SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO.	1	DATED	4/17/2025	ADDENDUM NO.		DATED	
ADDENDUM NO		DATED		ADDENDUM NO.		DATED	
ADDENDUM NO		DATED		ADDENDUM NO.		DATED	

Number	Description
1	Revised NTB Nos. 6756 & 6757; Amendment EBSx Download Required.

TOTAL ADDENDA: 1
(Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

_____
Contractor

BY _____
Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

_____
President Address

_____
Secretary Address

_____
Treasurer Address

The following is my (our) itemized proposal.

NHPP-0056-01(114)/ 109228301000 & SP-0056-01(124)/ 109228302000

Hinds County(ies)

Revised 01/26/2016

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 6756 **CODE: (SP)**

DATE: **04/17/2025**

SUBJECT: **Scope of Work**

PROJECT: **NHPP-0056-01(114) / 109228301 & SP-0056-01(124) / 109228302 – Hinds County**

The contract documents do not include an official set of construction plans but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, "Standard Drawings".

A general description of the work required on the project is as follows:

Milling/Overlaying and Concrete Rehabilitation of approximately 2.2 miles of existing pavement on SR 25 from B.O.M (BOP STA 28+53) to Rankin County Line (EOP STA 108+18) along with the adjacent frontage roads. Details of specific work are mentioned in the following sections.

NHPP-0056-01(114) / 109228301
SR 25 STA 28+53 (BOP) to STA 108+18 (EOP) (EQUATION) 48+10.69 BK to 9+94.3 AH (EQUATION) 87+88.13 BK to 88+04.74 AH

Work in this section shall consist of milling 1½" and variable on the mainline and shoulders, and inlaying with 1½" of 9.5-mm, MT asphalt on the mainline and shoulders. A dditional work includes full depth repairs of CRCP and drainage repairs. See attached tables for additional details. All guardrail not meeting current MDOT standards shall be replaced. Traffic will be allowed to run on the milled surfaces no more than five (5) consecutive days.

SP-0056-01(124) / 109228302 – SR 25 Frontage Roads

Work in this section shall consist of paving 1½" and variable of 9.5-mm, MT asphalt. Failed areas listed in the provided table shall be repaired full depth using 12.5-mm, MT, Leveling asphalt. Traffic will be allowed to run on the milled surfaces no more than five (5) consecutive days.

GENERAL NOTES:
MILLING

Milling/paving shall not begin until an **approved** asphalt mix design has been received, nor until such time that, in the opinion of the Engineer, weather conditions have been consistently suitable enough to allow placement of the asphalt pavement after the milling operations.

The reclaimed asphalt pavement (RAP) material removed by the milling operation shall become the property of the Contractor.

Where milling is required, the Contractor shall provide outlets in the existing shoulders at

sufficient intervals to prevent pooling or standing water on the milled surface; the cost of which shall be absorbed in other items bid.

Milling and paving operations shall be performed such that a -2% slope from centerline is provided in normal crown roadway sections. Super-Elevation through curves shall be maintained as it currently exists or improved as directed. Where slope correction is required, correction will be made by milling, paving, or combination thereof as directed by the Engineer. Milling correction: Mill outside edge of pavement to a depth of 1½" on a 2% slope towards the centerline. Paving Correction: Mill to depth of 1½" on existing slope and 2¼" and variable on centerline and 1½" on outside edge. Combination Method: Combination of both methods as directed by the Engineer to achieve the desired slope. In super elevated areas where correct SE exist milling will transition to thickness through curves. Where correct SE does not exist milling will transition at curves to correct SE as directed by the Engineer.

Milling operations shall be performed in accordance with the Contract documents and the MDOT Standard Specifications. Variable width and length transitions may be required for tie-ins at ramps, local roads, project limits.

Milling of driveway pads shall be conducted in a manner to prevent gouging or otherwise affecting the roadway pavement structure and slope. Milling of driveway pads shall not be done in simultaneous path with mainline milling.

Traffic will be allowed to travel on the mainline milled surface for five (5) days, and the Contractor will be assessed a penalty of $5,000.00 per calendar day afterwards until the mainline milled surface is covered with the next lift of asphalt. This includes frontage roads. Additionally, traffic will be allowed to run on all milled surfaces other than the mainline for 30 days unless otherwise stated, and the Contractor will be assessed a penalty of **$1,000.00** per calendar day afterwards until the non-mainline milled surface is covered with the next lift of asphalt. The additional allowance for the non-mainline milled surface is for the Contractor's convenience, and thus, the Contractor is responsible for any pavement failures or damage sustained during this period. Milling and paving of paved shoulders shall conform to Subsection 406.03.2 of the Standard Specifications.

PAVING

Per Subsection 401.02.3.2, the asphalt mix design shall be submitted to the Engineer at least 10 working days prior to its proposed use.

Prior to mainline milling and paving operations, failed areas in the existing concrete pavement shall be removed and repaired. CRCP concrete failed areas shall be repaired as per the attached details and be paid under 8" and Variable Continuously Reinforced Concrete Pavement, Broom Finish. The concrete pavement failures shall be removed by saw cutting and excavating the failed material. Any failures in the cement treated base shall be removed and replaced with Class "C" concrete. Payment will be made under pay item 503-D: Concrete for Base Repair. A list of the failed areas is shown in the attached tables. Pavement repairs shall be completed as a continuous operation in order to minimize traffic impacts. 12.5-mm, MT, Leveling asphalt shall be placed to grade over the concrete repair, prior to opening traffic. Payment will be

made under pay item 907-403-B. Lane closures shall remain in place until the failed area has been completely repaired and lane closures shall not be left unattended.

A table showing locations of underlying problems with the concrete pavement has been included. The following sequence of operations will be used to correct the underlying concrete pavement problems: Fill voids under the concrete pavement, joints at the centerline of the concrete pavement, and joints at the edge of pavement between the concrete pavement and the soil cement-treated shoulder by pressure grouting. After pressure grouting, mill and replace the asphalt over the joint to a maximum depth of six inches (6") at 4 feet in width. After milling and prior to replacement, if there is any remaining depth of asphalt over the joint, repair any failed asphalt by removing all loose/broken pieces. Replacement of milled area and any repair areas are to be made with 12.5-mm, MT, Leveling asphalt back to existing finish grade.

Failed areas in the existing pavement on the Frontage Road shall be removed and backfilled with 12.5-mm, MT, Leveling asphalt pavement as per the attached typical sections and details. Asphalt shall be placed in multiple lifts with a maximum lift thickness of 3". Any granular/chemically treated/stone/etc. base or subgrade material deemed unsuitable by the Engineer shall be removed as directed and backfilled with 12.5-mm, MT, Leveling asphalt. Payment for the excavation of the granular base and subgrade will be made using the 203-G: Excess Excavation pay item. A list of the failed areas is shown in the attached tables. Pavement repairs shall be completed as a continuous operation in order to minimize traffic impacts. Lane closures shall remain in place until the failed area has been completely repaired. Lane closures may not be left unattended.

The surface lift for failed area repair or concrete punchout repair shall have a maximum deviation of 3/8" as determined by a 10-foot straight edge. Any location that deviates more than this tolerance, as determined by the Engineer, shall be corrected at no additional cost to the State.

Publicly maintained roads and streets should be paved to the existing right-of-way and in accordance with the attached drawings.

Privately owned entrances shall be paved to the shoulder line per the included typical drawing unless otherwise directed. Pad dimensions shall match the existing lengths and widths unless otherwise directed. Pads shall be shaped horizontally and vertically to prevent excessive drop-offs. Any new driveway pads deemed necessary by the Engineer shall be placed according to specifications.

If traditional excavation methods are used, the removal area shall first be saw cut full depth including concrete, where applicable, to create a neat line and prevent damage to the adjacent pavement structure. Payment for saw cuts will be made using the appropriate items. If milling techniques are used, the area will not require saw cuts, but care should be exercised to create a neat removal line and to prevent damage to the adjacent pavement structure. If saw cuts are used in conjunction with milling, payment will be made using the appropriate pay items. Payment will not be made for saw cuts that are not performed.

GRANULAR SHOULDER MATERIAL

Where applicable, the existing shoulders shall be raised to match the new pavement elevation by placing variable depth granular material. The shoulders shall be graded and pulled up on a daily basis to eliminate drop-offs in excess of 2¼". Placement of the granular material on the finished asphalt course shall not be permitted. The existing shoulder shall be scarified to allow incorporation of the new shoulder material. The material shall be bladed, rolled, and compacted to a finished slope of four percent (4%) in normal crown sections. Placement of this material shall be performed to provide a uniform and compacted shoulder with a minimum depth and width of material placed. Shoulders with adequate shoulder material in place shall be bladed to a slope of four percent (4%) in normal crown sections. The cost of blading will be an absorbed item and shall be included in the price of other items bid.

Granular material (Class 5, Group C) shall be provided around driveway pads as directed to prevent shoulder drop-offs and shall be placed in a timely manner.

Drop-offs exceeding 2¼" shall be corrected within two (2) calendar days of the placement of the pad. Stabilizer aggregate shall be used as directed by the Engineer.

Any material excavated from the existing shoulder during pavement widening operations or as a result of shoulder blading shall be used on the existing shoulder to match the new pavement elevation and any surplus material shall be spread along the edge of the shoulders, fore slopes, or other adjacent areas as directed by the Engineer and will be an absorbed item. Material which cannot be suitably placed in adjacent areas and deemed to be excess excavation by the Engineer shall be removed from the project site. Payment for removal of excess material will be made using pay item 203-G: Excess Excavation.

TEMPORARY AND PERMANENT PAVEMENT MARKINGS

Temporary traffic stripe will be required immediately after the milling and/or required overlay and prior to opening area to traffic. Temporary stripe shall be placed in the same location and configuration as the permanent stripe except that it may be offset as required for milling and paving operations. If temporary stripe is offset, the Contractor shall conduct operations in a manner to insure the final temporary stripe is placed at the required location of the permanent stripe. If removal of temporary offset stripe is required in order to achieve the correct location and alignment of permanent stripe. The cost of removal will be absorbed in other items bid. Placing double temporary centerline will not be allowed.

Temporary striping shall conform to finished stripe specifications for alignment, neatness, and straightness.

The use of short strips of traffic tape will not be allowed unless approved by the Engineer.

All permanent striping will be double drop thermoplastic, 90-mil thickness unless otherwise specified in Subsection 907-626.03.2. Edge lines shall be placed to accommodate the lane widths shown on the attached applicable typical sections unless prevented by field conditions.

Per Subsection 907-626.01, a epoxy-sealer shall be applied to the concrete pavement or bridge surface prior to the placement of the thermoplastic material and shall be absorbed under the

thermoplastic pay items. The type and amount of epoxy-sealer used shall adhere to the thermoplastic manufacturer's recommendations.

Permanent raised pavement markers shall be installed on mainline and local public roads after completion of all paving operations.

Payment for edge stripe on local roads shall be made under pay item 907-626-G: Thermoplastic Double Drop Detail Stripe, White when the length of said stripe is less than 150 feet when measured from the end of the radius. If the measured length is greater than 150 feet, then payment shall be made under pay item 907-626-B: 6" Thermoplastic Double Drop Traffic Stripe, Continuous White.

Payment for centerline stripe on local roads shall be made under pay item 907-626-G: Thermoplastic Double Drop Detail Stripe, Yellow when the length of said stripe is less than 150 feet when measured from the stop bar. If the measured length is greater than 150 feet, then payment shall be made under pay item 907-626-E: 6" Thermoplastic Double Drop Traffic Stripe, Continuous Yellow. Centerline Stripe shall be omitted on local roads whose width is less than 20 feet.

The face of all existing undisturbed curbs shall be painted with at least two coats of white traffic paint with glass beads being required in the top coat. The cost associated with the painting of new or existing curb is to be included in other items bid.

Pavement section marking tape on this project shall be located prior to overlaying and placed back in the same location after paving operations have ceased. The section marking shall be 8-inch High-performance cold plastic detail stripe and shall be four (4) feet in length. The marking shall be centered across the centerline stripe. The cost of this item shall be absorbed in other items bid.

GUARDRAIL

Guardrails shall be replaced at the locations shown on the attached table. Removal of guardrail shall consist of removal of bridge end section, w-beam/thrie beam, terminal end section, posts, and all other appurtenances. All guardrail removed shall be replaced the same day and prior to reopening the adjacent lane of traffic. Voids created by the removal of posts, concrete anchors, footings, etc. shall be backfilled and compacted in accordance with Section 203 of the Standard Specifications.

The asphalt guardrail pad shall be milled and paved up to the face of the guardrail. The remaining asphalt guardrail pad behind the face of the guardrail shall be removed and shall be paid for using the fine milling pay item regardless of removal method. If the remaining asphalt is removed with conventional methods and not milled, saw cuts shall be made at the face of rail and paid under pay item 503-C: Saw Cuts, Full Depth. The guardrail pad shall be reconstructed using crushed stone granular material and shall be a minimum of 4" in depth. I f blading is required in order to meet the minimum depth, then said blading shall be an absorbed item and the excavated material shall be retained and used to raise the existing shoulder to match the new pavement elevation. Material which cannot be placed and blended in adjacent

areas and deemed to be excess excavation by the Engineer shall be removed under pay item 203-G: Excess Excavation. Prior to the placement of the crushed stone, a soil sterilant shall be applied as per Subsection 616.03.2 and Geotextile Stabilization, Type V, Non-Woven installed underneath the limits of the crushed stone. The installed guardrail shall meet all requirements in order to be MASH compliant.

Guardrail lengths are based on terminal end length of 37.5'. If terminal of length other than this is used, an adjustment in w-beam length is required.

All dimensions and spacings for bridge rail connectors shall be verified in the field by the Contractor prior to fabrication.

