

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO.	<u>1</u>	DATED	<u>5/23/2025</u>	ADDENDUM NO.	_____	DATED	_____
ADDENDUM NO.	_____	DATED	_____	ADDENDUM NO.	_____	DATED	_____
ADDENDUM NO.	_____	DATED	_____	ADDENDUM NO.	_____	DATED	_____

Number

Description

1 Revised NTB No. 6874; Revised Wage Rates; Revised Bid Items; Amendment EBSx Download Required.

TOTAL ADDENDA: 1

(Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

Contractor

BY _____

Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

President

Address

Secretary

Address

Treasurer

Address

The following is my (our) itemized proposal.

IM-0220-03(036)/ 109480301000 & IM-0220-03(036)/ 109480302000

Madison & Hinds County(ies)

Revised 01/26/2016

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 6874

CODE: (SP)

DATE: 05/23/2025

SUBJECT: Scope of Work

PROJECT: IM-0220-03(036) / 109480301 & 302. – Madison and Hinds Counties

The contract documents do not include an official set of construction plans but may, in reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, “Standard Drawings”

A general description of the work required on the project is for the milling/overlaying of approximately 11 miles of I-220 from John R. Lynch St (BOP Station 22+25) to I-55 Ramps (EOP Station 771+50) in Madison and Hinds Counties. Details of specific work are mentioned in the following sections.

(Hinds 302) From Station 22+25 (BOP) to Station 677+00:

(Madison 301) From Station 677+00 to Station 771+50 (EOP):

Work in this section shall consist of milling approximately 1” of OGFC and replacing with 1” of OGFC. The Contractor shall repair failed areas listed in the provided tables with full depth 19-mm, HT, Leveling asphalt. Similarly, the Contractor shall spot mill leveling areas listed in the provided tables with 9.5-mm SMA at variable depths. All guard rail not meeting current MDOT standards shall be replaced. Traffic will be allowed to run on the milled surfaces no more than two (2) consecutive days. The Contractor shall perform repairs and undersealing at the bridges listed in the provided tables. Random clearing of trees and vegetation shall be performed at the locations listed in the provided tables or as directed by the Engineer.

Milling

Milling/paving shall not begin until an approved asphalt mix design has been received, nor until such time that, in the opinion of the Engineer, weather conditions have been consistently suitable enough to allow placement of the asphalt pavement after the milling operations.

The reclaimed asphalt pavement (RAP) material removed by the milling operation shall become the property of the Contractor.

Where milling is required, the Contractor shall provide outlets in the existing shoulders at sufficient intervals to prevent pooling or standing water on the milled surface, the cost of which shall be absorbed in other items bid.

Milling and paving operations shall be performed such that a -2% slope from centerline is provided in normal crown roadway sections. Superelevation through curves shall be maintained as it currently exists or improved as directed.

Milling operations shall be performed in accordance with the Contract documents and the Standard Specifications. Variable width and length transitions may be required for ties at ramps, local roads, and project limits.

Traffic will be allowed to travel on the mainline milled surface for two (2) days, and the Contractor will be assessed a penalty of **\$5,000.00** per calendar day afterwards until the mainline milled surface is covered with the next lift of asphalt. This allowance for traffic on mainline milled surface is not a requirement. It shall be determined by the Contractor how many days up to two (2) days traffic is allowed on the milled surface. If the milled surface begins to deteriorate under traffic, the Contractor shall make the necessary adjustments to prevent the roadway deterioration. The Contractor shall be responsible for any claims due to the deteriorating roadway.

Traffic will be allowed to run on all milled surfaces other than the mainline for 30 days unless otherwise stated, and the Contractor will be assessed a penalty of **\$1,000.00** per calendar day afterwards until the non-mainline milled surface is covered with the next lift of asphalt. The additional allowance for the non-mainline milled surface is for the Contractor's convenience, and thus, the Contractor is responsible for any pavement failures or damage sustained during this period. Milling and paving of paved shoulders shall conform to Subsection 406.03.2 of the Standard Specifications.

Paving

Per Subsection 401.02.3.2, the asphalt mix design shall be submitted to the Engineer at least 10 working days prior to its proposed use.

Prior to mainline milling and paving operations, failed areas in the existing pavement shall be removed and backfilled with 19-mm, HT, Leveling asphalt as per the attached typical sections and details. Asphalt shall be placed in multiple lifts with a maximum lift thickness of 3". Any granular/chemically treated/stone/etc. base or subgrade material deemed unsuitable by the Engineer shall be removed as directed and backfilled with 19-mm, HT, Leveling asphalt. Payment for the excavation of the granular base and subgrade will be made using the 203-G: Excess Excavation pay item. A list of the failed areas is shown in the attached tables.

Pavement repairs shall be completed as a continuous operation in order to minimize traffic impacts. Lane closures shall remain in place until the failed area has been completely repaired. Lane closures may not be left unattended.

Prior to mainline milling and paving operations and subsequent to the repair of failed areas, spot milling and leveling shall be performed in the areas listed in the attached tables and at other areas as directed by the Engineer. Prior to the leveling operations, spot milling at variable depths shall be performed in the areas to remove cracked/oxidized asphalt. Subsequent to spot milling leveling shall be performed to correct the longitudinal and transverse profile of the roadway using 9.5-mm SMA. Work shall be conducted and coordinated in a manner to prevent a longitudinal joint of more than 2¼" where traffic is expected to cross. Adjacent lanes and shoulders shall be brought up to grade as required to prevent drop-offs and as specified in Subsection 618.03.3. Payment for milling, leveling, and granular shoulder work will be made using the appropriate pay items. Uneven Lanes signs shall be used as required and as shown on the MDOT Standard Drawings.

The surface lift for failed area repair or concrete punchout repair shall have a maximum deviation of 3/8" as determined by a 10-foot straight edge. Any location that deviates more than this tolerance, as determined by the Engineer, shall be corrected at no additional cost to the State.

If traditional excavation methods are used, the removal area shall first be saw cut full depth including concrete, where applicable, to create a neat line and prevent damage to the adjacent pavement structure. Payment for saw cuts will be made using the appropriate items. If milling techniques are used, the area will not require saw cuts but care should be exercised to create a neat removal line and to prevent damage to the adjacent pavement structure. If saw cuts are used in conjunction with milling, payment will be made using the appropriate pay items. Payment will not be made for saw cuts that are not performed.

Granular Shoulder Material

Where applicable, the existing shoulders shall be raised to match the new pavement elevation by placing variable depth granular material. The shoulders shall be graded and pulled up on a daily basis to eliminate drop-offs in excess of 2 1/4". Placement of the granular material on the finished asphalt course shall not be permitted. The existing shoulder shall be scarified to allow incorporation of the new shoulder material. The material shall be bladed, rolled, and compacted to a finished slope of four percent (4%) in normal crown sections. Placement of this material shall be performed to provide a uniform and compacted shoulder with a minimum depth and width of material placed. Shoulders with adequate shoulder material in place shall be bladed to a slope of four percent (4%) in normal crown sections. The cost of blading will be an absorbed item and is to be included in the price of other items bid. Crushed concrete will not be allowed for use as granular shoulder material.

Any material excavated from the existing shoulder as a result of shoulder blading shall be used on the existing shoulder to match the new pavement elevation and any surplus material shall be spread along the edge of the shoulders, fore slopes, or other adjacent areas as directed by the Engineer and will be an absorbed item. Material which cannot be suitably placed in adjacent areas and deemed to be excess excavation by the Engineer shall be removed from the project site. Payment for removal of excess material will be made using pay item 203-G: Excess Excavation.

Temporary and Permanent Pavement Markings

Temporary traffic stripe will be required immediately after the milling and/or required overlay and prior to opening area to traffic. Temporary stripe shall be placed in the same location and configuration as the permanent stripe except that it may be offset as required for milling and paving operations. If temporary stripe is offset, the Contractor shall conduct operations in a manner to ensure the final temporary stripe is placed at the required location of the permanent stripe. If removal of temporary offset stripe is required in order to achieve the correct location and alignment of permanent stripe, the cost of removal will be absorbed in other items bid.

Placing double temporary centerline will not be allowed.

Temporary striping shall conform to finished stripe specifications for alignment, neatness, and straightness.

The use of short strips of traffic tape will not be allowed unless approved by the Engineer.

All permanent striping will be double drop thermoplastic, 90-mil thickness unless otherwise specified in Subsection 907-626.03. Edge lines will be placed to accommodate the lane widths shown on the attached applicable typical sections unless prevented by field conditions.

Per Subsection 907-626.03, a binder-sealer shall be applied to the concrete pavement or bridge surface prior to the placement of the thermoplastic material and shall be absorbed under the thermoplastic pay items. The type and amount of binder-sealer used shall adhere to the thermoplastic manufacturer's recommendations.

Permanent raised pavement markers shall be installed on mainline after completion of all paving operations. Edgeline RPM's shall be installed as per Special Design Drawing RPM-1. If the usable space outside of the traffic stripe is insufficient to install the RPM's as per Special Design Drawing RPM-1, then the Contractor shall be allowed to install the outside edge of the RPM flush with the inside edge of the traffic stripe.

Pavement section marking tape on this project shall be located prior to overlaying and placed back in the same location after paving operations have ceased. The section marking shall be 8-inch high performance cold plastic detail stripe and shall be four feet (4') in length. The marking shall be centered across the centerline stripe. The cost of this item shall be absorbed in other items bid.

Guardrail

Guardrails shall be replaced at the locations shown on the attached table. Removal of guardrail shall consist of removal of bridge end section, w-beam/thrie beam, terminal end section, posts, and all other appurtenances.

All guardrail removed shall be replaced the same day and prior to reopening the adjacent lane of traffic.

Voids created by the removal of posts, concrete anchors, footings, etc. shall be backfilled and compacted in accordance with Section 203 of the Standard Specifications.

Up to ten thousand (10,000) linear feet of guard rail designated to be removed (metal rails and metal posts only) shall be delivered to the Whitfield Maintenance Headquarters and stockpiled, or as directed by the Engineer. The Contractor shall coordinate delivery of the material with MDOT Maintenance personnel two (2) days prior to delivery. MDOT Maintenance personnel may be reached at 601-683-3341. All wooden posts, blockouts, concrete anchors, damaged guard rail sections, etc. will be the property of the Contractor.

The asphalt guardrail pad shall be milled and paved up to the face of the guardrail. The existing asphalt pad to be paved shall be milled to accommodate 3'' of 9.5-mm, HT asphalt. The elevation of the finished surface of the asphalt pavement shall provide for the required MASH guardrail height (see Standard Drawings). The remaining asphalt guardrail pad behind the face of the guardrail shall be removed and shall be paid for using the milling pay item. The guardrail pad shall be reconstructed using crushed stone granular material and shall be a minimum of 4'' in depth. If blading is required in order to meet the minimum depth, then said blading shall be an absorbed item and the excavated material shall be retained and used to raise the existing shoulder to match

the new pavement elevation. Material which cannot be placed and blended in adjacent areas and deemed to be excess excavation by the Engineer shall be removed under pay item 203-G: Excess Excavation. Prior to the placement of the crushed stone, a soil sterilant shall be applied as per Subsection 616.03.2 and Geotextile Stabilization, Type V, Non-Woven installed underneath the limits of the crushed stone. The installed guardrail shall meet all requirements in order to be MASH compliant.

Guardrail lengths are based on terminal end length of 37.5'. If terminal of length other than this is used, an adjustment in w-beam length is required.

All dimensions and spacings for bridge rail connectors shall be verified in the field by the Contractor prior to fabrication.

The existing impact attenuators at STA 177+50, STA 200+00, and STA 137+00 shall be removed and replaced under the pay items 202-B: Removal of Impact Attenuator, and 629-A: Vehicular Impact Attenuator, 70 MPH. Similarly, the existing impact attenuators at STA 25+30, STA 35+00, STA 15+00 and STA 17+00 shall be removed and replaced under the pay items 202-B: Removal of Impact Attenuator, and 629-A: Vehicular Impact Attenuator, 50 MPH. The existing attenuators shall not be removed until the new one is on site and ready to be installed.

Payment made under pay item 629-A shall be considered complete compensation by the Department to the Contractor for all aspects of installation of the new attenuator. The attenuators shall be retained by MDOT and shall be delivered by the Contractor to the Clinton Maintenance Office. Delivery shall be coordinated with Trent Holbrook (601-946-7044) at least 24 hours prior to the expected delivery.

Permanent Signs

Object markers at bridge approaches and other locations shall be replaced as shown in the attached table. Removal of object markers shall be absorbed in the cost of other items bid.

Throughout the entire project, delineators shall be removed/replaced or installed in missing locations at all interchanges and as directed by the Engineer. Removal of post-mounted delineators shall be absorbed in the cost of other items bid.

Traffic Control

The Contractor shall erect and maintain construction signing and provide all signs and traffic control devices necessary to safely maintain traffic around and through the work areas in accordance with the Traffic Control Plan and the MUTCD. The cost shall be included in the price bid for pay item 907-618-A: Maintenance of Traffic. Fluorescent orange sheeting shall be used on all construction and traffic control signs except those designated in the plans to be black legend and border on white background.

Lane closures shall not be left unattended by the Contractor's personnel. Ramp closures will not be allowed without written approval from the Engineer.

Standard roadside construction signs, barricades, etc. shall be placed in accordance with the attached tables, drawings, and as directed by the Engineer. W20-1 signs shall be placed on all

public road approaches as shown or as directed. Payment for standard roadside construction signs, barricades, etc. will be made using the appropriate pay items.

The Contractor shall on a daily basis, remove all debris from within the roadway and a 30-foot clear zone which, in the opinion of the Engineer, is a hazard to the traveling public. This activity shall begin with the beginning of work or the beginning of the contract time, whichever comes first. No direct payment will be made for the debris removal; the cost is to be included in the prices of items bid. Failure of the Contractor to remove the debris as prescribed herein shall be just cause for withholding the monthly progress estimate payment or suspending active operations until the debris is satisfactorily removed by the Contractor.

