LN	236	4	105	30	11
988+08	990+00	CT LN	192	4	85	25	9
1023+42	1024+11	RT CL	69	4	31	9	3
1024+17	1024+55	LT CL	38	4	17	5	2
1025+58	1026+37	RT CL	79	4	35	10	4
1026+20	1026+76	LT CL	56	4	25	7	3
1064+00	1064+65	RRL	65	4	29	8	3
US 49 Southbound							
964+73	967+25	LT LN	252	4	112	32	11
964+73	972+00	RT LN	727	4	323	93	32
986+45	987+00	RT RAMP	55	4	24	7	2
986+50	987+00	RT CL	50	4	22	6	2
1021+20	1022+70	RT CL	150	4	67	19	7
1022+00	1024+00	LT CL	200	4	89	26	9
1025+36	1026+22	LRL	86	4	38	11	4
1026+70	1029+52	LT CL	282	4	125	36	13
1063+60	1064+70	LT LN	110	4	49	14	5
20 WB Ramp to 49 S							
707+00	707+65	RT LN	65	4	29	8	3
TOTAL			1912		1379	396	140

REMOVAL OF SIGN FOOTING 109444/301000			
Station	**Location**	**202-B214 Removal of Sign Footing, EA**	**Remarks**
US 49 South Bound			
1044+85	**LT**	**1**	**Concrete Footing & Stub.**
SURROUNDING AREA SHALL BE BACKFILLED WITH CRUSHED STONE, COSTS WHICH ARE TO BE PAID UNDER THE APPROPRIATE PAY ITEM.			

TYPICAL RAMP/PAD DETAIL



Milling and Paving Detail
County Roads
US 49 Rankin County



County Roads:

Carrier Blvd
Distribution Dr
Lake Dr
Interstate Dr
Davis Johnson Dr

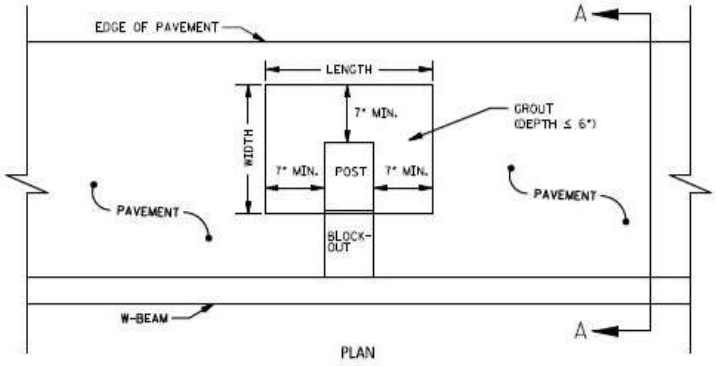
Notes:

-Mill limits of county/local roads at a depth of 1 1/2".

-Place 1 1/2" of HT 9.5mm Mixture to tie to mainline overlay.

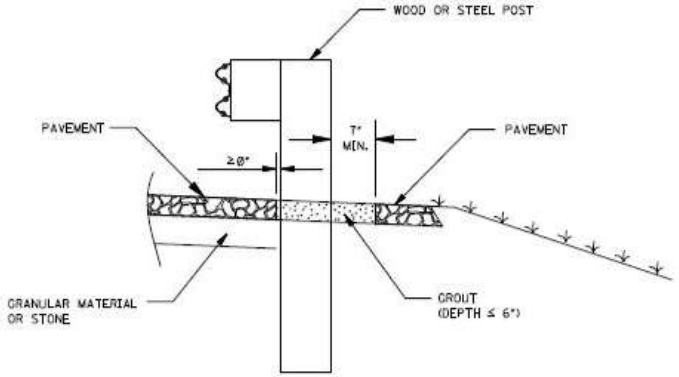
-Milling/Paving area = ▨

Guardrail Post Installation in Paved Areas



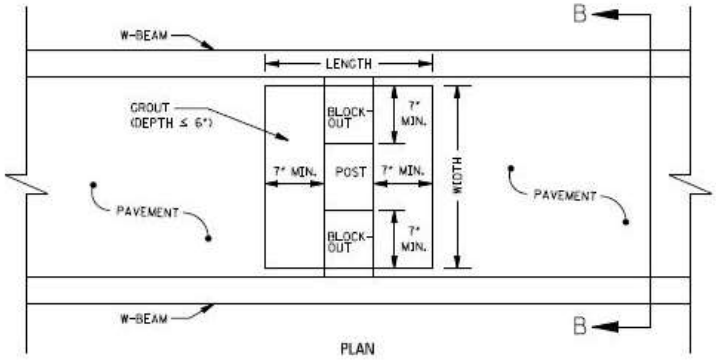
PLAN

PAVEMENT LEAVE-OUT AREA FOR GUARDRAIL POSTS
SINGLE-FACED GUARDRAIL



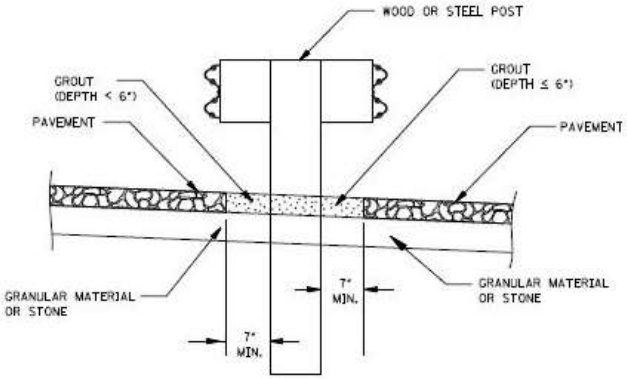
SECTION A–A

MIN. PAV'T LEAVE-OUT AREA				
POST	SINGLE-FACED		DOUBLE-FACED	
	LENGTH (IN.)	WIDTH (IN.)	LENGTH (IN.)	WIDTH (IN.)
6"x8" WOOD	20	15	20	22
8"x8" WOOD	22	15	22	22
10"x10" WOOD	24	17	24	24
W6x9 STEEL	18	13	18	20