Object markers at bridge approaches and other locations are to be replaced as shown in the attached table. Removal of object markers shall be absorbed in the cost of other items bid.

PERMANENT SIGNS

Permanent signs as listed on the attached tables shall be replaced. Unless otherwise listed in the attached tables, existing posts, anchors, angles/bars, and other components shall be reused. The Contractor shall use new bolts, screws, washers, nuts, etc. of the required sizes in the installation of signs. If required as part of the sign replacement activities, all post and I-beam lengths in these plans are estimated. Post lengths for all signs shall be verified in the field by the Contractor prior to fabrication. Installation dates shall be clearly written in bold black markings on the back bottom half off all signs with a permanent marking stick that is waterproof, fade resistant, and marks on wet or dry surfaces. If existing sign posts or footings are to be replaced, the existing posts and footings are to be removed and the area backfilled and compacted in accordance with Section 203 of the Standard Specifications. Removal of sign, post, and footing and backfilling will be paid using the removal of sign pay item.

Object markers at bridge approaches and other locations shall be replaced as shown in the attached table. Removal of object markers shall be absorbed in the cost of other items bid.

TRAFFIC CONTROL

The Contractor shall erect and maintain construction signing and provide all signs and traffic control devices necessary to safely maintain traffic around and through the work areas in accordance with the Traffic Control Plan and the MUTCD. The cost shall be included in the price bid for pay item 907-618-A: Maintenance of Traffic. Fluorescent orange sheeting shall be used on all construction and traffic control signs except those designated in the plans to be black legend and border on white background.

Standard roadside construction signs, barricades, etc. shall be placed in accordance with the attached tables, drawings, and as directed by the Engineer. W20-1 signs shall be placed on all public road approaches as shown or as directed. Payment for standard roadside construction signs, barricades, etc. will be made using the appropriate pay items.

The Contractor shall on a daily basis, remove all debris from within the roadway and a 30-foot clear zone which, in the opinion of the Engineer, is a hazard to the traveling public. This

activity shall begin with the beginning of work or the beginning of the contract time, whichever comes first. No direct payment will be made for the debris removal; the cost is to be included in the prices of items bid. Failure of the Contractor to remove the debris as prescribed herein shall be just cause for withholding the monthly progress estimate payment or suspending active operations until the debris is satisfactorily removed by the Contractor.

Temporary asphalt joints (aka paper joints) shall be employed at all locations requiring traffic to traverse an uneven, transverse, pavement joint. Paper joints shall be a minimum of nine feet (9') in length and for the full width of the milled/paved surface. Paper joints shall be adequately maintained.

Potholes that may exist or occur in the existing pavement shall be patched in a timely manner as required. Patching of potholes shall be considered an absorbed item.

Temporary portable rumble strips, as described in Special Provision No. 907-619, shall be used in advance of each lane closure on the frontage roads. Direct payment will not be made for this item and shall be considered absorbed under pay item 907-618-A: Maintenance of Traffic.

MISCELLANEOUS NOTES

It shall be the responsibility of the Contractor to protect existing structures such as pipes, inlets, aprons, bridges, etc. from damage which might occur during construction. T his includes manholes and other utilities in the roadway. The Contractor shall replace or repair, as directed by the Engineer, any structures damaged by the Contractor during the life of the contract. No payment will be made for replacement or repair of damaged items.

Drainage channels listed in the attached table shall have the existing debris and sediment removed by the Contractor and shall be paid for using pay item 202-B: Removal of Debris and Sand From Box Culvert, 10-Foot and Greater Width. The applicable pay item shall be measured along the length of the box culvert or in the case of multiple barrels along the length of each barrel of the box culvert, and along the length of the drainage channel. The depth of sediment listed for the drainage channel is for estimation purposes only, and the actual depth of the sediment shall be field verified by the Contractor prior to bidding the Project. The disposal of this material will not be measured for separate payment. Any work necessary to grade the ditches and/or restabilize any disturbed areas shall be absorbed under other items bid.

Any signs that are in conflict with construction of this project shall be removed and relocated by the Contractor as directed by the Engineer; the cost of which shall be absorbed in other items bid.

Removal of existing raised pavement markers shall be included in the prices for other items bid.

Incidental work such as removing vegetation, shaping and compacting shoulders, removing and resetting signs and/or mailboxes, removing excess asphalt material, project clean-up, and other items of incidental work necessary to complete the project will not be measured for separate payment and will be considered included in the prices of items bid.

Prior to the final inspection, bridges, islands, and areas with curb shall be swept/cleaned. Care

should be taken to prevent milled asphalt, asphalt debris, vegetative/granular debris, etc. from entering drainage structures or clogging other drainage ways. Disposal of material will not be measured for separate payments.

Following the overlaying operation the transverse joints in the pavement shall be sawed and sealed within seven (7) days. The details for sawing and sealing transverse joints for this section are in the Standard Specifications. The width of the sawing and sealing operation will be 14' on each side of centerline, unless otherwise directed by the Engineer, to prevent "sympathy cracking." It shall be the responsibility of the Contractor to locate and mark all existing joints that are to be sawed and sealed prior to the milling operation. The Contractor shall notify the Department when this is to take place so that they can oversee the work and determine the width that each joint will be sawed and sealed.

Box culverts and drainage channels listed in the attached table shall have the existing debris and sediment removed by the Contractor, and shall be paid for using pay item 202-B: Removal of Debris and Sand From Box Culvert, 10-Foot and Greater Width. The applicable pay item shall be measured along the length of the box culvert or in the case of multiple barrels along the length of each barrel of the box culvert, and along the length of the drainage channel. The depth of sediment listed for each box culvert/drainage channel is for estimation purposes only, and the actual depth of the sediment shall be field verified by the Contractor prior to bidding the Project. The disposal of this material will not be measured for separate payment.

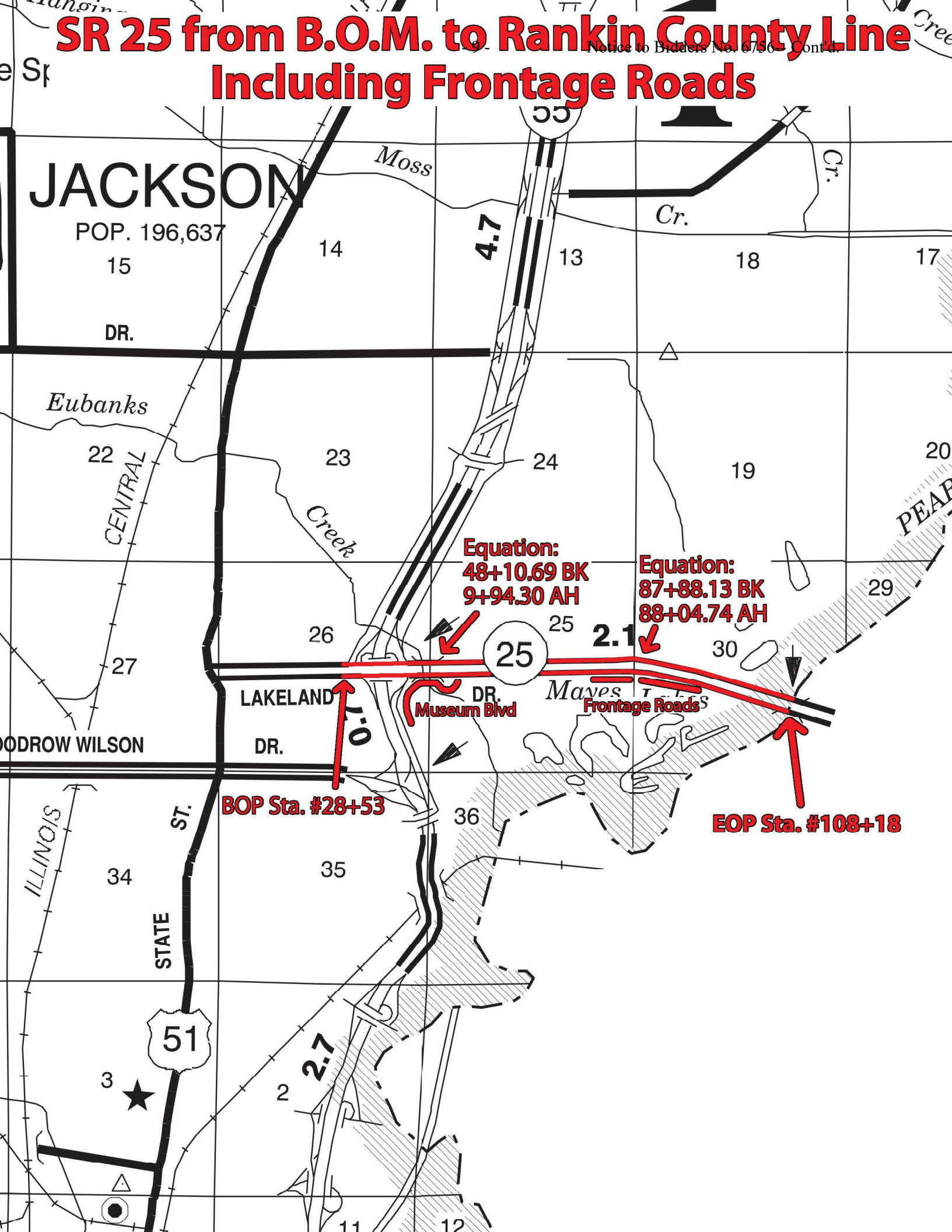
Riprap shall be required at the locations listed in the table, some of which may need to be hand placed due to field conditions. The furnished rock shall come from a pre-approved source and be visually approved prior to placement. Tree removal or clearing for equipment access will be at the Engineer's discretion and shall be absorbed in other items bid.

As per Subsection 512.03.2 (d), a colloidal mixer shall be required for the Type 5 pressure grout mixture. No exceptions shall be allowed for this requirement.

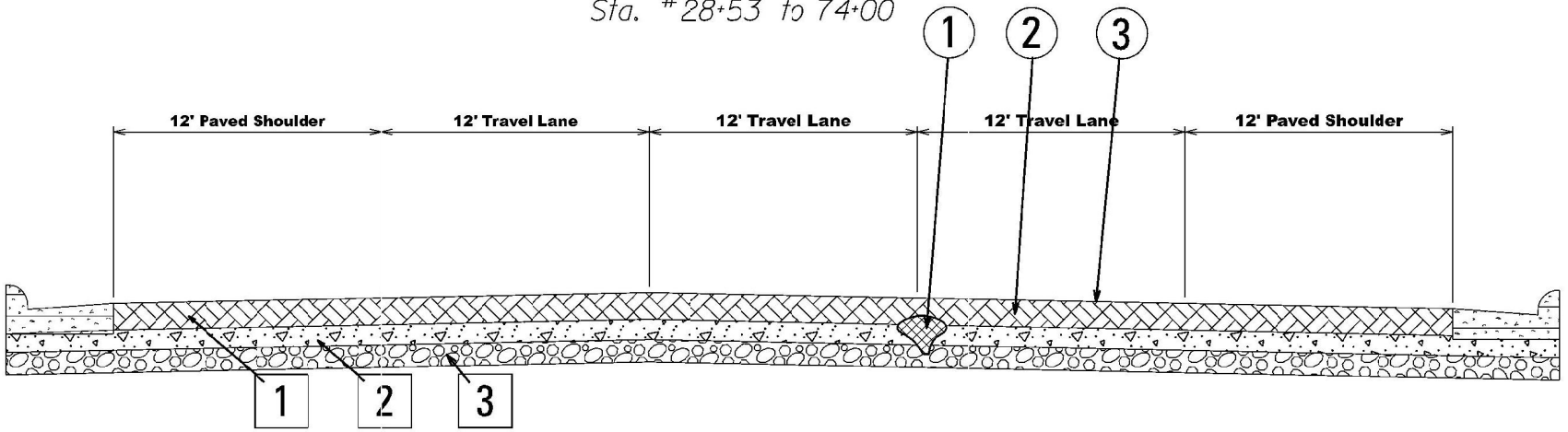
Removal of sign, post, and footing and backfilling will be paid using the removal of sign pay item.

Ramp closures will not be allowed without written approval from the Engineer.

