

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO. <u> 1 </u>	DATED <u> 1/22/2025 </u>	ADDENDUM NO. <u> </u>	DATED <u> </u>
ADDENDUM NO. <u> </u>	DATED <u> </u>	ADDENDUM NO. <u> </u>	DATED <u> </u>
ADDENDUM NO. <u> </u>	DATED <u> </u>	ADDENDUM NO. <u> </u>	DATED <u> </u>

Number	Description
1	Revised Table of Contents; Added Notice to Bidder No. 6195; Revised Notice to Bidder No. 6570; Revised Wage Rates; Section 903 Replaces Same; Amendment EBSx Download Required.

TOTAL ADDENDA: 1
(Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

Contractor

BY _____
Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

President Address

Secretary Address

Treasurer Address

The following is my (our) itemized proposal.

SP-0037-04(061)/ 107633301000

Rankin County(ies)

Revised 01/26/2016

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION
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PROJECT: SP-0037-04(061)/107633301 - Rankin

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(REVISIONS TO THE ABOVE WILL BE INDICATED ON THE SECOND SHEET
OF SECTION 905 AS ADDENDA)

01/22/2025 01:43 PM

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 6195

CODE: (SP)

DATE: 8/22/2024

SUBJECT: Retroreflectivity Requirements

The Bidder's attention is called to Subsection 907-626.03.3 – Reflectivity Requirements in Special Provision No. 907-626-11.

The value shown in Table 1, Minimum Dry Retroreflectivity for Yellow, 275 mcd/m²/lx is hereby revised to 225 mcd/m²/lx.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 6570

CODE: (SP)

DATE: 1/22/2025

SUBJECT: Scope of Work

PROJECT: STP-0037-04(061) / 107633301 – Rankin County

The contract documents do not include an official set of construction plans but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, “Standard Drawings”.

Work on the project shall consist of overlaying approximately 10 miles of SR 43 in Rankin County from the intersection of SR 481/SR43 (BOP Sta. 0+00) to the Madison County Line (EOP Sta. 529+35).

Project-Wide Work

Station 0+00 (BOP) to Station 292+14

Trench widening will be required from BOP station 0+00 to the end of the 4 lane section at station 292+14, to widen existing 12-foot lanes to 14 feet. Widening shall consist of excavating 5” in depth at the edge of pavement by 2’ wide. Two (2) lifts @ 2½” of 19-mm, MT, Trench Widening asphalt shall be placed. Trench widening shall be paid under pay item 403-C002 19-mm, MT, Asphalt Pavement, Trench Widening.

Station 0+00 (BOP) to Station 344+38 (End of 4-Lane)

The Contractor shall mill 2” minimum depth, and variable, in locations where ruts/dips/humps exist, where surface has previously been patched, or is severely alligator cracked, but not deemed to be a full depth failure. An overlay of 2” and variable of 12.5-mm, MT asphalt shall be placed.

Station 344+38 to Station 529+35

The Contractor shall mill 2” and overlay with an average of 2” of 12.5-mm, MT asphalt, correcting locations where the slope is 1½% to 2% by placing 2½” at centerline and 2” at the edge of pavement, extending new pavement to the face of guardrail.

GENERAL NOTES: These general notes are applicable to all sites.

Milling

Milling/paving will not begin until an **approved** asphalt mix design has been received, nor until such time that, in the opinion of the Engineer, weather conditions have been consistently suitable enough to allow placement of the asphalt pavement after the milling operations.

The reclaimed asphalt pavement (RAP) material removed by the milling operation shall become the property of the Contractor.

Fine milling shall be performed in accordance with the attached drawings, and on all mainline tie-ins, driveway pads, county roads, etc. Publicly maintained roads and streets shall be milled sufficiently, including around concrete gutters, to tie in overlay at the existing right-of-way. Intersecting roads shall be milled 1½” to End of State Maintenance, then replace with 2” of 12.5-mm, MT, asphalt transitioning into mainline overlay. Asphalt at concrete gutters at curbs and islands shall be milled sufficient to tie in and as directed by the Engineer. Payment for milling and asphalt will be under pay items 406-D001 Fine Milling of Bituminous Pavement, All Depths and 403-A002 12.5-mm, MT, Asphalt Pavement. A milling/paving transition is required to tie local roads to mainline overlay section.

Milling operations shall be performed in accordance with the Contract documents and the Standard Specifications. Variable width and length transitions may be required for ties at ramps, local roads, project limits.

Milling of driveway pads shall be conducted in a manner to prevent gouging or otherwise affecting the roadway pavement structure and slope. Milling of driveway pads shall not be performed in simultaneous path with main line milling.

Traffic will be allowed to travel on the mainline milled surface for five (5) days, and the Contractor will be assessed a penalty of **\$5,000.00** per calendar day afterwards until the mainline milled surface is covered with the next lift of asphalt. This period is applicable to mainline through lanes, mainline turn lanes, and mainline auxiliary lanes. Additionally, traffic will be allowed to run on all milled surfaces other than the mainline for 30 days unless otherwise stated, and the Contractor will be assessed a penalty of **\$1,000.00** per calendar day afterwards until the non-mainline milled surface is covered with the next lift of asphalt. The additional allowance for the non-mainline milled surface is for the Contractor’s convenience, and thus, the Contractor is responsible for any pavement failures or damage sustained during this period.

Paving

Per Subsection 401.02.3.2, the asphalt mix design shall be submitted to the Engineer at least 10 working days prior to its proposed use.

Prior to mainline milling and paving operations, failed areas in the existing pavement shall be removed and backfilled with 12.5-mm, MT, Leveling asphalt as per the attached typical sections and details. Asphalt shall be placed in multiple lifts with a maximum lift thickness of 3”. Any granular/chemically treated/stone/etc. base or subgrade material deemed unsuitable by the Engineer shall be removed as directed and backfilled with 12.5-mm, MT, Leveling asphalt. Payment for the excavation of the granular base and subgrade will be made using the 203-G: Excess Excavation pay item. A list of the failed areas is shown in the attached tables. Pavement repairs shall be completed as a continuous operation in order to minimize traffic impacts. Lane closures shall remain in place until the failed area has been completely repaired. Lane closures may not be left unattended.

Prior to mainline paving operations and subsequent to the repair of failed areas, spot milling shall be performed in the areas listed in the attached tables and at other areas as directed by the Engineer. Spot milling and leveling will be variable depth at each location and will be determined in the field by the engineer.

The surface lift for failed area repair or concrete punchout repair shall have a maximum deviation of 3/8" as determined by a 10 foot straight edge. Any location that deviates more than this tolerance, as determined by the Engineer, shall be corrected at no additional cost to the State.

Privately owned entrances shall be paved to the shoulder line as per the included typical drawing. Pads shall be shaped horizontally and vertically to prevent excessive drop-offs. All residential pads exceeding a 2" drop off from the edge of pavement to the pad shall be corrected before the end of the day using paper joints to minimize damage to vehicles.

If traditional excavation methods are used, the removal area shall first be saw cut full depth including concrete, where applicable, to create a neat line and prevent damage to the adjacent pavement structure. Payment for saw cuts will be made using the appropriate items. If milling techniques are used, the area will not require saw cuts but care should be exercised to create a neat removal line and to prevent damaged to the adjacent pavement structure. If saw cuts are used in conjunction with milling, payment will be made using the appropriate pay items. Payment will not be made for saw cuts that are not performed.

Granular Shoulder Material

Where applicable, the existing shoulders shall be raised to match the new pavement elevation by placing variable depth granular material. The shoulders shall be graded and pulled up on a daily basis to eliminate drop-offs in excess of 2¼". Placement of the granular material on the finished asphalt course shall not be permitted. The existing shoulder shall be scarified to allow incorporation of the new shoulder material. The material shall be bladed, rolled, and compacted to a finished slope of four percent (4%) in normal crown sections. Placement of this material shall be performed to provide a uniform and compacted shoulder with a minimum depth and width of material placed. Shoulders with adequate shoulder material in place shall be bladed to a slope of four percent (4%) in normal crown sections. The cost of blading will be an absorbed item and is to be included in the price of other items bid. Crushed concrete will not be allowed.

Any material excavated from the existing shoulder during pavement widening operations or as a result of shoulder blading shall be used on the existing shoulder to match the new pavement elevation and any surplus material shall be spread along the edge of the shoulders, fore slopes, or other adjacent areas as directed by the Engineer and will be an absorbed item. Material which cannot be suitably placed in adjacent areas and deemed to be excess excavation by the Engineer shall be removed from the project site. Payment for removal of excess material will be made using pay item 203-G: Excess Excavation.

Granular material (crushed stone) shall be provided around driveway pads as directed to prevent shoulder drop-offs and shall be placed in a timely manner. Drop-offs exceeding 2¼" shall be corrected within two (2) calendar days of the placement of the pad.

Temporary and Permanent Pavement Markings

Temporary traffic stripe will be required immediately after the milling and/or required overlay and prior to opening area to traffic. Temporary stripe shall be placed in the same location and configuration as the permanent stripe except that it may be offset as required for milling and paving operations. If temporary stripe is offset, the Contractor shall conduct operations in a manner to ensure the final temporary stripe is placed at the required location of the permanent stripe. If removal of temporary offset stripe is required in order to achieve the correct location and alignment of permanent stripe; the cost of removal will be absorbed in other items bid. Placing double temporary centerline will not be allowed.

Temporary striping shall conform to finished stripe specifications for alignment, neatness, and straightness.

The use of short strips of traffic tape will not be allowed unless approved by the Engineer.

All permanent striping will be double drop thermoplastic, 90-mil thickness unless otherwise specified in Subsection 907-626.03.2. Edge lines will be placed to accommodate the lane widths shown on the attached applicable typical sections unless prevented by field conditions.

Rumble strip shall be placed throughout the project limits in accordance with the attached details and Standard Drawings.

Existing raised pavement markers shall be removed prior to beginning the overlay operation. No measurement will be made for separate payment; the cost shall be included in the price for other items bid.

Permanent raised pavement markers shall be installed on mainline and local public roads after completion of all paving operations. Edge line RPM's shall be installed as per Special Design Drawing RPM-1. If the usable space outside of the traffic stripe is insufficient to install the RPM's as per Special Design Drawing RPM-1, the Contractor shall be allowed to install the outside edge of the RPM flush with the inside edge of the traffic stripe.

Payment for edge stripe on local roads shall be made under pay item 907-626-G006: Thermoplastic Double Drop Detail Stripe, White when the length of said stripe is less than 150 feet when measured from the end of the radius. If the measured length is greater than 150 feet, then payment shall be made under pay item 907-626-C012: 6" Thermoplastic Double Drop Edge Stripe, Continuous White.

Payment for centerline stripe on local roads shall be made under pay item 907-626-G007: Thermoplastic Double Drop Detail Stripe, Yellow when the length of said stripe is less than 150 feet when measured from the stop bar. If the measured length is greater than 150 feet, then payment shall be made under pay item 907-626-E003: 6" Thermoplastic Double Drop Traffic Stripe, Continuous Yellow.

Centerline Stripe shall be omitted on local roads whose width is less than 20 feet.

The face of all existing undisturbed curbs shall be painted with at least two coats of white traffic paint with glass beads being required in the top coat. The cost associated with the painting of new or existing curb is to be included in other items bid.

The face of all existing, painted, concrete islands shall be painted with at least two coats of white traffic paint with glass beads being required in the top coat. The cost associated with the painting of new or existing curb is to be included in other items bid.

Guardrail

Guardrails shall be replaced at the locations shown on the attached table. Removal of guardrail shall consist of removal of bridge end section, w-beam/thrie beam, terminal end section, posts, and all other appurtenances. All guardrail removed is to be replaced the same day and prior to reopening the adjacent lane of traffic. Voids created by the removal of posts, concrete anchors, footings, etc. shall be backfilled and compacted in accordance with Section 203 of the Standard Specifications.

The installed guardrail shall meet all requirements in order to be MASH compliant.

Guardrail lengths are based on terminal end length of 37.5'. If terminal of length other than this is used, an adjustment in w-beam length is required.

Delineators will be required on all guardrails within the project. Existing guardrail delineators shall be removed and replaced. The cost of removal shall be included in the price of items bid.

All dimensions and spacings for bridge rail connectors shall be verified in the field by the Contractor prior to fabrication.

Permanent Signs

Permanent signs as listed on the attached tables are to be replaced. Unless otherwise listed in the attached tables, existing posts, anchors, angles/bars, and other components are to be reused. The Contractor shall use new bolts, screws, washers, nuts, etc. of the required sizes in the installation of signs. If required as part of the sign replacement activities, all post and I-beam lengths in these plans are estimated. Post lengths for all signs shall be verified in the field by the Contractor prior to fabrication. Installation dates shall be clearly written in bold black markings on the back bottom half off all signs with a permanent marking stick that is waterproof, fade resistant, and marks on wet or dry surfaces. If existing sign posts or footings are to be replaced, the existing posts and footings shall be removed and the area backfilled and compacted in accordance with Section 203 of the Standard Specifications. Removal of sign, post, and footing and backfilling will be paid using the removal of sign pay item.

Object markers at bridge approaches and other locations shall be replaced as shown in the attached table. Removal of object markers shall be absorbed in the cost of other items bid

Traffic Control

The Contractor shall erect and maintain construction signing and provide all signs and traffic control devices necessary to safely maintain traffic around and through the work areas in accordance with the Traffic Control Plan and the MUTCD. The cost shall be included in the price bid for pay item 907-618-A: Maintenance of Traffic. Fluorescent orange sheeting shall be used on all construction and traffic control signs except those designated in the plans to be black legend and border on white background.