PLAN

PAVEMENT LEAVE-OUT AREA FOR GUARDRAIL POSTS
DOUBLE-FACED GUARDRAIL

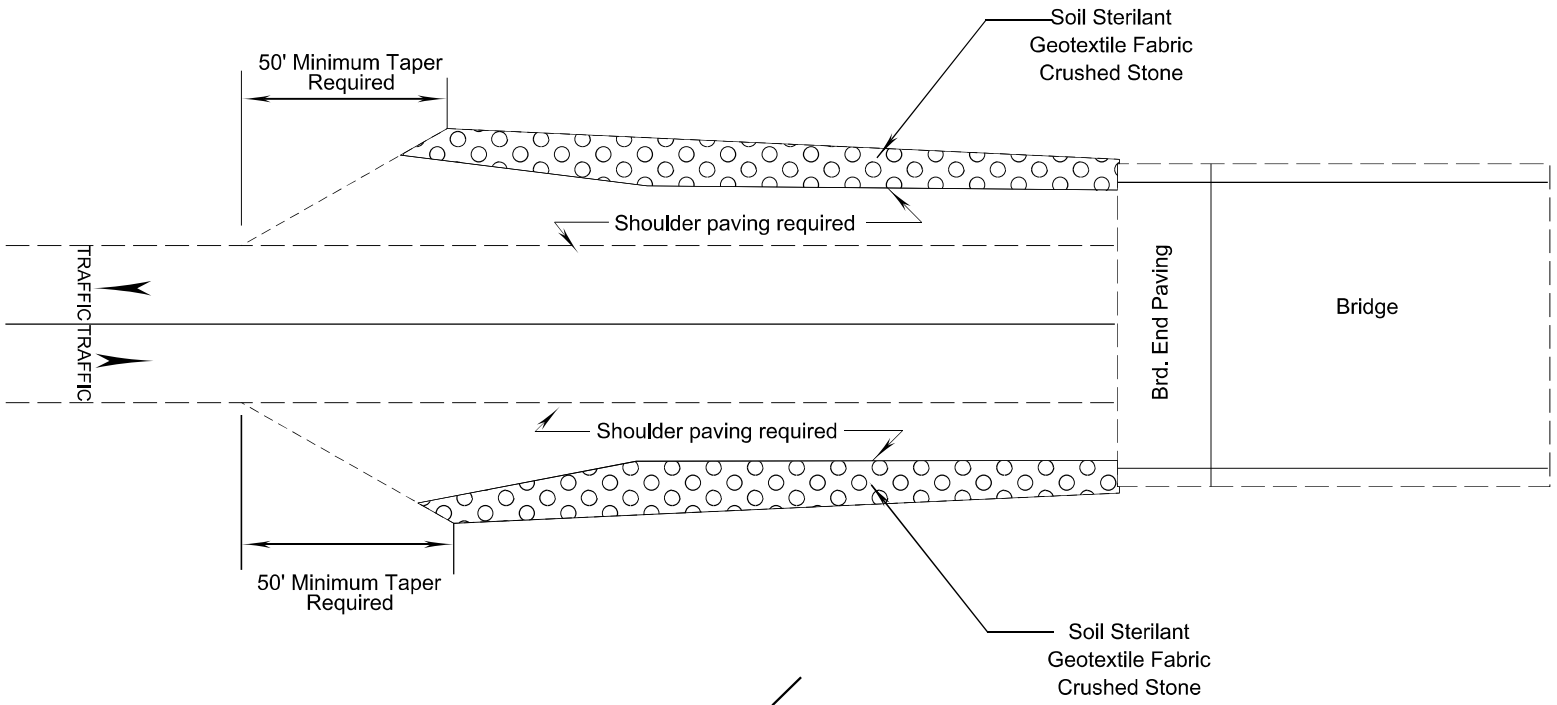


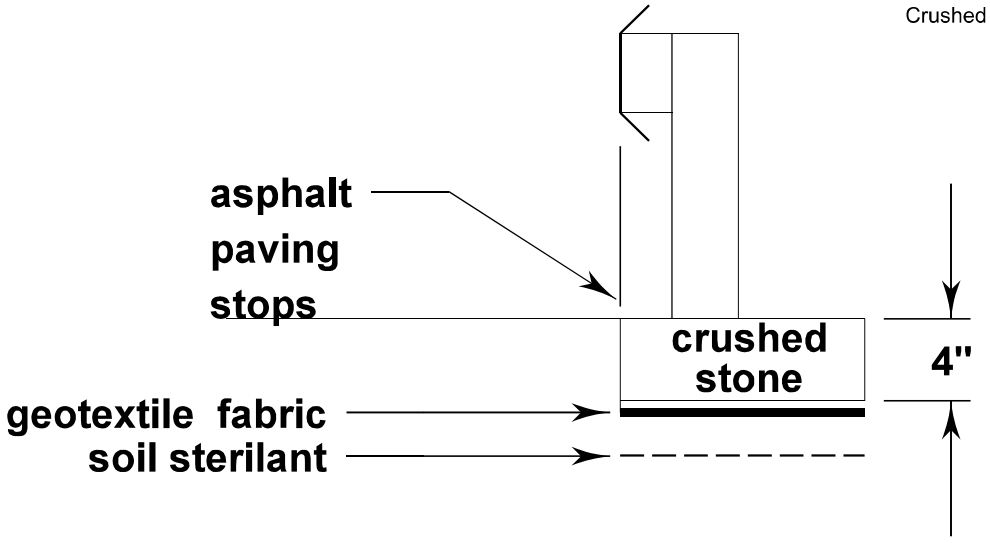
SECTION B–B

GENERAL NOTES

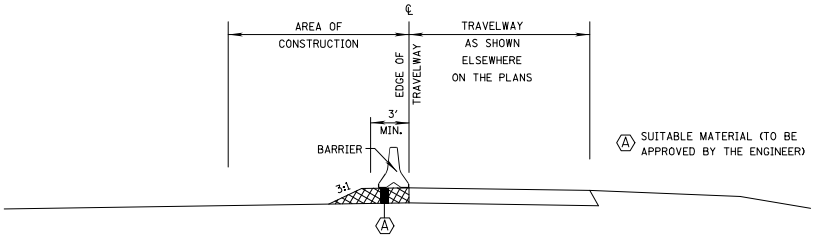
1. GUARDRAIL POSTS SHALL NOT BE COMPLETELY SURROUNDED BY PAVEMENT. THE AREA BEHIND AND LATERAL OF THE POST SHALL HAVE A MINIMUM 7" CLEARANCE FROM THE PAVEMENT. THIS AREA SHALL BE FILLED WITH A LOW STRENGTH GROUT WITH A MAXIMUM 28 DAY COMPRESSIVE STRENGTH OF 120 PSI.
2. GROUT SHALL BE INSTALLED AT A DEPTH EQUAL TO THE SURROUNDING PAVEMENT, UP TO A MAXIMUM OF 6". IF SURROUNDING PAVEMENT IS GREATER THAN 6", THE DIFFERNCE SHALL BE FILLED IN WITH SHOULDER GRANULAR MATERIAL.
3. COST OF GROUT SHALL BE ABSORBED IN THE COST OF OTHER ITEMS BID.
4. PAVEMENT LEAVE-OUT AREAS ARE REQUIRED FOR STEEL AND WOOD POSTS.
5. STANDARD EMBEDMENT DEPTHS STILL APPLY, MEASURED FROM THE TOP OF THE PROJECTED PAVEMENT SURFACE.

TYPICAL DETAIL OF ADDITIONAL SHOULDER PAVING REQUIRED AT GUARDRAIL LOCATIONS





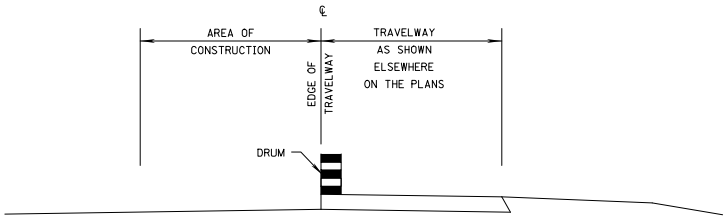
STATE	PROJECT NO.
MISS.	UPDATE



ELEVATION VIEW FOR POSITIVE BARRIER

NOTES:

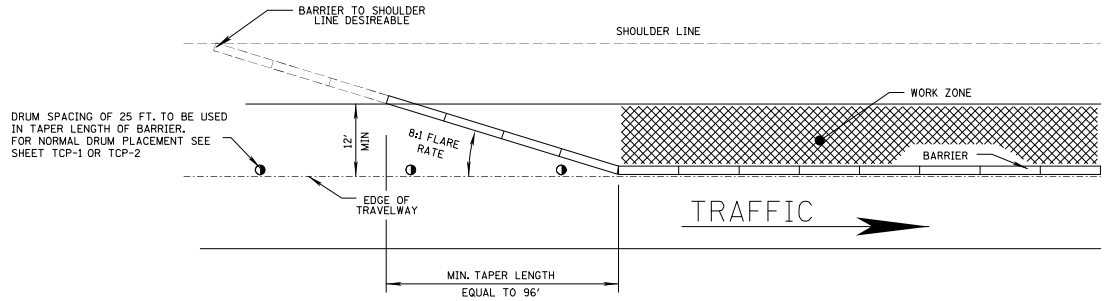
1. POSITIVE BARRIER IS REQ'D IN THE AREA OF OPEN PUNCH OUTS THAT ARE WITHIN SIX (6) FEET OF THE TRAVELWAY WHENEVER ACTUAL REPAIR WORK IS NOT BEING PERFORMED WITHIN THE LANE CLOSURE.
2. MATERIAL USED TO SUPPORT POSITIVE BARRIER MUST BE AT SAME ELEVATION AS PAVEMENT IN ADJACENT TRAVELWAY.
3. DELINEATORS REQUIRED ON ALL NON-REFLECTIVE BARRIER, AS SHOWN ON WORKING NO. CMB-3.