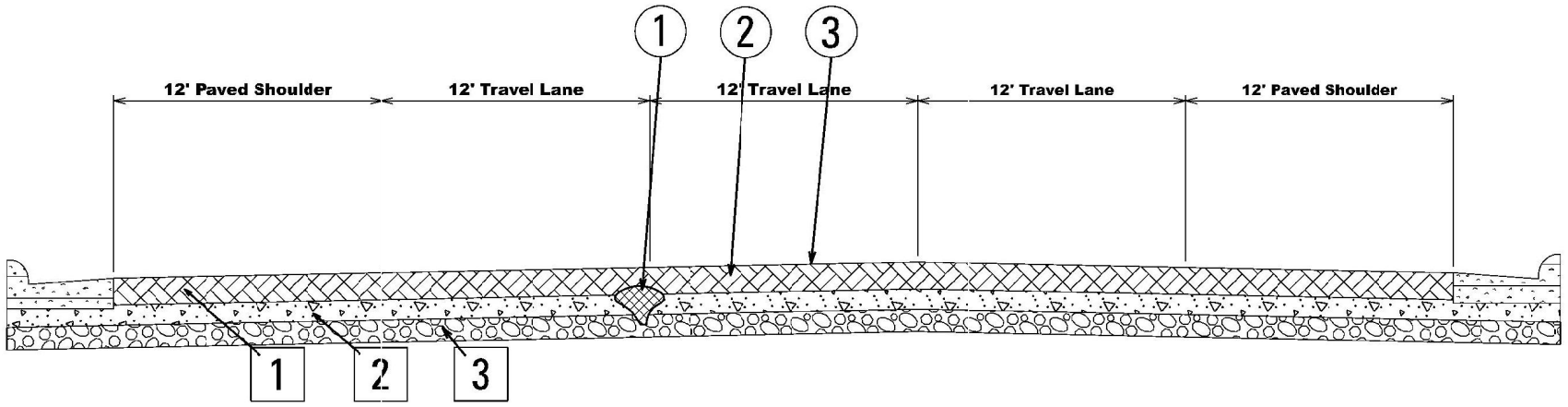
SR 25 from B.O.M. to Rankin County Line Including Frontage Roads



HWY 25 NORTHBOUND
Sta. #28+53 to 74+00



HWY 25 SOUTHBOUND
Sta. #28+53 to 40+43 AND
Sta. #59+83 to 74+00



EXISTING

1. 4½" HMA
2. 8" CRCP
3. 6" Cement Treated Base

PROPOSED

1. Repair any failed areas full depth per concrete punchout typical.
2. Mill 1½" and variable of existing asphalt pavement
3. Overlay with 1½ 9.5mm Mix, MT

MDOT
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

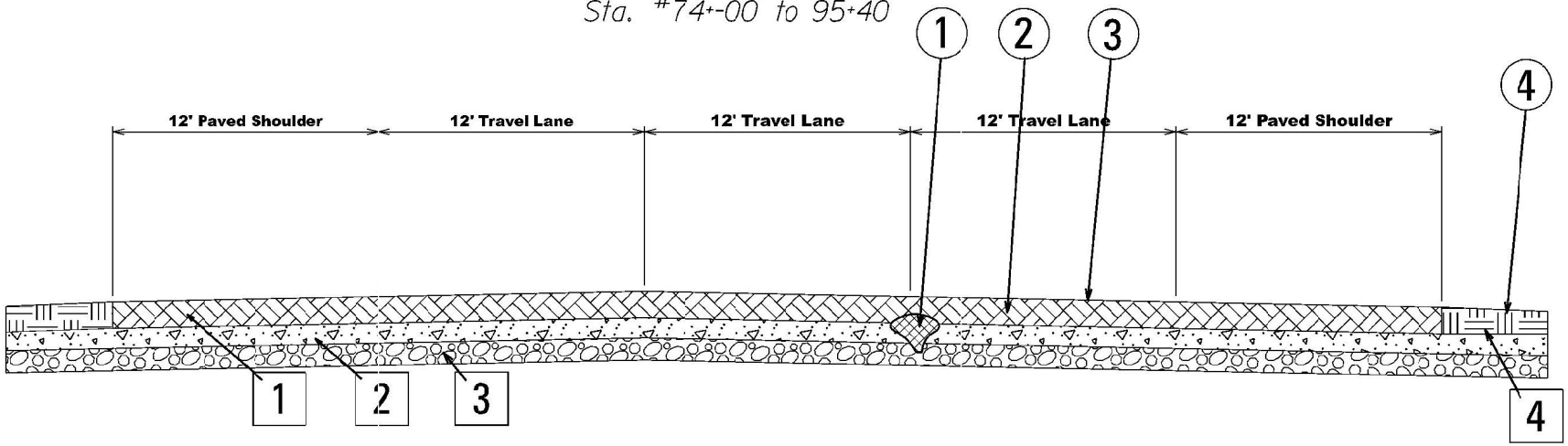
DESIGNED BY: Matthew Little
DETAILED BY: Matthew Little
CHECKED BY: Trent Holbrook
DATE: July 31, 2024

FMS CON: 109228/301000
PROJECT NO.: NHPP-0056-01(114)
COUNTY: HINDS

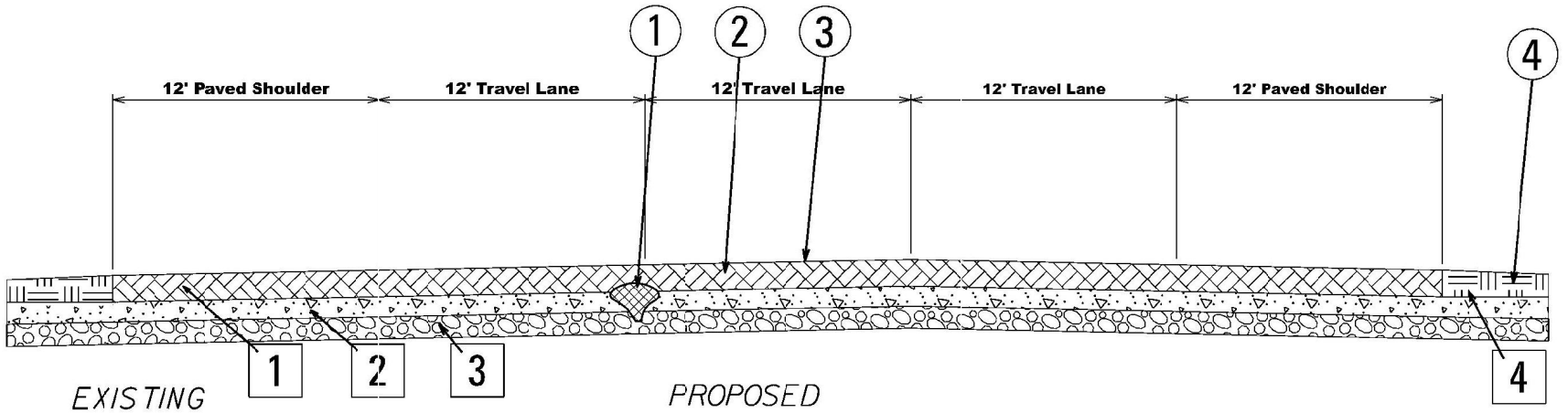
MAINLINE THREE LANE SECTIONS
HWY 25 NORTHBOUND AND SOUTHBOUND
Sta. #28+53 to 74+00

SHEET ID
15
SHEET NO.
1

HWY 25 NORTHBOUND
Sta. #74+00 to 95+40



HWY 25 SOUTHBOUND
Sta. #74+00 to 95+40



EXISTING

1. 4½" HMA
2. 8" CRCP
3. 6" Cement Treated Base
4. 7" Granular Material

PROPOSED

1. Repair any failed areas full depth per concrete punchout typical.
2. Mill 1½" and variable of existing asphalt pavement
3. Overlay with 1½ 9.5mm Mix, MT
4. Granular Material, Class 5, Group "C" to bring shoulders to grade.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

MDOT
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

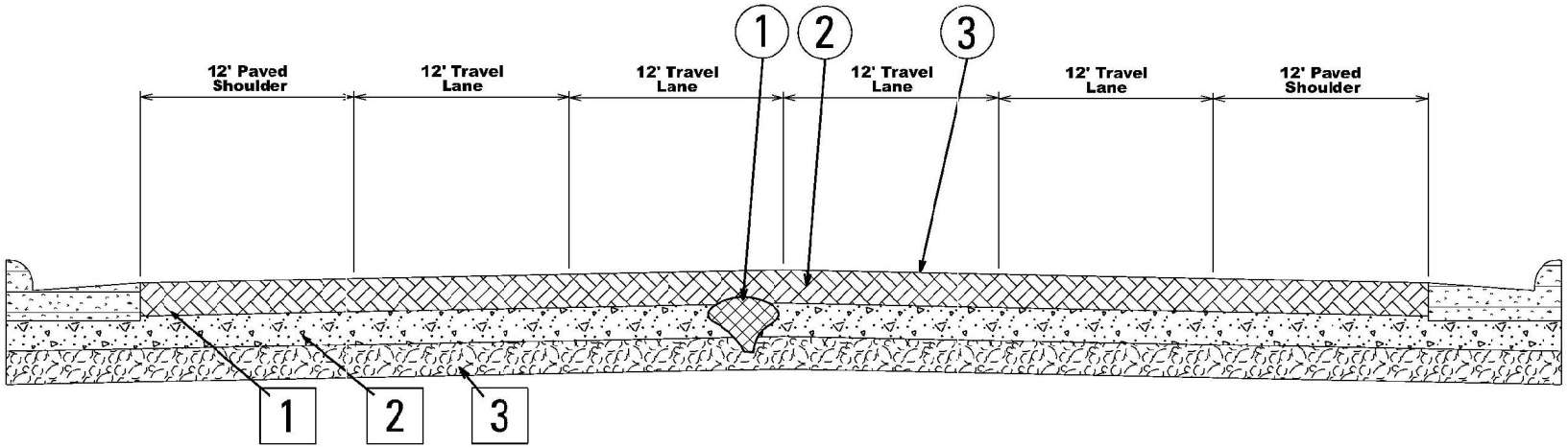
DESIGNED BY: Matthew Little
DETAILED BY: Matthew Little
CHECKED BY: Trent Holbrook
DATE: July 31, 2024

FMS CON: 109228/301000
PROJECT NO.: NHPP-0056-01(114)
COUNTY: HINDS

MAINLINE THREE LANE SECTIONS
NORTHBOUND AND SOUTHBOUND
Sta. #74+00 to 95+40

SHEET ID
SHEET NO. 2

HWY 25 SOUTHBOUND
Sta. #40+43 to 59+83



EXISTING

1 **4½" HMA**

2 **8" CRCP**

3 **6" Cement Treated Base**

PROPOSED

① **Repair any failed areas full depth per concrete punchout typical.**

② **Mill 1½" and variable of existing asphalt pavement**

③ **Overlay with 1½ 9.5mm Mix, MT**

MDOT
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

DESIGNED BY: Matthew Little
DETAILED BY: Matthew Little
CHECKED BY: Trent Holbrook
DATE: July 31, 2024

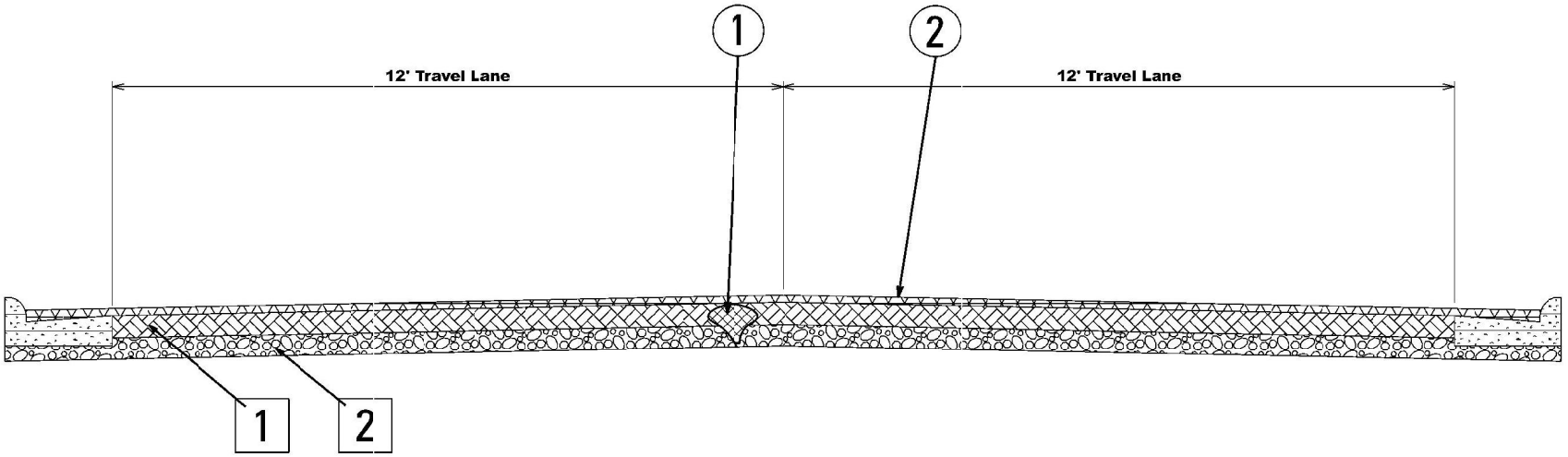
FMS CON: 109228/301000
PROJECT NO.: NHPP-0056-02(114)
COUNTY: HINDS