Temporary asphalt joints (aka paper joints) shall be employed at all locations requiring traffic to traverse an uneven, transverse, pavement joint. Paper joints shall be a minimum of nine feet (9') in length and for the full width of the milled/paved surface. Paper joints for 1" OGFC joints shall be a minimum of three feet (3') in length. Paper joints shall be adequately maintained.

Potholes that may exist or occur in the existing pavement are to be patched in a timely manner as required. Patching of potholes shall be considered an absorbed item.

Random Clearing

Random clearing shall be required at a distance of seventy feet (70') from the pavement edge or to the right-of-way line, whichever distance is less. In situations where the clearing limits will leave a strip of trees in the median twenty feet (20') wide or less, then said strip shall also be cleared. Tree limbing shall be required at a distance of up to thirty feet (30') vertically within the clearing limits. The random clearing shall encompass trees four inches (4") or greater in diameter, scrubs, and overhang within the clearing limits. Trees shall either be cut flush with the ground line or if the Contractor elects to cut the tree above the groundline then the tree stump shall be ground flush with the ground line. The cut material shall either be hauled away from the project site or mulched in place to a depth of four inches (4") maximum in any one location. If the Contractor elects to utilize other means of disposal for the cut material, then said method shall be approved by the Engineer prior to commencement. Random clearing of areas within the ROW or the 70' random clearing limits shall be paid using pay item 201-D: Random Clearing (Station).

Areas within the ROW that is outside of the 70' random clearing limits shall have all dead or dying trees removed and payment shall be made using pay item 907-201-D: Random Clearing (Mile). All dead or dying trees that are either cut or already downed shall be either mulched in place or become the property of the Contractor and removed from the right-of-way. If the Contractor elects to remove any of the trees from the right-of-way, then the Contractor shall be responsible for ensuring that all necessary requirements are met including but not limited to any permits needed. If any tree stumps are located outside of the tree line, then said tree stumps shall either be ground or cut to the ground line. Grubbing shall NOT be allowed for this Contract. If the Contractor elects to mulch the trees in place, then said mulch shall be spread to a maximum depth of four inches (4"). If the dead tree is located in areas maintained or mowed in front of homes or businesses, then said dead tree shall be removed and mulched out of the maintained or mowed area.

Random clearing by the acre shall be performed around bridge areas as per the attached table. The random clearing shall encompass trees four inches (4") or greater in diameter, scrubs, and

overhang within the clearing limits. Trees shall either be cut flush with the ground line or if the Contractor elects to cut the tree above the groundline then the tree stump shall be ground flush with the ground line. The cut material shall either be hauled away from the project site or mulched in place to a depth of four inches (4") maximum in any one location. If the Contractor elects to utilize other means of disposal for the cut material, then said method shall be approved by the Engineer prior to commencement. Payment for random clearing of areas around bridge areas shall be made using pay item 201-D: Random Clearing (Acre).

Any areas disturbed by the Contractor shall be restabilized to the satisfaction of the Engineer at no additional cost to the State. The Contractor shall not damage the slopes, other right-of-way features, private property, or above ground utilities. Any damage done shall be corrected by the Contractor at no additional cost to the State and shall include but not be limited to ditch regrading, slope regrading, grassing, etc. All ditches and slopes shall be restored to their original conditions and the Contractor shall ensure that mulch shall not be left in the ditches and on the shoulders/slopes on the roadway side of the ditches. Additionally, it shall be the responsibility of the Contractor to protect existing structures such as pipes, inlets, aprons, bridges, etc. from damage which might occur during construction. The Contractor shall replace or repair, as directed by the Engineer, any structures damaged during the life of the contract. No payment will be made for replacement or repair of damaged items.

Throughout the life of the project and until the partial or full maintenance release, all dead or dying trees within the ROW outside of the random clearing areas as identified by the Project Engineer shall be cut and removed or mulched in place. The Contractor shall cut the trees as directed by the Engineer and shall take precautions to prevent damage to the roadway, stripe, guardrails, and other structures. The stumps of such trees shall be cut off or mulched flush with the groundline. Payment shall be made under pay item 202-B: Removal of Trees. This shall be considered full compensation for all items of work associated with the removal of any dead or dying trees outside of the defined clearing limits in the attached table. The Contractor shall coordinate the activities with local utilities if any trees pose danger to utility lines. Appropriate traffic control shall be used for all tree cutting and disposal operations.

Bridge Repairs

All epoxy repairs shall be performed in accordance with the attached details shown on the epoxy repair drawings and in accordance with the notes herein. Concrete spalled areas on the bridge shall be repaired as directed by the Project Engineer and at the locations listed in the attached tables. The Contractor shall determine the depth of reinforcement prior to any saw cutting. Spalled areas where pack rust has developed around or on reinforcement shall be blasted clean prior to repairing the spalled location. All areas of the bridge repaired with epoxy mortar shall be restored to the original dimensions as shown in the information plans, unless noted otherwise.

Materials:

1. Epoxy Resin: Resin shall be selected from the MDOT Approved Products List.
2. Silica Sand: the materials shall be bagged general purpose cleaning sand.
3. Epoxy Mortar Mix: the epoxy mortar mix shall consist of part liquid epoxy and part clean dry sand mixed in the ratio recommended by the Manufacturer.

Applications:

1. A Representative of the Epoxy Manufacture must be present for sufficient time to ensure that the Contractor is properly schooled in the use of the epoxy material.
2. Prior to placement of the mortar mix, the prepared surface shall be lightly primed with neat epoxy.
3. Acetone alcohol may be used to clean and lubricate trowels.
4. Curing time shall be in accordance with the Manufacturer's recommendations.

All items of work related to epoxy repair shall be paid for under pay item 907-824-A: General Epoxy Repair. Epoxy repair under this pay item is for general concrete spall repairs, and shall be bid such that the item may be increased, decreased, or eliminated as directed by the Project Engineer.

The joint repair shall include installation of the preformed joint seal and other necessary work per the included tables, standard drawings, or as directed by the Engineer.

The joint shall be sealed by one of the three approved Manufacturers listed in Special Provision 907-823 and installed according to the Manufacturer's specifications.

Miscellaneous Notes

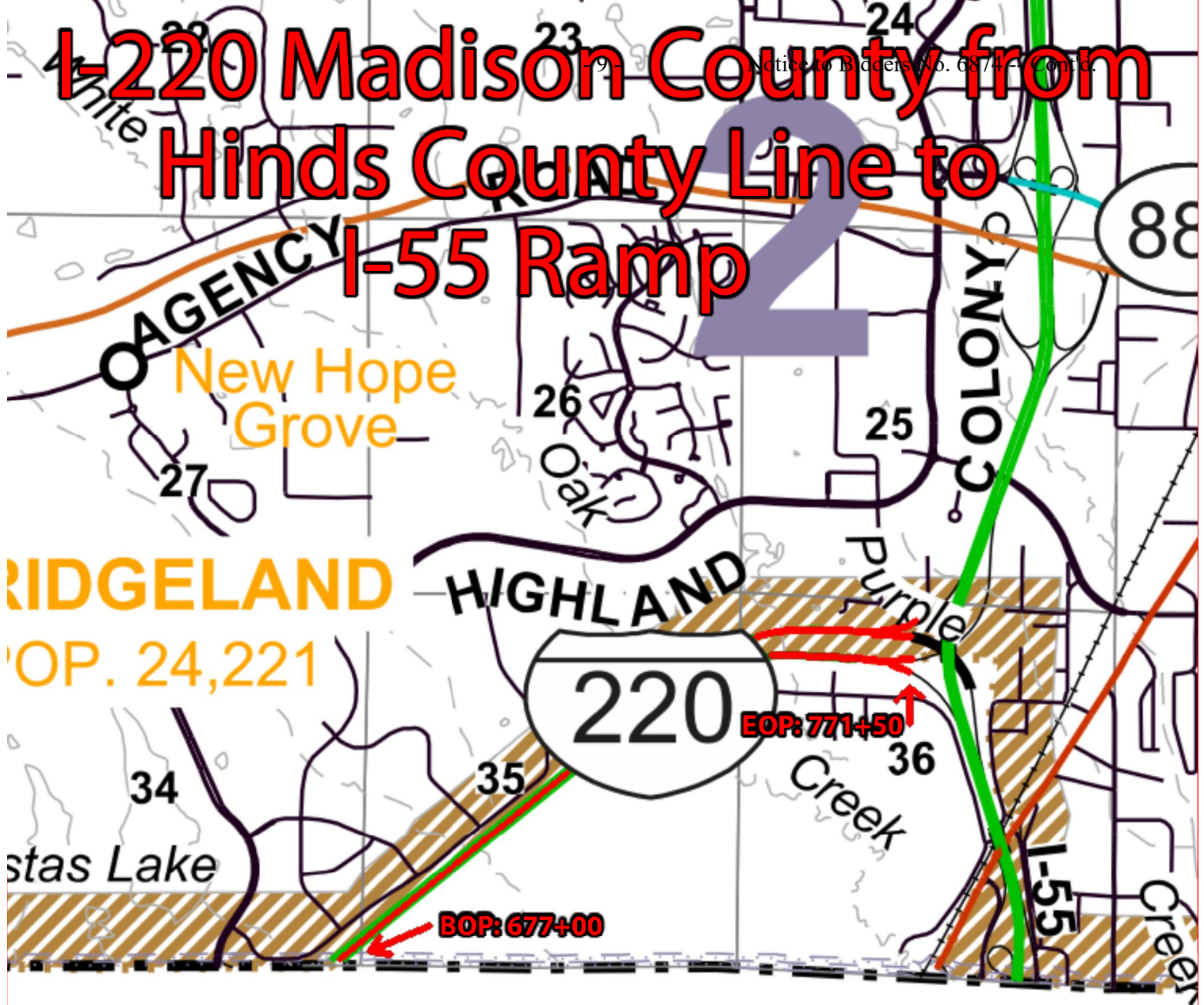
It shall be the responsibility of the Contractor to protect existing structures such as pipes, inlets, aprons, bridges, etc. from damage which might occur during construction. The Contractor shall replace or repair, as directed by the Engineer, any structures damaged by the Contractor during the life of the contract. No payment will be made for replacement or repair of damaged items.

Any signs that are in conflict with construction of this project shall be removed and relocated by the Contractor as directed by the Engineer; the cost of which is to be absorbed in other items bid.

Removal of existing raised pavement markers shall be included in the prices for other items bid.

Incidental work such as removing vegetation, shaping and compacting shoulders, removing and resetting signs and/or mailboxes, removing excess asphalt material, project clean-up, and other items of incidental work necessary to complete the project will not be measured for separate payment and will be considered included in the prices of items bid.

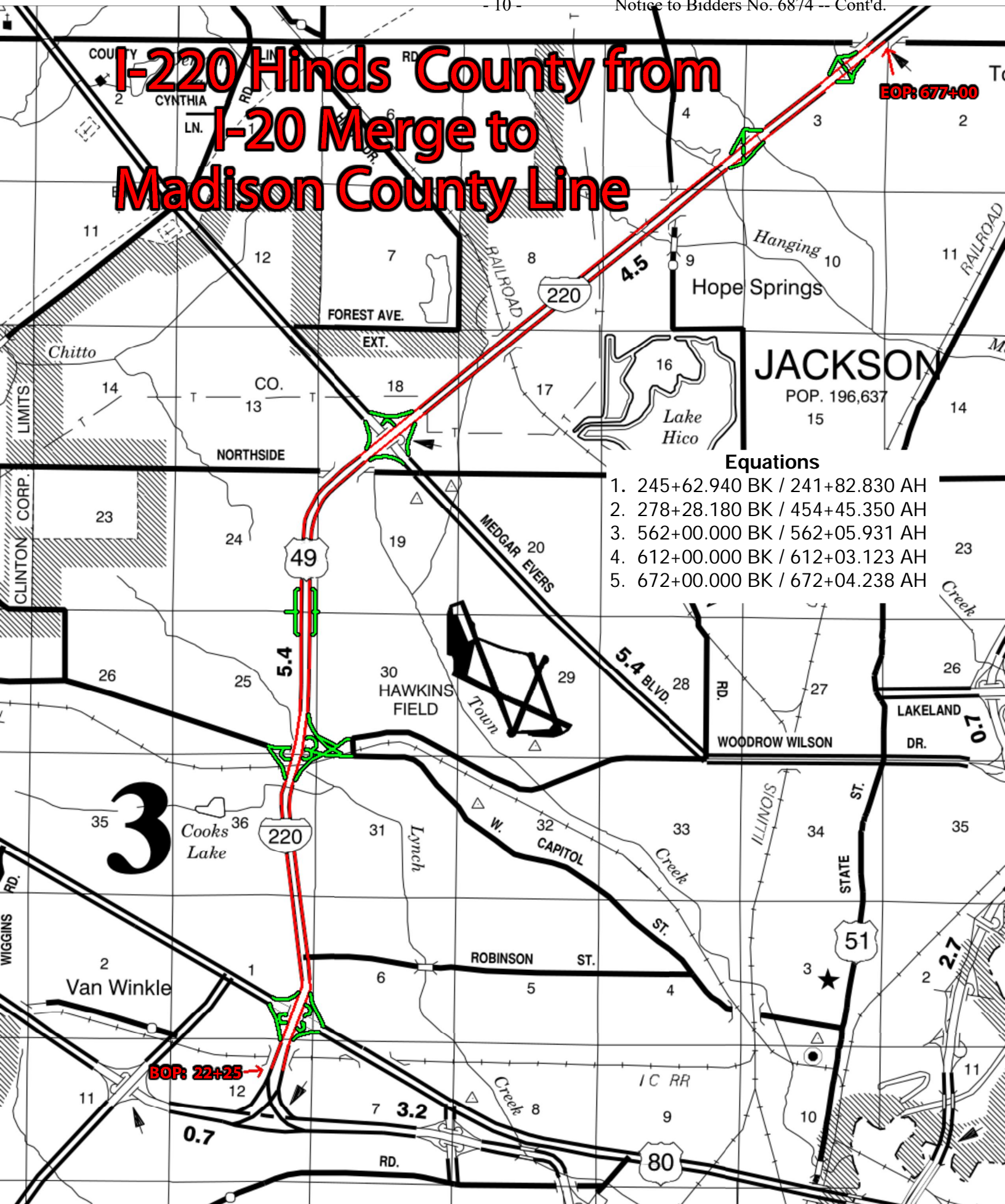
Prior to the final inspection, bridges, islands, and areas with curb shall be swept/cleaned. Care should be taken to prevent milled asphalt, asphalt debris, vegetative/granular debris, etc. from entering drainage structures or clogging other drainage ways. Disposal of material will not be measured for separate payments.