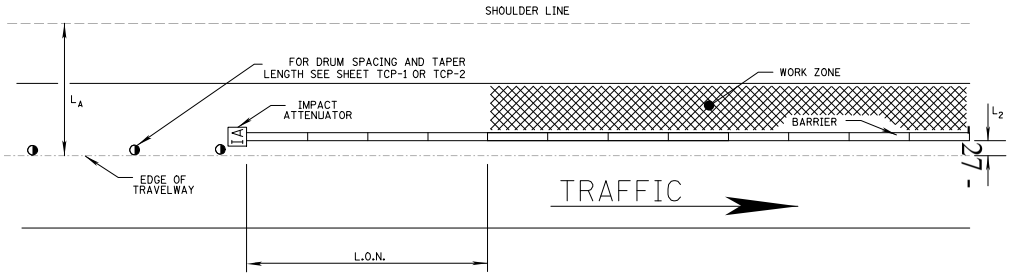
ELEVATION VIEW FOR DRUM

NOTES:

1. WHILE WORK IS BEING PERFORMED WITHIN THE LANE CLOSURE, DROP-OFFS MUST BE PROTECTED WITH DRUMS, ETC. IN EMERGENCIES EXCAVATED SECTION MAY BE BACKFILLED WITH GRANULAR MATERIAL, STONE OR OTHER APPROVED MATERIAL TO AVOID OVERNIGHT DROP-OFFS.
2. LANE CLOSURES WITH OPEN PUNCH OUT AREAS MAY NOT BE LEFT UNATTENDED WHEN DRUMS ARE BEING USED FOR LANE CLOSURE



DETAIL OF POSITIVE BARRIER WITH TAPER



DETAIL OF POSITIVE BARRIER WITH IMPACT ATTENUATOR

NOTES:

1. LENGTH OF NEED, L.O.N. = $\frac{L_R(L_A - L_2)}{L_A}$

WHERE: L_A = LATERAL EXTENT OF THE AREA OF CONCERN
L_R = RUNOUT LENGTH
L_2 = LATERAL OFFSET FROM EDGE OF TRAVELED WAY TO BARRIER.

2. RUNOUT LENGTH (L_R) IS TO BE DETERMINED USING THE FOLLOWING TABLE:

RUNOUT LENGTH (L_R) GIVEN TRAFFIC VOLUME (ADT) (ft)				
DESIGN SPEED (mph)	OVER 10,000 veh/day	5,000-10,000 veh/day	1,000-5,000 veh/day	UNDER 1,000 veh/day
70	360	330	290	250
60	300	250	210	200
50	230	190	160	150
40	160	130	110	100
30	110	90	80	70

GENERAL NOTES:

1. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER OTHER BID ITEMS.
2. FOR DETAILS OF DRUM PLACEMENT SEE OTHER TRAFFIC CONTROL PLANS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

LANE CLOSURE DETAILS FOR GREATER THAN 3 INCH DROPOFF

PROJ. NO.: UPDATE
COUNTY: UPDATE

REVISION | BY | DATE

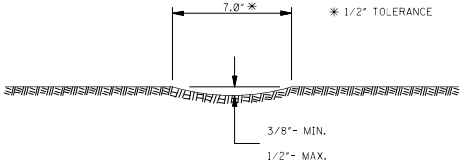
FILENAME: SDTCP-C.DGN
DESIGN TEAM UPDATE CHECKED UPDATE DATE UPDATE

PRELIMINARY
NOT FOR CONSTRUCTION

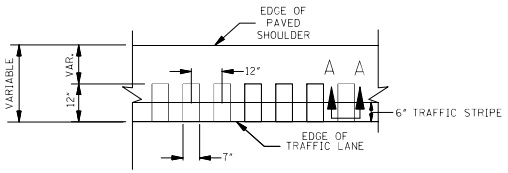
WORKING NUMBER
SDTCP-C

SHEET NUMBER
PG

STATE	PROJECT NO.
MISS.	



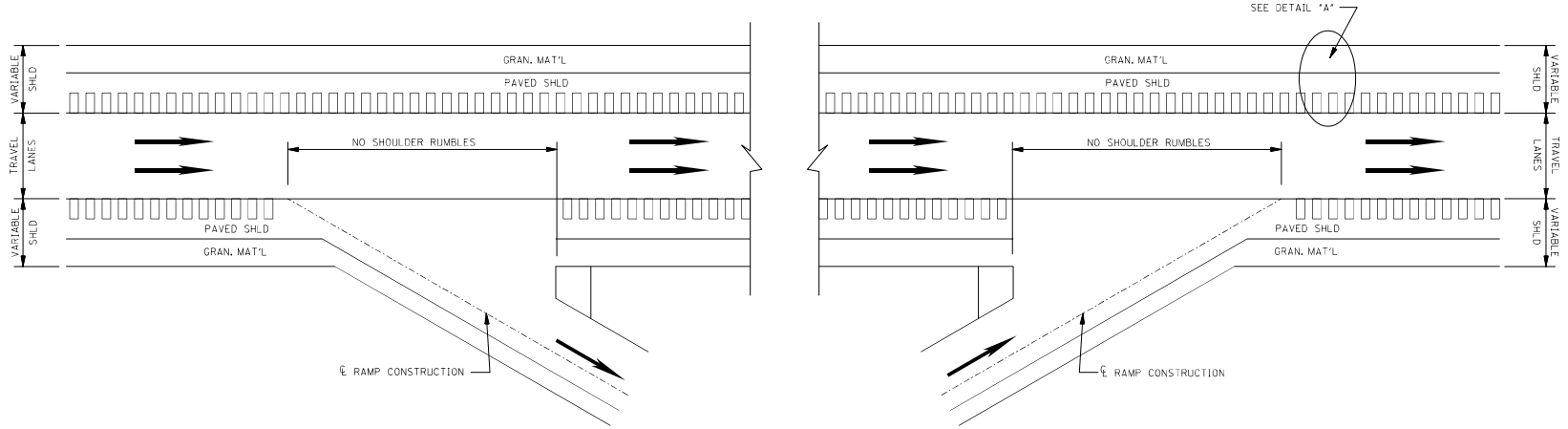
SECTION "A–A"



DETAIL "A"

GENERAL NOTES

1. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT
2. GROUND-IN RUMBLE STRIPES SHALL BE OMITTED ACROSS PUBLIC INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
3. COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS
4. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO:
   - A. MAINLINE
   - B. INTERSECTING ROADWAY IF OVERLAID OR RECONSTRUCTED BEYOND NORMAL MAINLINE R.O.W.
   - C. ANY ROADWAY WITH EXISTING RUMBLE STRIPES PRIOR TO CONSTRUCTION.