MAINLINE FOUR LANE SECTION
HWY 25 SOUTHBOUND
Sta. #40+43 to 59+83

SHEET ID
SHEET NO. 3

Notice to Bidders -- Contract No. 110756

HWY 25 FRONTAGE ROADS



EXISTING

1 **6" HMA**

2 **6" Untreated Clay Gravel Granular Material**

PROPOSED

① **Repair any failed areas full depth with 12.5mm Mix, MT, Leveling**

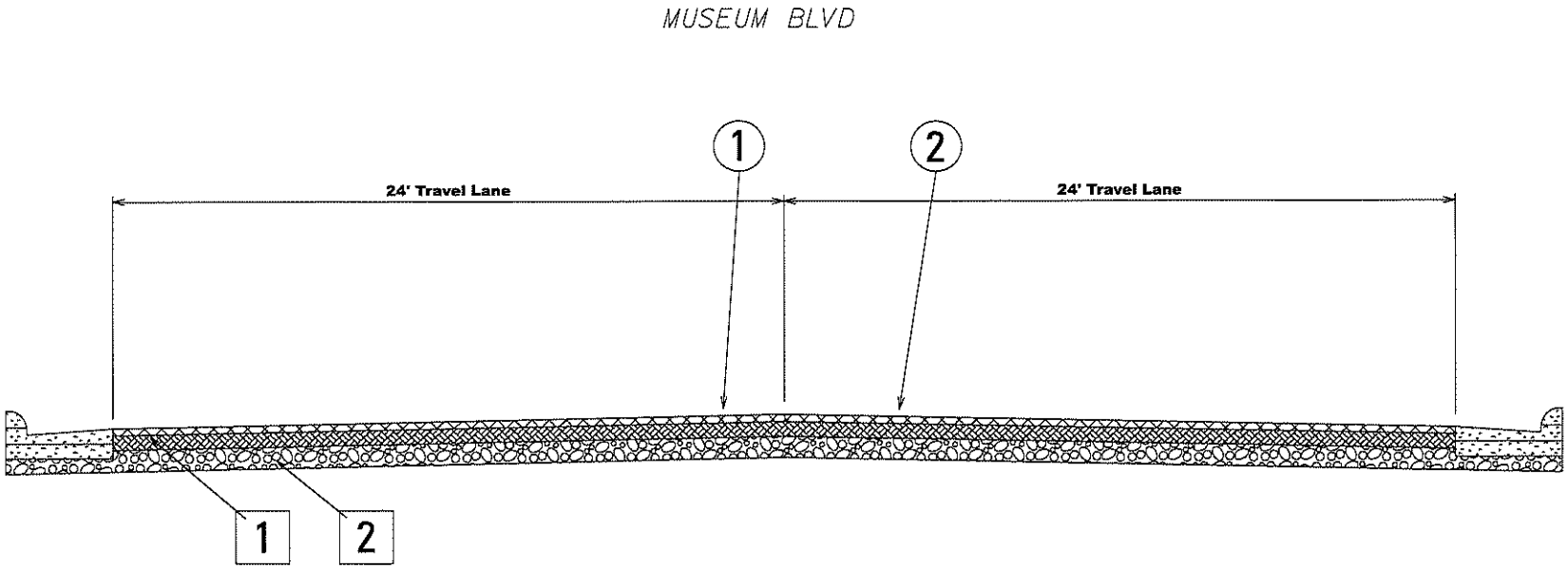
② **Overlay with 1½ 9.5mm Mix, MT**

DESIGNED BY: Matthew Little
DETAILED BY: Matthew Little
CHECKED BY: Trent Holbrook
DATE: January 22, 2025

FMS CON: 109228/302000
PROJECT NO.: SP-0056-01(27) - 13
COUNTY: HINDS

Notice to Bidders No. 6756 -- Contract

SHEET ID
SHEET NO. 4



EXISTING

1 **6" HMA**

2 **6" Untreated Clay Gravel Granular Material**

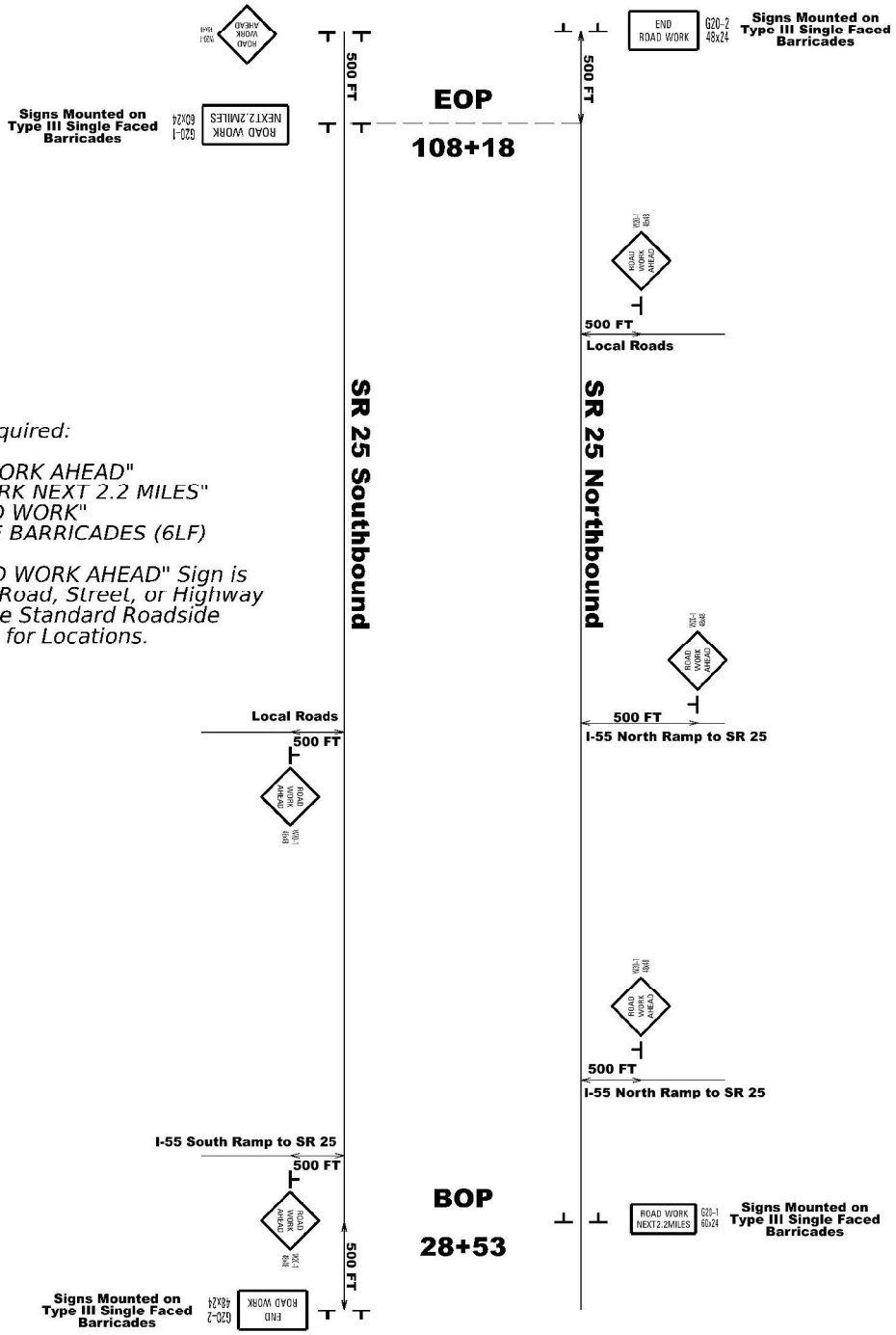
PROPOSED

① **Mill 1½" and variable of existing asphalt pavement**

② **Overlay with 1½, 9.5mm Mix, MT**

DESIGNED BY: Matthew Little
DETAILED BY: Matthew Little
CHECKED BY: Zachary Horton
DATE: April 17, 2025
FMS CON: 109228/302000
PROJECT NO.: SP-0056-01(123)
COUNTY: HINDS
MUSEUM BLVD
SHEET ID TS
SHEET NO. 5

Construction Signing Detail
SR 25 Hinds County



ESTIMATED
Traffic Control Signs Required:

18 - W20-1 "ROAD WORK AHEAD"
2 - G20-1 "ROAD WORK NEXT 2.2 MILES"
2 - G20-2 "END ROAD WORK"
8 - TYPE III SGL. FACE BARRICADES (6LF)

NOTES: One (1) W20-1 "ROAD WORK AHEAD" Sign is Required at each Local Road, Street, or Highway Entering the Project. See Standard Roadside Construction Sign Table for Locations.

SHEET NO. 1	SHEET ID SD1	Construction Signing Detail SR 25 Hinds County	FMS CON: 109228/301000	DESIGNED BY: Matthew Little
			PROJECT NO.: NHPP-0056-07(114)	DETAILED BY: Matthew Little
			COUNTY: Hinds	CHECKED BY: Trent Holbrook
				DATE: July 23, 2024

MDOT MISSISSIPPI DEPARTMENT OF TRANSPORTATION

Removal of Debris and Sand From Box Culvert, 10-foot and Greater Width

202-B096

Location	Sta.	Width	Estimated Depth	300lb Riprap	Length	Total Length
Triple Box	10+50	51	0.5 ft	300 TON	249	747
					TOTAL	**747**

FAILED AREAS QUANTITIES 109228/302000								
Beginning Station	**Ending Station**	**Direction**	**Lane**	**Length (FT)**	**Width (FT)**	**202-B009 Removal of Asphalt, Failed Areas (SY)**	**403-B002 12.5-mm, MT, Asphalt Pavement, Leveling, Ton**	**503-C010 Saw Cut, Full Depth**
71+09	71+55	FRONTAGE	RT/LN	46	14	71.6	17.7	120
70+89	71+09	FRONTAGE	RT/LN	20	14	31.1	7.7	68.0
70+28	70+48	FRONTAGE	RT/LN	20	14	31.1	7.7	68
				Totals:		**134**	**33**	**256**
Notes: Locations and Measurements are Approximate and may Vary With Field Conditions.								
All failed area locations facing upstation direction.								

GUARD RAIL QUANTITIES 109228/301000

GUARDRAIL				TERMINAL END		CABLE	BRIDGE END		DELINEATORS					
STATION	LOCATION (LT/RT)	W-BEAM (LF)	Impact Attenuator, 50 MPH (EA.)	FLARED END SECTION	NON-FLARED END SECTION	ANCHOR TYPE 1	TYPE "I"	Type "H	WHITE	YELLOW	TYPE 3 OBJECT MARKERS	GUARDRAIL REMOVAL	BRIDGE #	REMARKS
38+28	RRL	325		1				1	3	7	1	387.5	0.0A	Guardrail to be replaced in kind. Modifications necessary.
38+40	LLL	75		1				1	7		1	137.5	0.0A	Some posts embedded in concrete. Modifications necessary.
80+80	RRL	150		1			1		7		1	212.5	1.6B	
81+75			1										1.6A/B	
87+03	LLL	150		1			1		7		1	212.5	1.6A	
88+65			1										1.6A/B	
93+30	RRL	150		1			1		7		1	212.5	1.7B	
93+68			1										1.7A/B	
108+18	LLL	150		1			1		7		1	212.5	1.7A	
108+18			1										1.7A/B	
TOTAL =		**1000.00**	**4.00**	**6.00**	**0.00**	**0.00**	**4.00**	**2.00**	**38.00**	**7.00**	**6.00**	**1375.00**		
		LF.	**EA.**	**EA.**	**EA.**	**EA.**	**EA.**		**EA.**	**EA.**	**EA.**	**LF.**		

* REMOVAL OF ALL GUARDRAIL (BRIDGE END SECTIONS, W-BEAM, TYPE-I CABLE ANCHORAGE, TERMINAL END SECTIONS, ETC.) WILL BE PAID UNDER PAY ITEM 202-B REMOVAL OF GUARD RAIL.

* REMOVAL OF GUARDRAIL DELINEATORS ARE CONSIDERED INCIDENTAL TO THE REMOVAL OF GUARDRAIL AND WILL NOT BE MEASURED AS A SEPARATE PAY ITEM.

* ALL GUARDRAIL, POSTS, BLOCKOUTS, CONCRETE ANCHORS, HARDWARE, ETC. WILL BE THE PROPERTY OF THE CONTRACTOR.

* TOTAL GUARDRAIL LENGTH IS BASED ON A TERMINAL END SECTION 37.5' LONG. IF A TERMINAL END SECTION OF A DIFFERENT LENGTH IS USED, THE LENGTH OF THE W-BEAM MAY HAVE TO BE ADJUSTED.

REMOVAL OF OBJECT MARKERS WILL NOT BE MEASURED AS A SEPARATE PAY ITEM AND SHALL BE ABSORBED IN OTHER ITEMS

PUNCHOUT CRCP QUANTITIES 109228/301000

SOUTHBOUND NORTHBOUND

Station	Station	Location	Width (ft)	Length (ft)	503-B001 Saw Cut Longitudinal Joint, LF	503-C004 Saw Cut, 3 - inch	503-C010 Saw Cut, Full Depth, LF	202-B069 Removal of Concrete Pavement w/ Variable Depth Overlay	202-B045 Removal of Cement Treated Base, All Depths, SY	503-D001 Concrete for Base Repair, CY	503-E002 Tie Bars, NO.5 Deformed, Drilled And Epoxied or Grouted, EA	503-A001 8" and Variable Continuously Reinforced Concrete Pavement, Broom Finish, SY	907-403-B002 12.5-mm, MT, Asphalt Pavement, Leveling, Ton
45+92	45+98	Left	12	6	6	24	30	8	4	0.791	4	8	2
45+92	45+98	Center	12	6	6	24	30	8	4	0.791	4	8	1
28+70	28+76	Left CL	12	6	6	24	30	8	4	0.791	4	8	1
28+86	28+92	Left	12	6	6	24	30	8	4	0.791	4	8	1
34+60	34+66	Left	12	6	6	24	30	8	4	0.791	4	8	1
34+60	34+66	Center	12	6	6	24	30	8	4	0.791	4	8	1
45+97	46+03	Left CL	12	6	6	24	30	8	4	0.791	4	8	1
45+97	46+03	Right CL	12	6	6	24	30	8	4	0.791	4	8	1
45+97	46+03	Right	6	6	6	12	18	4	2	0.395	4	4	1
50+30	50+36	Left CL	12	6	6	24	30	8	4	0.791	4	8	1
50+24	50+30	Left	12	6	6	24	30	8	4	0.791	4	8	1
75+50	75+73	Center	12	23	23	24	47	31	15.5	5.830	13	31	5
75+50	75+73	Right	12	23	23	24	47	31	15.5	5.830	13	31	7
TOTAL					**112**	**300**	**412**	**146**	**73**	**19.96**	**70**	**146**	**24**

Quantities were rounded on estimate, quantities to be used as directed by the Engineer. CRCP repairs were estimated using the PR-1B Typical CRC Pavement Repair Standard. If the Contractor elects to use PR-1A Optional Welding Method, then the pay item quantities will be adjusted accordingly. The load transfer device neccessary for jointed concrete pavement repair is to be included in the cost of the 503-A001 pay item.