Equations

1. 733+00.001 BK / 733+07.611 AH

I-220 Hinds County from I-20 Merge to Madison County Line



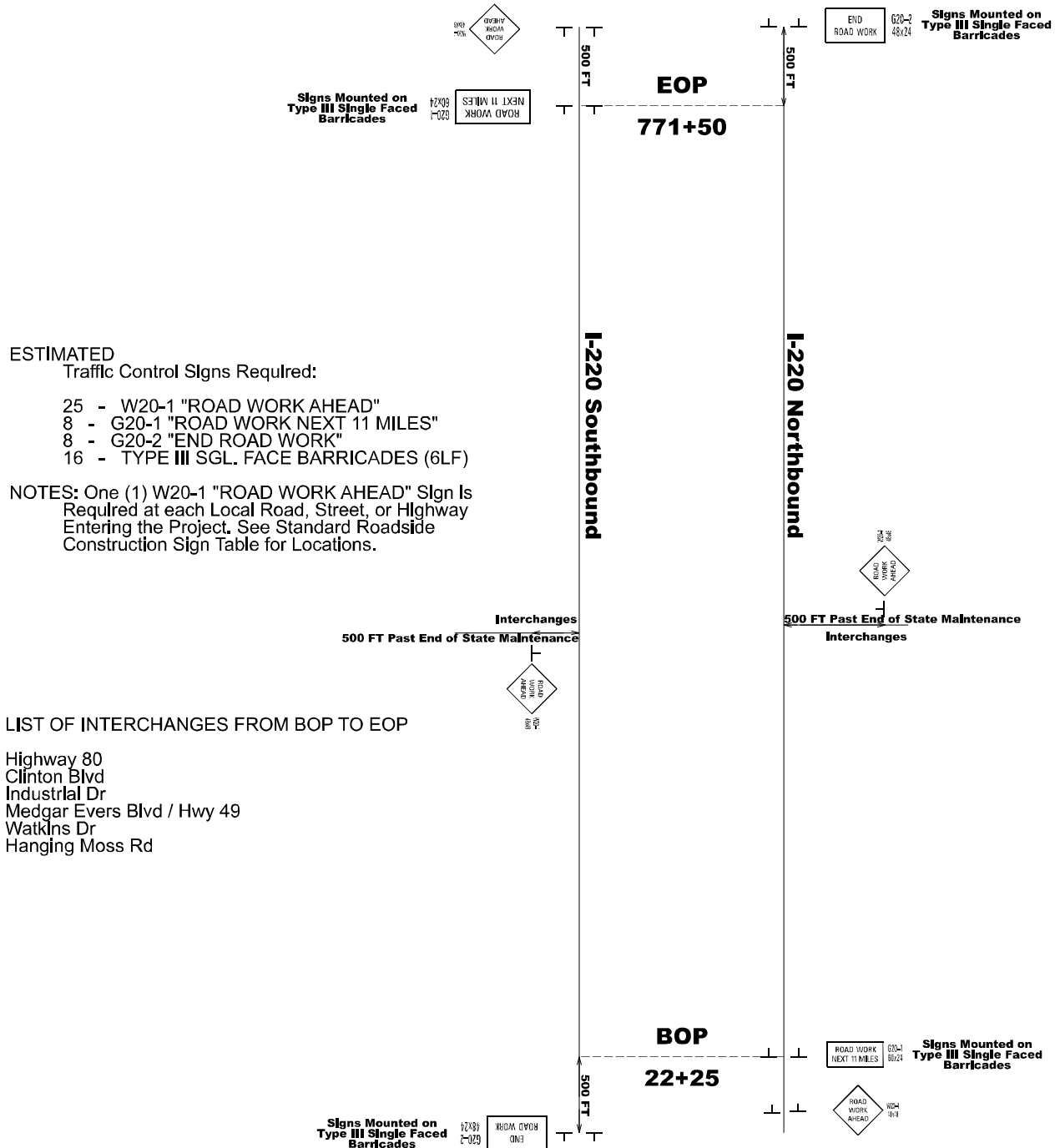
EOP: 677+00

Equations

1. 245+62.940 BK / 241+82.830 AH
2. 278+28.180 BK / 454+45.350 AH
3. 562+00.000 BK / 562+05.931 AH
4. 612+00.000 BK / 612+03.123 AH
5. 672+00.000 BK / 672+04.238 AH

BOP: 22+25


Construction Signing Detail I-220 Madison and Hinds Counties



STATE	PROJECT NO.
MISS	IM-0220-03(036)

SUMMARY OF QUANTITIES (SHEET 1)

PAY ITEM NO.	PAY ITEM	UNIT	HINDS : 109480-302000	
			Prelim	Final
907-201-D001	Random Clearing	MI	6	
201-D001	Random Clearing	STA	138	
201-D002	Random Clearing	ACRE	8	
202-B009	Removal of Asphalt Pavement, Failed Areas	SY	301	
202-B158	Removal of Guard Rail, Including Rails, Posts and Terminal Ends	LF	24,181	
202-B162	Removal of Impact Attenuator	EA	7	
202-B169	Removal of Joint Material	LF	106	
202-B240	Removal of Traffic Stripe	LF	44,419	
202-B244	Removal of Trees	EA	50	
203-G002	Excess Excavation, LVM, AH	CY	1,471	
209-A005	Geotextile Stabilization, Type V, Non-Woven	SY	8,521	
223-A001	Mowing	ACRE	1,392	
304-D002	Granular Material, Crushed Stone	TON	1,597	
907-402-A002	Open Graded Friction Course, 9.5-mm Mixture	TON	10,450	
907-402-B001	Bituminous Tack Coat	GAL	31,093	
907-403-A013	9.5-mm, HT, Asphalt Pavement	TON	1,943	
907-403-B004	19-mm, HT, Asphalt Pavement, Leveling	TON	306	
907-403-S002	Joint Sealant	LF	13,600	
907-405-A001	Stone Matrix Asphalt, 9.5 mm Mixture	TON	6,612	
406-D001	Fine Milling of Bituminous Pavement, All Depths	SY	304,640	
407-A001	Asphalt for Tack Coat	GAL	8,383	
907-420-A001	Undersealing	LBS	45,000	
503-C010	Saw Cut, Full Depth	LF	22,627	
606-B003	Guard Rail, Class A, Type 1, 'W' Beam, Metal Post	LF	19,040	
606-C003	Guard Rail, Cable Anchor, Type 1	EA	15	
606-D012	Guard Rail, Bridge End Section, Type D Modified	EA	24	
606-D018	Guard Rail, Bridge End Section, Type G, Modified	EA	24	
606-D020	Guard Rail, Bridge End Section, Type H, Metal Post	EA	21	
606-D022	Guard Rail, Bridge End Section, Type I	EA	6	
606-E005	Guard Rail, Terminal End Section, Flared	EA	29	
606-E007	Guard Rail, Terminal End Section, Non-Flared	EA	59	
907-618-A001	Maintenance of Traffic	LS	1	
907-618-M2001	Work Zone Law Enforcement	HRS	1	
619-A1001	Temporary Traffic Stripe, Continuous White	MI	56	
619-A2001	Temporary Traffic Stripe, Continuous Yellow	MI	37	
619-A3001	Temporary Traffic Stripe, Skip White	MI	54	
619-A5001	Temporary Traffic Stripe, Detail	LF	248,571	
619-D1001	Standard Roadside Construction Signs, Less than 10 Square Feet	SF	64	
619-D2001	Standard Roadside Construction Signs, 10 Square Feet or More	SF	480	
619-G4005	Barricades, Type III, Single Faced	LF	96	
620-A001	Mobilization	LS	1	
907-626-A007	6" Thermoplastic Double Drop Traffic Stripe, Skip White	MI	27	
907-626-B004	6" Thermoplastic Double Drop Traffic Stripe, Continuous White	MI	28	
907-626-E003	6" Thermoplastic Double Drop Traffic Stripe, Continuous Yellow	MI	13	

By	MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
	SUMMARY OF QUANTITIES	
Revision	PROJ NO: IM-0220-03(036) COUNTY: HINDS	
	Working Number	SQ-1
Date	FILENAME: I-220 Hinds County	Sheet Number
	Design Team	2
	Checked	Date

STATE	PROJECT NO.
MISS	IM-0220-03(036)

SUMMARY OF QUANTITIES (SHEET 2)				
PAY ITEM NO.	PAY ITEM	UNIT	HINDS : 109480-302000	
			Prelim	Final
907-626-G004	Thermoplastic Detail Stripe, White	LF	66,612	
907-626-G005	Thermoplastic Detail Stripe, Yellow	LF	16,245	
907-626-H006	Thermoplastic Double Drop Legend, White	SF	320	
907-627-A001	Type A, Non-Reflective White Raised Jiggle Markers	EA	2,395	
907-627-A002	Type AY, Non-Reflective Yellow Raised Jiggle Markers	EA	1,185	
907-627-K001	Red-Clear Reflective High Performance Raised Markers	EA	8,468	
907-627-K002	Red-Yellow Reflective High Performance Raised Markers	EA	2,456	
629-A002	Vehicular Impact Attenuator, 50 MPH	EA	4	
629-A005	Vehicular Impact Attenuator, 70 MPH	EA	3	
630-F006	Delineators, Guard Rail, White	EA	332	
630-F007	Delineators, Guard Rail, Yellow	EA	371	
630-F010	Delineators, Post Mounted, Double White	EA	350	
630-F011	Delineators, Post Mounted, Double Yellow	EA	100	
630-F012	Delineators, Post Mounted, Single White	EA	475	
630-F013	Delineators, Post Mounted, Single Yellow	EA	450	
630-G005	Type 3 Object Markers, OM-3R or OM-3L, Post Mounted	EA	73	
907-808-A002	Joint Repair	LF	1,528	
907-823-A001	Preformed Joint Seal, Type I	LF	764	
907-823-A002	Preformed Joint Seal, Type II	LF	53	
907-823-B001	Saw Cut, Type I	LF	1,528	
907-823-B002	Saw Cut, Type II	LF	106	
907-824-A003	General Epoxy Repair	SF	220	
907-824-PP007	Bridge Repair, Elastomeric Concrete	CY	2	

	By	MISSISSIPPI DEPARTMENT OF TRANSPORTATION SUMMARY OF QUANTITIES					
	Revision	<div style="float: left; width: 30%;">PROJ NO: IM-0220-03(036)</div> <div style="float: right; width: 30%;">COUNTY: HINDS</div> <div style="clear: both;"></div>					
	Date	<div style="float: left; width: 30%;">FILENAME: I-220 Hinds County</div> <div style="float: right; width: 30%;">Working Number SQ-2</div> <div style="clear: both;"></div>					
		<div style="float: left; width: 30%;">Design Team</div> <div style="float: right; width: 30%;">Checked</div> <div style="clear: both;"></div>					
		<div style="float: left; width: 30%;">Date</div> <div style="float: right; width: 30%;">Sheet Number 3</div> <div style="clear: both;"></div>					

STATE	PROJECT NO.
MISS	IM-0220-03(036)

SUMMARY OF QUANTITIES (SHEET 1)

PAY ITEM NO.	PAY ITEM	UNIT	MADISON : 109480-301000	
			Prelim	Final
907-201-D001	Random Clearing	MI	3	
201-D001	Random Clearing	STA	39	
201-D002	Random Clearing	ACRE	3	
202-B158	Removal of Guard Rail, Including Rails, Posts and Terminal Ends	LF	1,619	
202-B240	Removal of Traffic Stripe	LF	900	
202-B244	Removal of Trees	EA	50	
209-A005	Geotextile Stabilization, Type V, Non-Woven	SY	664	
223-A001	Mowing	ACRE	196	
304-D002	Granular Material, Crushed Stone	TON	125	
907-402-A002	Open Graded Friction Course, 9.5-mm Mixture	TON	2,510	
907-402-B001	Bituminous Tack Coat	GAL	7,473	
907-403-A013	9.5-mm, HT, Asphalt Pavement	TON	159	
907-403-S002	Joint Sealant	LF	1,313	
907-405-A001	Stone Matrix Asphalt, 9.5 mm Mixture	TON	450	
406-D001	Fine Milling of Bituminous Pavement, All Depths	SY	59,089	
407-A001	Asphalt for Tack Coat	GAL	476	
907-420-A001	Undersealing	LBS	5,000	
503-C010	Saw Cut, Full Depth	LF	1,897	
606-B003	Guard Rail, Class A, Type 1, 'W' Beam, Metal Post	LF	1,194	
606-C003	Guard Rail, Cable Anchor, Type 1	EA	1	
606-D012	Guard Rail, Bridge End Section, Type D Modified	EA	6	
606-E005	Guard Rail, Terminal End Section, Flared	EA	5	
606-E007	Guard Rail, Terminal End Section, Non-Flared	EA	2	
907-618-M2001	Work Zone Law Enforcement	HRS	1	
619-A1001	Temporary Traffic Stripe, Continuous White	MI	11	
619-A2001	Temporary Traffic Stripe, Continuous Yellow	MI	8	
619-A3001	Temporary Traffic Stripe, Skip White	MI	4	
619-A5001	Temporary Traffic Stripe, Detail	LF	6,990	
907-626-A007	6" Thermoplastic Double Drop Traffic Stripe, Skip White	MI	4	
907-626-B004	6" Thermoplastic Double Drop Traffic Stripe, Continuous White	MI	4	
907-626-E003	6" Thermoplastic Double Drop Traffic Stripe, Continuous Yellow	MI	4	
907-626-G004	Thermoplastic Detail Stripe, White	LF	2,250	
907-626-G005	Thermoplastic Detail Stripe, Yellow	LF	350	
907-626-H006	Thermoplastic Double Drop Legend, White	SF	120	
907-627-A001	Type A, Non-Reflective White Raised Jiggle Markers	EA	125	
907-627-A002	Type AY, Non-Reflective Yellow Raised Jiggle Markers	EA	55	
907-627-K001	Red-Clear Reflective High Performance Raised Markers	EA	1,000	
907-627-K002	Red-Yellow Reflective High Performance Raised Markers	EA	500	
630-F006	Delineators, Guard Rail, White	EA	22	
630-F007	Delineators, Guard Rail, Yellow	EA	31	
630-G005	Type 3 Object Markers, OM-3R or OM-3L, Post Mounted	EA	6	
907-808-A002	Joint Repair	LF	320	
907-823-A001	Preformed Joint Seal, Type I	LF	160	
907-823-B001	Saw Cut, Type I	LF	320	