PLAN

NOT TO SCALE

DETAILS OF RUMBLE STRIPS

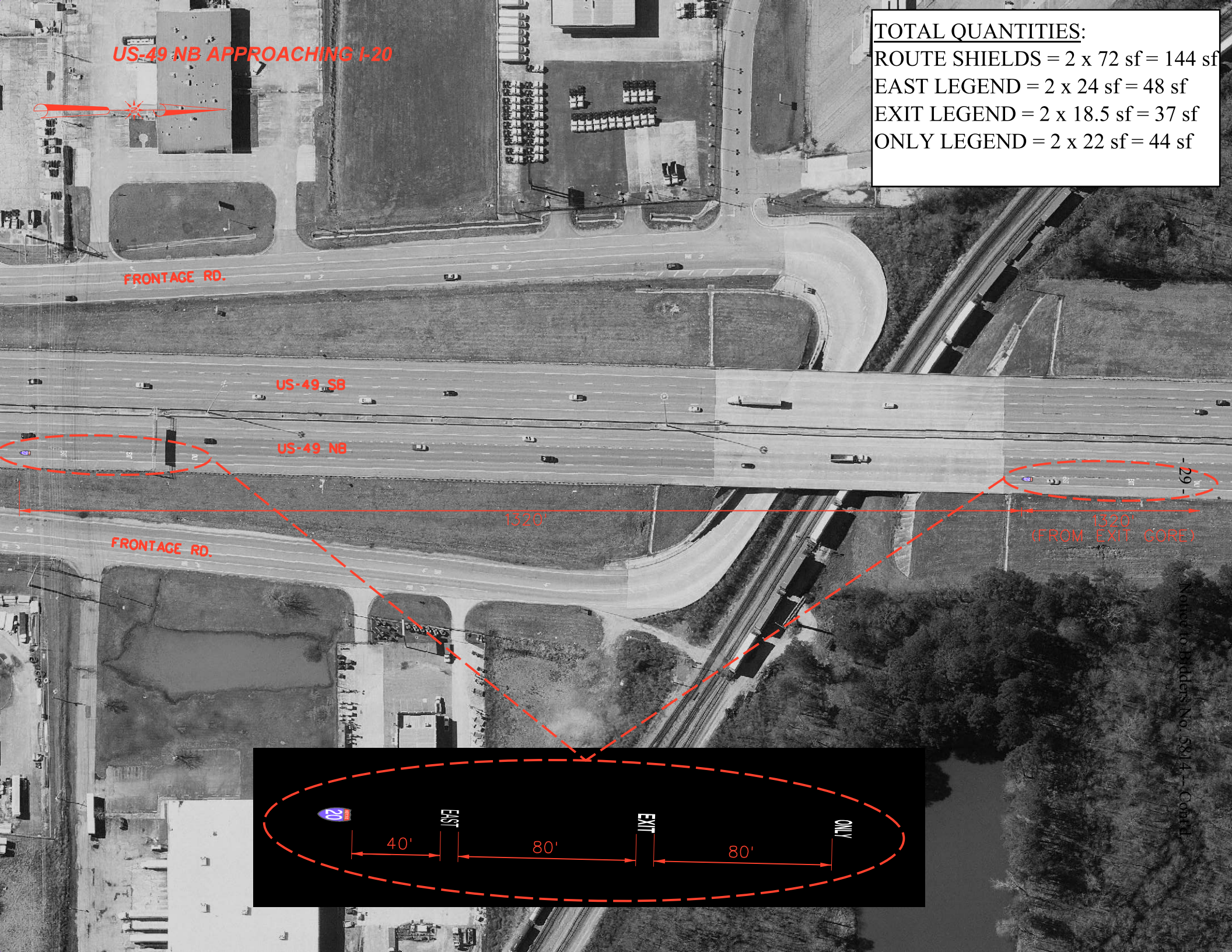
DATE	REVISION	BY
2/8/21	REVISED STRIPE LOCATION	DSP

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

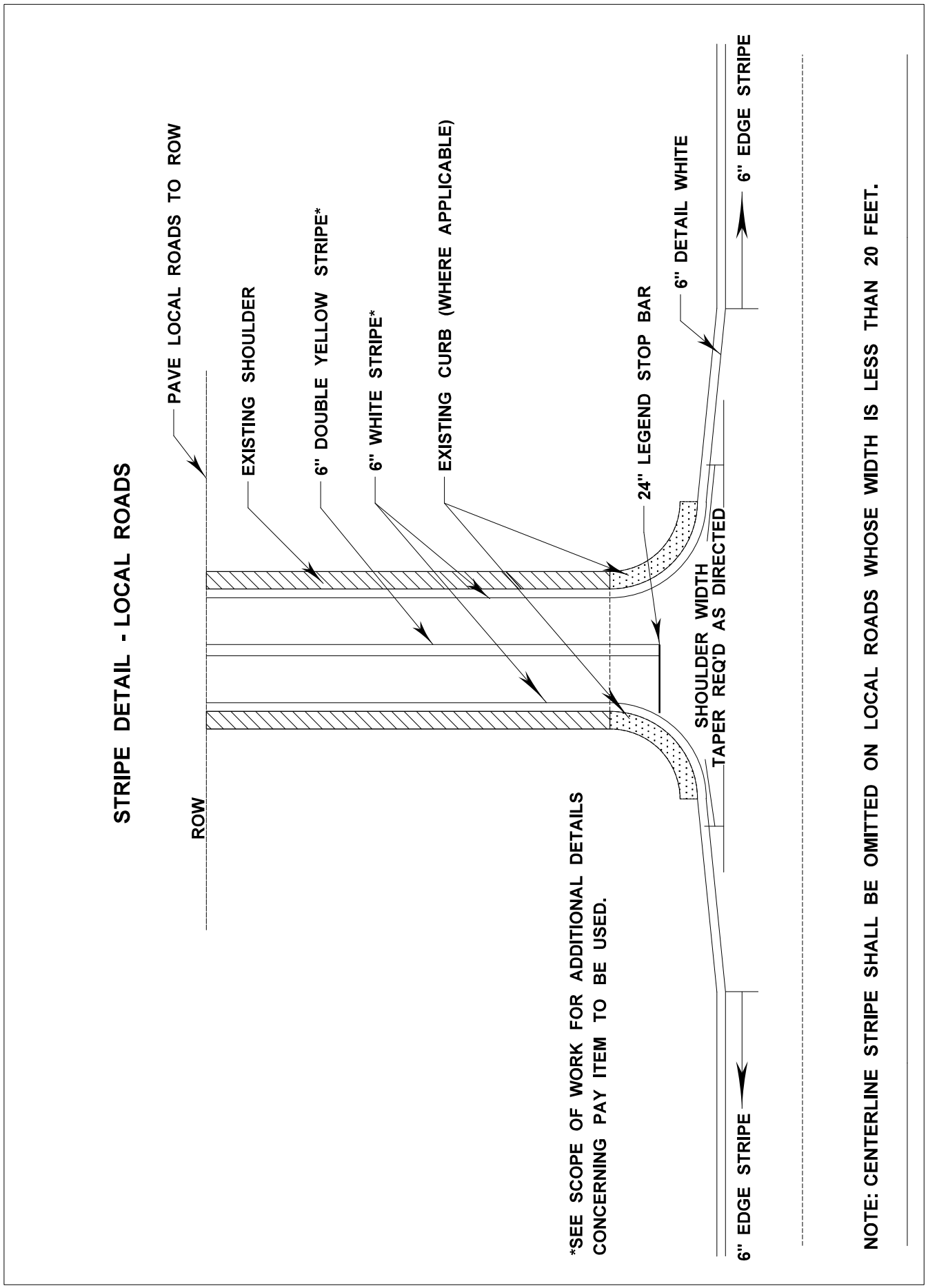
RUMBLE STRIPES 4-LANE HIGHWAYS (ASPHALT LANES, 2-FT OR WIDER ASPHALT SHOULDERS)