Standard roadside construction signs, barricades, etc. shall be placed in accordance with the attached tables, drawings, and as directed by the Engineer. W20-1 signs shall be placed on all public road approaches as shown or as directed. Payment for standard roadside construction signs, barricades, etc. will be made using the appropriate pay items.

The Contractor shall on a daily basis, remove all debris from within the roadway and a 30-foot clear zone which, in the opinion of the Engineer, is a hazard to the traveling public. This activity shall begin with the beginning of work or the beginning of the contract time, whichever comes first. No direct payment will be made for the debris removal; the cost is to be included in the prices of items bid. Failure of the Contractor to remove the debris as prescribed herein shall be just cause for withholding the monthly progress estimate payment or suspending active operations until the debris is satisfactorily removed by the Contractor.

Temporary asphalt joints (aka paper joints) shall be employed at all locations requiring traffic to traverse an uneven, transverse, pavement joint. Paper joints shall be a minimum of nine feet (9') in length and for the full width of the milled/paved surface. Paper joints shall be adequately maintained.

Potholes that may exist or occur in the existing pavement shall be patched in a timely manner as required. Patching of potholes shall be considered an absorbed item.

Temporary portable rumble strips, as described in Special Provision No. 907-619, shall be used in advance of each lane closure. Direct payment will not be made for this item and shall be considered absorbed under pay item 907-618-A001: Maintenance of Traffic.

Miscellaneous Notes

It shall be the responsibility of the Contractor to protect existing structures such as pipes, inlets, aprons, bridges, etc. from damage which might occur during construction. The Contractor shall replace or repair, as directed by the Engineer, any structures damaged by the Contractor during the life of the contract. No payment will be made for replacement or repair of damaged items.

Any signs that are in conflict with construction of this project shall be removed and relocated by the Contractor as directed by the Engineer, the cost of which is to be absorbed in other items bid. Removal of existing raised pavement markers shall be included in the prices for other items bid.

Incidental work such as removing vegetation, shaping and compacting shoulders, removing and resetting signs and/or mailboxes, removing excess asphalt material, project clean-up, and other

items of incidental work necessary to complete the project will not be measured for separate payment and will be considered included in the prices of items bid.

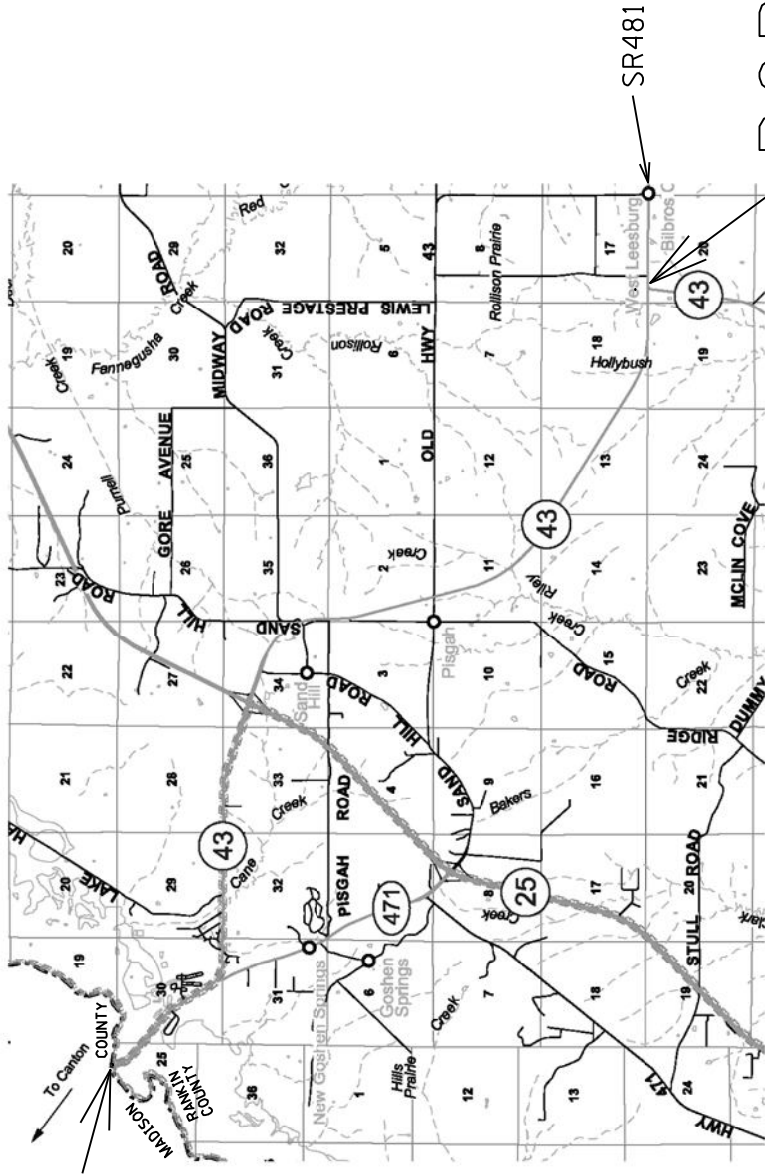
Prior to the final inspection, bridges, islands, and areas with curb shall be swept/cleaned. Care should be taken to prevent milled asphalt, asphalt debris, vegetative/granular debris, etc. from entering drainage structures or clogging other drainage ways. Disposal of material will not be measured for separate payments.

The 48" RCP cross drain in front of Pisgah High School at approximately station 272+80 shall be removed and replaced. Details of the work required are included in the remove and replace pipe table. The cross drain repair at Station 272+80 will involve removal and replacement of the existing pipe which shall be completed as a continuous operation in order to minimize traffic impacts and will require special traffic control according to the current edition of the Manual on Uniform Traffic Control Devices. Lane closures shall remain in place until the failed area has been completely repaired. Lane closures shall not be left unattended. The Contractor will be allowed to place the crushed stone to the top of the roadway temporarily until asphalt can be placed. If the Contractor elects to place the crushed stone to the top of the roadway, the crushed stone shall be adequately maintained as directed by the Engineer in order to provide a safe riding surface for the traveling public. Any crushed stone needed in order to accomplish this shall not be paid for directly and shall be absorbed under other items bid. Once the asphalt operations are ready to commence, the crushed stone shall be removed to a depth of 6" below the roadway surface and 6" (3 lifts at 2" apiece) of 12.5-mm, MT, Leveling asphalt shall be placed. The removal of the crushed stone shall not be paid for directly and shall be absorbed under other items bid. If the Contractor elects to utilize the removed crushed stone elsewhere on the project, it shall not be paid for directly and shall be absorbed under other items bid. If the Contractor elects to perform this work over a weekend closure, crushed stone shall not be used and the asphalt structure shall be placed. In lieu of lift holes for this pipe, the producer may cast an approved lifting device during the manufacturing process. Should a lifting device be included with the pipe, the Contractor shall cut off or grind down the lifting device flush with the pipe surface after placement of the pipe. The area around the lifting device shall be coated with a sealer approved by the Engineer. Work related to the lifting device will be considered incidental and absorbed in other items bid.

Pipe repairs and paved ditch repairs are required at the locations included in the tables provided.

The Madison County/Pearl River sign at the EOP shall be straightened up to be perpendicular to the roadway. The cost for all work involved shall be included in other items bid.

SR43 OVERLAY PROJECT
FROM SR481 TO MADISON COUNTY LINE
RANKIN COUNTY
107633/301000

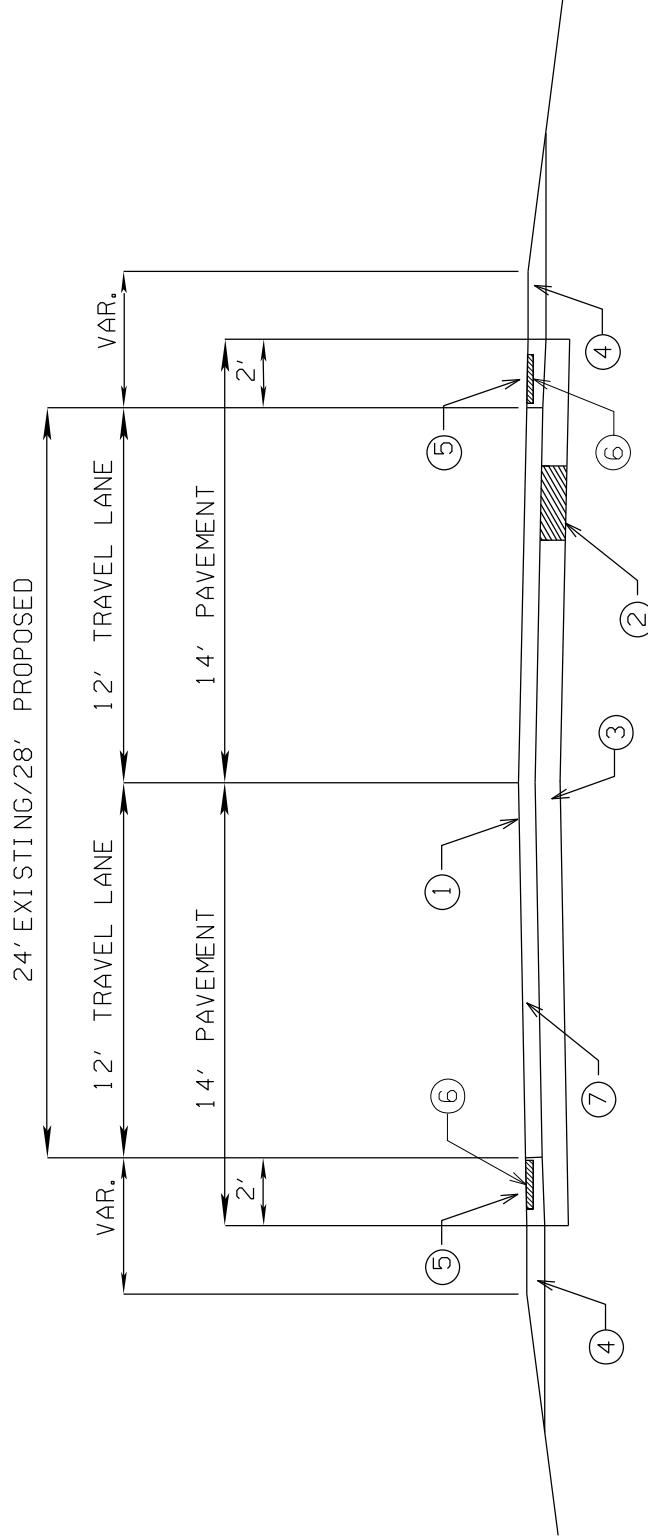


EOP 529+35

BOP 0+00

107633/301000
RANKIN COUNTY
TYPICAL SECTION
HWY 43 OVERLAY

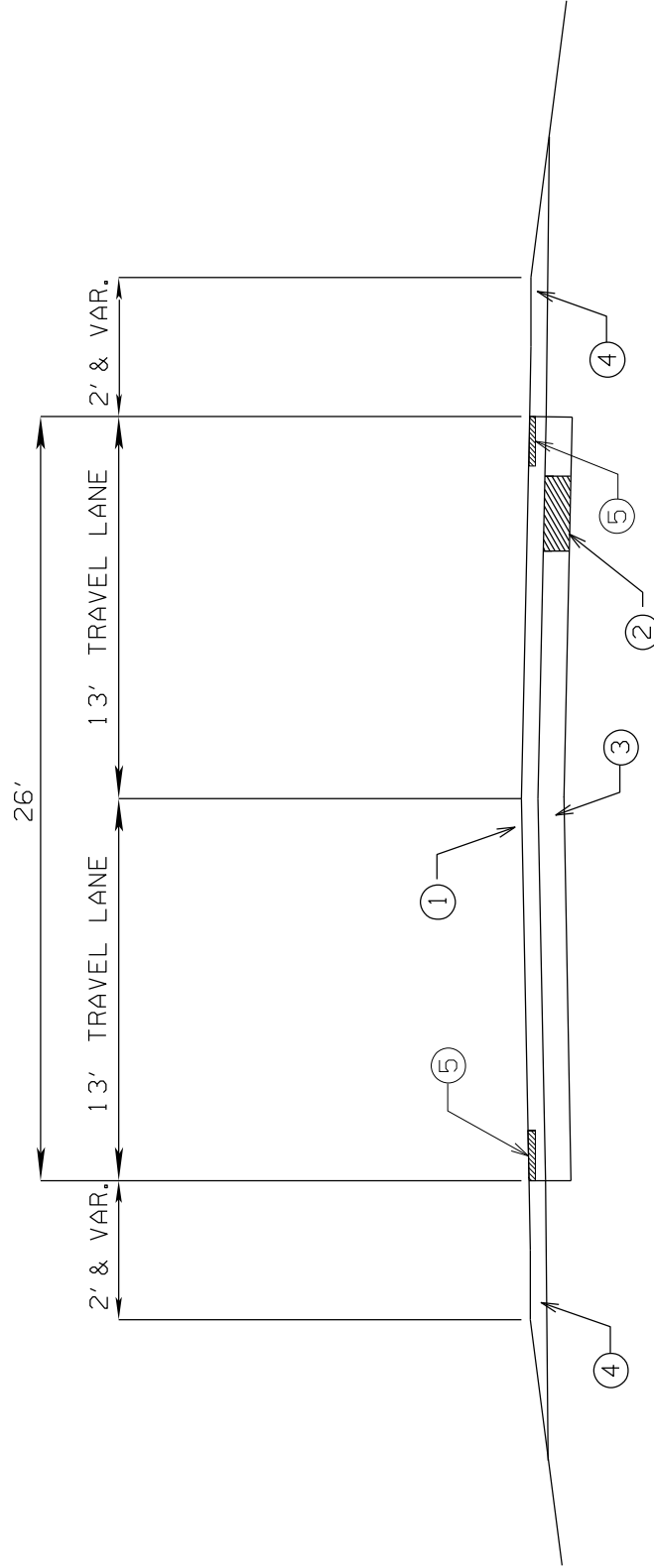
STA 0+00 TO STA 292+14



- ① Mill 2" & Variable in locations where ruts/dips/humps exist then Overlay 2" Asphalt Pavement 12.5mm,MT
- ② Failed areas to be removed and back filled with asphalt pavement 12.5mm Mix,MT leveling as directed.
- ③ Existing Pavement Structure (6-18" & VARIABLE ASPHALT PAVEMENT OVER 6-8" OF LIME TREATED OR UNTREATED GRANDULAR MATERIAL)
- ④ 2' & VAR. Crushed Stone as Required
- ⑤ 2' Trench Widening pavement structure
- ⑥ Rumble Stripe Required
- ⑦ Variable Depth Spot Milling required at locations in table provided.