MISSISSIPPI DEPARTMENT OF TRANSPORTATION			
SUMMARY OF QUANTITIES			
By	PROJ NO: IM-0220-03(036) COUNTY: MADISON		
Revision			
	Working Number SQ-1		Sheet Number 1
Date	FILENAME: I-220 Madison Co. Revised 4		
	Design Team	Checked	Date



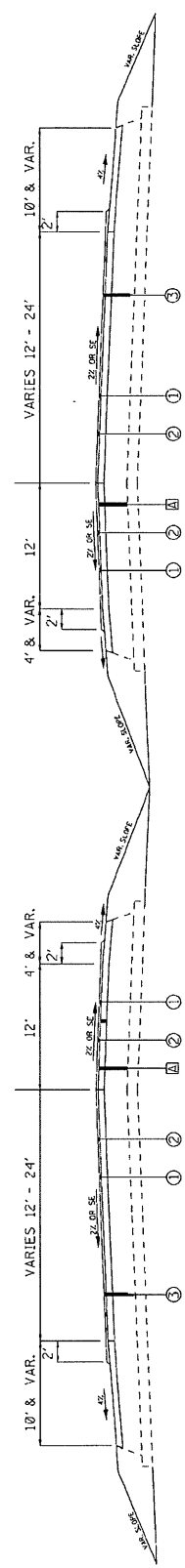
DESIGNED BY: Matthew Little
CHECKED BY: Trent Holbrook
DATE: April 4, 2025

FMS CON: 109480 3016302 000
PROJECT NO.: IM-0220-03(036)
COUNTY: MADISON & HINDS

Notice to Bidders No. 68/4
MAINLINE I-220

SHEET ID
TS
SHEET NO. 1

MAINLINE I-220
MADISON COUNTY 109480/301000 STA. 677+00 to 771+50 (EOP)
HINDS COUNTY 109480/302000 STA. 22+25 (BOP) TO 677+00



EXISTING

- 16.25" & VAR. HMA PAVEMENT

PROPOSED

- 1" & VAR. FINE MILLING
- 1" & VAR. OGFC, 9.5-mm MIX
- VAR. DEPTH ASPHALT, 19-mm, HT, LEVELING
REQ'D (FOR FAILED AREA)

9.5-MM SMA, Asphalt Pavement, Leveling							
Hinds County - 109480 / 302000							
Repairs			Spot Mill			Replace	
Location	Start Sta.	End Sta.	Width	Length	Area (S.Y.)	Asph. Tack Coat (GAL.)	9.5mm, SMA
NB	68+10	69+60	40	150	666.67	66.67	73.33
NB	75+00	76+50	40	150	666.67	66.67	73.33
NB	85+00	90+50	40	550	2,444.44	244.44	268.89
NB	521+00	526+00	40	500	2,222.22	222.22	244.44
NB	537+50	541+00	40	350	1,555.56	155.56	171.11
NB	545+00	549+50	40	450	2,000.00	200.00	220.00
NB	650+00	654+00	40	400	1,777.78	177.78	195.56
SB	28+00	30+00	40	200	888.89	88.89	97.78
SB	46+25	52+00	40	575	2,555.56	255.56	281.11
SB	73+75	89+50	40	1575	7,000.00	700.00	770.00
SB	119+25	122+50	40	325	1,444.44	144.44	158.89
SB	520+50	529+00	40	850	3,777.78	377.78	415.56
SB	538+50	540+00	40	150	666.67	66.67	73.33
SB	554+00	555+00	40	100	444.44	44.44	48.89
Total					28,111.111	2,811.111	3,092.222

9.5-MM SMA, Asphalt Pavement, Leveling							
Madison County - 109480 / 301000							
Repairs			Spot Mill			Replace	
Location	Start Sta.	End Sta.	Width	Length	Area (S.Y.)	Asph. Tack Coat (GAL.)	9.5mm, SMA
SB	684+00	685+50	40	150	666.67	66.67	73.33
Total					666.67	66.67	73.33

Northbound				Southbound			
Hinds/Madison - 109480 / 302 & 301				Hinds/Madison - 109480 / 302 & 301			
Random Clearing - Dead Trees				Random Clearing - Dead Trees			
907-201-D001				907-201-D001			
Beginning	Ending	Length (MI-Mile)	Location	Beginning	Ending	Length (MI-Mile)	Location
0.20	0.21	0.01	Hinds	0.30	1.10	0.80	Madison
1.30	1.60	0.30	Hinds	1.40	1.60	0.20	Madison
1.80	2.00	0.20	Hinds	1.80	1.81	0.01	Madison
2.40	2.41	0.01	Hinds	2.90	3.00	0.10	Hinds
4.70	4.71	0.01	Hinds	3.70	4.00	0.30	Hinds
5.00	5.10	0.10	Hinds	4.30	4.50	0.20	Hinds
5.30	5.31	0.01	Hinds	5.20	5.30	0.10	Hinds
5.80	5.81	0.01	Hinds	5.50	5.80	0.30	Hinds
6.00	6.50	0.50	Hinds	5.90	6.30	0.40	Hinds
7.40	7.41	0.01	Hinds	6.40	6.60	0.20	Hinds
7.80	7.90	0.10	Hinds	7.00	7.10	0.10	Hinds
8.00	8.20	0.20	Hinds	7.50	7.70	0.20	Hinds
9.20	9.30	0.10	Hinds	8.30	8.40	0.10	Hinds
10.40	10.41	0.01	Madison	8.50	8.90	0.40	Hinds
10.60	10.80	0.20	Madison	9.20	9.30	0.10	Hinds
10.90	11.00	0.10	Madison	9.50	10.60	1.10	Hinds
11.10	11.20	0.10	Madison	Total		4.61	
11.30	11.40	0.10	Madison				
11.60	11.80	0.20	Madison				
Total		2.27					

Median			
Hinds/Madison - 109480 / 302 & 301			
Random Clearing - Dead Trees			
907-201-D001			
Beginning	Ending	Length (MI-Mile)	Location
0.40	0.41	0.01	Hinds
10.80	11.30	0.50	Madison
Total		0.51	

Notes

*For Northbound/Median, random clearing to begin at the I-20 WB and I-220 Split.

*For Southbound, random clearing to begin at the I-55 SB and I-220 Split.

Random Clearing				
Hinds County - 302				
Beginning STA	Ending STA	Avg. Depth (ft)	ACRE	Location
69+30	85+85	35	1.33	NB
90+70	101+25	52	1.26	NB
461+00	484+00	70	3.70	NB
568+80	573+00	30	0.29	NB
638+85	646+80	42	0.77	NB
567+00	572+50	37	0.47	SB
494+50	498+50	60	0.55	NB
485+80	493+20	31	0.53	SB
463+50	476+50	37	1.10	SB
89+00	112+00	47	2.48	SB
64+85	87+00	31	1.58	SB
Total (By STA)		137.30		
Madison County - 301				
739+75	744+20	52	0.53	SB
730+50	735+00	38	0.39	SB
Total (By STA)		8.95		

Random Clearing - Median				
Madison County - 301				
Beginning STA	Ending STA	Avg. Depth (ft)	ACRE	Location
743+50	750+25	102	1.58	Median
752+90	775+96	171	9.05	Median
Total (By STA)		29.81		

- * REMOVAL OF ALL GUARDRAIL (BRIDGE END SECTIONS, W-BEAM, TYPE-I CABLE ANCHORAGE, TERMINAL END SECTIONS, ETC.) WILL BE PAID UNDER PAY ITEM 202-B REMOVAL OF GUARD RAIL.
- * REMOVAL OF GUARDRAIL DELINEATORS ARE CONSIDERED INCIDENTAL TO THE REMOVAL OF GUARDRAIL AND WILL NOT BE MEASURED AS A SEPARATE PAY ITEM.
- * ALL GUARDRAIL, POSTS, BLOCKOUTS, CONCRETE ANCHORS, HARDWARE, ETC. WILL BE THE PROPERTY OF THE CONTRACTOR.
- * TOTAL GUARDRAIL LENGTH IS BASED ON A TERMINAL END SECTION 37.5' LONG. IF A TERMINAL END SECTION OF A DIFFERENT LENGTH IS USED, THE LENGTH OF THE W-BEAM MAY HAVE TO BE ADJUSTED.
- * REMOVAL OF OBJECT MARKERS WILL NOT BE MEASURED AS A SEPARATE PAY ITEM AND SHALL BE ABSORBED IN OTHER ITEMS

- * REMOVAL OF ALL GUARDRAIL (BRIDGE END SECTIONS, W-BEAM, TYPE-I CABLE ANCHORAGE, TERMINAL END SECTIONS, ETC.) WILL BE PAID UNDER PAY ITEM 202-B REMOVAL OF GUARD RAIL.
- * REMOVAL OF GUARDRAIL DELINEATORS ARE CONSIDERED INCIDENTAL TO THE REMOVAL OF GUARDRAIL AND WILL NOT BE MEASURED AS A SEPARATE PAY ITEM.
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REMOVAL OF OBJECT MARKERS WILL NOT BE MEASURED AS A SEPARATE PAY ITEM AND SHALL BE ABSORBED IN OTHER ITEMS

GUARDRAIL QUANTITIES I-220, CROSSROADS AND RAMPS - 109480 / 302000																			
GUARDRAIL				TERMINAL				CABLE				BRIDGE END SECTION				DELINEATORS			
Location (LT/RT)		W-BEAM (LF)	THRIE BEAM TRANS. SECT. (LF)	THRIE BEAM (LF)	FLARED END SECT. (EA)	NON-FLARED END SECT. (EA)	ANCHOR TYPE 1 (EA)	TYPE "D", MOD (EA)	TYPE "H" (EA)	TYPE "H" MOD. (EA)	TYPE "I" (EA)	TYPE "G" MOD(EA)	WHITE (EA)	YELLOW (EA)	TYPE 3 OBJECT MARKERS (EA)	Vehicular Impact attenuator 50 MPH (EA)	GUARDRAIL REMOVAL (LF)		REMARKS
NB 220 to EB Capital St	LT	150				1						1		7	1		212.5		
NB 220 to EB Capital St	RT	150				1			1				7		1		212.5		
NB 220 to EB Capital St	RT	150				1						1	7		1		212.5		
WB Capital St Under 220	LT	175			1		1						7				225		
WB Capital St Under 220	RT	250			1		1						7				300		
Bolling st to NB 220	LT	150				1						1	7		1		212.5		
Clinton Blvd to SB 220	LT	150			1							1	7		1		212.5		
Clinton Blvd to SB 220	RT	150				1			1				5		1		137.5		
WB Hanging Moss Rd	RT	75				1		1					7		1		212.5		
EB Hanging Moss Rd	LT	150				1						1	4		1		75		
McCain Ave		12.5				1						1	4		1		75		
McCain Ave		12.5				1						1	4		1		75		
McCain Ave		12.5				1						1	4		1		75		
Officer Thomas Catchings		12.5				1						1	4		1		77		
Officer Thomas Catchings		12.5				1						1	4		1		77		
Officer Thomas Catchings		12.5				1						1	4		1		77		
Officer Thomas Catchings		12.5				1						1	4		1		77		
25+30	EB Clinton Blvd																		
35+00	Capitol St															1		Flared	
15+00	Watkins Dr.	Median														1		Flared	
17+00	Watkins Dr.	Median														1		Flared	
TOTAL =		1,650.00	0.00	0.00	3.00	15.00	2.00	2.00	2.00	0.00	0.00	12.00	93.00	7.00	16.00	4.00	2,756.00	LF.	

* REMOVAL OF ALL GUARDRAIL (BRIDGE END SECTIONS, W-BEAM, TYPE-I CABLE ANCHORAGE, TERMINAL END SECTIONS, ETC.) WILL BE PAID UNDER PAY ITEM 202-B REMOVAL OF GUARD RAIL.

* REMOVAL OF GUARDRAIL DELINEATORS ARE CONSIDERED INCIDENTAL TO THE REMOVAL OF GUARDRAIL AND WILL NOT BE MEASURED AS A SEPARATE PAY ITEM.

* ALL GUARDRAIL, POSTS, BLOCKOUTS, CONCRETE ANCHORS, HARDWARE, ETC. WILL BE THE PROPERTY OF THE CONTRACTOR.

* TOTAL GUARDRAIL LENGTH IS BASED ON A TERMINAL END SECTION 37.5' LONG. IF A TERMINAL END SECTION OF A DIFFERENT LENGTH IS USED, THE LENGTH OF THE W-BEAM MAY HAVE TO BE ADJUSTED.