ISSUE DATE: AUGUST 01, 2017

MDOT
DRAWING NUMBER
RRS-2
SHEET NUMBER
6065

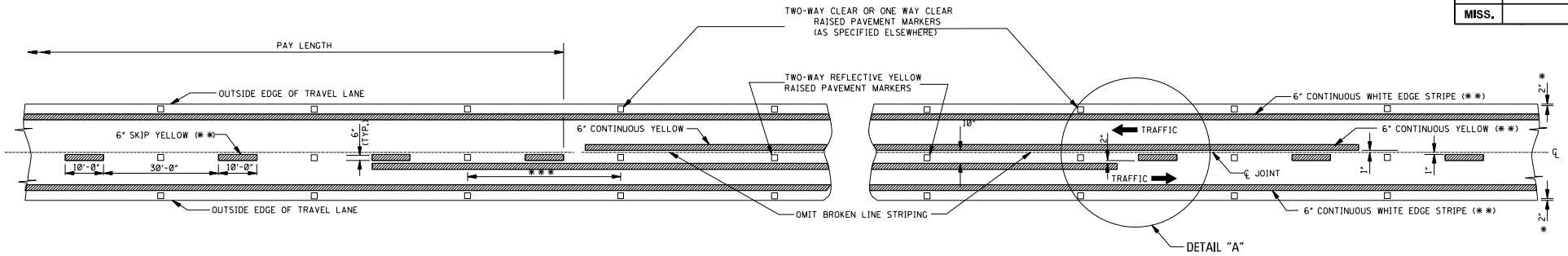


TOTAL QUANTITIES:
ROUTE SHIELDS = 2 x 72 sf = 144 sf
EAST LEGEND = 2 x 24 sf = 48 sf
EXIT LEGEND = 2 x 18.5 sf = 37 sf
ONLY LEGEND = 2 x 22 sf = 44 sf


STRIPE DETAIL - LOCAL ROADS
PAVE LOCAL ROADS TO ROW
EXISTING SHOULDER
6" DOUBLE YELLOW STRIPE*
6" WHITE STRIPE*
EXISTING CURB (WHERE APPLICABLE)
24" LEGEND STOP BAR
6" DETAIL WHITE
6" EDGE STRIPE
ROW
SHOULDER WIDTH TAPER REQ'D AS DIRECTED
6" EDGE STRIPE
*SEE SCOPE OF WORK FOR ADDITIONAL DETAILS CONCERNING PAY ITEM TO BE USED.
NOTE: CENTERLINE STRIPE SHALL BE OMITTED ON LOCAL ROADS WHOSE WIDTH IS LESS THAN 20 FEET.

FMS CON:

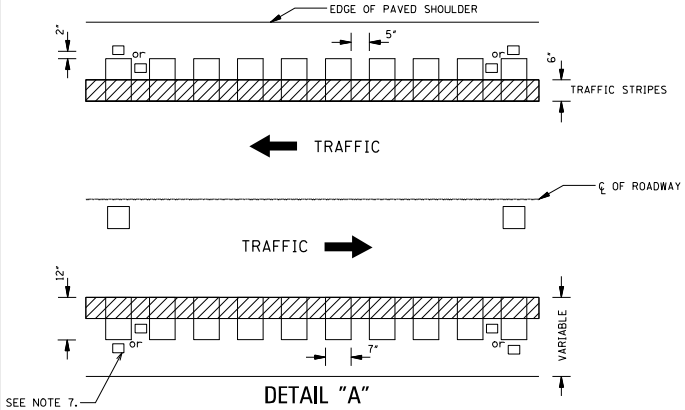
STATE	PROJECT NO.
MISS.	



TWO–WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)

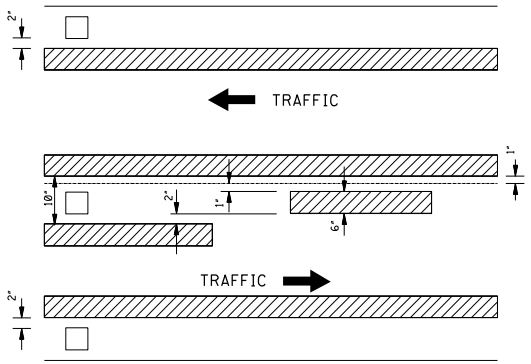
DIRECTION OF TRAFFIC

* TWO-WAY TRAFFIC
TWO-WAY CLEAR OR ONE-WAY CLEAR
RAISED PAVEMENT MARKERS
(AS SPECIFIED ELSEWHERE)



DETAIL "A"
PLACEMENT OF RPMs ON
PAVEMENTS WITH RUMBLE STRIPS

* TWO-WAY TRAFFIC
TWO-WAY CLEAR OR ONE-WAY CLEAR
RAISED PAVEMENT MARKERS
(AS SPECIFIED ELSEWHERE)



DETAIL "A"
PLACEMENT OF RPMs ON
PAVEMENTS WITHOUT RUMBLE STRIPS

GENERAL NOTES:

* 1. 2" UNLESS SHOWN ELSEWHERE ON THE PLANS. FOR STRIPING ON RUMBLE STRIP SECTIONS REFER TO WK. SHEETS RS-1, RS-2, AND RS-3.

* * 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR PLASTIC AS INDICATED IN PAY ITEMS).

* * * 3. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

	URBAN AREA (ft-in)	RURAL AREA (ft-in)
TANGENT SECTIONS	40'-0"	80'-0"
HORIZONTAL CURVES	40'-0"	40'-0"
INTERCHANGE LIMITS	40'-0"	† 40'-0"

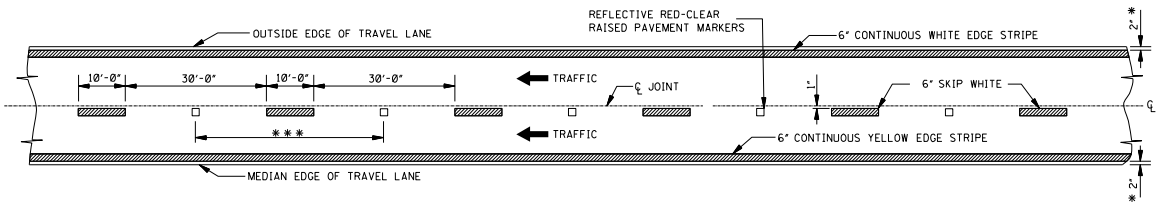
† NOTE: ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINE(S) THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.

4. EDGE LINE RAISED PAVEMENT MARKERS ARE TO BE PLACED AT THE SAME SPACING AS THE CENTERLINE MARKERS. EDGE LINE MARKERS WILL BE OMITTED WHERE CONFLICTS OCCUR WITHIN INTERSECTIONS, CROSSOVERS, ETC. EDGE LINE RPMs WILL NOT BE PLACED IN URBAN AREAS UNLESS OTHERWISE SPECIFIED

5. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MDOT "APPROVED SOURCES OF MATERIALS."

6. THE CRITERIA FOR NO-PASSING ZONES CAN BE FOUNDIN THE MDOTROADWAY DESIGN MANUAL, SECTION 11-1.01.

7. WHERE RUMBLE STRIPS ARE PRESENT, THE EDGE LINE MARKERS WILL BE PLACED OUTSIDE THE RUMBLE STRIPS OR BETWEEN THE RUMBLE STRIPS AS DIRECTED BY THE ENGINEER.



4–LANE WITH ONE–WAY TRAFFIC

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

EDGE LINE
RAISED PAVEMENT MARKING
PLACEMENT
2-LANE ROADWAYS

PROJ. NO.:
COUNTY:

WORKING NUMBER
RPM-1

SHEET NUMBER

BY | REVISION | DATE

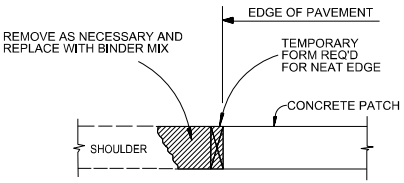
FILE NAME: RPM-1.dgn
DESIGN TEAM_____CHECKED_____DATE_____

PLAN
ROADWAY DESIGN DIVISION
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

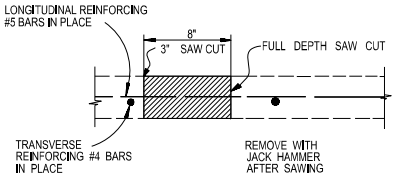
2/15/2023 1:31:11 PM RPM-1.DGN

Notice to Bidders No. 8814 - Cont.