107633/301000
RANKIN COUNTY
TYPICAL SECTION
HWY 43 OVERLAY

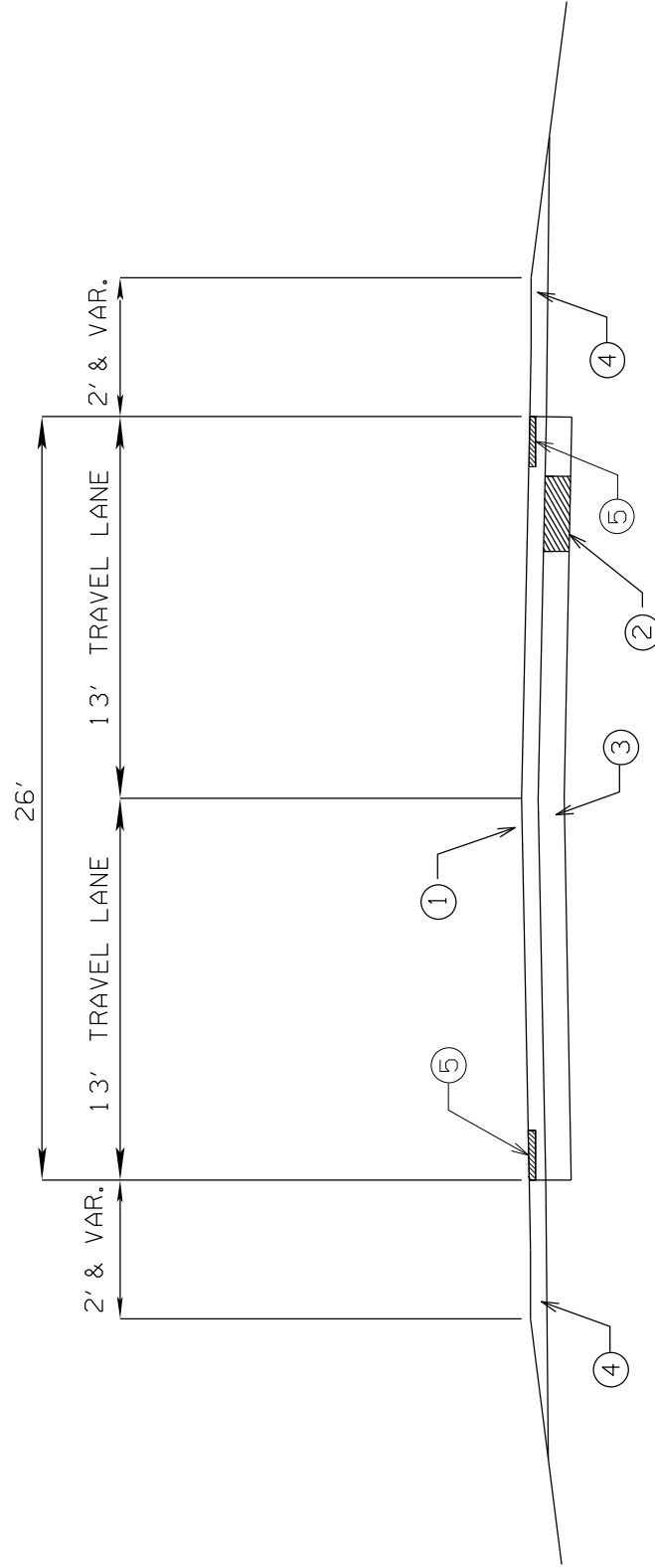
STA 292+14 TO STA 344+38 NB & SB



- ① Mill 2" & Overlay 2" HMA Asphalt Pavement 12.5mm,MT
- ② Failed areas to be removed and back filled with asphalt pavement 12.5mm Mix,MT leveling as directed.
- ③ Existing Pavement Structure (6-18" & VARIABLE ASPHALT PAVEMENT OVER 6-8" OF LIME TREATED OR UNTREATED GRANDULAR MATERIAL)
- ④ 2' & VAR. Crushed Stones as Required
- ⑤ Rumble Stripe Required

107633/301000
RANKIN COUNTY
TYPICAL SECTION
HWY 43 OVERLAY

STA 344+38 TO STA 529+35



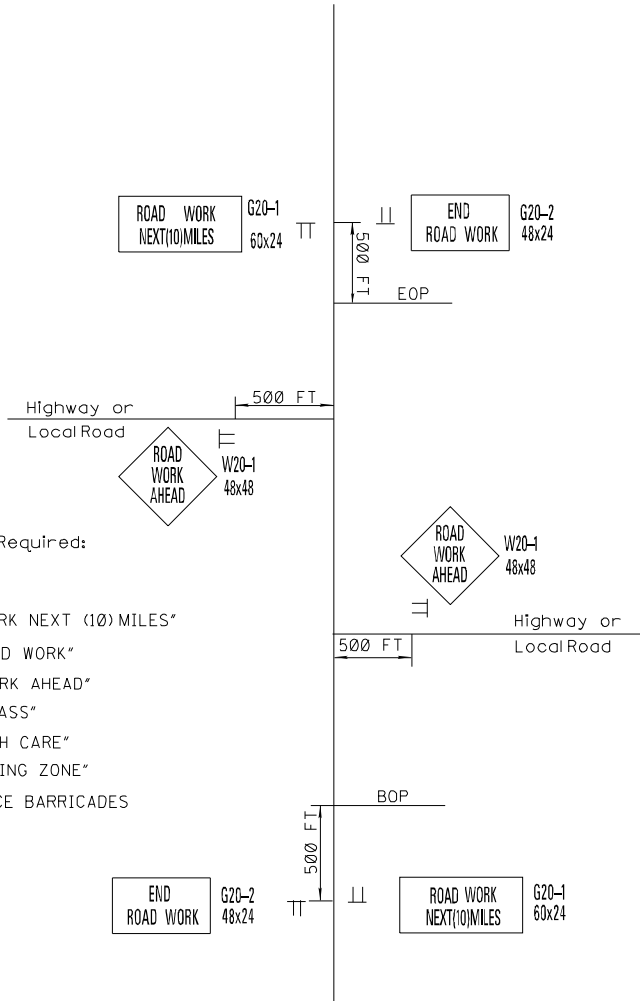
- ① Mill 2" & Overlay 2" HMA Asphalt Pavement 12.5mm,MT
- ② Failed areas to be removed and back filled with asphalt pavement 12.5mm Mix,MT leveling as directed.
- ③ Existing Pavement Structure (6-18" & VARIABLE ASPHALT PAVEMENT OVER 6-8" OF LIME TREATED OR UNTREATED GRANDULAR MATERIAL)
- ④ 2' & VAR. Crushed Stone as Required
- ⑤ Rumble Stripe Required

CONSTRUCTION SIGNING DETAIL
 SR 43
 RANKIN COUNTY
 107633/301000

ESTIMATED

Traffic Control Signs Required:

- 2 - G20-1 "ROAD WORK NEXT (10) MILES"
- 2 - G20-2 "END ROAD WORK"
- 26 - W20-1 "ROAD WORK AHEAD"
- 32 - R4-1 "DO NOT PASS"
- 17 - R4-2 "PASS WITH CARE"
- 32 - W14-3 "NO PASSING ZONE"
- 24 - TYPE III DBL. FACE BARRICADES



NOTES: One (1) W20-1 "ROAD WORK AHEAD" Sign is Required at each Local Road, Street or Highway Entering the Project.

G20-1 and G20-2 signs mounted on Type III Double Faced Barricade.

R4-1 "DO NOT PASS", R4-2 "PASS WITH CARE" AND W14-3 "NO PASSING ZONE" signs are required in accordance with Subsection 618.03.3 and as specified in the MUTCD. If No Passing zones are 1000 ft or more, install additional "DO NOT PASS" signs on maximum spacing of 750 ft.

Payment for these signs will be under the appropriate pay item numbers in the summary of quantities

PROJECT NO. 107633-301000
 COUNTY: Rankin

STANDARD ROADSIDE SIGNS - 0.080" THICKNESS												
STATION	SIGN NUMBER	SIZE (in. x in.)	AREA (sf)	PIPE POSTS (lf)				U POST (lf)		" x 2-1/2" (lf)	Class "B" Conc (cy)	REMARKS
				3"	3-1/2"	4"	5"	4 lb/ft	2 lb/ft			
523+65	W8-13	36 x 36	9						15.00			
458+38	M3-3	12 x 24	2						15.00			
458+38	M1-4	24 x 24	4						15.00			
458+00	D9-3a	24 x 24	4									
458+00	M6-3	12 x 9	0.75									
458+00	Drawing	66 x 48	22								3	
458+00	Drawing		0									
343+43	M1-4	24 x 24	4						15.00			
343+43	M3-1	12 x 24	2						15.00			
339+57	R2-1	30 x 24	5						15.00			
295+05	R2-1	30 x 24	5						15.00			
282+00	R2-1	30 x 24	5						15.00			
277+80	S1-1	36 x 36	9						15.00			
255+17	S1-1	36 x 36	9						15.00			
462+06	W1-8	18 x 24	3.00						15.00			
462+06	W1-8	18 x 24	3.00						15.00			
462+06	W1-8	18 x 24	3.00						15.00			
462+06	W1-8	18 x 24	3.00						15.00			
273+27	R1-2	36 x 36	3.900						15.00			
271+90	R1-2	36 x 36	3.900						15.00			
Total this sheet =			100.55	0.00	0.00	0.00	0.00	0.00	240.00		3.00	

107633/301000 Rankin County					
Spot Milling/ Leveling Locations					
Location	Length	Width	CY	12.5mm-MT Leveling (Ton)	Spot Milling SY
150+70 - 173+83	2313	12	257	509	800
234+18 - 244+40	1022	12	114	225	1067
			Total	734	1867

PROJECT NO. 107633-301000
 COUNTY: Rankin

STANDARD ROADSIDE SIGNS - 0.125" THICKNESS												
STATION	SIGN NUMBER	SIZE (in. x in.)	AREA (sf)	PIPE POSTS (lf)				U POST (lf)		" x 2-1/2" (lf)	Class "B" Conc (cy)	REMARKS
				3"	3-1/2"	4"	5"	4 lb/ft	2 lb/ft			
494+96	D1-1	6 x 36	1.5						15.00			
473+04	W1-2L	36 x 36	9						15.00			
448+41	W2-1	36 x 36	9						15.00			
448+41	W1-2R	36 x 36	9						15.00			
349+57	W6-1	48 x 48	16						15.00			
341+91	W6-3	36 x 36	9						15.00			
330+00	W4-2L	48 x 48	16						15.00			
326+88	W9-1L	48 x 48	16						15.00			
309+31	W9-1L	48 x 48	16						15.00			
304+14	W4-2L	48 x 48	16						15.00			
298+36	W6-1	36 x 36	9						15.00			
278+82	W2-1	36 x 36	9						15.00			
277+12	D1-1a	6 x 18	0.75						15.00			
268+69	D1-1	6 x 18	0.75						15.00			
267+24	W2-1	36 x 36	9						15.00			
208+82	D1-1a	6 x 18	0.75						15.00			
199+13	D1-1a	6 x 18	0.75						15.00			
Total this sheet =			147.50	0.00	0.00	0.00	0.00	0.00	255.00		0.00	

**107633-301000
202-B009 Removal of Failed Areas
Rankin County**

12.5mm MT, Leveling

Location	STA	to	STA	Length (ft)	Depth (ft)	Width (ft)	Saw Cuts (ft)	Removal of Asphalt	Estimated Asphalt Req. (TONS)	Estimated Excess	Tack (Gal.)
Rt	374+00		377+00	300	2	9	318	300.000	396.900	317.52	33.00
Rt	386+50		388+00	150	2	9	168	150.000	198.450	158.76	16.50
Rt	392+25		393+00	75	2	9	93	75.000	99.225	79.38	8.25
Rt	398+25		404+00	275	2	9	293	275.000	363.825	291.06	30.25
LT	158+50		156+50	200	2	9	218	200.000	264.600	211.68	22.00
LT	168+50		170+00	150	2	9	168	150.000	198.450	158.76	16.50
LT	311+00		313+00	200	2	14	228	311.111	411.600	329.28	34.22
LT	327+00		328+25	125	2	9	143	125.000	165.375	132.30	13.75
LT	352+00		352+50	50	2	9	68	50.000	66.150	52.92	5.50
LT	354+00		354+50	50	2	9	68	50.000	66.150	52.92	5.50
Totals								1854.7222	2230.725	1784.58	185

*Stations are approximate and may be adjusted to match field conditions

Hwy 43
Rankin County
107633-301000

Guardrail Quantities

STATION	LOCATION (L-T/RT)	GUARDRAIL		FLARED TERMINAL END SECT. (EA)	TANGENT TERMINAL END SECT. (EA)	Cable Anchor TYPE I (EA)	BRIDGE END SECTION			DELINEATORS		Type 3 Object Markers (EA)	GUARDRAIL REMOVAL (LF)	REMARKS
		(W-BEAM) (LF)	THRIE BEAM (LF)				TRANS. SECT. (LF)	TYPE "A" (EA)	TYPE "C" (EA)	TYPE "F" (EA)	SPEC. DESIGN BR END CONN. (EA)			
497+76	LT	3225		1			1		1		70	2	3225	
497+76	RT	3225		1			1		1		70	2	3225	
423+52	LT			2							14	2		New
423+52	RT			2							14	2		New
TOTAL =		6870 L.F.	0 EA.	6 EA.	0 EA.	0 EA.	2 EA.	0 EA.	0 EA.	0 EA.	168 EA.	8 EA.	6450 L.F.	