FAILED AREAS AND PRESSURE GROUT QUANTITIES 109228/301000										
Beginning Station	Ending Station	Direction	Lane	Length (FT)	Width (FT)	202-B009 Removal of Asphalt, Failed Areas (SY)	403-B002 12.5-mm, MT, Asphalt Pavement, Leveling, Ton	407-A001 ASPHALT FOR TACK COAT	512-A001 Holes (EACH)	512-B001 Cement Pressure Grout Slurry, Type 5 (POUNDS)
74+30	78+30	SB	RT/LN	400	4	177.8	44.0	17.8	81	400
74+30	77+30	SB	CT/LN	300	4	133.3	33.0	13.3	61	300
				Totals:		**311**	**77**	**31**	**142**	**700**
Notes: Locations and Measurements are Approximate and may Vary With Field Conditions.										
All failed areas and pressure grouting locations facing upstation direction.										

Curb and Gutter

Location	Begin	End	609-D007 Combination Concrete Curb and Gutter Type 3 Modified	202-B008 Removal of Curb & Gutter, All Types
LLL	39+90	40+08	18	18
LRL	45+84	46+12	28	28
TOTAL =			**46**	**46**
			LF.	**LF.**

Removal of Debris and Sand From Box Culvert, 10-foot and Greater Width						
202-B096						
Location	**Sta.**	**Width**	**Estimated Depth**	**300lb Riprap**	**Length**	**Total Length**
Triple Box	10+50	51	0.5 ft	**Location**	249	747
					TOTAL	**747**

Removal of Debris and Sand From Pipe, All Sizes				
202-B114				
Location	**Sta.**	**Estimated Depth**	***Length	
60" RCP LT	28+45	0.5 ft	20	
			TOTAL	**20**

*** Note: Length is to clean out upstream ditch at pipe

Sawing and Sealing Transverse Joints in Asphalt Pavement

NORTH BOUND		Width
15+36	LT Turn	12
15+87	LT Turn	13
18+88	LT Turn	12
57+64	LT Turn	24
60+15	LT Turn	24
61+11	LT Turn	24
62+40	LT Turn	24
79+40	FULL WIDTH	56
79+78	FULL WIDTH	56
80+16	FULL WIDTH	46
81+97	FULL WIDTH	60
87+23	FULL WIDTH	60
94+35	FULL WIDTH	60
		471

CROSSOVERS	Width
16+28	29
26+92	29
30+68	29
43+54	29
79+78	34
80+16	34
	184

SOUTH BOUND		Width
16+83	LT/LN	15
31+54	LT Turn	14
36+94	FULL WIDTH	56
37+56	FULL WIDTH	56
37+83	FULL WIDTH	56
44+20	LT Turn	12
79+40	FULL WIDTH	56
79+78	FULL WIDTH	46
80+16	FULL WIDTH	46
82+82	FULL WIDTH	60
88+24	FULL WIDTH	60
94+35	FULL WIDTH	60
		537

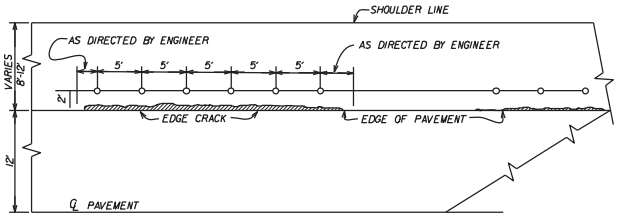
Total =	1192

619-D2001 Standard Roadside Construction Signs, 10 Square Feet or More					
Station	Location	Description	Quantity	Unit	Remarks
31+00	LLL	W20-1 (Road Work Ahead)	16	SF	I-55S -> SR 25
43+50	LLL	W20-1 (Road Work Ahead)	16	SF	I-55S -> SR 25
20+00	LLL	W20-1 (Road Work Ahead)	16	SF	I-55S -> SR 25
20+00	LLL	W20-1 (Road Work Ahead)	16	SF	I-55S -> SR 25
27+00	LLL	W20-1 (Road Work Ahead)	16	SF	Cool Papa Bell
35+00	LLL	W20-1 (Road Work Ahead)	16	SF	Lakeland Ln
35+00	RRL	W20-1 (Road Work Ahead)	16	SF	Lakeland Terrace
43+50	RRL	W20-1 (Road Work Ahead)	16	SF	Lakeward Dr
57+75	LLL	W20-1 (Road Work Ahead)	16	SF	I-55S -> SR 25
59+75	LLL	W20-1 (Road Work Ahead)	16	SF	I-55S -> SR 25
63+00	LLL	W20-1 (Road Work Ahead)	16	SF	I-55S -> SR 25
63+00	LLL	W20-1 (Road Work Ahead)	16	SF	I-55S -> SR 25
74+00	LLL	W20-1 (Road Work Ahead)	16	SF	I-55S -> SR 25
90+50	LLL	W20-1 (Road Work Ahead)	16	SF	I-55S -> SR 25
90+50	LLL	W20-1 (Road Work Ahead)	16	SF	I-55S -> SR 25
94+00	LLL	W20-1 (Road Work Ahead)	16	SF	I-55S -> SR 25
28+53	LLL	W20-1 (Road Work Ahead)	16	SF	I-55S -> SR 25
108+18	LLL	W20-1 (Road Work Ahead)	16	SF	I-55S -> SR 25
113+18	LLL	W20-1 (Road Work Ahead)	16	SF	I-55S -> SR 25
28+53	LLL	W20-1 (Road Work Ahead)	16	SF	I-55S -> SR 25
108+18	LLL	W20-1 (Road Work Ahead)	16	SF	I-55S -> SR 25
113+18	LLL	W20-1 (Road Work Ahead)	16	SF	I-55S -> SR 25
TOTAL			**328**	**SF**	

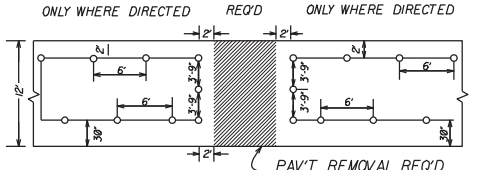
619-D1001 Standard Roadside Construction Signs, Less than 10 Square Feet					
Station	Location	Description	Quantity	Unit	Remarks
23+53	LRL & RRL	G20-2 (End Road Work)	16	SF	500' North of EOP
113+18	LLL & RLL	G20-2 (End Road Work)	16	SF	500' South of BOP
TOTAL			**32**	**SF**	

619-G4005 Barricades. Type III, Single Faced					
Station	Location	Description	Quantity	Unit	Remarks
28+53	LRL, RRL	Mounted on G20-1	12	LF	BOP
108+18	LLL, RLL	Mounted on G20-1	12	LF	EOP
23+53	LRL, RRL	Mounted on G20-2	12	LF	BOP
113+18	LLL, RLL	Mounted on G20-2	12	LF	EOP
TOTAL			**48**	**LF**	

STATE	PROJECT NO.
MISS.	



TYPICAL PRESSURE GROUTING REQUIREMENTS
FOR EDGE CRACKS



TYPICAL PRESSURE GROUTING REQUIREMENTS

NOTES:

1. DO NOT LOCATE HOLES WITHIN 18" OF JOINT, BREAK, OR PAVEMENT EDGE. ADDITIONAL HOLES MAY BE REQUIRED IF THE SLAB IS BROKEN. KNOWN CAVITY LOCATIONS SHOULD TAKE PRECEDENCE IN LOCATING HOLES.
2. THE ABOVE HOLE LOCATIONS FOR THE CONDITIONS CITED ARE TO BE PERFORMED IN THE INITIAL GROUTING OPERATION.
3. THE DIMENSIONS & LOCATIONS SHOWN ARE TYPICAL. THE EXACT DIMENSIONS & LOCATIONS MAY VARY DURING CONSTRUCTION AS DIRECTED BY THE ENGINEER.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

PRESSURE GROUTING DETAILS

PROJECT NO.:
COUNTY:
FILENAME:
DESIGN TEAM _____ CHECKED _____ DATE _____

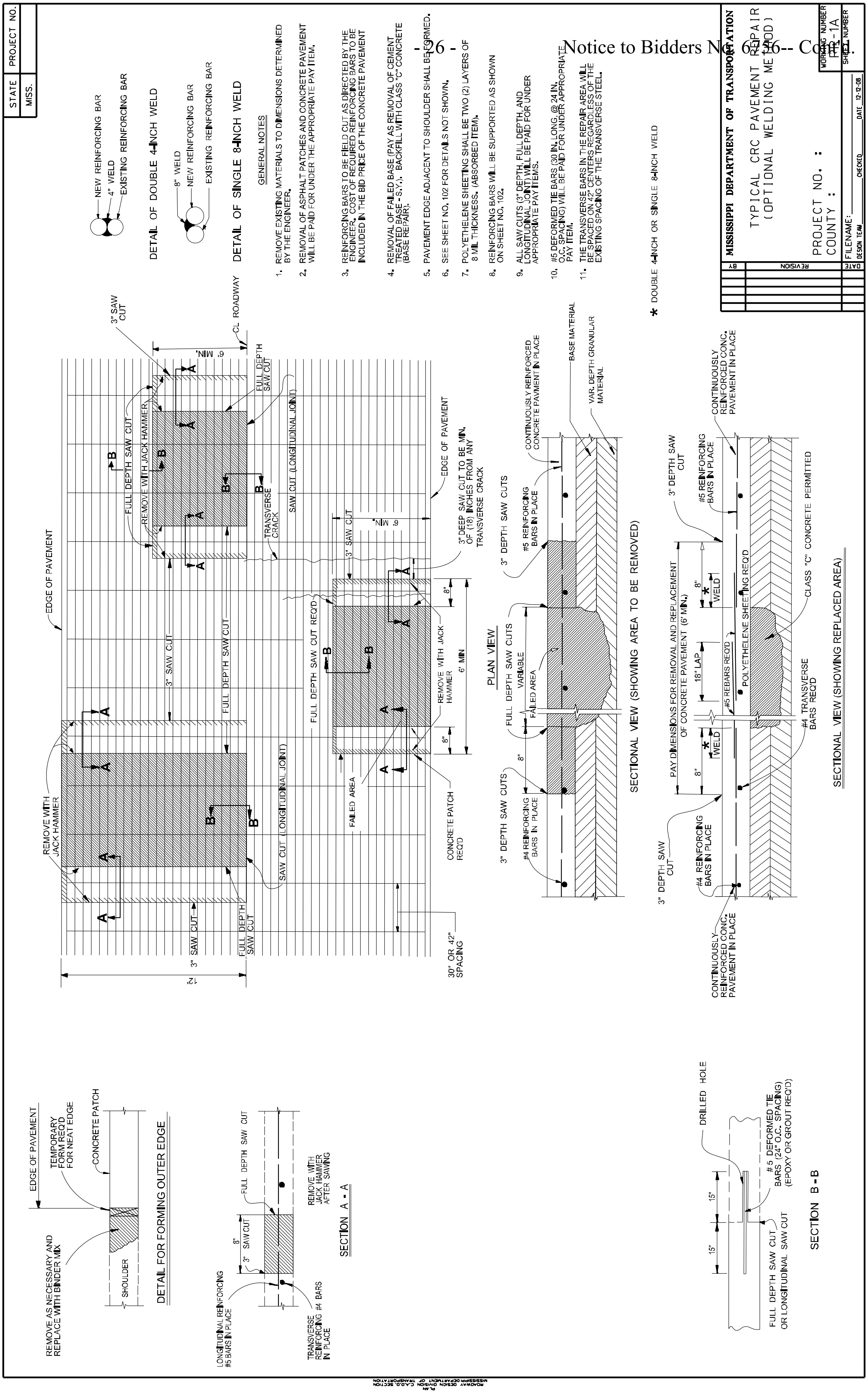
SHEET ADDED
REVISION

WORKING NUMBER PG-1

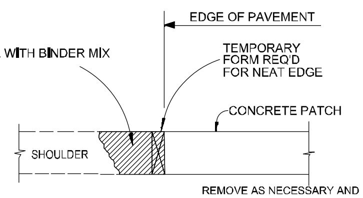
SHEET NUMBER 69

PLANS PRODUCT DEVELOPMENT DIVISION
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

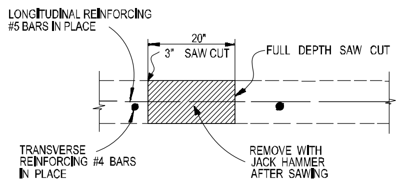
11/7/2018 4:54:17 PM PRESSURE GROUTING DETAIL.DGN