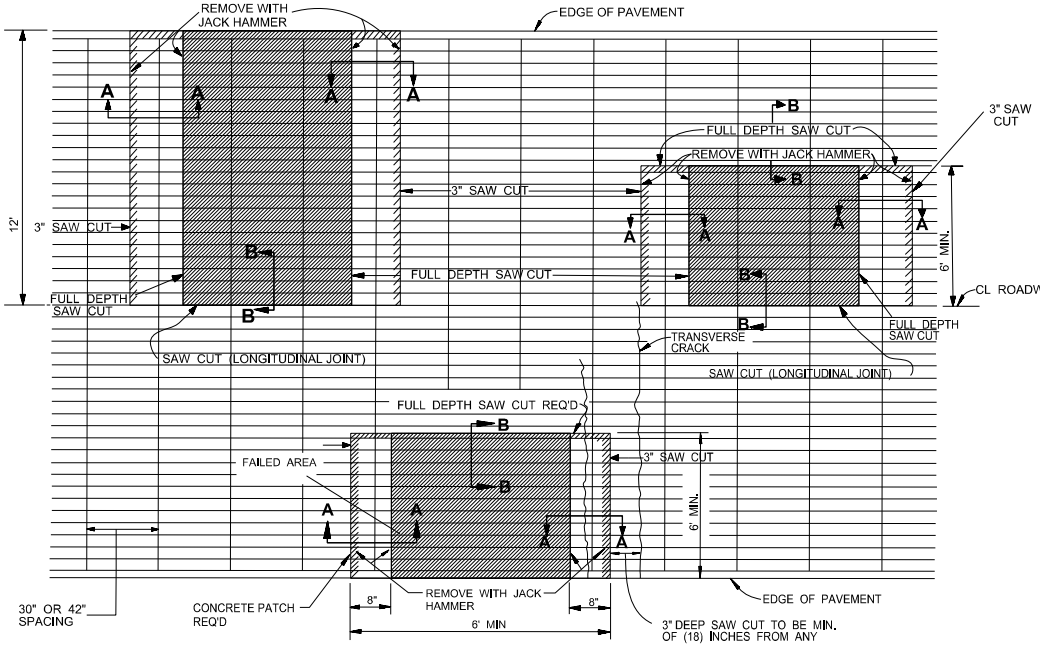
STATE	PROJECT NO.
MISS.	



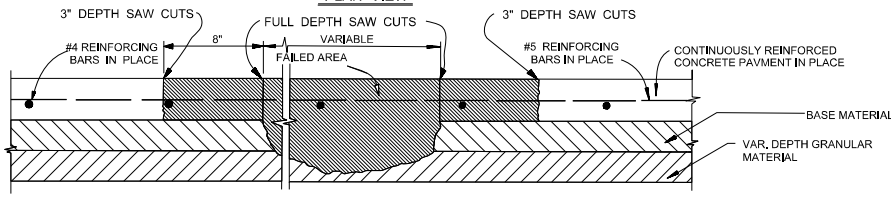
DETAIL FOR FORMING OUTER EDGE



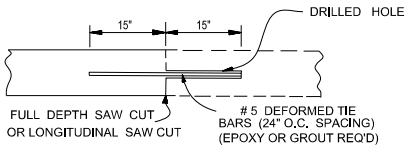
SECTION A - A



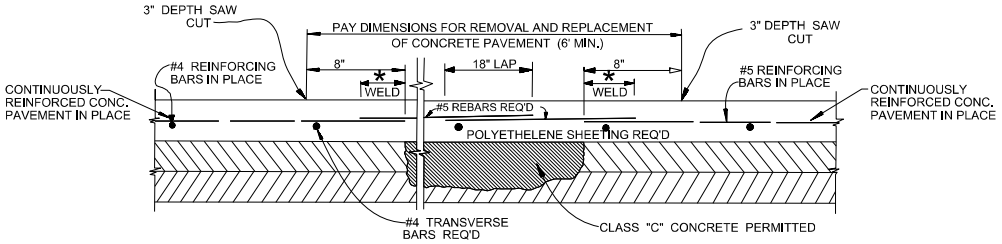
PLAN VIEW



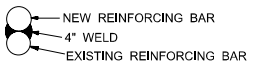
SECTIONAL VIEW (SHOWING AREA TO BE REMOVED)



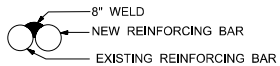
SECTION B - B



SECTIONAL VIEW (SHOWING REPLACED AREA)



DETAIL OF DOUBLE 4-INCH WELD



DETAIL OF SINGLE 8-INCH WELD

GENERAL NOTES

1. REMOVE EXISTING MATERIALS TO DIMENSIONS DETERMINED BY THE ENGINEER.
2. REMOVAL OF ASPHALT PATCHES AND CONCRETE PAVEMENT WILL BE PAID FOR UNDER THE APPROPRIATE PAY ITEM.
3. REINFORCING BARS TO BE FIELD CUT AS DIRECTED BY THE ENGINEER. COST OF REQUIRED REINFORCING BARS TO BE INCLUDED IN THE BID PRICE OF THE CONCRETE PAVEMENT
4. REMOVAL OF FAILED BASE (PAY AS REMOVAL OF CEMENT TREATED BASE - S.Y.). BACKFILL WITH CLASS "C" CONCRETE (BASE REPAIR).
5. PAVEMENT EDGE ADJACENT TO SHOULDER SHALL BE FORMED.
6. SEE SHEET NO. 102 FOR DETAILS NOT SHOWN.
7. POLYETHELENE SHEETING SHALL BE TWO (2) LAYERS OF 8 MIL THICKNESS. (ABSORBED ITEM).
8. REINFORCING BARS WILL BE SUPPORTED AS SHOWN ON SHEET NO. 102.
9. ALL SAW CUTS (3" DEPTH, FULL DEPTH, AND LONGITUDINAL JOINT) WILL BE PAID FOR UNDER APPROPRIATE PAY ITEMS.
10. #5 DEFORMED TIE BARS (30 IN. LONG, @ 24 IN. O.C. SPACING) WILL BE PAID FOR UNDER APPROPRIATE PAY ITEM.
11. THE TRANSVERSE BARS IN THE REPAIR AREA WILL BE SPACED ON 42" CENTERS REGARDLESS OF THE EXISTING SPACING OF THE TRANSVERSE STEEL.

* DOUBLE 4-INCH OR SINGLE 8-INCH WELD

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

TYPICAL CRC PAVEMENT REPAIR (OPTIONAL WELDING METHOD)

PROJECT NO. :
COUNTY :

REVISION	BY	DATE

FILENAME:
DESIGN TEAM CHECKED DATE 12-12-08

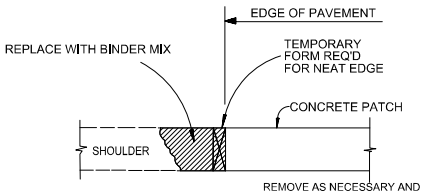
WORKING NUMBER R-1A
SHEET NUMBER

- 52 -

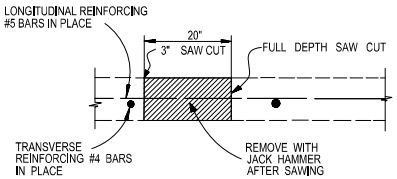
Notice to Bidders No. 584 -- Cont'd.