REMOVAL OF OBJECT MARKERS WILL NOT BE MEASURED AS A SEPARATE PAY ITEM AND SHALL BE ABSORBED IN OTHER ITEMS

GUARDRAIL QUANTITIES I-220 NB																
Madison County - 109480 / 301000																
STATION	LOCATION (LT/RT)	GUARDRAIL		TERMINAL		BRIDGE END SECTION					DELINEATORS		REMARKS			
		W-BEAM (LF)	THRIE BEAM TRANS. SECT. (LF)	FLARED END SECT. (EA)	NON-FLARED END SECT. (EA)	ANCHOR TYPE 1 (EA)	TYPE "D", MOD (EA)	TYPE "H" (EA)	TYPE "I" (EA)	TYPE "G" MOD (EA)	WHITE (EA)	YELLOW (EA)	TYPE 3 OBJECT MARKERS (EA)	Vehicular impact attenuator		
														(70 MPH)	(EA)	
711+75	LT	293.75			1	1						9			343.75	
733+07	RT	150		1			1				8		1		212.5	
733+07	LT	150			1		1					8	1		212.5	
TOTAL =		593.75	0.00	1.00	2.00	1.00	2.00	0.00	0.00	0.00	8.00	17.00	2.00		768.75	
		LF.	LF.	EA.	EA.	EA.	EA.	EA.	EA.	EA.	EA.	EA.	EA.		EA	

GUARDRAIL QUANTITIES I-220 SB																
Madison County - 109480 / 301000																
STATION	location (LT/RT)	GUARDRAIL		TERMINAL		BRIDGE END SECTION					DELINEATORS		REMARKS			
		W-BEAM (LF)	THRIE BEAM TRANS. SECT. (LF)	FLARED END SECT. (EA)	NON-FLARED END SECT. (EA)	ANCHOR TYPE 1 (EA)	TYPE "D", MOD (EA)	TYPE "H" (EA)	TYPE "I" (EA)	TYPE "G" MOD (EA)	WHITE (EA)	YELLOW (EA)	TYPE 3 OBJECT MARKERS (EA)	Vehicular impact attenuator		
														(70 MPH)	(EA)	
736+63	LT	150		1			1				7		1		212.5	
736+63	RT	150		1			1					7	1		212.5	Exit 9
677+52	LT	150		1			1				7		1		212.5	Exit 8
678+12	RT	150		1			1					7	1		212.5	
TOTAL =		600.00	0.00	4.00	0.00	0.00	4.00	0.00	0.00	0.00	14.00	14.00	4.00		850.00	
		LF.	LF.	EA.	EA.	EA.	EA.	EA.	EA.	EA.	EA.	EA.	EA.		EA	

* REMOVAL OF ALL GUARDRAIL (BRIDGE END SECTIONS, W-BEAM, TYPE-I CABLE ANCHORAGE, TERMINAL END SECTIONS, ETC.) WILL BE PAID UNDER PAY ITEM 202-B REMOVAL OF GUARD RAIL.

* REMOVAL OF GUARDRAIL DELINEATORS ARE CONSIDERED INCIDENTAL TO THE REMOVAL OF GUARDRAIL AND WILL NOT BE MEASURED AS A SEPARATE PAY ITEM.

* ALL GUARDRAIL, POSTS, BLOCKOUTS, CONCRETE ANCHORS, HARDWARE, ETC. WILL BE THE PROPERTY OF THE CONTRACTOR.

* TOTAL GUARDRAIL LENGTH IS BASED ON A TERMINAL END SECTION 37.5' LONG. IF A TERMINAL END SECTION OF A DIFFERENT LENGTH IS USED, THE LENGTH OF THE W-BEAM MAY HAVE TO BE ADJUSTED.

REMOVAL OF OBJECT MARKERS WILL NOT BE MEASURED AS A SEPARATE PAY ITEM AND SHALL BE ABSORBED IN OTHER ITEMS

Hinds County - 109480 / 302000			
Undersealing			
Northbound			
Bridge Name	STA	907-420-A001 Undersealing (LBS)	Remarks
.5B	22+00	5000	South End
4.8B	244+00	5000	South End
7.7B	577+00	5000	South End
7.9B	593+00	5000	North End
Total		20000	

Hinds County - 109480 / 302000			
Undersealing			
Southbound			
Bridge Name	STA	907-420-A001 Undersealing (LBS)	Remarks
3.2A	168+00	5000	North End
4.8A	254+00	5000	North End
5.1A	264+00	5000	South End
7.7A	582+00	5000	North End
8.3A	617+00	5000	North End
Total		25000	

Madison County - 109480 / 301000			
Undersealing			
Southbound			
Bridge Name	STA	907-420-A001 Undersealing (LBS)	Remarks
10.6A	738+00	5000	North End
Total		5000	

Bridge Repairs								
Madison County - 109480 / 301000								
Bridge ID	Bridge #	907-808-A002 Joint Repair (LF)	907-823-A001 Preformed Joint Seal, Type I (LF)	907-823-B001 Saw Cut Type I (LF)	202-B276 Removal of Debris from Drainage Channel (LF)	201-D002 Random Clearing (ACRE)	907-824-A003 General Epoxy Repairs (SF)	General Epoxy Locations
12996	10.1	0	0	0	600	0.5	0	
12997	10.6A	160	80	160	0	0.5	0	
12993	10.6B	160	80	160	0	0.5	0	
12994	11.1A	0	0	0	0	0.5	0	
12995	11.1B	0	0	0	0	0.5	0	
Totals		320	160	320	600	2.5	0	

*Any areas disturbed by the Contractor shall be re-stabilized at no additional cost to the State.

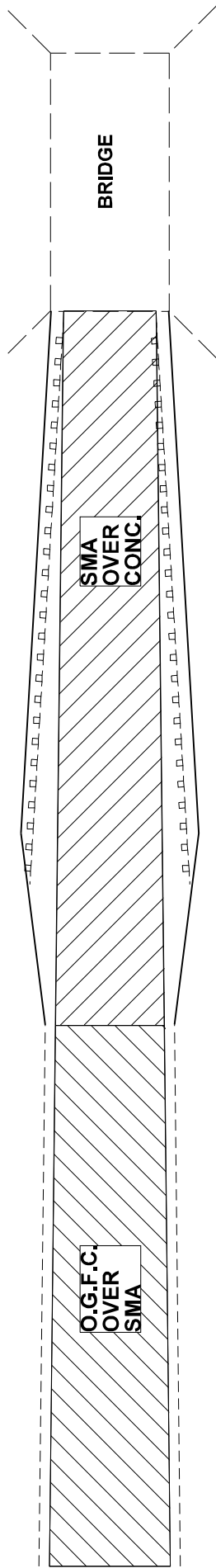
Bridge Repairs								
Hinds County - 109480 / 302000								
Bridge ID	Bridge #	907-808-A002 Joint Repair (LF)	907-823-A001 Preformed Joint Seal, Type I (LF)	907-823-B001 Saw Cut Type I (LF)	202-B276 Removal of Debris from Drainage Channel (LF)	201-D002 Random Clearing (ACRE)	907-824-A003 General Epoxy Repairs (SF)	General Epoxy Locations
11434	0.8B	0	0	0	0	0	4	Header Bent 4
11435	1.2A	164	82	164	0	0	0	
11452	1.2B	0	0	0	0	0	11	Rail Span 2, Deck Span 1
11453	1.8A	0	0	0	0	0.5	12	Rail SE and NW Corners
11436	1.8B	172	86	172	0	0.5	1	Rail NW Corner
11437	2.5	0	0	0	800	0.5	0	
11438	2.6B	0	0	0	0	0	21	Deck Various Locations
11439	2.6C	0	0	0	0	0	1	Header Bent 6
11496	2.6D	0	0	0	0	2	0	
11456	2.7A	0	0	0	0	0	12	Header Bent 1
11457	2.7B	0	0	0	446	0.5	0	
11450	2.7C	0	0	0	0	0	3	Rail at Soffit NW Corner
11463	4.8A	0	0	0	0	0	7	Rail Spans 3 and 5
11464	4.8B	0	0	0	0	0	2	Deck Spans 1 and 4
11440	5.1B	0	0	0	0	0	6	Deck Span 1, Header Bent 1
11441	5.6	0	0	0	670	0.5	0	
11444	6.0	0	0	0	0	0.5	0	
11449	7.4A	0	0	0	0	0.5	0	
11466	7.4B	0	0	0	0	0.5	0	
11455	7.9A	270	135	270	0	0	0	
11460	7.9B	270	135	270	0	0	66	Deck Various Locations
11461	8.2	0	0	0	0	0.5	0	
11474	8.5	0	0	0	1200	0.5	0	
11475	9.5B	0	0	0	0	0.5	0	
11472	9.6A	328	164	328	0	0	72	Deck Various Locations
11473	9.6B	324	162	324	0	0	2	Deck Span 3
Totals		1528	764	1528	3116	8	220	

*Any areas disturbed by the Contractor shall be re-stabilized at no additional cost to the State.


Bridge Repairs					
Hinds County - 109480 / 302000					
Bridge ID	Bridge #	202-B169 Removal of Joint Material (LF)	907-823-A002 Preformed Joint Seal, Type II (LF)	907-823-B002 Saw Cut Type II (LF)	907-824-PP007 Bridge Repair, Elastomeric Concrete (CY)
11448	6.1B	106	53	106	2

Thermoplastic Double Drop Legend Locations Hinds County - 109480 / 302000				
Station	Location	Legend/Symbol	QTY	Unit
58+29	I-220 NB RRL	Lane-Reduction Arrow	40	SF
122+56	I-220 SB LLL	Lane-Reduction Arrow	40	SF
208+47	I-220 NB RRL	Lane-Reduction Arrow	40	SF
211+47	I-220 NB RRL	Lane-Reduction Arrow	40	SF
243+30	I-220 SB LLL	Lane-Reduction Arrow	40	SF
248+80	I-220 SB LLL	Lane-Reduction Arrow	40	SF
264+35	I-220 SB LLL	Lane-Reduction Arrow	40	SF
675+40	I-220 NB RRL	Lane-Reduction Arrow	40	SF
TOTAL			320	SF
Thermoplastic Double Drop Legend Locations Madison County - 109480 / 301000				
Station	Location	Legend/Symbol	QTY	Unit
680+55	I-220 NB RRL	Lane-Reduction Arrow	40	SF
751+36	I-220 SB LLL	Lane-Reduction Arrow	40	SF
761+86	I-220 SB LLL	Lane-Reduction Arrow	40	SF
TOTAL			120	SF
Remarks: Tables reference new thermoplastic legend locations to be added. Changes in legend locations shall not be made without approval from the Engineer. Short skips (2' - 12') shall be required at locations where lane begins to reduce from 12'. Payment for detail stripe shall be made using the appropriate pay item.				

The diagram illustrates the construction details for a bridge approach. The cross-section (top) shows a bridge deck labeled "Bridge" with "Brd. End Paving" at its edge. On either side of the bridge is a shoulder area. The shoulder is constructed with layers: "Soil Sterilant", "Geotextile Fabric", and "Crushed Stone". A note "Shoulder paving required" points to the shoulder surface. A "50' Minimum Taper Required" is indicated for the transition from the bridge end paving to the shoulder. The plan view (bottom) shows the bridge deck and the approach on both sides. The approach is labeled "asphalt paving stops" and "crushed stone". Below the crushed stone is a layer of "geotextile fabric" and "soil sterilant". A "4\"



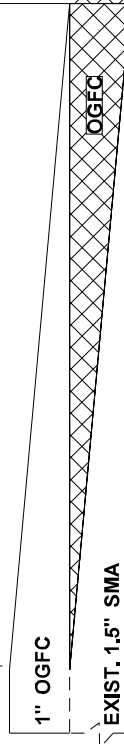
 -OGFC OVER SMA PAVING LIMITS

 - SMA PAVING LIMITS

BRIDGE LIMITS - APPROACH/DEPARTURE

 MILL 0" to 1" for 100' for O.G.F.C. OVERLAY Over existing SMA

 - SMA Area to be milled and replaced with SMA



Notice to Bidders No. _____

1.5" SMA

BRIDGE DECK

EXISTING CONC. PAVEMENT

SMA MILLING REQUIREMENTS

(NOT TO SCALE)

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
AB	REVISION
PAVING LIMITS SMA MILLING REQUIREMENTS @ BRIDGES	
PROJ. NO.:	
COUNTY:	
FILE NAME:	DATE
DESIGN TEAM	DATE
WORKING NUMBER PLBR-1	
SHEET NUMBER	

Note:
 Weep holes shall be required through the shoulder to prevent water from ponding in the milled areas as directed by Engineer.