STATE MISS. PROJECT NO.
NEW REINFORCING BAR
4" WELD
EXISTING REINFORCING BAR
DETAIL OF DOUBLE 4-INCH WELD
8" WELD
NEW REINFORCING BAR
EXISTING REINFORCING BAR
DETAIL OF SINGLE 8-INCH WELD
GENERAL NOTES
1. REMOVE EXISTING MATERIALS TO DIMENSIONS DETERMINED BY THE ENGINEER.
2. REMOVAL OF ASPHALT PATCHES AND CONCRETE PAVEMENT WILL BE PAID FOR UNDER THE APPROPRIATE PAY ITEM.
3. REINFORCING BARS TO BE FIELD CUT AS DIRECTED BY THE ENGINEER. COST OF REQUIRED REINFORCING BARS TO BE INCLUDED IN THE BID PRICE OF THE CONCRETE PAVEMENT
4. REMOVAL OF FAILED BASE (PAY AS REMOVAL OF CEMENT TREATED BASE - S.Y.). BACKFILL WITH CLASS "C" CONCRETE (BASE REPAIR).
5. PAVEMENT EDGE ADJACENT TO SHOULDER SHALL BE FORMED.
6. SEE SHEET NO. 102 FOR DETAILS NOT SHOWN.
7. POLYETHELENE SHEETING SHALL BE TWO (2) LAYERS OF 8 MIL THICKNESS. (ABSORBED ITEM).
8. REINFORCING BARS WILL BE SUPPORTED AS SHOWN ON SHEET NO. 102.
9. ALL SAW CUTS (3" DEPTH, FULL DEPTH, AND LONGITUDINAL JOINT) WILL BE PAID FOR UNDER APPROPRIATE PAY ITEMS.
10. #5 DEFORMED TIE BARS (30 IN. LONG, @ 24 IN. O.C. SPACING) WILL BE PAID FOR UNDER APPROPRIATE PAY ITEM.
11. THE TRANSVERSE BARS IN THE REPAIR AREA WILL BE SPACED ON 42" CENTERS REGARDLESS OF THE EXISTING SPACING OF THE TRANSVERSE STEEL.
* DOUBLE 4-INCH OR SINGLE 8-INCH WELD
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
TYPICAL CRC PAVEMENT REPAIR
(OPTIONAL WELDING METHOD)
PROJECT NO. :
COUNTY :
WORKING NUMBER
SHEET NUMBER
FILENAME:
DESIGN TEAM
CHECKED
DATE 12-12-08
DATE
REVISION
BY
EDGE OF PAVEMENT
REMOVE WITH JACK HAMMER
3" SAW CUT
FULL DEPTH SAW CUT
SAW CUT (LONGITUDINAL JOINT)
TRANSVERSE CRACK
6' MIN.
CL ROADWAY
12'
30" OR 42" SPACING
FAILED AREA
CONCRETE PATCH REQ'D
FULL DEPTH SAW CUT REQ'D
8"
6' MIN
3" DEEP SAW CUT TO BE MIN. OF (18) INCHES FROM ANY TRANSVERSE CRACK
PLAN VIEW
3" DEPTH SAW CUTS
#4 REINFORCING BARS IN PLACE
FULL DEPTH SAW CUTS
VARIABLE
#5 REINFORCING BARS IN PLACE
CONTINUOUSLY REINFORCED CONCRETE PAVMENT IN PLACE
BASE MATERIAL
VAR. DEPTH GRANULAR MATERIAL
SECTIONAL VIEW (SHOWING AREA TO BE REMOVED)
3" DEPTH SAW CUT
PAY DIMENSIONS FOR REMOVAL AND REPLACEMENT OF CONCRETE PAVEMENT (6' MIN.)
WELD
18" LAP
#5 REBARS REQ'D
POLYETHELENE SHEETING REQ'D
#4 TRANSVERSE BARS REQ'D
CLASS "C" CONCRETE PERMITTED
CONTINUOUSLY REINFORCED CONC. PAVEMENT IN PLACE
SECTIONAL VIEW (SHOWING REPLACED AREA)
REMOVE AS NECESSARY AND REPLACE WITH BINDER MIX
TEMPORARY FORM REQ'D FOR NEAT EDGE
CONCRETE PATCH
SHOULDER
DETAIL FOR FORMING OUTER EDGE
LONGITUDINAL REINFORCING #5 BARS IN PLACE
TRANSVERSE REINFORCING #4 BARS IN PLACE
REMOVE WITH JACK HAMMER AFTER SAWING
SECTION A - A
DRILLED HOLE
15"
5 DEFORMED TIE BARS (24" O.C. SPACING) (EPOXY OR GROUT REQ'D)
FULL DEPTH SAW CUT OR LONGITUDINAL SAW CUT
SECTION B - B

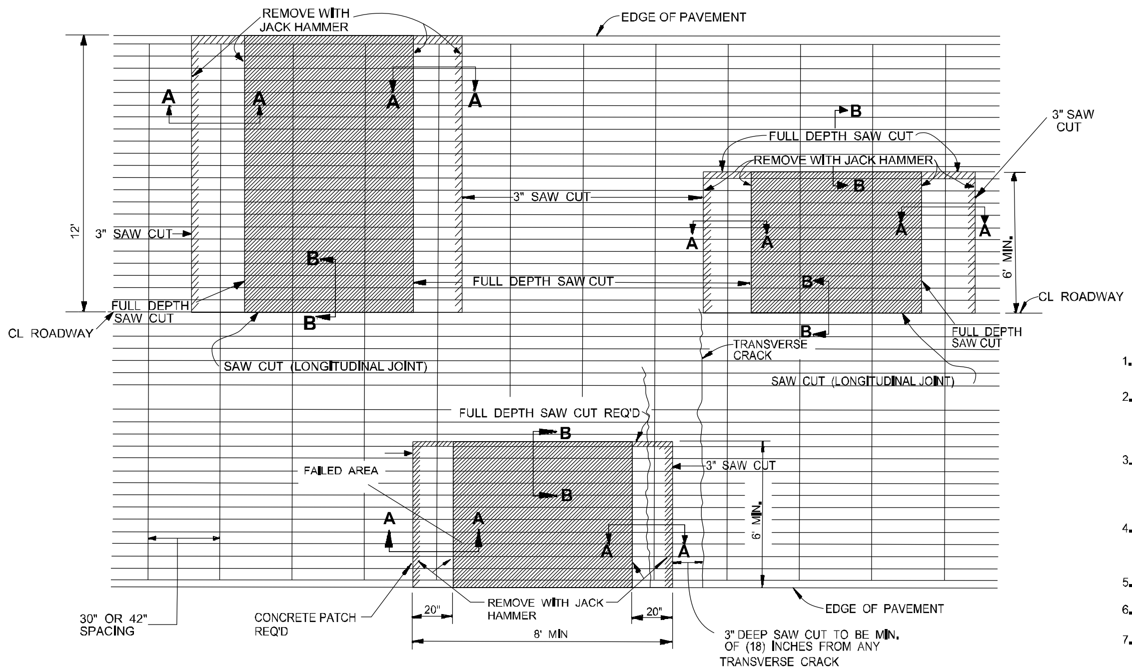
STATE	PROJECT NO.
MISS.	



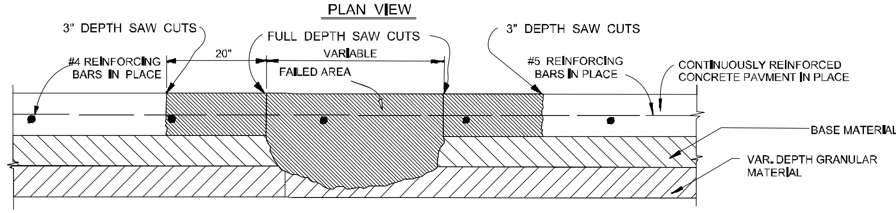
DETAIL FOR FORMING OUTER EDGE



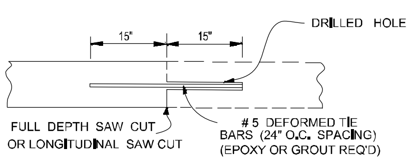
SECTION A - A



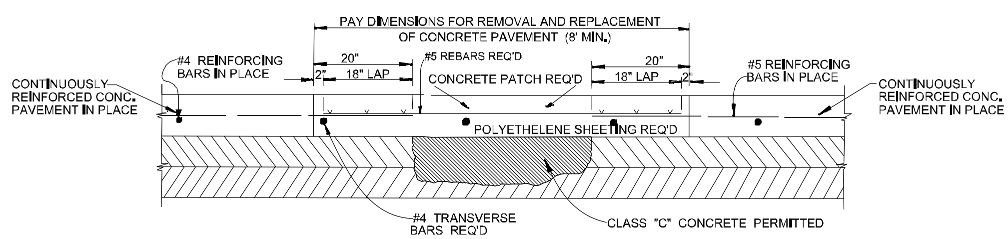
PLAN VIEW



SECTIONAL VIEW (SHOWING AREA TO BE REMOVED)



SECTION B - B



SECTIONAL VIEW (SHOWING REPLACED AREA)

GENERAL NOTES

1. REMOVE EXISTING MATERIALS TO DIMENSIONS DETERMINED BY THE ENGINEER.
2. REMOVAL OF ASPHALT PATCHES AND CONCRETE PAVEMENT WILL BE PAID FOR UNDER THE APPROPRIATE PAY ITEM.
3. REINFORCING BARS TO BE FIELD CUT AS DIRECTED BY THE ENGINEER. COST OF REQUIRED REINFORCING BARS TO BE INCLUDED IN THE BID PRICE OF THE CONCRETE PAVEMENT
4. REMOVAL OF FAILED BASE (PAY AS REMOVAL OF CEMENT TREATED BASE - S.Y.). BACKFILL WITH CLASS "C" CONCRETE (BASE REPAIR).
5. PAVEMENT EDGE ADJACENT TO SHOULDER SHALL BE FORMED.
6. SEE SHEET NO. 102 FOR DETAILS NOT SHOWN.
7. POLYETHELENE SHEETING SHALL BE TWO (2) LAYERS OF 8 MIL THICKNESS. (ABSORBED ITEM).
8. REINFORCING BARS WILL BE SUPPORTED AS SHOWN ON SHEET NO. 102.
9. ALL SAW CUTS (3" DEPTH, FULL DEPTH, AND LONGITUDINAL JOINT) WILL BE PAID FOR UNDER APPROPRIATE PAY ITEMS.
10. #5 DEFORMED TIE BARS (30 IN. LONG, @ 24 IN. O.C. SPACING) WILL BE PAID FOR UNDER APPROPRIATE PAY ITEM.
11. THE TRANSVERSE BARS IN THE REPAIR AREA WILL BE SPACED ON 42" CENTERS REGARDLESS OF THE EXISTING SPACING OF THE TRANSVERSE STEEL.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

TYPICAL CRC PAVEMENT REPAIR

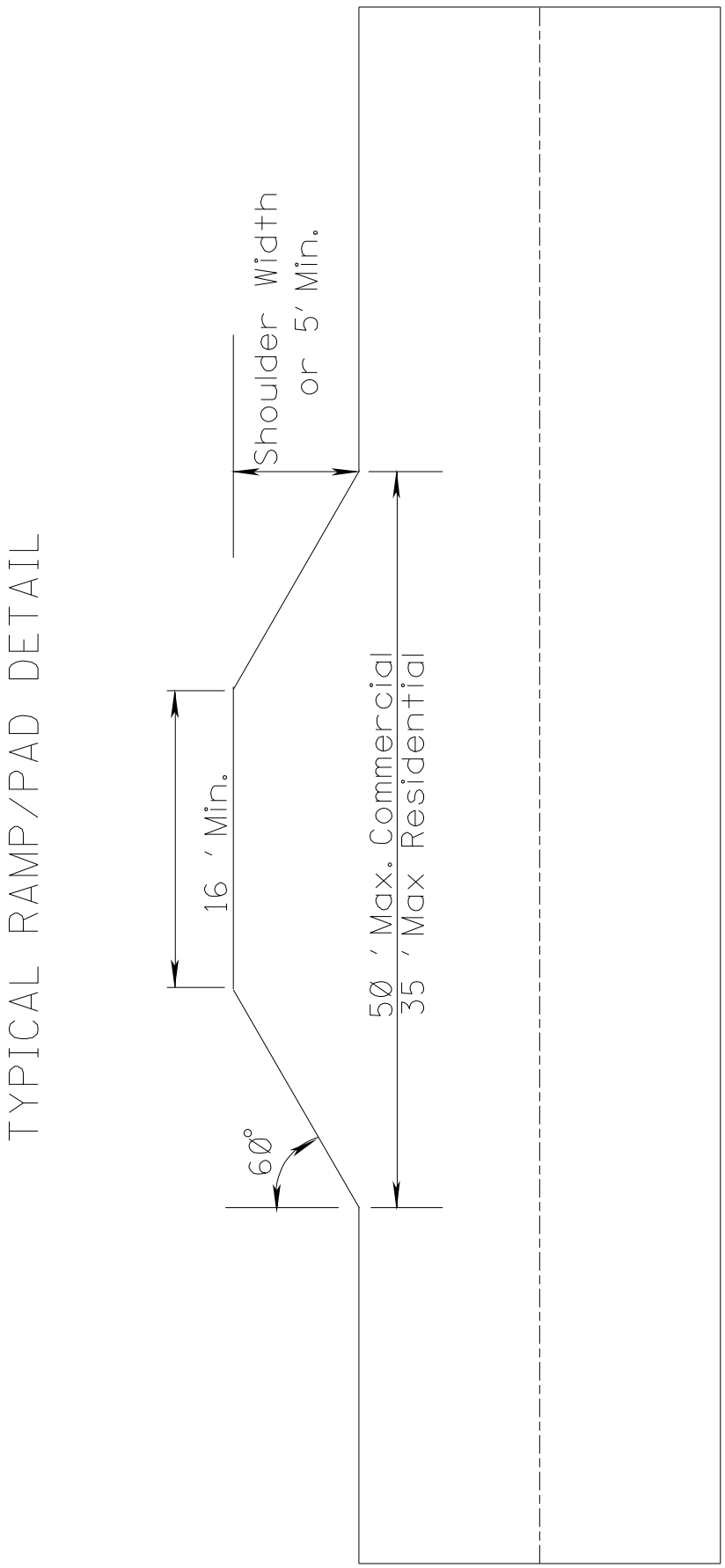
DATE | REVISION | BY

PROJECT NO.:
COUNTY:
FILENAME:
DESIGN TEAM ____ CHECKED ____ DATE ____

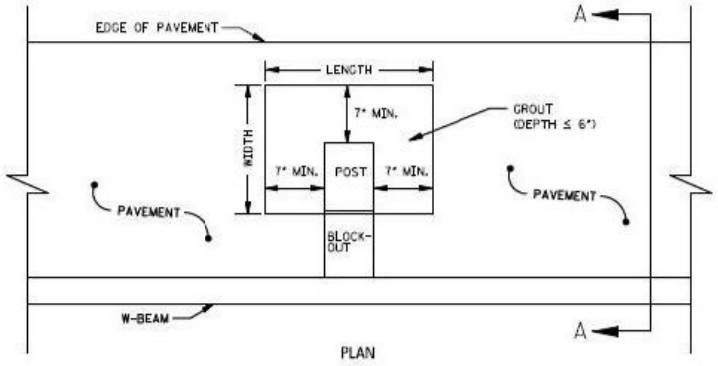
WORKING NUMBER: CR-1B
SHEET NUMBER

PLAN
ROADWAY DESIGN DIVISION
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