PLAN ROADWAY DESIGN DIVISION CLASS SECTION MISSISSIPPI DEPARTMENT OF TRANSPORTATION

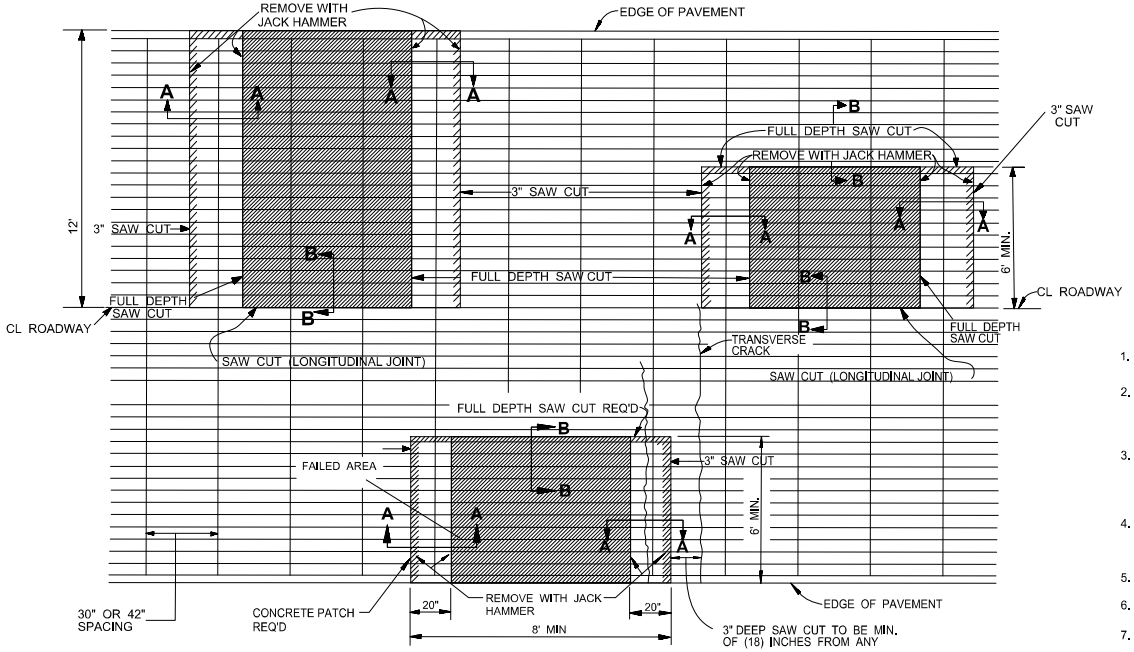
STATE	PROJECT NO.
MISS.	



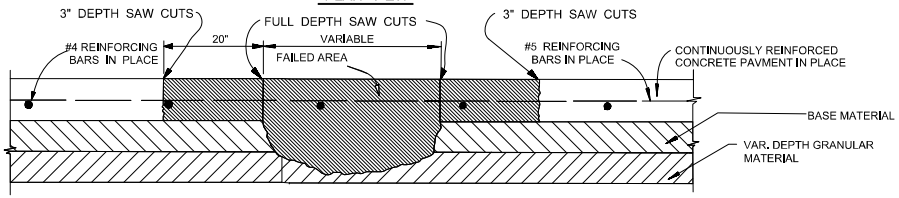
DETAIL FOR FORMING OUTER EDGE



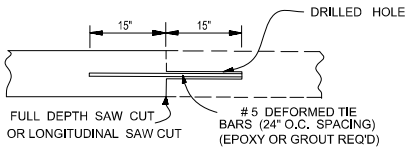
SECTION A - A



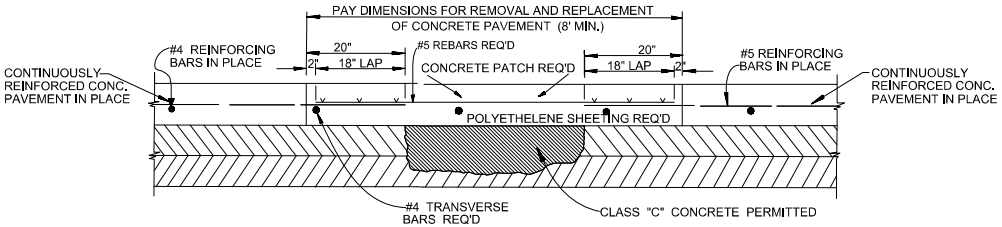
PLAN VIEW



SECTIONAL VIEW (SHOWING AREA TO BE REMOVED)



SECTION B - B



SECTIONAL VIEW (SHOWING REPLACED AREA)

GENERAL NOTES

1. REMOVE EXISTING MATERIALS TO DIMENSIONS DETERMINED BY THE ENGINEER.
2. REMOVAL OF ASPHALT PATCHES AND CONCRETE PAVEMENT WILL BE PAID FOR UNDER THE APPROPRIATE PAY ITEM.
3. REINFORCING BARS TO BE FIELD CUT AS DIRECTED BY THE ENGINEER. COST OF REQUIRED REINFORCING BARS TO BE INCLUDED IN THE BID PRICE OF THE CONCRETE PAVEMENT
4. REMOVAL OF FAILED BASE (PAY AS REMOVAL OF CEMENT TREATED BASE - S.Y.). BACKFILL WITH CLASS "C" CONCRETE (BASE REPAIR).
5. PAVEMENT EDGE ADJACENT TO SHOULDER SHALL BE FORMED.
6. SEE SHEET NO. 102 FOR DETAILS NOT SHOWN.
7. POLYETHELENE SHEETING SHALL BE TWO (2) LAYERS OF 8 MIL THICKNESS. (ABSORBED ITEM).
8. REINFORCING BARS WILL BE SUPPORTED AS SHOWN ON SHEET NO. 102.
9. ALL SAW CUTS (3" DEPTH, FULL DEPTH, AND LONGITUDINAL JOINT) WILL BE PAID FOR UNDER APPROPRIATE PAY ITEMS.
10. #5 DEFORMED TIE BARS (30 IN. LONG, @ 24 IN. O.C. SPACING) WILL BE PAID FOR UNDER APPROPRIATE PAY ITEM.
11. THE TRANSVERSE BARS IN THE REPAIR AREA WILL BE SPACED ON 42" CENTERS REGARDLESS OF THE EXISTING SPACING OF THE TRANSVERSE STEEL.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

TYPICAL CRC PAVEMENT REPAIR

PROJECT NO.:
COUNTY:
FILENAME:
DESIGN TEAM CHECKED DATE

WORKING NUMBER
CRC-1B
SHEET NUMBER

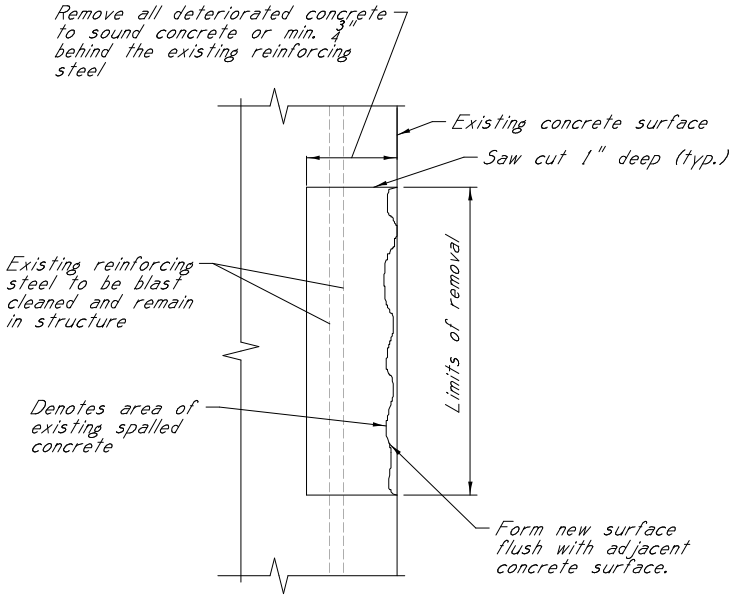
REVISION BY DATE

ROADWAY DESIGN DIVISION
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

M:\DDIVY 08\ 08AMPM DGN FILENAME

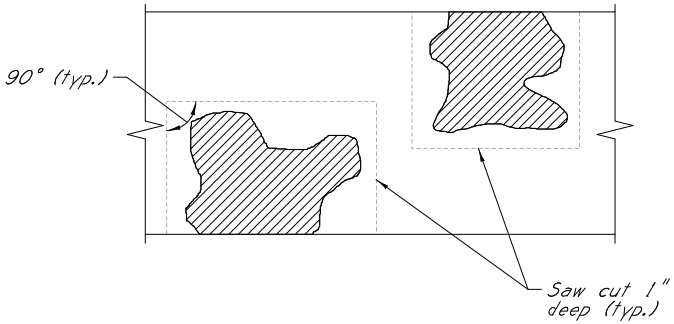
- 123 -

Notice to Bidders No. 814 -- Contract



~ Denotes areas of existing spalled concrete

*NOTE:
Saw cut existing concrete 1" deep so as to obtain a rectangular area. All existing reinforcement shall be carefully preserved and blast cleaned.