* REMOVAL OF ALL GUARDRAIL (BRIDGE END SECTIONS, W-BEAM, TYPE-I CABLE ANCHORAGE, TERMINAL END SECTIONS, ETC.) WILL BE PAID UNDER PAY ITEM 202-B REMOVAL OF GUARD RAIL.
 * REMOVAL OF GUARDRAIL DELINEATORS ARE CONSIDERED INCIDENTAL TO THE REMOVAL OF GUARDRAIL AND WILL NOT BE MEASURED AS A SEPARATE PAY ITEM.
 * ALL GUARDRAIL (METAL RAIL AND METAL POSTS ONLY), WOODEN POSTS, ALL BLOCKOUTS, CONCRETE ANCHORS, ETC. WILL BE THE PROPERTY OF THE CONTRACTOR.
 * TOTAL GUARDRAIL LENGTH IS BASED ON A TERMINAL END SECTION 37.5' LONG. IF A TERMINAL END SECTION OF A DIFFERENT LENGTH IS USED, THE LENGTH OF THE W-BEAM MAY HAVE TO BE ADJUSTED.

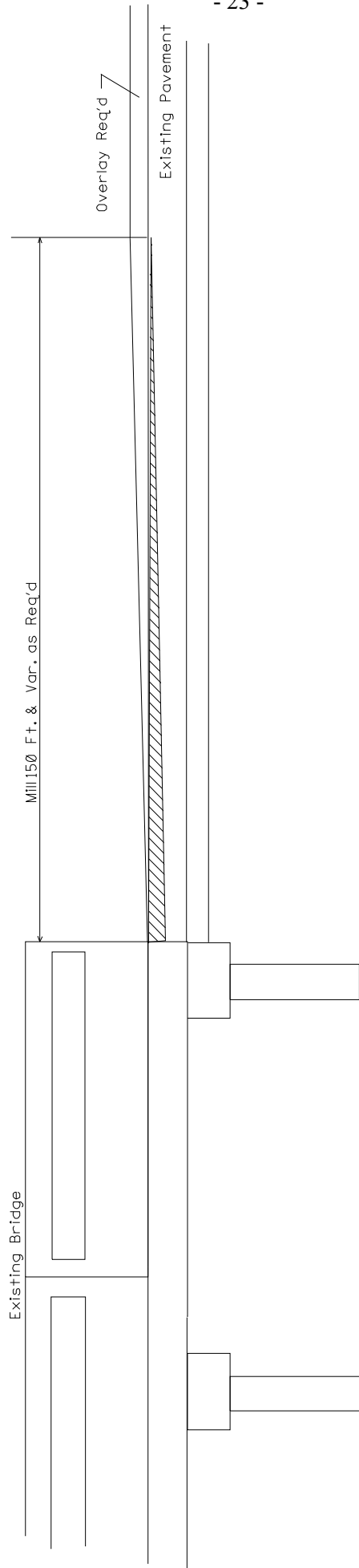
107633/301000 Rankin County			
Paved Ditch Repair			
Location	300 lb Rip Rap	Geotextile Fabric	Minor Structure Concrete
17+86	16 Ton	20 SY	0.114 CY Toewall

107633/301000 Rankin County					
Reinforced Concrete Pipe Repair					
Location	Pipe Size	300lb RipRap	Minor Structure Concrete	Geotextile Fabric	Borrow Excavation
16+54 L	18"	16 Ton	0.316 CY Toewall & Collar	20 SY	
253+84 R	48"		0.85 CY Collar		
308+79 Median	36"		0.369 CY Collar (3)		20 CY

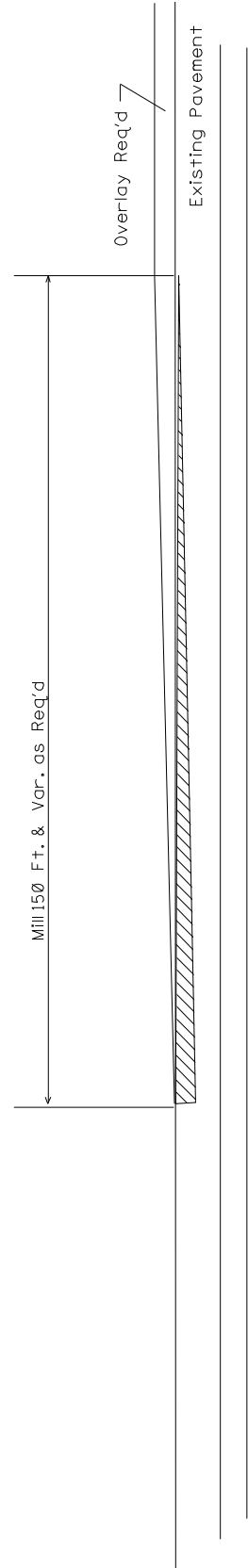
107633/301000 Rankin County		
202-B052 Removal of Concrete Driveways, All Depths		
Location	Saw Cut LF	Removal SY
205+97	10 LF	7 SY

107633/301000 Rankin County											
Remove and Replace Pipe											
Location	Pipe Size	300lb RipRap	Minor Structure Concrete	Geotextile Fabric	Structure Excavation	Removal Of Pipe	Crushed Stone	48" RCP, Class III	48" End Sections	Removal of Asphalt	Saw Cut
272+80	48"	16 Ton (Headwalls)	0.145 CY Toewall & 1.7 CY Collar (2)	20 SY	155	115 LF	110	104 LF	2 EA	22 SY	48 LF

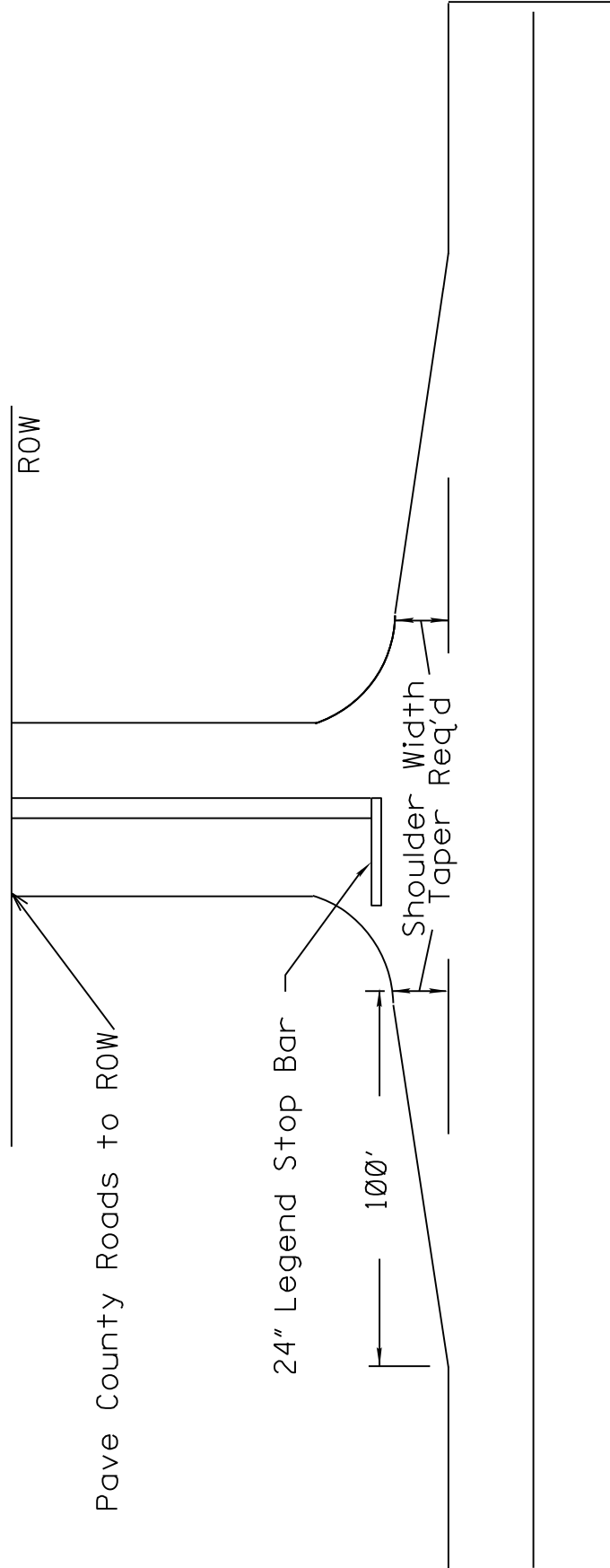
**107633/301000
RANKIN COUNTY
BRIDGE END PAVING DETAIL**



BOP & EOP PAVING DETAIL

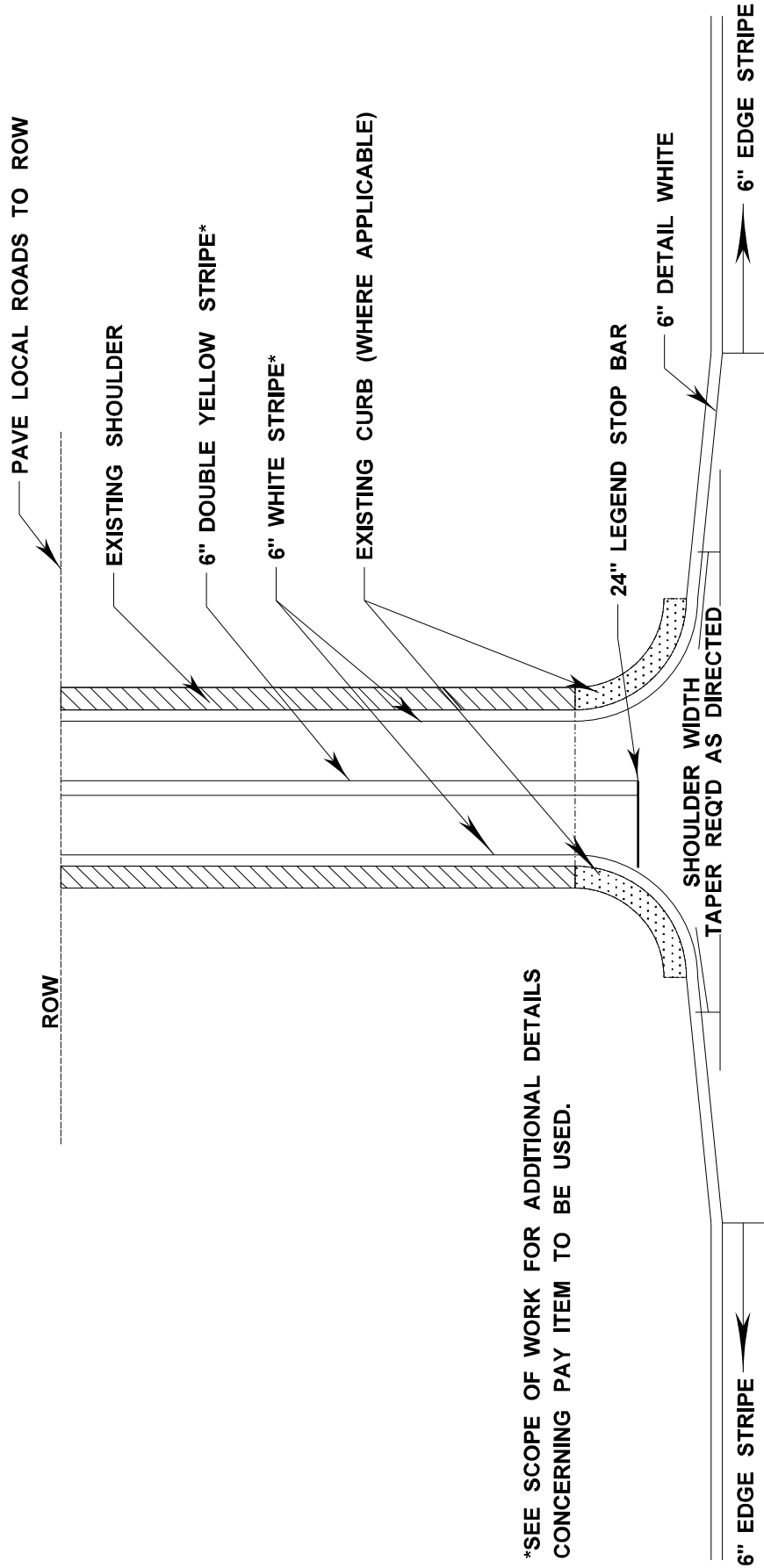


Typical Section - County Roads



**SR 43
RANKIN COUNTY
107633/301000**

STRIPE DETAIL - LOCAL ROADS



*SEE SCOPE OF WORK FOR ADDITIONAL DETAILS CONCERNING PAY ITEM TO BE USED.