EPOXY MORTAR REPAIR NOTES:

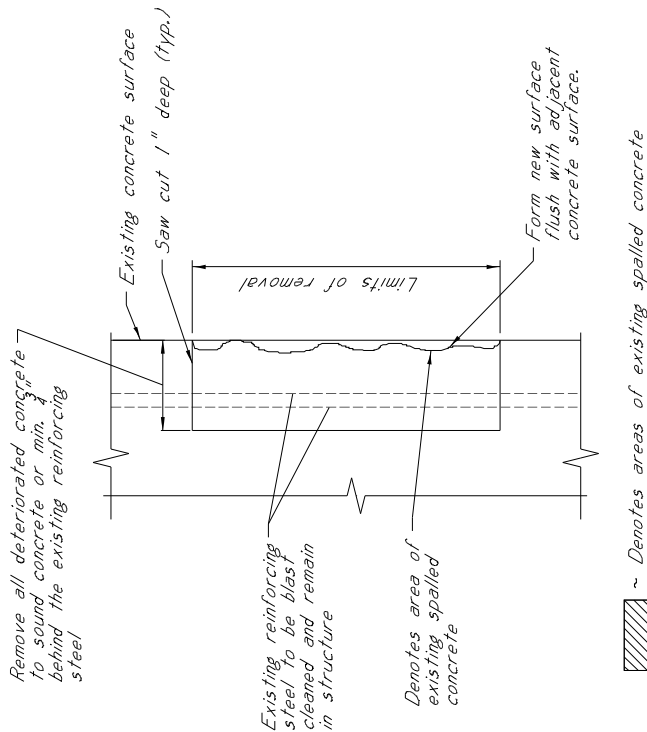
1. Repair concrete spalled areas on the bridge as directed by the Project Engineer using epoxy mortar.
2. Repair all concrete spalled areas listed on this page and as directed by the Project Engineer.
3. Repair any additional concrete spalled areas not listed on this page as directed by the Project Engineer.
4. Contractor shall sawcut around the perimeter of the damaged and unsound concrete.
5. The Contractor shall determine the depth of reinforcement prior to any saw cutting.
6. Spalled areas where pack rust has developed around or on reinforcement shall be removed by small hand tools or pressure washing (using 3500 psi pressure). Hammers used to remove concrete shall be limited to 30 pounds.
7. All areas of the bridge repaired with epoxy mortar shall be restored to the original dimensions and details as shown in the information plans, unless noted otherwise.
8. Materials:
 - a. Epoxy Resin: Resin shall be selected from the MDOT approved materials list.
 - b. Silica Sand: Silica sand material shall be bagged general purpose blast cleaning sand.
 - c. Epoxy Mortar Mix: Epoxy mortar mix shall consist of part liquid epoxy and part clean, dry sand mixed in the ratio recommended by the manufacturer.
9. Application:
 - a. A representative of the epoxy manufacturer must be present for sufficient time to ensure the Contractor is properly schooled in the use of the epoxy materials.
 - b. Prior to placement of the mortar mix the prepared surface shall be lightly primed with neat epoxy.
 - c. Curing time shall be in accordance with manufacturer's recommendations.
10. The cost of saw cutting, removing spalled or cracked concrete, cleaning exposed reinforcing steel, patching material, labor and any miscellaneous materials necessary to complete the repairs as shown shall be paid for on a square foot basis as Bridge Repair, Epoxy Repair. This item shall be bid such that this item may be increased, decreased, or eliminated as directed by the Project Engineer.

EPOXY BINDER

Contact areas where new concrete or epoxy mortar is placed against old concrete shall be cleaned then coated with an approved epoxy binder designed to bond new concrete to old. The binder shall be applied in accordance with the Manufacturer's recommendations.

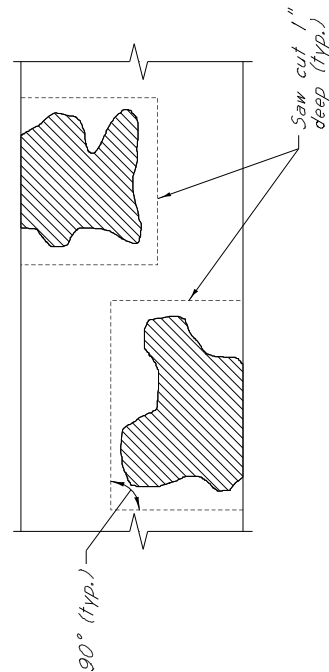
1" SAWCUT NOTES:

All 1" sawcuts shall be considered an absorbed item of work. The Contractor shall verify depth of reinforcing steel before making any sawcuts. The depth of the sawcut shall be no more than the depth of the reinforcing steel. Any damage to reinforcing steel shall be repaired to the satisfaction of the Engineer at no cost to the State.



*NOTE:

Saw cut existing concrete 1" deep so as to obtain a rectangular area. All existing reinforcement shall be carefully preserved and blast cleaned.



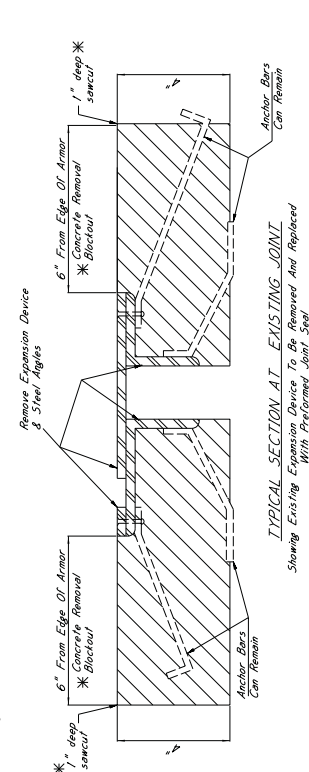
EPOXY MORTAR SPALL REPAIR DETAIL

★ 1" SAWCUT NOTES:

All 1" Sawcuts Shall Be Considered An Absorbed Item of Work. The Contractor Shall Verify Depth Of Reinforcing Steel Before Making Any Sawcuts. The Depth Of The Reinforcing Steel Shall Be Repaired To The Satisfaction Of The Engineer At No Cost To The State.

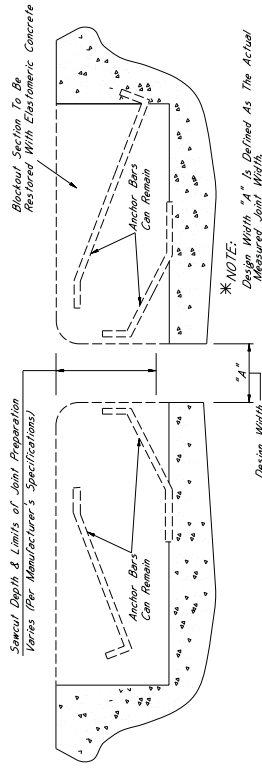
★ CONCRETE REMOVAL BLOCKOUT NOTES

Removal Of The Concrete Blockout Area Shall Be Considered An Absorbed Item Of Work Under Pay Item 202-B169. The Contractor Shall Use Hand Tools To Cut No Deeper Than 30 Lbs. To Complete This Work.



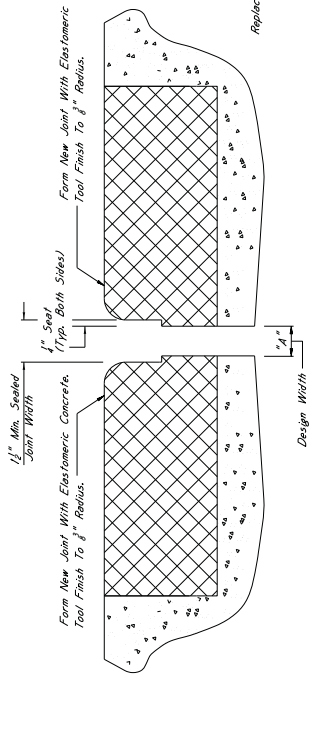
TYPICAL SECTION AT EXISTING JOINT

Showing Existing Expansion Device To Be Removed And Replaced With Preformed Joint Seal



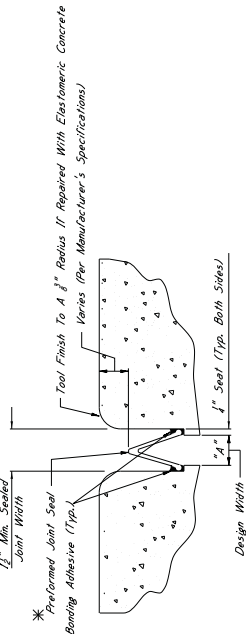
TYPICAL SECTION AT JOINT AFTER REMOVAL OF EXISTING SEAL

Showing Limits Of Joint Preparation For Application Of New Joint Seal Materials



TYPICAL SECTION AT SAWCUT & JOINT REPAIR

Showing Area Where Repairs Are Made After Sawcut With Elastomeric Concrete

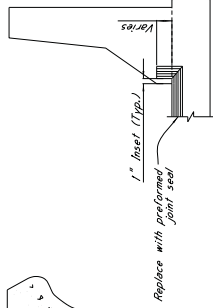


TYPICAL SECTION AT SAWCUT & SEALED JOINT

Showing Sealed Joint After Sawcut And Repair With Elastomeric Concrete

★ NOTES:

- The Preformed Joint Seal Shall Be One Of The Following, Installed According To The Manufacturer's Specifications:
 - A. Silcoflex Joint Sealing System Manufactured By R.J. Watson, Inc. In Alden, NY www.rjwatson.com
 - B. Wabco SP5 Joint System Manufactured By Wabco Bowman Acme Corporation In Amherst, NY www.wabcorp.com
 - C. Silspec S55 Silicone Strip Seal Manufactured By SSI Commercial & Highway Construction Materials www.ssi.com
- For Elastomeric Repairs, The R.J. Watson Silcoflex Joint Sealing System Was Selected. However, Should Another Supplier Be Chosen, It Is The Contractor's Responsibility To Ensure That The Manufacturer's Recommendations Are Followed. The Manufacturer's Recommendations Shall Be Provided To The Engineer And Any Other Variance Between The Specifications Provided By The Manufacturer, A Manufacturer Representative Shall Be Present At The Time Joint Sealing Begins Material. The Contractor Is Properly Schooled In Installation Of The Joint Material.
- Which Shall Be Sealed At Their Design Width, Dimension "A", Which Is Defined As:
 - The Actual Width Of The Joint Opening. This Width Does Not Account For The Seal Applied On Both Sides Of The Joint. The Preformed Joint Seal, Type I, Shall Be Used On Joints Greater Than 1/2" Deep. The Sealant Shall Be Applied To The Joint Using A Tool With A Depth Greater Than 1/2" To Seal To The Minimum Required Joint Seal Dimension Being 3". Type III Seal Be Used For Design Width Greater Than 1/2" Design Width. The Minimum Design Width Being 3". In Cases Where The Design Width Is Greater Than 3", The Contractor Shall Be Responsible To Ensure That The Size Selected Is Appropriate For The Width Of The Joint.



ELEVATION AT END OF SPAN

NOTES ON ASSOCIATED ITEMS OF WORKS:

202-B169 REMOVAL OF EXISTING JOINT MATERIAL

Description: Shall Include The Removal Of Material Associated With Existing Joint Material. The Contractor Shall Remove All Material As Designated In The Detail Drawings Provided. Removal Of The Concrete Blockout Area Shall Be Absorbed Under This Item of Work. On Work, The Contractor Shall Not Be Included Under This Item of Work. The Contractor Shall Be Responsible For The Engineer's Removal Of Joint Material And Any Trash And Debris (Including But Not Limited To Compacted Dirt, Vegetation And Trash) Located At Any Depth Within The Joint Shall Be Included Under This Item of Work.

Basis Of Payment: Removal Of Armor And Slicing Plate Joint Material Will Be Paid For In Linear Feet At The Contract Unit Price Of The Centerline Joint, While Removal Of Nonarmor Joint Material Will Only Be Paid For As The Length Along The Centerline Of The Joint.

907-823-0001 SAW CUT, TYPE I,
907-823-0002 SAW CUT, TYPE II,
907-823-0003 SAW CUT, TYPE III

Description: The Saw Cut Depth Shall Be Equivalent To The Installation Depth Required By The Manufacturer's Specifications. The Saw Cut Type Shall Be The Same As The Preformed Joint Seal Selected.

Basis Of Payment: The Accepted Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Bridge Deck On Each Side Of The Centerline Joint.

907-823-0001 PREFORMED JOINT SEAL, TYPE I,
907-823-0002 PREFORMED JOINT SEAL, TYPE II,
907-823-0003 PREFORMED JOINT SEAL, TYPE III

Description: Shall Include The Manufacturer's Required Joint Preparation Including Sandblasting Both Sides Of The Joint And Blowing The Joint Free Of Debris With Compressed Air And Placement Of The New Preformed Joint Seal

Basis Of Payment: The Accepted Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Centerline Joint.

ELASTOMERIC CONCRETE NOTES

907-824-PP007 BRIDGE REPAIR, ELASTOMERIC CONCRETE

Description: Elastomeric Concrete Shall Be One Of The Following Products, Installed According To The Manufacturer's Specifications:

- A. Poly-Tron Elastomeric Concrete Manufactured By R.J. Watson, Inc. In Alden, NY www.rjwatson.com
- B. WabcoCrete II Manufactured By Wabco Bowman Acme Corporation In Amherst, NY www.wabcorp.com
- C. Delcrete Elastomeric Concrete Manufactured By The D.S. Brown Company In North Baltimore, OH www.dsbrown.com

Basis Of Payment: The Accepted Quantities Will Be Paid For In Cubic Yards At The Contract Unit Price.

GENERAL NOTES:

- Specifications: Mississippi Standard Specifications For Road And Bridge Construction, 2017.
- Approval Of The Director Of Structures, State Bridge Engineer, May Be Authorized By The Bridge Engineer Provided Such Changes Do Not Affect The Safety Or Structural Integrity Of The Work For Which No Pay Item Is Provided In The Proposal Will Not Be Paid For Directly And Shall Therefore Be Considered An Absorbed Item of Work.

"General Decision Number: MS20250134 05/02/2025

Superseded General Decision Number: MS20240134

State: Mississippi

Construction Type: Highway

County: Hinds County in Mississippi.

HIGHWAY CONSTRUCTION PROJECTS

Note: Contracts subject to the Davis-Bacon Act are generally required to pay at least the applicable minimum wage rate required under Executive Order 14026 or Executive Order 13658. Please note that these Executive Orders apply to covered contracts entered into by the federal government that are subject to the Davis-Bacon Act itself, but do not apply to contracts subject only to the Davis-Bacon Related Acts, including those set forth at 29 CFR 5.1(a)(1).

If the contract is entered into on or after January 30, 2022, or the contract is renewed or extended (e.g., an option is exercised) on or after January 30, 2022:	<ul style="list-style-type: none"> . Executive Order 14026 generally applies to the contract. . The contractor must pay all covered workers at least \$17.75 per hour (or the applicable wage rate listed on this wage determination, if it is higher) for all hours spent performing on the contract in 2025.
If the contract was awarded on or between January 1, 2015 and January 29, 2022, and the contract is not renewed or extended on or after January 30, 2022:	<ul style="list-style-type: none"> . Executive Order 13658 generally applies to the contract. . The contractor must pay all covered workers at least \$13.30 per hour (or the applicable wage rate listed on this wage determination, if it is higher) for all hours spent performing on that contract in 2025.