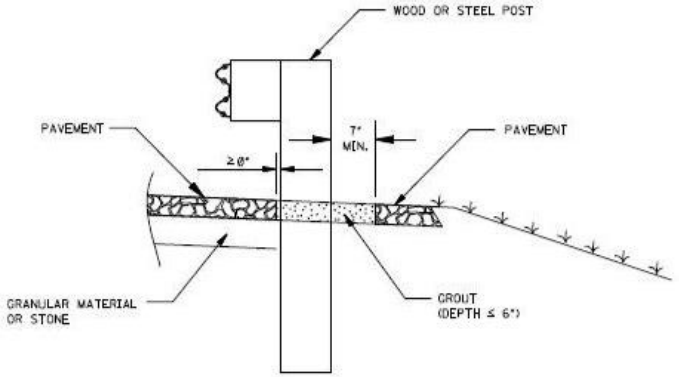
MMDDYY 00:00AMPM DGNFILENAME


TYPICAL RAMP/PAD DETAIL
Shoulder Width
or 5' Min.
16 ' Min.
50 ' Max. Commercial
35 ' Max Residential
60°

Guardrail Post Installation in Paved Areas

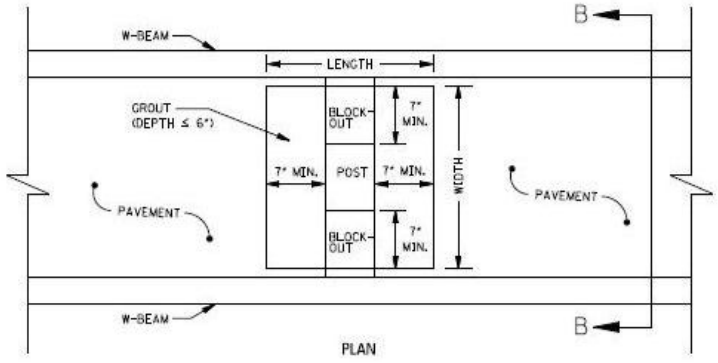


PAVEMENT LEAVE-OUT AREA FOR GUARDRAIL POSTS
SINGLE-FACED GUARDRAIL

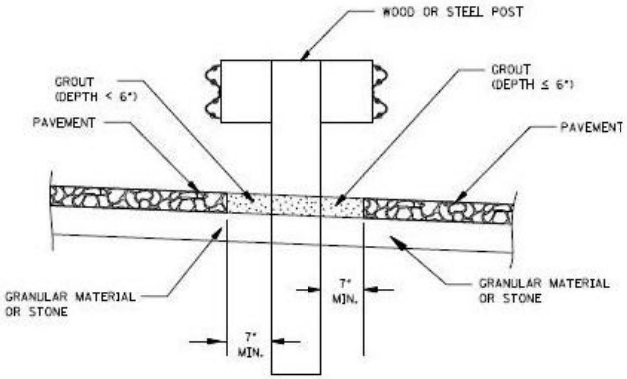


SECTION A–A

MIN. PAV'T LEAVE-OUT AREA				
POST	SINGLE-FACED		DOUBLE-FACED	
	LENGTH (IN.)	WIDTH (IN.)	LENGTH (IN.)	WIDTH (IN.)
6"x8" WOOD	20	15	20	22
8"x8" WOOD	22	15	22	22
10"x10" WOOD	24	17	24	24
W6x9 STEEL	18	13	18	20



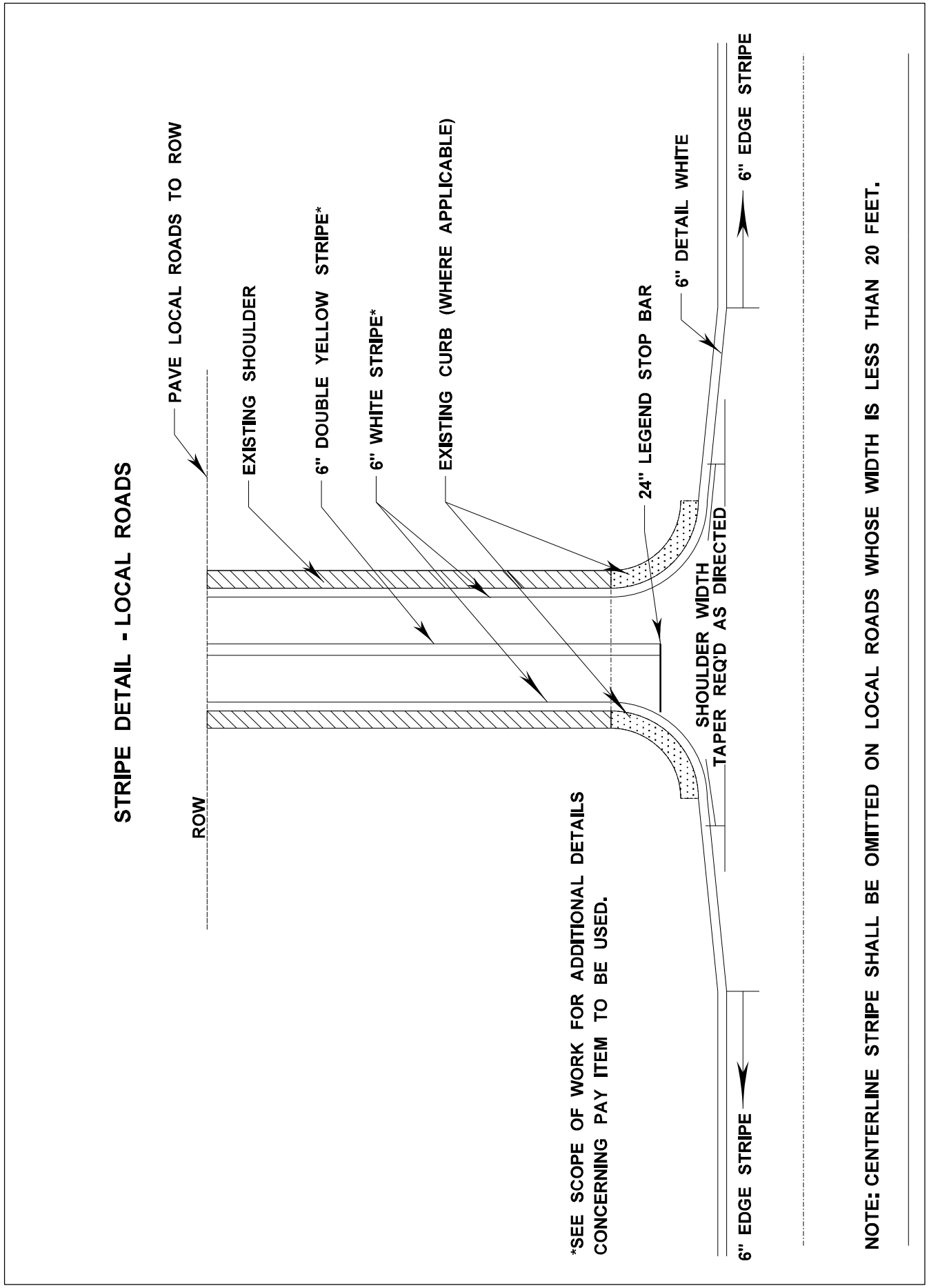
PAVEMENT LEAVE-OUT AREA FOR GUARDRAIL POSTS
DOUBLE-FACED GUARDRAIL



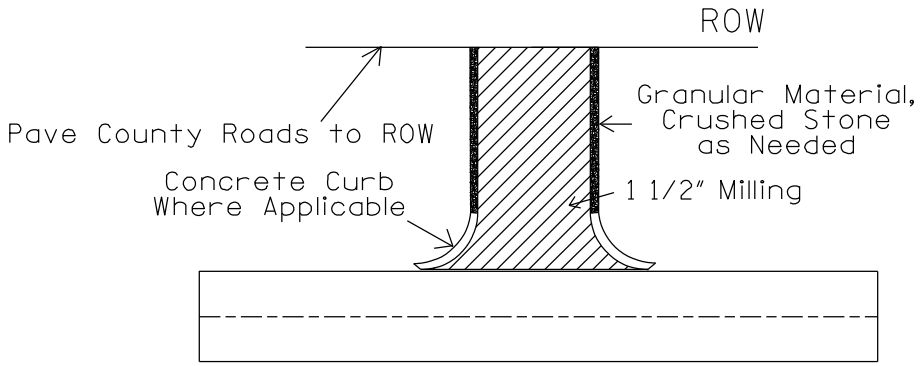
SECTION B–B

GENERAL NOTES

1. GUARDRAIL POSTS SHALL NOT BE COMPLETELY SURROUNDED BY PAVEMENT. THE AREA BEHIND AND LATERAL OF THE POST SHALL HAVE A MINIMUM 7" CLEARANCE FROM THE PAVEMENT. THIS AREA SHALL BE FILLED WITH A LOW STRENGTH GROUT WITH A MAXIMUM 28 DAY COMPRESSIVE STRENGTH OF 120 PSI.
2. GROUT SHALL BE INSTALLED AT A DEPTH EQUAL TO THE SURROUNDING PAVEMENT, UP TO A MAXIMUM OF 6". IF SURROUNDING PAVEMENT IS GREATER THAN 6", THE DIFFERNCE SHALL BE FILLED IN WITH SHOULDER GRANULAR MATERIAL.
3. COST OF GROUT SHALL BE ABSORBED IN THE COST OF OTHER ITEMS BID.
4. PAVEMENT LEAVE-OUT AREAS ARE REQUIRED FOR STEEL AND WOOD POSTS.
5. STANDARD EMBEDMENT DEPTHS STILL APPLY, MEASURED FROM THE TOP OF THE PROJECTED PAVEMENT SURFACE.


STRIPE DETAIL - LOCAL ROADS
PAVE LOCAL ROADS TO ROW
EXISTING SHOULDER
6" DOUBLE YELLOW STRIPE*
6" WHITE STRIPE*
EXISTING CURB (WHERE APPLICABLE)
24" LEGEND STOP BAR
6" DETAIL WHITE
6" EDGE STRIPE
ROW
SHOULDER WIDTH
TAPER REQ'D AS DIRECTED
6" EDGE STRIPE
*SEE SCOPE OF WORK FOR ADDITIONAL DETAILS
CONCERNING PAY ITEM TO BE USED.
NOTE: CENTERLINE STRIPE SHALL BE OMITTED ON LOCAL ROADS WHOSE WIDTH IS LESS THAN 20 FEET.

Milling and Paving Detail
County Roads
SR25 Hinds County



County Roads:

Cool Papa Bell
Museum Blvd
Cool Papa Bell
Lakeland Ln
Lakeland Terrace
Lakeward Dr
Southerland Dr.
River Hills Dr.
Ridgewood Rd.
Ridgewood Rd.
Crane Ridge Dr.
Lakeland Cir
River Ridge Dr.
Lakeland Cir.

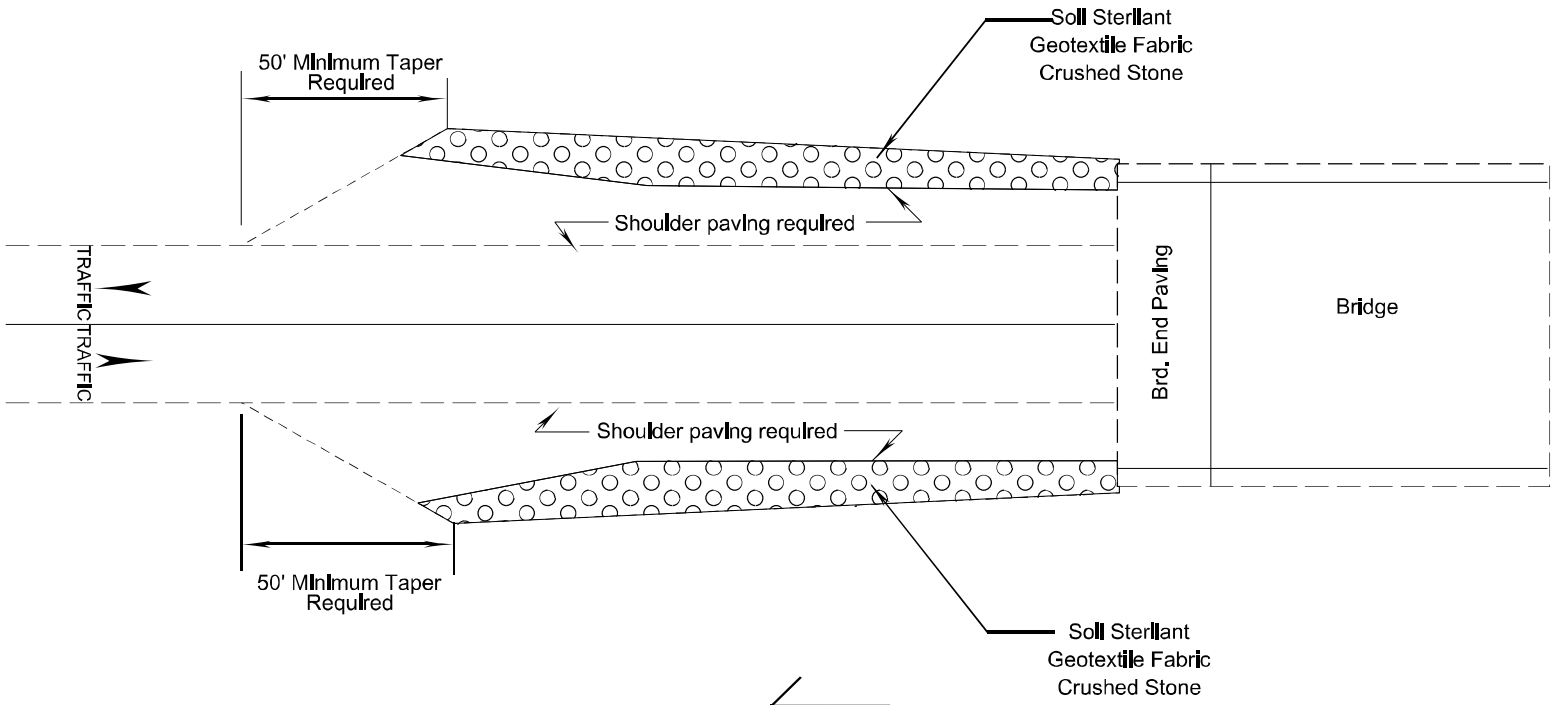
Notes:

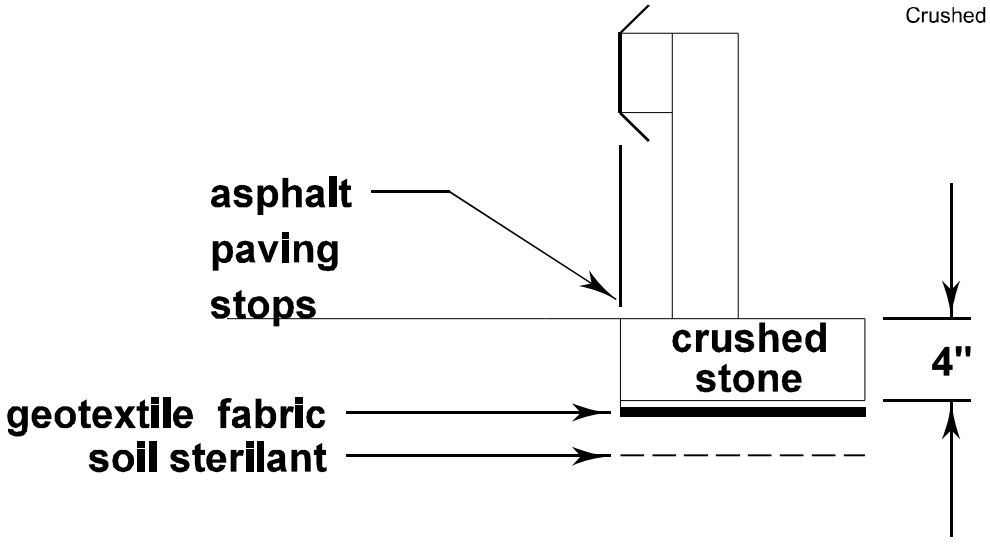
-Mill limits of county/local roads at a depth of 1 1/2".

-Place 1 1/2" of MT 9.5mm Mixture to tie to mainline overlay.

-Milling/Paving area = ▨

TYPICAL DETAIL OF ADDITIONAL SHOULDER PAVING REQUIRED AT GUARDRAIL LOCATIONS

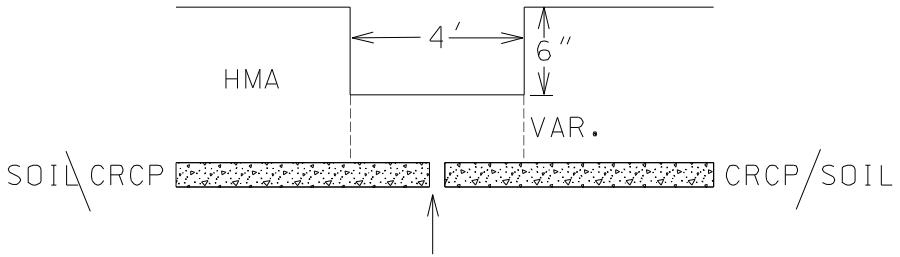




FMS CON:

STATE	PROJECT NO.
MISS.	

LONGITUDINAL JOINT REPAIR



Prior to mill/overlay of the lanes, where condition of existing HMA surface course indicates underlying problems with the CRCP: Fill voids under the CRCP, joints at the centerline of the CRCP, and joints at the edge of pavement between the CRCP and soil cement treated shoulder by pressure grouting. Subsequent to pressure grouting, mill and replace HMA over the joint to a maximun depth of 6". After milling and prior to replacement, if there is any remaining depth of HMA over the joint, repair any failed HMA by removing all loose/broken pieces. Replacement of milled area and any repair area to be made with 12.5mm Mix, MT, Asphahlt Pavement, Leveling back to existing finish grade.

NOTE: REFER TO PRESSURE GROUTING DETAIL SHEET.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

LONGITUDINAL JOINT REPAIR

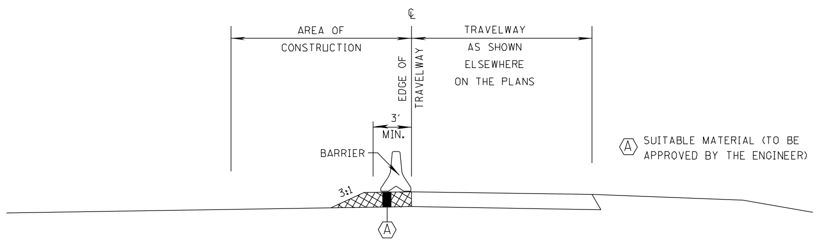
COUNTY:
PROJECT NO:
FILENAME: LJR-1 LONGITUDINAL JOINT REPAIR.DGN
DESIGN TEAM_____CHECKED_____DATE_____

WORKING NUMBER LJR-1
SHT. NUMBER XX

DATE REVISION BY

MMDDYY BRI BB AMPM DGNFILENAME

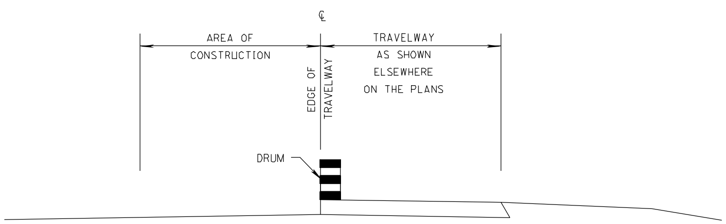
STATE	PROJECT NO.
MISS.	



ELEVATION VIEW FOR POSITIVE BARRIER

NOTES:

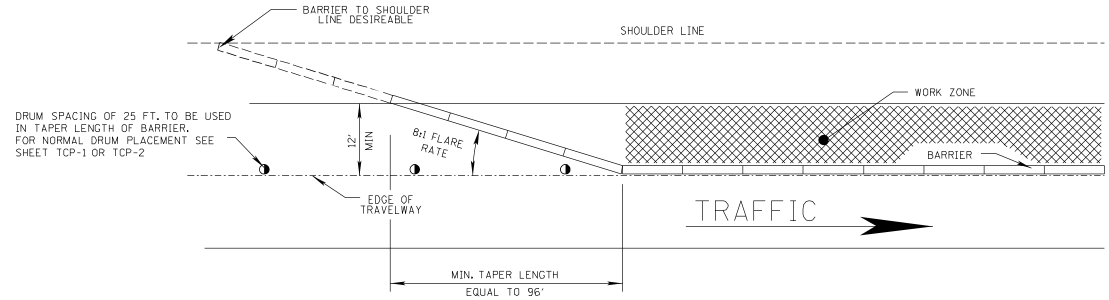
1. POSITIVE BARRIER IS REQ'D IN THE AREA OF OPEN PUNCH OUTS THAT ARE WITHIN SIX (6) FEET OF THE TRAVELWAY WHENEVER ACTUAL REPAIR WORK IS NOT BEING PERFORMED WITHIN THE LANE CLOSURE.
2. MATERIAL USED TO SUPPORT POSITIVE BARRIER MUST BE AT SAME ELEVATION AS PAVEMENT IN ADJACENT TRAVELWAY.
3. DELINEATORS REQUIRED ON ALL NON-REFLECTIVE BARRIER, AS SHOWN ON WORKING NO. CMB-3.



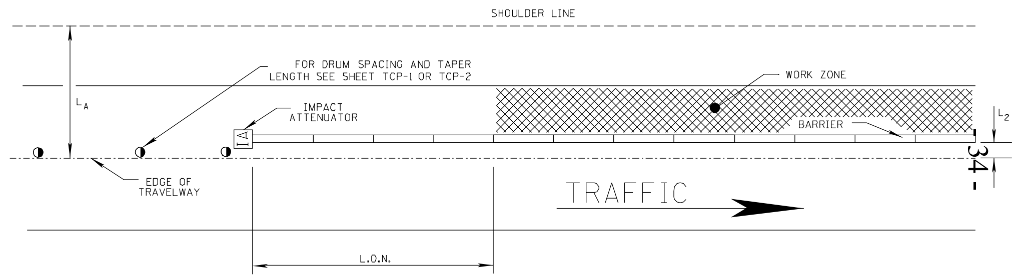
ELEVATION VIEW FOR DRUM

NOTES:

1. WHILE WORK IS BEING PERFORMED WITHIN THE LANE CLOSURE, DROP-OFFS MUST BE PROTECTED WITH DRUMS, ETC. IN EMERGENCIES EXCAVATED SECTION MAY BE BACKFILLED WITH GRANULAR MATERIAL, STONE OR OTHER APPROVED MATERIAL TO AVOID OVERNIGHT DROP-OFFS.
2. LANE CLOSURES WITH OPEN PUNCH OUT AREAS MAY NOT BE LEFT UNATTENDED WHEN DRUMS ARE BEING USED FOR LANE CLOSURE



DETAIL OF POSITIVE BARRIER WITH TAPER



DETAIL OF POSITIVE BARRIER WITH IMPACT ATTENUATOR

NOTES:

1. LENGTH OF NEED, L.O.N. = $\frac{L_R(L_A - L_2)}{L_A}$

   WHERE: L_A = LATERAL EXTENT OF THE AREA OF CONCERN
   L_R = RUNOUT LENGTH
   L_2 = LATERAL OFFSET FROM EDGE OF TRAVELED WAY TO BARRIER.

2. RUNOUT LENGTH (L_R) IS TO BE DETERMINED USING THE FOLLOWING TABLE:

RUNOUT LENGTH (L_R) GIVEN TRAFFIC VOLUME (ADT) (ft)				
DESIGN SPEED (mph)	OVER 10,000 veh/day	5,000-10,000 veh/day	1,000-5,000 veh/day	UNDER 1,000 veh/day
70	360	330	290	250
60	300	250	210	200
50	230	190	160	150
40	160	130	110	100
30	110	90	80	70

GENERAL NOTES:

1. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER OTHER BID ITEMS.
2. FOR DETAILS OF DRUM PLACEMENT SEE OTHER TRAFFIC CONTROL PLANS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

LANE CLOSURE DETAILS FOR GREATER THAN 3 INCH DROPOFF

PROJ. NO.:
COUNTY:

WORKING NUMBER SDTCP-C
SHEET NUMBER

BY | REVISION | DATE

FILENAME: SDTCP-C.DGN
DESIGN TEAM UPDATE CHECKED UPDATE DATE UPDATE

MMDDYY 00:00AMPM DGNFILENAME

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION PLAN

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 6757 **CODE: (SP)**

DATE: 4/17/2025

SUBJECT: Lane Closure Restrictions

PROJECT: NHPP-0056-01(114) / 109228301 –Hinds County

Bidders are hereby advised of the following lane closure restrictions on the above captioned project:

The following conditions apply for Concrete Pavement Punchout Repair Operations.

SR 25 Eastbound and Westbound Single Lane Closures:

- Lane closures shall be allowed on weekends from 6:00 PM Friday to 6:00 AM Monday.

SR 25 Eastbound and Westbound Multi-Lane Closures:

- Lane closures shall be allowed on weekends from 6:00 PM Friday to 12:00 PM Saturday and from 6:00 PM Saturday to 12:00 PM Sunday.

The following conditions apply for All Operations,

SR 25 Eastbound and Westbound Single Lane Closures:

- Lane closures shall NOT be allowed Monday through Friday from 6:00 AM to 6:00 PM.
- Lane closures shall NOT be allowed Saturday from 6:00 AM to 7:00 PM.

SR 25 Eastbound Multi-Lane Closures:

- Lane closures shall NOT be allowed Monday through Friday from 6:00 AM to 6:00 PM.
- Lane closures shall NOT be allowed Saturday from 6:00 AM to 7:00 PM.

SR 25 Westbound Multi-Lane Closures:

- Lane closures shall NOT be allowed Monday through Thursday from 6:00 AM to 7:00 PM.
- Lane closures shall NOT be allowed Friday from 6:00 AM to 8:00 PM.
- Lane closures shall NOT be allowed Saturday from 6:00 AM to 7:00 PM.

No lane closures shall be allowed during the State Fair (from October 1st to October 14th), and during Jackson Preparatory School or Jackson State University football games.

No lane closures shall be permitted on Sunday. Sunday is defined as 7:00 PM Saturday to 7:00 PM Sunday

The Contractor will be charged a fee of **$500.00** for each full or partial 5-minute period until the roadway is back in compliance with the requirements stated above.

Official time can be obtained by calling the following Jackson area phone number: 601-355-9311.