EPOXY MORTAR SPALL REPAIR DETAIL

EPOXY MORTAR REPAIR NOTES:

1. Repair concrete spalled areas on the bridge as directed by the Project Engineer using epoxy mortar.
2. Repair all concrete spalled areas listed on this page and as directed by the Project Engineer.
3. Repair any additional concrete spalled areas not listed on this page as directed by the Project Engineer.
4. Contractor shall sawcut around the perimeter of the damaged and unsound concrete.
5. The Contractor shall determine the depth of reinforcement prior to any saw cutting.
6. Spalled areas where pack rust has developed around or on reinforcement shall be removed by small hand tools or pressure washing (using 3500 psi pressure). Hammers used to remove concrete shall be limited to 30 pounds.
7. All areas of the bridge repaired with epoxy mortar shall be restored to the original dimensions and details as shown in the information plans, unless noted otherwise.
8. Materials:
   a. Epoxy Resin: Resin shall be selected from the MDOT approved materials list.
   b. Silica Sand: Silica sand material shall be bagged general purpose blast cleaning sand.
   c. Epoxy Mortar Mix: Epoxy mortar mix shall consist of part liquid epoxy and part clean, dry sand mixed in the ratio recommended by the manufacturer.
9. Application:
   a. A representative of the epoxy manufacturer must be present for sufficient time to ensure the Contractor is properly schooled in the use of the epoxy materials.
   b. Prior to placement of the mortar mix the prepared surface shall be lightly primed with neat epoxy.
   c. Curing time shall be in accordance with manufacturer's recommendations.
10. The cost of saw cutting, removing spalled or cracked concrete, cleaning exposed reinforcing steel, patching material, labor and any miscellaneous materials necessary to complete the repairs as shown shall be paid for on a square feet basis as Bridge Repair, Epoxy Repair. This item shall be bid such that this item may be increased, decreased, or eliminated as directed by the Project Engineer.

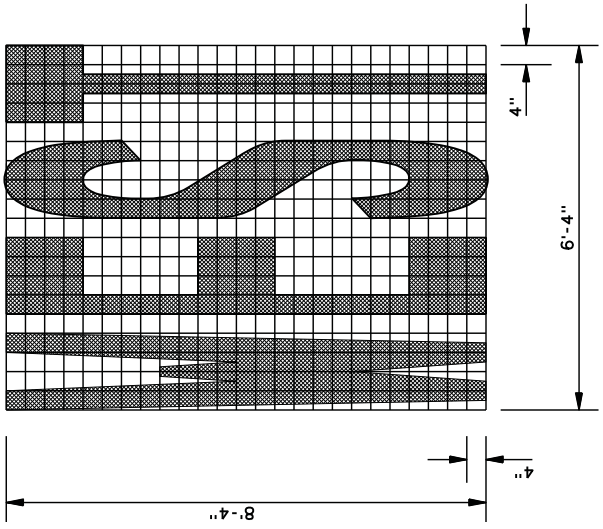
EPOXY BINDER

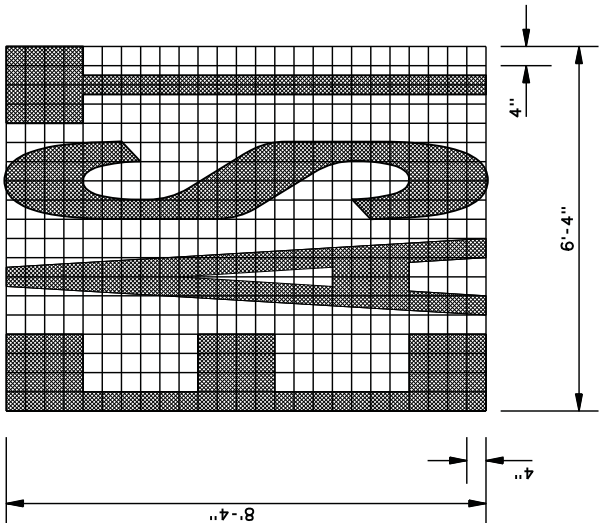
Contact areas where new concrete or epoxy mortar is placed against old concrete shall be cleaned then coated with an approved epoxy binder designed to bond new concrete to old. The binder shall be applied in accordance with the Manufacturer's recommendations.

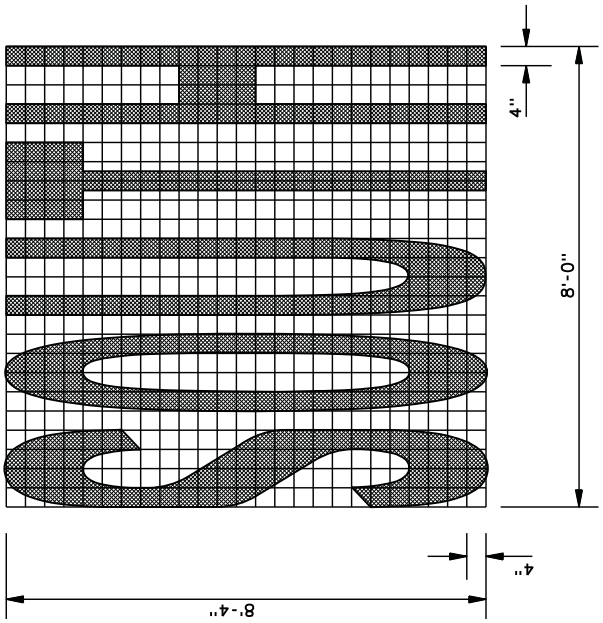
1" SAWCUT NOTES:

All 1" sawcuts shall be considered an absorbed item of work. The Contractor shall verify depth of reinforcing steel before making any sawcuts. The depth of the sawcut shall be no more than the depth of the reinforcing steel. Any damage to reinforcing steel shall be repaired to the satisfaction of the Engineer at no cost to the State.

PAVEMENT MARKINGS
LEGEND DETAILS







PAVEMENT MARKINGS
LEGEND DETAILS

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-618-12 **CODE: (SP)**

DATE: 05/03/2024

SUBJECT: Traffic Control Management

Section 618, Maintenance of Traffic and Traffic Control Plan, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-618.01--Description.

907-618.01.2--Traffic Control Management. Delete subparagraph (g) of Subsection 618.01.2 on page 441, and substitute the following.

g) Perform a minimum of once-a-week inspections from the Notice to Proceed until a Partial or Final Maintenance Release is obtained. Once work begins, daily daytime inspections and weekly nighttime inspections are required on projects with predominantly daytime work, and daily nighttime inspections and weekly daytime inspections are required on projects with predominantly nighttime work. Weekly inspections will be allowed for periods outside of active construction. When lane closures are present or any non-fixed signs or traffic handling devices such as cones or barrels are in place, inspections shall be performed daily whether work is being performed or not.

907-618.05--Basis of Payment. Delete pay item 618-A on page 449 and substitute the following.

907-618-A: Maintenance of Traffic - lump sum