The applicable Executive Order minimum wage rate will be adjusted annually. If this contract is covered by one of the Executive Orders and a classification considered necessary for performance of work on the contract does not appear on this wage determination, the contractor must still submit a conformance request.

Additional information on contractor requirements and worker protections under the Executive Orders is available at <http://www.dol.gov/whd/govcontracts>.

Modification Number	Publication Date
0	01/03/2025
1	05/02/2025

* ELEC0480-010 01/01/2025

	Rates	Fringes
TRAFFIC SIGNALIZATION		
Electrician.....	\$ 30.35	11.39

* SUMS2010-057 08/04/2014		

	Rates	Fringes
CARPENTER (Form Work Only).....	\$ 13.73 **	0.00
CEMENT MASON/CONCRETE FINISHER...	\$ 13.93 **	0.00
ELECTRICIAN.....	\$ 24.04	5.87
HIGHWAY/PARKING LOT STRIPING:		
Truck Driver (Line Striping		
Truck).....	\$ 11.81 **	0.00
INSTALLER - GUARDRAIL.....	\$ 12.07 **	0.00
INSTALLER - SIGN.....	\$ 12.13 **	0.00
IRONWORKER, REINFORCING.....	\$ 15.47 **	0.00
LABORER: Common or General,		
Including Asphalt Raking,		
Shoveling, Spreading; and		
Grade Checking.....	\$ 10.32 **	0.00
LABORER: Flagger.....	\$ 9.69 **	0.00
LABORER: Luteman.....	\$ 12.88 **	0.00
LABORER: Mason Tender -		
Cement/Concrete.....	\$ 12.11 **	0.00
LABORER: Pipelayer.....	\$ 13.44 **	0.00
LABORER: Laborer-Cones/		
Barricades/Barrels -		
Setter/Mover/Sweeper.....	\$ 10.39 **	0.00
OPERATOR: Asphalt Spreader.....	\$ 14.71 **	0.00
OPERATOR:		
Backhoe/Excavator/Trackhoe.....	\$ 16.01 **	0.00
OPERATOR: Bobcat/Skid		
Steer/Skid Loader.....	\$ 11.64 **	0.00
OPERATOR: Broom/Sweeper.....	\$ 9.75 **	0.00
OPERATOR: Bulldozer.....	\$ 13.87 **	0.00
OPERATOR: Concrete Saw.....	\$ 14.38 **	0.00
OPERATOR: Crane.....	\$ 21.33	0.00
OPERATOR: Distributor.....	\$ 10.25 **	0.00
OPERATOR: Grader/Blade.....	\$ 14.31 **	0.00

OPERATOR: Grinding/Grooving Machine.....	\$ 15.90 **	0.00
OPERATOR: Loader.....	\$ 11.96 **	0.00
OPERATOR: Mechanic.....	\$ 15.20 **	0.00
OPERATOR: Milling Machine.....	\$ 14.68 **	0.00
OPERATOR: Mixer.....	\$ 14.25 **	0.00
OPERATOR: Oiler.....	\$ 12.13 **	0.00
OPERATOR: Paver (Asphalt, Aggregate, and Concrete).....	\$ 11.59 **	0.00
OPERATOR: Roller (All Types)....	\$ 11.53 **	0.00
OPERATOR: Scraper.....	\$ 12.25 **	0.00
OPERATOR: Tractor.....	\$ 11.81 **	0.00
TRUCK DRIVER: Flatbed Truck.....	\$ 14.06 **	0.00
TRUCK DRIVER: Lowboy Truck.....	\$ 12.56 **	0.00
TRUCK DRIVER: Mechanic.....	\$ 13.00 **	0.00
TRUCK DRIVER: Water Truck.....	\$ 10.00 **	0.00
TRUCK DRIVER: Dump Truck (All Types).....	\$ 11.39 **	0.00
TRUCK DRIVER: Semi/Trailer Truck.....	\$ 14.60 **	0.00

WELDERS - Receive rate prescribed for craft performing operation to which welding is incidental.

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** Workers in this classification may be entitled to a higher minimum wage under Executive Order 14026 (\$17.75) or 13658 (\$13.30). Please see the Note at the top of the wage determination for more information. Please also note that the minimum wage requirements of Executive Order 14026 are not currently being enforced as to any contract or subcontract to which the states of Texas, Louisiana, or Mississippi, including their agencies, are a party.

Note: Executive Order (EO) 13706, Establishing Paid Sick Leave for Federal Contractors applies to all contracts subject to the Davis-Bacon Act for which the contract is awarded (and any solicitation was issued) on or after January 1, 2017. If this contract is covered by the EO, the contractor must provide employees with 1 hour of paid sick leave for every 30 hours they work, up to 56 hours of paid sick leave each year. Employees must be permitted to use paid sick leave for their own illness, injury or other health-related needs, including preventive care; to assist a family member (or person who is like family to the employee) who is ill, injured, or has other health-related needs, including preventive care; or for reasons resulting from, or to assist a family member (or person who is like family to the employee) who is a victim of, domestic

violence, sexual assault, or stalking. Additional information on contractor requirements and worker protections under the EO is available at <https://www.dol.gov/agencies/whd/government-contracts>.

Unlisted classifications needed for work not included within the scope of the classifications listed may be added after award only as provided in the labor standards contract clauses (29CFR 5.5 (a) (1) (iii)).

The body of each wage determination lists the classifications and wage rates that have been found to be prevailing for the type(s) of construction and geographic area covered by the wage determination. The classifications are listed in alphabetical order under rate identifiers indicating whether the particular rate is a union rate (current union negotiated rate), a survey rate, a weighted union average rate, a state adopted rate, or a supplemental classification rate.

Union Rate Identifiers

A four-letter identifier beginning with characters other than ""SU"", ""UAVG"", ?SA?, or ?SC? denotes that a union rate was prevailing for that classification in the survey. Example: PLUM0198-005 07/01/2024. PLUM is an identifier of the union whose collectively bargained rate prevailed in the survey for this classification, which in this example would be Plumbers. 0198 indicates the local union number or district council number where applicable, i.e., Plumbers Local 0198. The next number, 005 in the example, is an internal number used in processing the wage determination. The date, 07/01/2024 in the example, is the effective date of the most current negotiated rate.

Union prevailing wage rates are updated to reflect all changes over time that are reported to WHD in the rates in the collective bargaining agreement (CBA) governing the classification.

Union Average Rate Identifiers

The UAVG identifier indicates that no single rate prevailed for those classifications, but that 100% of the data reported for the classifications reflected union rates. EXAMPLE: UAVG-OH-0010 01/01/2024. UAVG indicates that the rate is a weighted union average rate. OH indicates the State of Ohio. The next number, 0010 in the example, is an internal number used in producing the wage determination. The date, 01/01/2024 in the example, indicates the date the wage determination was updated to reflect the most current union average rate.

A UAVG rate will be updated once a year, usually in January, to reflect a weighted average of the current rates in the collective bargaining agreements on which the rate is based.

Survey Rate Identifiers

The ""SU"" identifier indicates that either a single non-union rate prevailed (as defined in 29 CFR 1.2) for this classification in the survey or that the rate was derived by computing a weighted average rate based on all the rates reported in the survey for that classification. As a weighted

average rate includes all rates reported in the survey, it may include both union and non-union rates. Example: SUFL2022-007 6/27/2024. SU indicates the rate is a single non-union prevailing rate or a weighted average of survey data for that classification. FL indicates the State of Florida. 2022 is the year of the survey on which these classifications and rates are based. The next number, 007 in the example, is an internal number used in producing the wage determination. The date, 6/27/2024 in the example, indicates the survey completion date for the classifications and rates under that identifier.

?SU? wage rates typically remain in effect until a new survey is conducted. However, the Wage and Hour Division (WHD) has the discretion to update such rates under 29 CFR 1.6(c)(1).

State Adopted Rate Identifiers

The ""SA"" identifier indicates that the classifications and prevailing wage rates set by a state (or local) government were adopted under 29 C.F.R 1.3(g)-(h). Example: SAME2023-007 01/03/2024. SA reflects that the rates are state adopted. ME refers to the State of Maine. 2023 is the year during which the state completed the survey on which the listed classifications and rates are based. The next number, 007 in the example, is an internal number used in producing the wage determination. The date, 01/03/2024 in the example, reflects the date on which the classifications and rates under the ?SA? identifier took effect under state law in the state from which the rates were adopted.

WAGE DETERMINATION APPEALS PROCESS

1) Has there been an initial decision in the matter? This can be:

- a) a survey underlying a wage determination
- b) an existing published wage determination
- c) an initial WHD letter setting forth a position on a wage determination matter
- d) an initial conformance (additional classification and rate) determination

On survey related matters, initial contact, including requests for summaries of surveys, should be directed to the WHD Branch of Wage Surveys. Requests can be submitted via email to davisbaconinfo@dol.gov or by mail to:

Branch of Wage Surveys
Wage and Hour Division
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

Regarding any other wage determination matter such as conformance decisions, requests for initial decisions should be directed to the WHD Branch of Construction Wage Determinations. Requests can be submitted via email to BCWD-Office@dol.gov or by mail to:

Branch of Construction Wage Determinations
Wage and Hour Division
U.S. Department of Labor
200 Constitution Avenue, N.W.

Washington, DC 20210

2) If an initial decision has been issued, then any interested party (those affected by the action) that disagrees with the decision can request review and reconsideration from the Wage and Hour Administrator (See 29 CFR Part 1.8 and 29 CFR Part 7). Requests for review and reconsideration can be submitted via email to dba.reconsideration@dol.gov or by mail to:

Wage and Hour Administrator
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

The request should be accompanied by a full statement of the interested party's position and any information (wage payment data, project description, area practice material, etc.) that the requestor considers relevant to the issue.

3) If the decision of the Administrator is not favorable, an interested party may appeal directly to the Administrative Review Board (formerly the Wage Appeals Board). Write to:

Administrative Review Board
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210.

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END OF GENERAL DECISION"

"General Decision Number: MS20250137 05/02/2025

Superseded General Decision Number: MS20240137

State: Mississippi

Construction Type: Highway

County: Madison County in Mississippi.

HIGHWAY CONSTRUCTION PROJECTS

Note: Contracts subject to the Davis-Bacon Act are generally required to pay at least the applicable minimum wage rate required under Executive Order 14026 or Executive Order 13658. Please note that these Executive Orders apply to covered contracts entered into by the federal government that are subject to the Davis-Bacon Act itself, but do not apply to contracts subject only to the Davis-Bacon Related Acts, including those set forth at 29 CFR 5.1(a)(1).

If the contract is entered into on or after January 30, 2022, or the contract is renewed or extended (e.g., an option is exercised) on or after January 30, 2022:	<ul style="list-style-type: none"> . Executive Order 14026 generally applies to the contract. . The contractor must pay all covered workers at least \$17.75 per hour (or the applicable wage rate listed on this wage determination, if it is higher) for all hours spent performing on the contract in 2025.
If the contract was awarded on or between January 1, 2015 and January 29, 2022, and the contract is not renewed or extended on or after January 30, 2022:	<ul style="list-style-type: none"> . Executive Order 13658 generally applies to the contract. . The contractor must pay all covered workers at least \$13.30 per hour (or the applicable wage rate listed on this wage determination, if it is higher) for all hours spent performing on that contract in 2025.

The applicable Executive Order minimum wage rate will be adjusted annually. If this contract is covered by one of the Executive Orders and a classification considered necessary for performance of work on the contract does not appear on this wage determination, the contractor must still submit a conformance request.

Additional information on contractor requirements and worker protections under the Executive Orders is available at <http://www.dol.gov/whd/govcontracts>.

Modification Number	Publication Date
0	01/03/2025
1	05/02/2025

* ELEC0480-010 01/01/2025

	Rates	Fringes
TRAFFIC SIGNALIZATION		
Electrician.....	\$ 30.35	11.39

* SUMS2010-060 08/04/2014

	Rates	Fringes
CARPENTER (Form Work Only).....	\$ 12.32 **	0.00
CEMENT MASON/CONCRETE FINISHER...	\$ 12.85 **	0.39
ELECTRICIAN.....	\$ 24.04	5.87
HIGHWAY/PARKING LOT STRIPING: Truck Driver (Line Striping Truck).....	\$ 10.86 **	0.00
INSTALLER - GUARDRAIL.....	\$ 12.07 **	0.00
INSTALLER - SIGN.....	\$ 11.54 **	0.00
IRONWORKER, REINFORCING.....	\$ 15.52 **	0.00
LABORER: Common or General, Including Asphalt Raking, Shoveling, Spreading; and Grade Checking.....	\$ 10.38 **	0.00
LABORER: Flagger.....	\$ 9.91 **	0.00
LABORER: Luteman.....	\$ 12.88 **	0.00
LABORER: Mason Tender - Cement/Concrete.....	\$ 11.27 **	0.00
LABORER: Pipelayer.....	\$ 13.44 **	0.00
LABORER: Laborer-Cones/ Barricades/Barrels - Setter/Mover/Sweeper.....	\$ 10.26 **	0.00
OPERATOR: Asphalt Spreader.....	\$ 14.71 **	0.00
OPERATOR: Backhoe/Excavator/Trackhoe.....	\$ 14.37 **	0.00
OPERATOR: Bobcat/Skid Steer/Skid Loader.....	\$ 11.64 **	0.00
OPERATOR: Broom/Sweeper.....	\$ 10.48 **	0.00
OPERATOR: Bulldozer.....	\$ 13.88 **	0.00
OPERATOR: Concrete Saw.....	\$ 15.50 **	0.00
OPERATOR: Crane.....	\$ 15.00 **	0.00
OPERATOR: Distributor.....	\$ 10.95 **	0.00
OPERATOR: Grader/Blade.....	\$ 14.28 **	0.00

OPERATOR: Grinding/Grooving Machine.....	\$ 15.90 **	0.00
OPERATOR: Loader.....	\$ 13.28 **	0.00
OPERATOR: Mechanic.....	\$ 13.00 **	0.00
OPERATOR: Milling Machine.....	\$ 14.68 **	0.00
OPERATOR: Mixer.....	\$ 14.25 **	0.00
OPERATOR: Oiler.....	\$ 12.35 **	0.00
OPERATOR: Paver (Asphalt, Aggregate, and Concrete).....	\$ 11.74 **	0.00
OPERATOR: Roller (All Types)....	\$ 10.85 **	0.00
OPERATOR: Scraper.....	\$ 12.25 **	0.00
OPERATOR: Tractor.....	\$ 10.56 **	0.00
TRUCK DRIVER: Flatbed Truck.....	\$ 14.06 **	0.00
TRUCK DRIVER: Lowboy Truck.....	\$ 12.08 **	0.00
TRUCK DRIVER: Mechanic.....	\$ 13.00 **	0.00
TRUCK DRIVER: Water Truck.....	\$ 10.00 **	0.00
TRUCK DRIVER: Dump Truck (All Types).....	\$ 11.02 **	0.00
TRUCK DRIVER: Semi/Trailer Truck.....	\$ 12.50 **	0.00

WELDERS - Receive rate prescribed for craft performing operation to which welding is incidental.