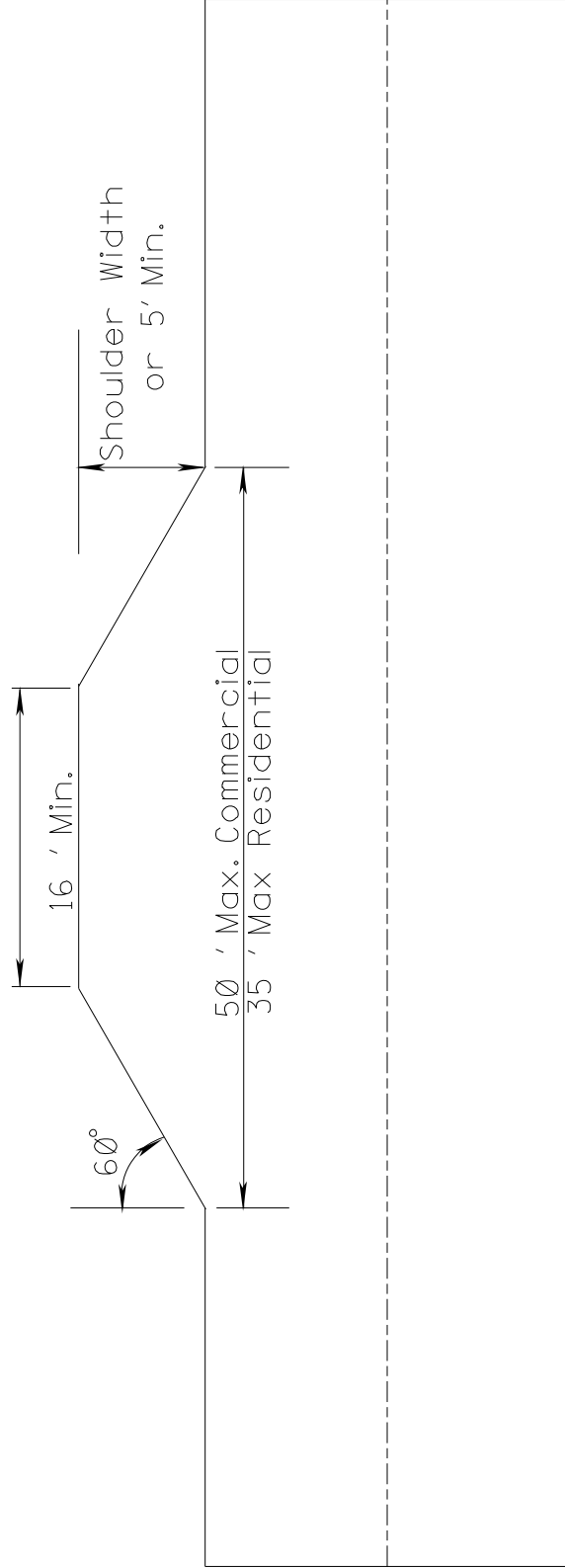
NOTE: CENTERLINE STRIPE SHALL BE OMITTED ON LOCAL ROADS WHOSE WIDTH IS LESS THAN 20 FEET.

107633/301000

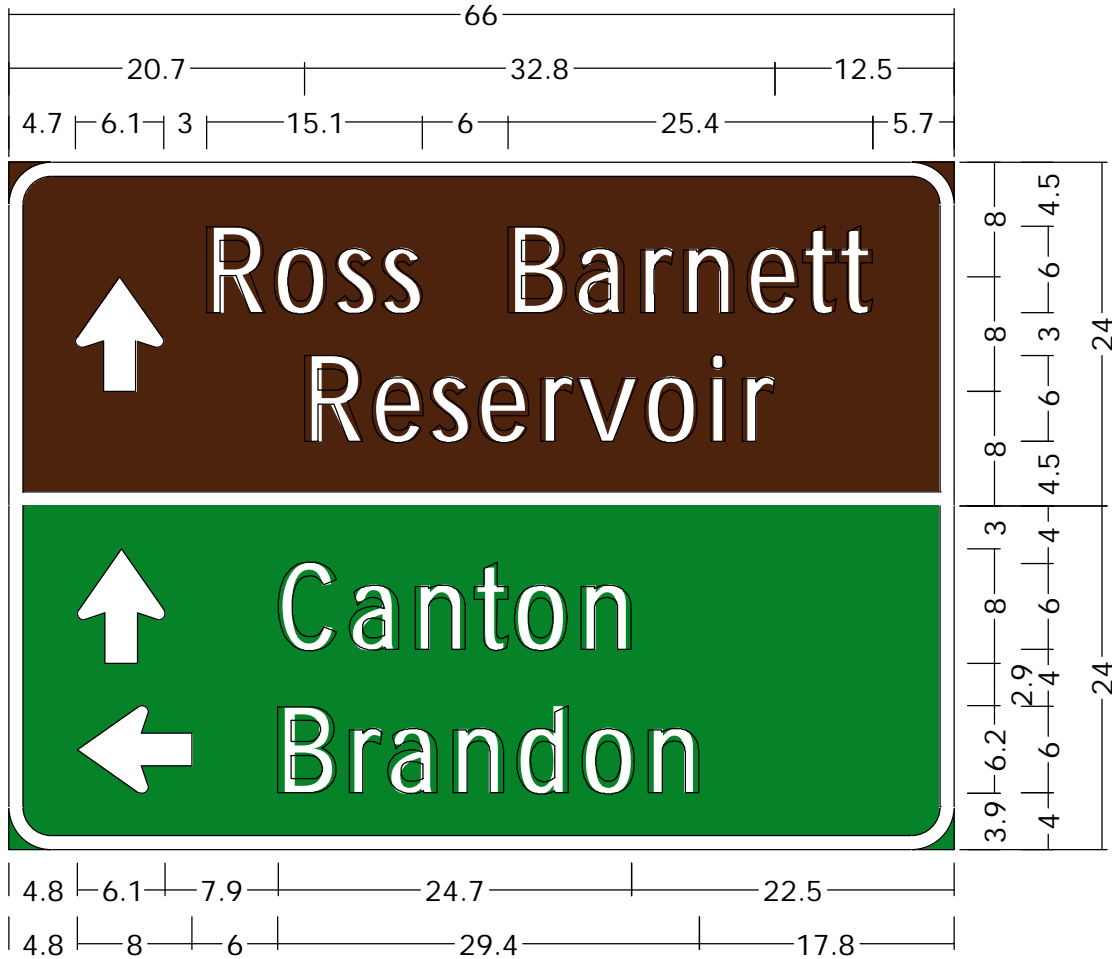
SR 43

**RANKIN COUNTY
DRIVEWAY PAD DETAIL**

TYPICAL RAMP/PAD DETAIL



107633/301000 Notice to Bidders No. 6570 -- Cont'd.
RANKIN COUNTY
STA. 458+00 RTLN



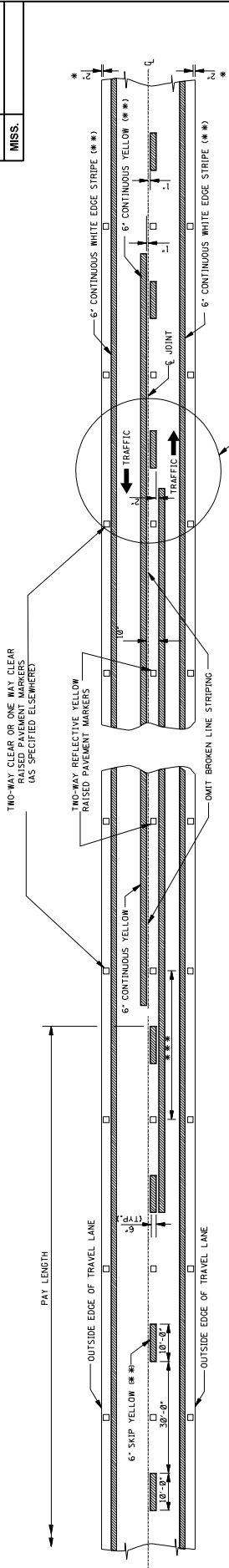
3.0" Radius, 1.0" Border, White on, Brown;
 Standard Arrow Custom 8.0" X 6.1" 90'; "Ross Barnett", D 2K;
 "Reservoir", D 2K;

3.0" Radius, 1.0" Border, White on, Green;
 Standard Arrow Custom 8.0" X 6.1" 90'; "Canton", D 2K;
 Standard Arrow Custom 8.0" X 6.1" 180'; "Brandon", D 2K;
 Table of widths and spaces

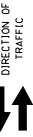
4.7	↑	6.1	3.0	R	4.1	0.7	o	3.6	0.6	s	2.8	0.5	s	2.8													
		6.0	B	4.1	0.6	a	3.4	1.1	r	2.3	0.7	n	3.6	0.9	e	3.5	0.3	t	2.4	0.1	t	2.4	5.7				
20.7	R	4.0	0.7	e	3.5	0.5	s	2.8	0.6	e	3.5	0.9	r	2.2	0.3	v	4.2	0.5	o	3.7	0.9	i	1.0	1.2	r	2.3	12.5
4.8	↑	6.1	7.9	C	4.1	0.9	a	3.4	1.2	n	3.5	0.6	t	2.4	0.4	o	3.7	0.9	n	3.6	22.5						
4.8	←	8.0																									
		6.0	B	4.1	0.9	r	2.3	0.4	a	3.4	1.1	n	3.6	0.9	d	3.6	0.9	o	3.7	1.0	n	3.5	17.8				

SR 43
RANKIN COUNTY
107633/301000

FMS CONG.	STATE PROJECT NO.
	MISS.



TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)

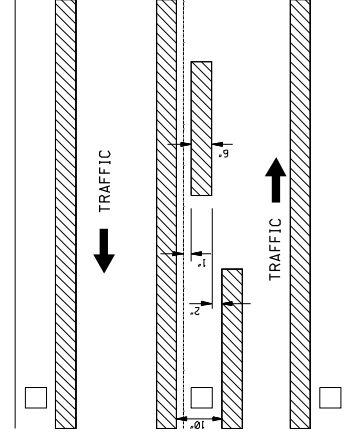


DIRECTION OF TRAFFIC

TWO-WAY TRAFFIC
RAISED PAVEMENT MARKERS
(AS SPECIFIED ELSEWHERE)

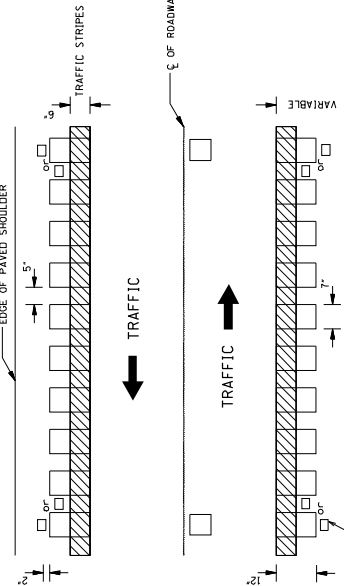
TWO-WAY TRAFFIC
RAISED PAVEMENT MARKERS
(AS SPECIFIED ELSEWHERE)

TWO-WAY TRAFFIC
RAISED PAVEMENT MARKERS
(AS SPECIFIED ELSEWHERE)



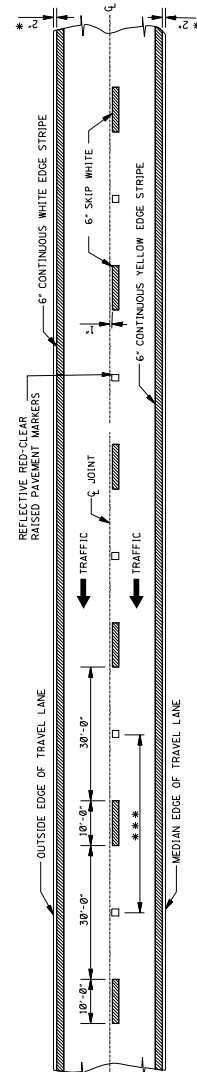
DETAIL "A"

PLACEMENTS OF RPMs ON
PAVEMENTS WITHOUT RUMBLE STRIPS



DETAIL "A"

PLACEMENTS OF RPMs ON
PAVEMENTS WITH RUMBLE STRIPS



4-LANE WITH ONE-WAY TRAFFIC

GENERAL NOTES:

1. UNLESS SHOWN ELSEWHERE ON THE PLANS, FOR STRIPING ON RUMBLE STRIP SECTIONS REFER TO WK. SHEETS RS-1, RS-2, AND RS-3.
2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR PLASTIC AS INDICATED IN PAY ITEMS).
3. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

TANGENT SECTIONS	40'-0"	URBAN AREA	44'-0"
HORIZONTAL CURVES	40'-0"	RURAL AREA	44'-0"
INTERCHANGE LIMITS	40'-0"		44'-0"

NOTE: ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON ALL RUMBLE STRIP SECTIONS. THE MARKERS SHALL BE PLACED 1000' IN ADVANCE ON DIRECTION OF TRAFFIC OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.

EDGE LINE RAISED PAVEMENT MARKERS ARE TO BE PLACED AT THE SAME SPACING AS THE CENTERLINE MARKERS. EDGE LINE MARKERS WILL BE OMITTED WHERE CONFLICTS OCCUR WITHIN INTERSECTIONS, CROSSOVERS, ETC. EDGE LINE RPMs WILL NOT BE PLACED IN URBAN AREAS UNLESS OTHERWISE SPECIFIED.

PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MOOT APPROVED SOURCES OF MATERIALS.

THE CRITERIA FOR NO-PASSING ZONES CAN BE FOUND IN THE MOTORWAY DESIGN MANUAL, SECTION 17-100.

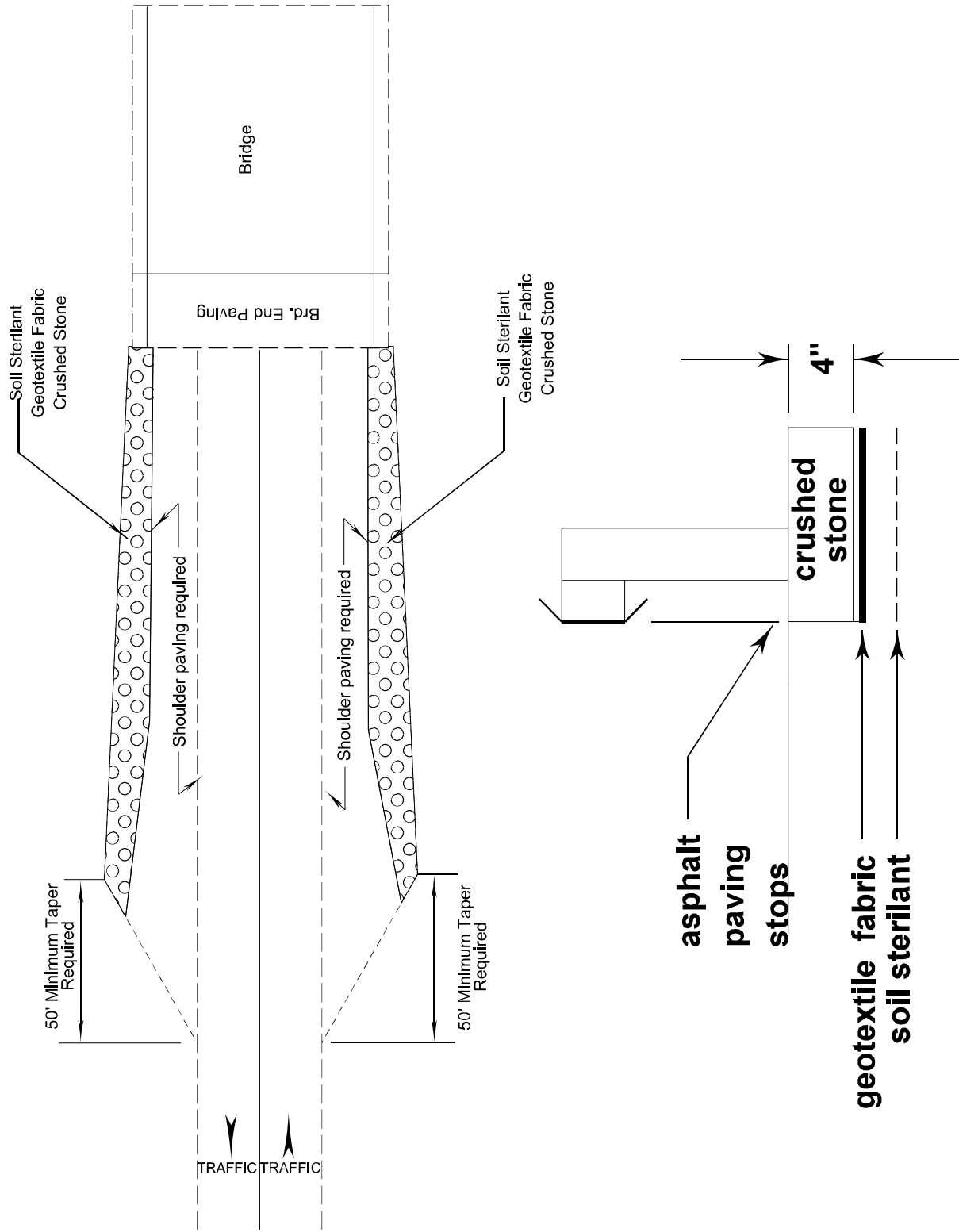
WHERE RUMBLE STRIPS ARE PRESENT, THE EDGE LINE MARKERS WILL BE PLACED AT THE END OF THE RUMBLE STRIPS AS DIRECTED BY THE ENGINEER.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
MISSISSIPPI HIGHWAY DEPARTMENT
STATE HIGHWAY PAVEMENT MARKING
RAISED PAVEMENT MARKING
2-LANE ROADWAYS
PROJECT NO.:
COUNTY:
WORKING NUMBER
RPM-1
SHEET NUMBER

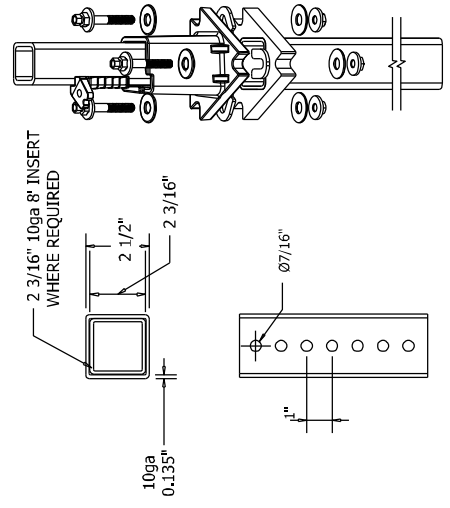


DATE	CHANGED
FILE NAME: RPM-1.DWG	
DESIGN TEAM	

**SR 43
RANKIN COUNTY
107633/301000**

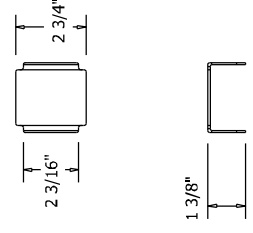


SIGN POSTS



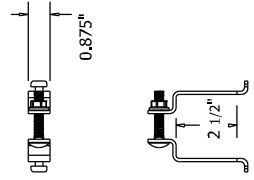
example slip base assembly (detailed below)

POST CAP



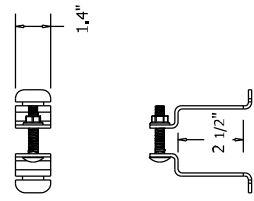
MADE OF 16GA PREGALVANIZED STEEL WITH AN LD CAPABLE OF FITTING INTO THE INSIDE OF A 2-1/2" SQUARE STEEL POST WITHOUT ANY FASTENERS. A CAST ALUMINUM FRICTION FIT CAP PIN ALSO BE USED.

EXTRUDED PANEL SIGN CLAMP



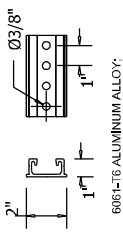
MATERIALS: 1.4" WIDE x 11 ga. TYPE 304, #2B FINISHED STAINLESS STEEL BRACKET. INCLUDES STAINLESS STEEL POST AND SEPARATED FLANGED NUT, ONE CLAMP REQUIRED FOR EACH BRACE TO POST JUNCTION.

FLAT PANEL SIGN CLAMP



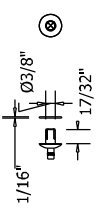
MATERIALS: 1.4" WIDE x 11ga. TYPE 304, #2B FINISHED STAINLESS STEEL BRACKET. INCLUDES STAINLESS STEEL POST AND SEPARATED FLANGED NUT, ONE CLAMP REQUIRED FOR EACH BRACE TO POST JUNCTION.

FLAT PANEL SIGN BRACE



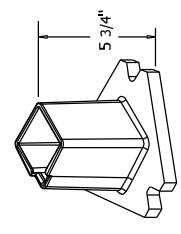
6061-T6 ALUMINUM ALLOY; 2" MOUNTING SURFACE x 7/8" DEPTH x 1/8" NOMINAL WALL THICKNESS. 3/8" HOLES ON 1" CENTERS. CENTERED ALONG ENTIRE LENGTH OF BRACE. MINIMUM OF TWO HORIZONTAL BRACES MUST BE USED.

SIGN DRIVE RIVET W/WASHER



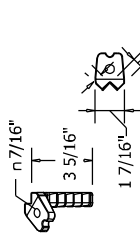
3/8" ALUMINUM SHELL; STEEL CORE RIVET WITH NYLON WASHER. MAXIMUM DISTANCE BETWEEN RIVETS SHOULD BE 12". WASHER MUST BE CENTERED ON RIVET. SIGN BRACE SECURE ALUMINUM SIGN PANEL. ALL EXTRUDED PANEL SIGNS SHALL USE 3/16"x3/8" GRIP RANGE ALUMINUM POP RIVETS (#65).

2-1/2" SQUARE POST RECEIVER



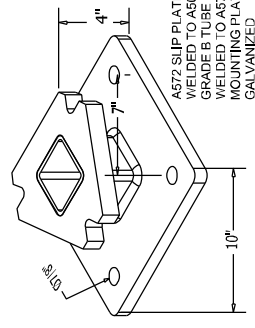
RECEIVER TO BE CAST FROM DUCTILE IRON ASTM A536 CLASS 65-45-12. GALVANIZED PER ASTM A153.

POST LOCKING WEDGE



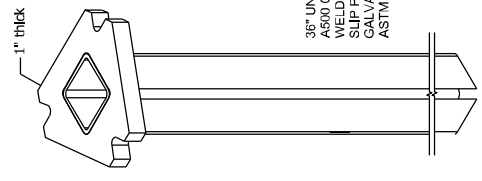
LOCKING WEDGE FORGED STEEL PER ASTM A153. GALVANIZED PER ASTM A153.

SURFACE MOUNT BASE



A572 SLIP PLATE WELDED TO A500 GRADE B TUBE WELDED TO A572 MOUNTING PLATE. GALVANIZED PER ASTM A153.

UNIBASE ANCHOR STUB



36" UNIBASE ANCHOR A500 GRADE B TUBE WELDED TO A572 SLIP PLATE. GALVANIZED PER ASTM A153.

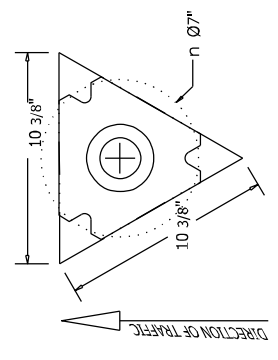
	1/2" x 3/8" GRADE 8, DOUBLE HEX BOLT, GALVANIZE PER ASTM B865 (3 EACH PER KIT)
	5/8" U.S.S. FLAT WASHER ANSI B 18.22.1, GALVANIZE PER ASTM B865 (8 EACH PER KIT)
	1/2" x 3/8" SQUARE x 3/16" THICK TETLON COATED, HARDENED SLIP WASHER (3 EACH PER KIT)
	1/2" x 3/8" SQUARE x 3/16" THICK TETLON COATED, HARDENED SLIP WASHER (3 EACH PER KIT)

SLIP BASE MATCH PLATE HARDWARE KIT

Notice to Bidders No. 6570-00

- NOTES:
- A. CONCRETE FOOTING MUST BE 12" DIAMETER x 48" DEEP.
 - B. BRASS SHIMS MAY BE USED BETWEEN SLIP PLATES TO LEVEL THE UPPER SLIP PLANE.
 - C. SIGN BRACE CLAMPS ALLOW SIGN BRACE TO BE ADJUSTED UP, DOWN, LEFT OR RIGHT IN ORDER TO ACHIEVE PERFECT POSITION OF SIGN PANEL.