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** Workers in this classification may be entitled to a higher minimum wage under Executive Order 14026 (\$17.75) or 13658 (\$13.30). Please see the Note at the top of the wage determination for more information. Please also note that the minimum wage requirements of Executive Order 14026 are not currently being enforced as to any contract or subcontract to which the states of Texas, Louisiana, or Mississippi, including their agencies, are a party.

Note: Executive Order (EO) 13706, Establishing Paid Sick Leave for Federal Contractors applies to all contracts subject to the Davis-Bacon Act for which the contract is awarded (and any solicitation was issued) on or after January 1, 2017. If this contract is covered by the EO, the contractor must provide employees with 1 hour of paid sick leave for every 30 hours they work, up to 56 hours of paid sick leave each year. Employees must be permitted to use paid sick leave for their own illness, injury or other health-related needs, including preventive care; to assist a family member (or person who is like family to the employee) who is ill, injured, or has other health-related needs, including preventive care; or for reasons resulting from, or to assist a family member (or person who is like family to the employee) who is a victim of, domestic

violence, sexual assault, or stalking. Additional information on contractor requirements and worker protections under the EO is available at <https://www.dol.gov/agencies/whd/government-contracts>.

Unlisted classifications needed for work not included within the scope of the classifications listed may be added after award only as provided in the labor standards contract clauses (29CFR 5.5 (a) (1) (iii)).

The body of each wage determination lists the classifications and wage rates that have been found to be prevailing for the type(s) of construction and geographic area covered by the wage determination. The classifications are listed in alphabetical order under rate identifiers indicating whether the particular rate is a union rate (current union negotiated rate), a survey rate, a weighted union average rate, a state adopted rate, or a supplemental classification rate.

Union Rate Identifiers

A four-letter identifier beginning with characters other than ""SU"", ""UAVG"", ?SA?, or ?SC? denotes that a union rate was prevailing for that classification in the survey. Example: PLUM0198-005 07/01/2024. PLUM is an identifier of the union whose collectively bargained rate prevailed in the survey for this classification, which in this example would be Plumbers. 0198 indicates the local union number or district council number where applicable, i.e., Plumbers Local 0198. The next number, 005 in the example, is an internal number used in processing the wage determination. The date, 07/01/2024 in the example, is the effective date of the most current negotiated rate.

Union prevailing wage rates are updated to reflect all changes over time that are reported to WHD in the rates in the collective bargaining agreement (CBA) governing the classification.

Union Average Rate Identifiers

The UAVG identifier indicates that no single rate prevailed for those classifications, but that 100% of the data reported for the classifications reflected union rates. EXAMPLE: UAVG-OH-0010 01/01/2024. UAVG indicates that the rate is a weighted union average rate. OH indicates the State of Ohio. The next number, 0010 in the example, is an internal number used in producing the wage determination. The date, 01/01/2024 in the example, indicates the date the wage determination was updated to reflect the most current union average rate.

A UAVG rate will be updated once a year, usually in January, to reflect a weighted average of the current rates in the collective bargaining agreements on which the rate is based.

Survey Rate Identifiers

The ""SU"" identifier indicates that either a single non-union rate prevailed (as defined in 29 CFR 1.2) for this classification in the survey or that the rate was derived by computing a weighted average rate based on all the rates reported in the survey for that classification. As a weighted

average rate includes all rates reported in the survey, it may include both union and non-union rates. Example: SUFL2022-007 6/27/2024. SU indicates the rate is a single non-union prevailing rate or a weighted average of survey data for that classification. FL indicates the State of Florida. 2022 is the year of the survey on which these classifications and rates are based. The next number, 007 in the example, is an internal number used in producing the wage determination. The date, 6/27/2024 in the example, indicates the survey completion date for the classifications and rates under that identifier.

?SU? wage rates typically remain in effect until a new survey is conducted. However, the Wage and Hour Division (WHD) has the discretion to update such rates under 29 CFR 1.6(c)(1).

State Adopted Rate Identifiers

The ""SA"" identifier indicates that the classifications and prevailing wage rates set by a state (or local) government were adopted under 29 C.F.R 1.3(g)-(h). Example: SAME2023-007 01/03/2024. SA reflects that the rates are state adopted. ME refers to the State of Maine. 2023 is the year during which the state completed the survey on which the listed classifications and rates are based. The next number, 007 in the example, is an internal number used in producing the wage determination. The date, 01/03/2024 in the example, reflects the date on which the classifications and rates under the ?SA? identifier took effect under state law in the state from which the rates were adopted.

----- WAGE DETERMINATION APPEALS PROCESS

1) Has there been an initial decision in the matter? This can be:

- a) a survey underlying a wage determination
- b) an existing published wage determination
- c) an initial WHD letter setting forth a position on a wage determination matter
- d) an initial conformance (additional classification and rate) determination

On survey related matters, initial contact, including requests for summaries of surveys, should be directed to the WHD Branch of Wage Surveys. Requests can be submitted via email to davisbaconinfo@dol.gov or by mail to:

Branch of Wage Surveys
Wage and Hour Division
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

Regarding any other wage determination matter such as conformance decisions, requests for initial decisions should be directed to the WHD Branch of Construction Wage Determinations. Requests can be submitted via email to BCWD-Office@dol.gov or by mail to:

Branch of Construction Wage Determinations
Wage and Hour Division
U.S. Department of Labor
200 Constitution Avenue, N.W.

Washington, DC 20210

2) If an initial decision has been issued, then any interested party (those affected by the action) that disagrees with the decision can request review and reconsideration from the Wage and Hour Administrator (See 29 CFR Part 1.8 and 29 CFR Part 7). Requests for review and reconsideration can be submitted via email to dba.reconsideration@dol.gov or by mail to:

Wage and Hour Administrator
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

The request should be accompanied by a full statement of the interested party's position and any information (wage payment data, project description, area practice material, etc.) that the requestor considers relevant to the issue.

3) If the decision of the Administrator is not favorable, an interested party may appeal directly to the Administrative Review Board (formerly the Wage Appeals Board). Write to:

Administrative Review Board
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210.

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END OF GENERAL DECISION"

Remove & Replace OGFC Lift on approximately 2 miles of I-220 from the Hinds County Line to near the I-55 Divide Ramp & approximately 9 miles from the merge with I-20 to the Madison County Line, known as Federal Aid Project Nos. IM-0220-03(036) / 109480301 & IM-0220-03(036) / 109480302 in Madison & Hinds Counties.

Line No.	Item Code	Adj Code	Quantity	Units	Description [Fixed Unit Price]
Roadway Items					
0010	201-D001		177	Station	Random Clearing
0020	201-D002		11	Acre	Random Clearing
0030	202-B009		301	Square Yard	Removal of Asphalt Pavement, Failed Areas
0040	202-B158		25,800	Linear Feet	Removal of Guard Rail, Including Rails, Posts and Terminal Ends
0050	202-B162		7	Each	Removal of Impact Attenuator
0060	202-B169		106	Linear Feet	Removal of Joint Material
0070	202-B240		45,319	Linear Feet	Removal of Traffic Stripe
0080	202-B244		100	Each	Removal of Trees
0090	203-G002	(E)	1,471	Cubic Yard	Excess Excavation, LVM, AH
0100	209-A005		9,185	Square Yard	Geotextile Stabilization, Type V, Non-Woven
0110	223-A001		1,588	Acre	Mowing [\$50.00]
0120	304-D002	(GT)	1,722	Ton	Granular Material, Crushed Stone
0130	406-D001		363,729	Square Yard	Fine Milling of Bituminous Pavement, All Depths
0140	407-A001	(A2)	8,859	Gallon	Asphalt for Tack Coat
0150	503-C010		24,524	Linear Feet	Saw Cut, Full Depth
0160	606-B003		20,234	Linear Feet	Guard Rail, Class A, Type 1, 'W' Beam, Metal Post
0170	606-C003		16	Each	Guard Rail, Cable Anchor, Type 1
0180	606-D012		30	Each	Guard Rail, Bridge End Section, Type D Modified
0190	606-D018		24	Each	Guard Rail, Bridge End Section, Type G, Modified
0200	606-D020		21	Each	Guard Rail, Bridge End Section, Type H, Metal Post
0210	606-D022		6	Each	Guard Rail, Bridge End Section, Type I
0220	606-E005		34	Each	Guard Rail, Terminal End Section, Flared
0230	606-E007		61	Each	Guard Rail, Terminal End Section, Non-Flared
0240	619-A1001		67	Mile	Temporary Traffic Stripe, Continuous White
0250	619-A2001		45	Mile	Temporary Traffic Stripe, Continuous Yellow
0260	619-A3001		58	Mile	Temporary Traffic Stripe, Skip White
0270	619-A5001		255,561	Linear Feet	Temporary Traffic Stripe, Detail
0280	619-D1001		64	Square Feet	Standard Roadside Construction Signs, Less than 10 Square Feet
0290	619-D2001		480	Square Feet	Standard Roadside Construction Signs, 10 Square Feet or More
0300	619-G4005		96	Linear Feet	Barricades, Type III, Single Faced
0310	620-A001		1	Lump Sum	Mobilization
0320	629-A002		4	Each	Vehicular Impact Attenuator, 50 MPH

Line No.	Item Code	Adj Code	Quantity	Units	Description [Fixed Unit Price]
0330	629-A005		3	Each	Vehicular Impact Attenuator, 70 MPH
0340	630-F006		354	Each	Delineators, Guard Rail, White
0350	630-F007		402	Each	Delineators, Guard Rail, Yellow
0360	630-F010		350	Each	Delineators, Post Mounted, Double White
0370	630-F011		100	Each	Delineators, Post Mounted, Double Yellow
0380	630-F012		475	Each	Delineators, Post Mounted, Single White
0390	630-F013		450	Each	Delineators, Post Mounted, Single Yellow
0400	630-G005		79	Each	Type 3 Object Markers, OM-3R or OM-3L, Post Mounted
0410	907-201-D001		9	Mile	Random Clearing
0420	907-402-A002	(BA1)	12,960	Ton	Open Graded Friction Course, 9.5-mm Mixture
0430	907-402-B001	(A3)	38,566	Gallon	Bituminous Tack Coat
0440	907-403-A013	(BA1)	2,102	Ton	9.5-mm, HT, Asphalt Pavement
0450	907-403-B004	(BA1)	306	Ton	19-mm, HT, Asphalt Pavement, Leveling
0460	907-403-S002		14,913	Linear Feet	Joint Sealant
0470	907-405-A001	(BA1)	7,062	Ton	Stone Matrix Asphalt, 9.5 mm Mixture
0480	907-420-A001		50,000	Pounds	Undersealing
0490	907-618-A001		1	Lump Sum	Maintenance of Traffic
0500	907-618-M2001		760	Hours	Work Zone Law Enforcement [\$60.00]
0510	907-626-A007		31	Mile	6" Thermoplastic Double Drop Traffic Stripe, Skip White
0520	907-626-B004		32	Mile	6" Thermoplastic Double Drop Traffic Stripe, Continuous White
0530	907-626-E003		17	Mile	6" Thermoplastic Double Drop Traffic Stripe, Continuous Yellow
0540	907-626-G004		68,862	Linear Feet	Thermoplastic Detail Stripe, White
0550	907-626-G005		16,595	Linear Feet	Thermoplastic Detail Stripe, Yellow
0560	907-626-H006		440	Square Feet	Thermoplastic Double Drop Legend, White
0580	907-627-A001		2,520	Each	Type A, Non-Reflective White Raised Jiggle Markers
0590	907-627-A002		1,240	Each	Type AY, Non-Reflective Yellow Raised Jiggle Markers
0600	907-627-K001		9,468	Each	Red-Clear Reflective High Performance Raised Markers
0610	907-627-K002		2,956	Each	Red-Yellow Reflective High Performance Raised Markers
Bridge Items					
0620	907-808-A002	(S)	1,848	Linear Feet	Joint Repair
0630	907-823-A001		924	Linear Feet	Preformed Joint Seal, Type I
0640	907-823-A002		53	Linear Feet	Preformed Joint Seal, Type II
0650	907-823-B001		1,848	Linear Feet	Saw Cut, Type I
0660	907-823-B002		106	Linear Feet	Saw Cut, Type II

Section 905

Proposal (Sheet 2 - 3)

IM-0220-03(036)/109480301, IM-0220-03(036)/109480302

MADISON, HINDS

Line No.	Item Code	Adj Code	Quantity	Units	Description [Fixed Unit Price]
0670	907-824-A003		220	Square Feet	General Epoxy Repair
0680	907-824-PP007		2	Cubic Yard	Bridge Repair, Elastomeric Concrete