SLIP PLATE DIMENSION



BY	REVISION	DATE

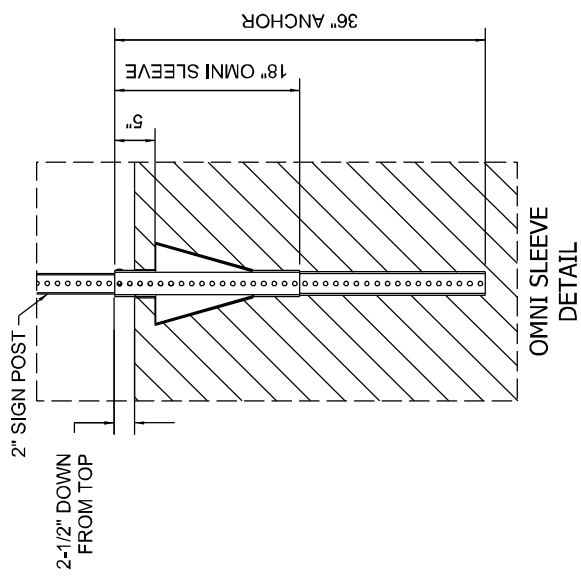
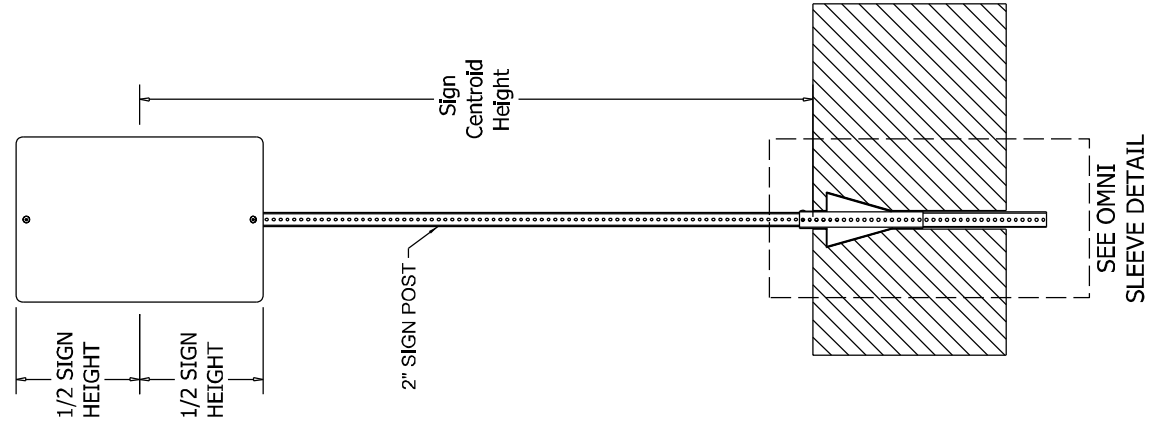
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
SIGN SUPPORT HARDWARE

PROJECT NO.: 2.5" SQUARE POST
COUNTY: ---
FILE NAME: ISS-1.DGN
DESIGN TEAM: ---
DATE: ---

PRIMARY
NOT FOR CONSTRUCTION
WORKING NUMBER
ISS-1
SHEET NUMBER
\$PC\$

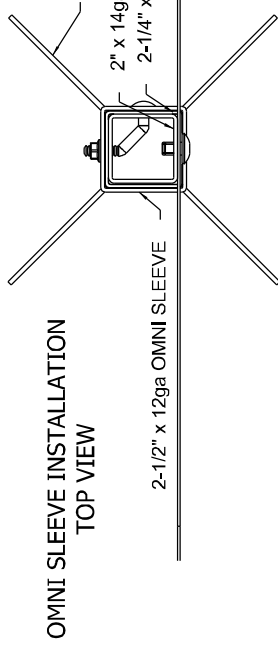
FMS CON: _____
 STATE PROJECT NO: _____
 MISS. _____

SOIL INSTALLATION



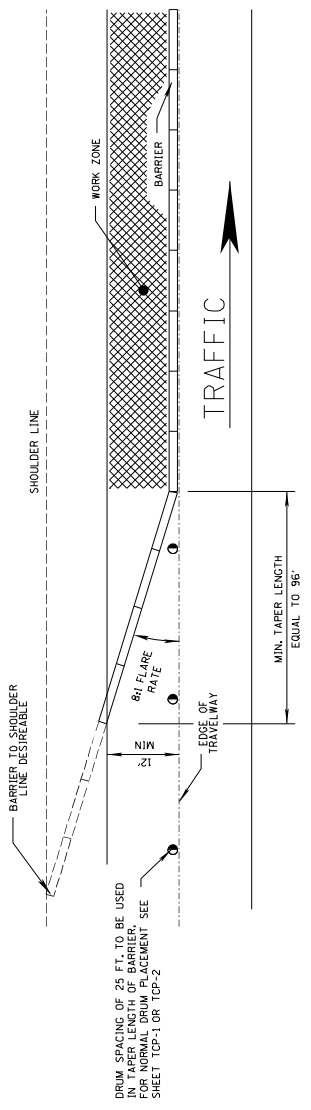
**OMNI SLEEVE
 DETAIL**

MATERIALS: PERFORATED SQUARE STEEL TUBE Post and Anchor (PSST):
 Must be ASTM A1011, Grade 50 steel, with an average minimum yield strength after cold-forming must be 60,000 psi. It must be corner welded, scarfed after welding, then zinc coated after scarfing. It must be coated with a chromate conversion coating and a clear organic polymer topcoat. Its interior and exterior will be galvanized. It must have 7/16" holes spaced 1" apart along the centerline of each of its four sides.
POST: must be 2" x 14ga x appropriate length.
ANCHOR: must be 2-1/4" x 12ga x 36".
OMNI SLEEVE: must be:
 2-1/2" x 12ga x 18" and contain four 4" x 12" x 10ga ASTM A569 steel fins welded to each of the four corner edges of the square sleeve with 4" flat end of each fin positioned 5" down from one end, and must be galvanized. A medium corner bolt must be used to secure the assembly.

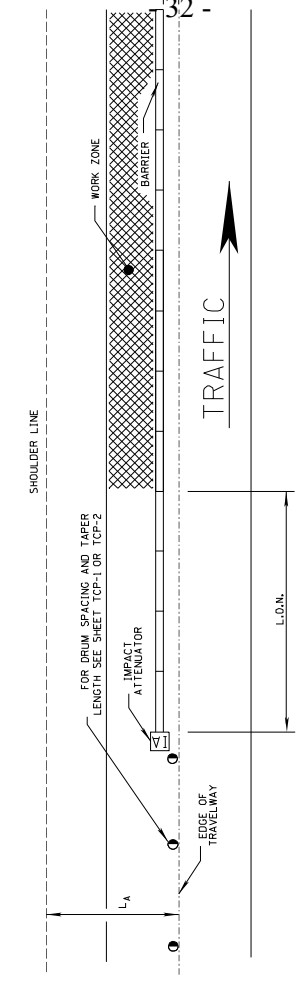


**OMNI SLEEVE INSTALLATION
 TOP VIEW**

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
SIGN SUPPORT HARDWARE	
BY	DATE
REVISION	PROJ. NO.: _____
	COUNTY: _____
	FILENAME: ISS-2.DGN
	DESIGN TEAM _____
	DATE _____
	DWG. NUMBER _____
	SHEET NUMBER _____
	ISS-2
	PRICE \$ _____
	PER UNIT \$ _____



DETAIL OF POSITIVE BARRIER WITH TAPER



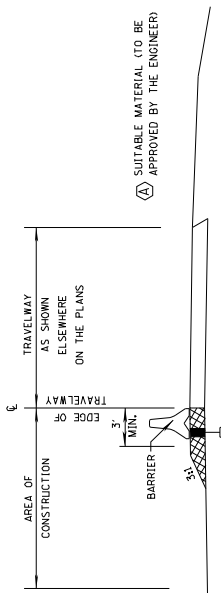
DETAIL OF POSITIVE BARRIER WITH IMPACT ATTENUATOR

NOTES:
1. LENGTH OF NEED, L.O.N. = $\frac{L_1 + L_2}{L_1}$

WHERE: L_1 = LATERAL EXTENT OF THE AREA OF CONCERN
 L_2 = RUNOUT LENGTH
 L_3 = LATERAL OFFSET FROM EDGE OF TRAVELED WAY TO BARRIER.

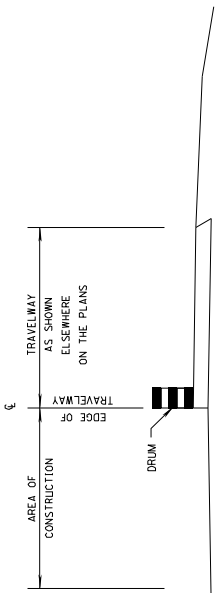
GENERAL NOTES:
1. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER OTHER BID ITEMS.
2. FOR DETAILS OF DRUM PLACEMENT SEE OTHER TRAFFIC CONTROL PLANS.

DESIGN SPEED (mph)	OVER 10,000 veh/dy	5,000-10,000 veh/dy	1,000-5,000 veh/dy	UNDER 1,000 veh/dy
70	360	330	290	250
60	300	250	210	200
50	230	190	160	150
40	160	130	110	100
30	110	90	80	70



ELEVATION VIEW FOR POSITIVE BARRIER

NOTES:
1. POSITIVE BARRIER IS REQUIRED IN THE AREA OF OPEN PUNCH OUTS THAT ARE WITHIN SIX (6) FEET OF THE TRAVELWAY WHENEVER ACTUAL REPAIR WORK IS NOT BEING PERFORMED WITHIN THE LANE CLOSURE.
2. MATERIAL USED TO SUPPORT POSITIVE BARRIER MUST BE AT SAME ELEVATION AS PAVEMENT IN ADJACENT TRAVELWAY.
3. DELINEATORS REQUIRED ON ALL NON-REFLECTIVE BARRIER, AS SHOWN ON WORKING NO. OMB-3.



ELEVATION VIEW FOR DRUM

NOTES:
1. WHILE WORK IS BEING PERFORMED WITHIN THE LANE CLOSURE DROP-OFFS MUST BE PROTECTED WITH DRUMS, ETC. IN EMERGENCIES EXCAVATED SECTION MAY BE BACKFILLED WITH GRANULAR MATERIAL, STONE OR OTHER APPROVED MATERIAL TO AVOID OVERNIGHT DROP-OFFS.
2. LANE CLOSURES WITH OPEN PUNCH OUT AREAS MAY NOT BE LEFT UNATTENDED WHEN DRUMS ARE BEING USED FOR LANE CLOSURE

Notice to Bidders No. 6570 -- Co

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
LANE CLOSURE DETAILS FOR GREATER THAN 3 INCH DROPOFF

PROJ. NO.: _____
COUNTY: _____

WORKING NUMBER: _____
SHEET NUMBER: _____

FILE NAME: SDTOP-CADGN
DESIGN TEAM: _____
CHECKED: _____
DATE: _____

REVISION: _____
DATE: _____

"General Decision Number: MS20250140 01/03/2025

Superseded General Decision Number: MS20240140

State: Mississippi

Construction Type: Highway

County: Rankin County in Mississippi.

HIGHWAY CONSTRUCTION PROJECTS

Note: Contracts subject to the Davis-Bacon Act are generally required to pay at least the applicable minimum wage rate required under Executive Order 14026 or Executive Order 13658. Please note that these Executive Orders apply to covered contracts entered into by the federal government that are subject to the Davis-Bacon Act itself, but do not apply to contracts subject only to the Davis-Bacon Related Acts, including those set forth at 29 CFR 5.1(a)(1).

<p>If the contract is entered into on or after January 30, 2022, or the contract is renewed or extended (e.g., an option is exercised) on or after January 30, 2022:</p>	<ul style="list-style-type: none"> . Executive Order 14026 generally applies to the contract. . The contractor must pay all covered workers at least \$17.75 per hour (or the applicable wage rate listed on this wage determination, if it is higher) for all hours spent performing on the contract in 2025.
<p>If the contract was awarded on or between January 1, 2015 and January 29, 2022, and the contract is not renewed or extended on or after January 30, 2022:</p>	<ul style="list-style-type: none"> . Executive Order 13658 generally applies to the contract. . The contractor must pay all covered workers at least \$13.30 per hour (or the applicable wage rate listed on this wage determination, if it is higher) for all hours spent performing on that contract in 2025.

The applicable Executive Order minimum wage rate will be adjusted annually. If this contract is covered by one of the Executive Orders and a classification considered necessary for performance of work on the contract does not appear on this wage determination, the contractor must still submit a conformance request.

Additional information on contractor requirements and worker protections under the Executive Orders is available at <http://www.dol.gov/whd/govcontracts>.

Modification Number Publication Date
 0 01/03/2025

ELEC0480-010 01/01/2024

	Rates	Fringes
TRAFFIC SIGNALIZATION		
Electrician.....	\$ 29.35	10.46

SUMS2010-063 08/04/2014		

	Rates	Fringes
CARPENTER (Form Work Only).....	\$ 15.47 **	0.00
CEMENT MASON/CONCRETE FINISHER...	\$ 14.02 **	0.00
ELECTRICIAN.....	\$ 24.04	5.87
HIGHWAY/PARKING LOT STRIPING:		
Truck Driver (Line Striping Truck).....	\$ 12.04 **	0.00
INSTALLER - GUARDRAIL.....	\$ 12.07 **	0.00
INSTALLER - SIGN.....	\$ 11.92 **	0.00
IRONWORKER, REINFORCING.....	\$ 15.47 **	0.00
LABORER: Common or General, Including Asphalt Raking, Shoveling, Spreading; and Grade Checking.....	\$ 10.65 **	0.00
LABORER: Flagger.....	\$ 10.22 **	0.00
LABORER: Luteman.....	\$ 12.88 **	0.00
LABORER: Mason Tender - Cement/Concrete.....	\$ 11.27 **	0.00
LABORER: Pipelayer.....	\$ 13.44 **	0.00
LABORER: Laborer-Cones/ Barricades/Barrels - Setter/Mover/Sweeper.....	\$ 11.29 **	0.00
OPERATOR: Asphalt Spreader.....	\$ 14.71 **	0.00
OPERATOR: Backhoe/Excavator/Trackhoe.....	\$ 15.36 **	0.00
OPERATOR: Bobcat/Skid Steer/Skid Loader.....	\$ 11.64 **	0.00
OPERATOR: Broom/Sweeper.....	\$ 11.57 **	0.00
OPERATOR: Bulldozer.....	\$ 15.41 **	0.00
OPERATOR: Concrete Saw.....	\$ 14.38 **	0.00
OPERATOR: Crane.....	\$ 19.22	0.00
OPERATOR: Distributor.....	\$ 10.95 **	0.00
OPERATOR: Grader/Blade.....	\$ 14.41 **	0.00
OPERATOR: Grinding/Grooving		

Machine.....	\$ 15.90 **	0.00
OPERATOR: Loader.....	\$ 12.57 **	0.00
OPERATOR: Mechanic.....	\$ 19.27	0.00
OPERATOR: Milling Machine.....	\$ 14.68 **	0.00
OPERATOR: Mixer.....	\$ 14.25 **	0.00
OPERATOR: Oiler.....	\$ 12.35 **	0.00
OPERATOR: Paver (Asphalt, Aggregate, and Concrete).....	\$ 12.15 **	0.00
OPERATOR: Roller (All Types)....	\$ 12.64 **	0.00
OPERATOR: Scraper.....	\$ 12.25 **	0.00
OPERATOR: Tractor.....	\$ 11.22 **	0.00
TRUCK DRIVER: Flatbed Truck.....	\$ 14.06 **	0.00
TRUCK DRIVER: Lowboy Truck.....	\$ 11.00 **	0.00
TRUCK DRIVER: Mechanic.....	\$ 13.00 **	0.00
TRUCK DRIVER: Water Truck.....	\$ 10.98 **	0.00
TRUCK DRIVER: Dump Truck (All Types).....	\$ 12.56 **	0.00
TRUCK DRIVER: Semi/Trailer Truck.....	\$ 14.60 **	0.00

WELDERS - Receive rate prescribed for craft performing operation to which welding is incidental.

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 ** Workers in this classification may be entitled to a higher minimum wage under Executive Order 14026 (\$17.75) or 13658 (\$13.30). Please see the Note at the top of the wage determination for more information. Please also note that the minimum wage requirements of Executive Order 14026 are not currently being enforced as to any contract or subcontract to which the states of Texas, Louisiana, or Mississippi, including their agencies, are a party.

Note: Executive Order (EO) 13706, Establishing Paid Sick Leave for Federal Contractors applies to all contracts subject to the Davis-Bacon Act for which the contract is awarded (and any solicitation was issued) on or after January 1, 2017. If this contract is covered by the EO, the contractor must provide employees with 1 hour of paid sick leave for every 30 hours they work, up to 56 hours of paid sick leave each year. Employees must be permitted to use paid sick leave for their own illness, injury or other health-related needs, including preventive care; to assist a family member (or person who is like family to the employee) who is ill, injured, or has other health-related needs, including preventive care; or for reasons resulting from, or to assist a family member (or person who is like family to the employee) who is a victim of, domestic violence, sexual assault, or stalking. Additional information

on contractor requirements and worker protections under the EO is available at <https://www.dol.gov/agencies/whd/government-contracts>.

Unlisted classifications needed for work not included within the scope of the classifications listed may be added after award only as provided in the labor standards contract clauses (29CFR 5.5 (a) (1) (iii)).

The body of each wage determination lists the classifications and wage rates that have been found to be prevailing for the type(s) of construction and geographic area covered by the wage determination. The classifications are listed in alphabetical order under rate identifiers indicating whether the particular rate is a union rate (current union negotiated rate), a survey rate, a weighted union average rate, a state adopted rate, or a supplemental classification rate.

Union Rate Identifiers

A four-letter identifier beginning with characters other than ""SU"", ""UAVG"", ?SA?, or ?SC? denotes that a union rate was prevailing for that classification in the survey. Example: PLUM0198-005 07/01/2024. PLUM is an identifier of the union whose collectively bargained rate prevailed in the survey for this classification, which in this example would be Plumbers. 0198 indicates the local union number or district council number where applicable, i.e., Plumbers Local 0198. The next number, 005 in the example, is an internal number used in processing the wage determination. The date, 07/01/2024 in the example, is the effective date of the most current negotiated rate.

Union prevailing wage rates are updated to reflect all changes over time that are reported to WHD in the rates in the collective bargaining agreement (CBA) governing the classification.

Union Average Rate Identifiers

The UAVG identifier indicates that no single rate prevailed for those classifications, but that 100% of the data reported for the classifications reflected union rates. EXAMPLE: UAVG-OH-0010 01/01/2024. UAVG indicates that the rate is a weighted union average rate. OH indicates the State of Ohio. The next number, 0010 in the example, is an internal number used in producing the wage determination. The date, 01/01/2024 in the example, indicates the date the wage determination was updated to reflect the most current union average rate.

A UAVG rate will be updated once a year, usually in January, to reflect a weighted average of the current rates in the collective bargaining agreements on which the rate is based.

Survey Rate Identifiers

The ""SU"" identifier indicates that either a single non-union rate prevailed (as defined in 29 CFR 1.2) for this classification in the survey or that the rate was derived by computing a weighted average rate based on all the rates reported in the survey for that classification. As a weighted average rate includes all rates reported in the survey, it may

include both union and non-union rates. Example: SUFL2022-007 6/27/2024. SU indicates the rate is a single non-union prevailing rate or a weighted average of survey data for that classification. FL indicates the State of Florida. 2022 is the year of the survey on which these classifications and rates are based. The next number, 007 in the example, is an internal number used in producing the wage determination. The date, 6/27/2024 in the example, indicates the survey completion date for the classifications and rates under that identifier.

?SU? wage rates typically remain in effect until a new survey is conducted. However, the Wage and Hour Division (WHD) has the discretion to update such rates under 29 CFR 1.6(c)(1).

State Adopted Rate Identifiers

The ""SA"" identifier indicates that the classifications and prevailing wage rates set by a state (or local) government were adopted under 29 C.F.R 1.3(g)-(h). Example: SAME2023-007 01/03/2024. SA reflects that the rates are state adopted. ME refers to the State of Maine. 2023 is the year during which the state completed the survey on which the listed classifications and rates are based. The next number, 007 in the example, is an internal number used in producing the wage determination. The date, 01/03/2024 in the example, reflects the date on which the classifications and rates under the ?SA? identifier took effect under state law in the state from which the rates were adopted.

WAGE DETERMINATION APPEALS PROCESS

1) Has there been an initial decision in the matter? This can be:

- a) a survey underlying a wage determination
- b) an existing published wage determination
- c) an initial WHD letter setting forth a position on a wage determination matter
- d) an initial conformance (additional classification and rate) determination

On survey related matters, initial contact, including requests for summaries of surveys, should be directed to the WHD Branch of Wage Surveys. Requests can be submitted via email to davisbaconinfo@dol.gov or by mail to:

Branch of Wage Surveys
 Wage and Hour Division
 U.S. Department of Labor
 200 Constitution Avenue, N.W.
 Washington, DC 20210

Regarding any other wage determination matter such as conformance decisions, requests for initial decisions should be directed to the WHD Branch of Construction Wage Determinations. Requests can be submitted via email to BCWD-Office@dol.gov or by mail to:

Branch of Construction Wage Determinations
 Wage and Hour Division
 U.S. Department of Labor
 200 Constitution Avenue, N.W.
 Washington, DC 20210

2) If an initial decision has been issued, then any interested party (those affected by the action) that disagrees with the decision can request review and reconsideration from the Wage and Hour Administrator (See 29 CFR Part 1.8 and 29 CFR Part 7). Requests for review and reconsideration can be submitted via email to dba.reconsideration@dol.gov or by mail to:

Wage and Hour Administrator
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

The request should be accompanied by a full statement of the interested party's position and any information (wage payment data, project description, area practice material, etc.) that the requestor considers relevant to the issue.

3) If the decision of the Administrator is not favorable, an interested party may appeal directly to the Administrative Review Board (formerly the Wage Appeals Board). Write to:

Administrative Review Board
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210.

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END OF GENERAL DECISION"

**SECTION 903
PERFORMANCE BOND**

PERFORMANCE BOND FOR THE FOLLOWING CONTRACT:

Project No.: _____

For the construction of: _____

Contract date: _____ Contract Price: _____

FOR OWNER: MISSISSIPPI TRANSPORTATION COMMISSION, 401 N. WEST STREET, JACKSON, MISSISSIPPI 39201.

CONTRACTOR (full legal name, contact person, phone number and address):

SURETY (legal name, phone number, principal place of business and address *for notice purposes*):

Second Surety (if applicable):

The Contractor and Surety, jointly and severally, bind themselves, their heirs, executors, administrators, successors and assigns, to the Owner for the performance of the Contract, which is incorporated herein by reference, and subject to the following terms:

1. If the Contractor fully and faithfully performs the Contract, the Surety and the Contractor shall have no obligation under this Bond.
2. The Surety's obligation under this Bond shall arise after:
 - (a) the Owner first provides notice to the Contractor and the Surety that termination is imminent, pursuant to the current edition of the Mississippi Standard Specifications for Road and Bridge Construction, which is a part of the Contract; and
 - (b) the Owner declares a Contractor Default, terminates the Contract, and notifies the Surety.
3. Within 20 calendar days as set forth in Section 108.08 of the current edition of the Mississippi Standard Specifications for Road and Bridge Construction, the Surety shall, after discussions with and consent from the Owner, and at the Surety's expense, elect to take one of the following actions:
 - (a) Arrange for the Contractor, with the consent of the Owner, to perform and complete the Contract;
 - (b) Undertake to perform and complete the Contract itself, through its agents or independent contractors;
 - (c) Waive its right to perform and complete, arrange for completion, or obtain a new contractor, and after investigation, determine the amount for which it may be liable to the Owner (subject to the consent of the Owner) and as soon as practicable after the amount is determined, make payment to the Owner.

4. If the Surety does not proceed, within a reasonable time frame, to enact and carry out the election made in Paragraph 3, then the Surety shall be deemed to be in default on this Bond, and the Owner shall be entitled to enforce any remedy available to it under the Contract and applicable law.
5. Subject to the commitment by the Owner to pay the Balance of the Contract Price, the Surety is obligated, without duplication, for
 - (a) the responsibilities of the Contractor for correction of defective work and completion of the Contract;
 - (b) additional legal, design professional and delay costs resulting from the Contractor's Default, and resulting from the actions or failure to act of the Surety under Paragraph 3; and
 - (c) liquidated damages, or if no liquidated damages are specified in the Contract, actual damages caused by delayed performance or non-performance of the Contractor.
6. The Surety hereby waives notice of any change, including changes of time, to the Contract or to related subcontracts, purchase orders and other obligations.
7. The penal sum of the Bond shall be equal to the Contract Price; however, the penal sum may be increased or decreased as the result of any subsequent Supplemental Agreements and/or final contract quantities.
8. Notice to the Surety, the Owner or the Contractor shall be mailed or delivered to the address listed for notice purposes on the first page of this Bond.

CONTRACTOR AS PRINCIPAL

Company: _____

Signature: _____

Name: _____

Title: _____

Address: _____

SURETY

Company: _____

Signature: _____

MS Insurance ID # _____

Name: _____

Title: _____

Address: _____

SURETY (if applicable)

Company: _____

Signature: _____

MS Insurance ID # _____

Name: _____

Title: _____

Address: _____

**SECTION 903
PAYMENT BOND**

PAYMENT BOND FOR THE FOLLOWING CONTRACT:

Project No.: _____

For the construction of: _____

Contract date: _____ Contract Price: _____

**FOR OWNER: MISSISSIPPI TRANSPORTATION COMMISSION, 401 N. WEST STREET,
JACKSON, MISSISSIPPI 39201.**

CONTRACTOR (full legal name, contact person, phone number and address):

SURETY (legal name, phone number, principal place of business and address *for notice purposes*):

Second Surety (if applicable):

The Contractor and Surety, jointly and severally, bind themselves, their heirs, executors, administrators, successors and assigns, to the Owner for payment of labor, materials and equipment furnished for use in the performance of the Contract, which is incorporated herein by reference, subject to the following terms:

1. If the Contractor promptly makes payment of all sums due to any and all subcontractors, sub-subcontractors, suppliers to the Contractor, suppliers to subcontractors and/or laborers who have performed work on the project site, and defends, indemnifies and holds harmless the Owner from claims, demands, liens or suits by any person or entity seeking payment for labor, materials or equipment furnished for use in the performance of the Contract, then the Surety and the Contractor shall have no obligation under this Bond.
2. The Owner shall provide notice to the Surety of any claims, demands, liens or suits against the Owner or the Owner's property that it receives from any person or entity ("Claimants") seeking payment for labor, materials or equipment furnished for use in the performance of the Contract.
3. Upon notice of any claims, demands, liens or suits provided by the Owner or Contractor or given to the Surety by a Claimant, the Surety shall promptly and at the Surety's expense, defend, indemnify and hold harmless the Owner against said claim, demand, lien or suit and shall take the following additional actions:
 - (a) Send an answer to the Claimant, with a copy to the Owner, within sixty (60) days after receipt of the Claim, stating the amounts that are undisputed and the basis for challenging any amounts that are disputed; and
 - (b) Pay or arrange for payment of any undisputed amounts.

4. The Surety shall not be liable to the Owner, Claimants or others for obligations of the Contractor that are unrelated to the Contract. The Owner shall not be liable for the payment of any costs or expenses of any Claimant under this Bond and shall have no obligation under this Bond to make payments to, or give notice on behalf of, Claimants, or otherwise have any obligations to Claimants under this Bond.
5. The Surety hereby waives notice of any change, including changes of time, to the Contract or to related subcontracts, purchase orders and other obligations.
6. The penal sum of the Bond shall be equal to the Contract Price; however, the penal sum may be increased or decreased as the result of any subsequent Supplemental Agreements and/or final contract quantities.

CONTRACTOR AS PRINCIPAL

Company: _____
Signature: _____
Name: _____
Title: _____
Address: _____

SURETY

Company: _____
Signature: _____
Name: _____
Title: _____
Address: _____

MS Insurance ID # _____

SURETY (if applicable)

Company: _____
Signature: _____
Name: _____
Title: _____
Address: _____

MS Insurance ID # _____