

## SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO.	<u>1</u>	DATED	<u>8/21/2025</u>	ADDENDUM NO.	_____	DATED	_____
ADDENDUM NO.	_____	DATED	_____	ADDENDUM NO.	_____	DATED	_____
ADDENDUM NO.	_____	DATED	_____	ADDENDUM NO.	_____	DATED	_____

Number

Description

1 Revised Notice to Bidder No. 7156; Amendment EBSx  
Download Required.

TOTAL ADDENDA: 1

(Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE \_\_\_\_\_

Contractor

BY \_\_\_\_\_

Signature

TITLE \_\_\_\_\_

ADDRESS \_\_\_\_\_

CITY, STATE, ZIP \_\_\_\_\_

PHONE \_\_\_\_\_

FAX \_\_\_\_\_

E-MAIL \_\_\_\_\_

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of \_\_\_\_\_ and the names, titles and business addresses of the executives are as follows:

President

Address

Secretary

Address

Treasurer

Address

The following is my (our) itemized proposal.

SP-2175-00(017)/ 109048301000

Pike County(ies)

Revised 01/26/2016

# MISSISSIPPI DEPARTMENT OF TRANSPORTATION

**SECTION 904 – NOTICE TO BIDDERS NO. 7156**

**CODE: (SP)**

**DATE:** 8/19/2025

**SUBJECT:** Scope of Work

**PROJECT:** SP-2175-00(017) / 109048301 -- Pike County

The contract documents do not include an official set of construction plans, but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, “Standard Drawings.”

The work to be accomplished using the pay items and corresponding specifications set forth in the contract is to overlay the following section of US 51 in Pike County from Osyka to Magnolia.

<u>Route</u>	<u>Length</u> (Mile)	<u>Width</u> (Feet)	<u>Top Lift</u>	<u>Thickness</u> (Inches)
US 51	8	28' & var.	9.5-mm, ST	1½"

See sheet TS-1 for details.

Work on the Project shall consist of the following:

1. The Contractor shall erect and maintain construction signing, provide all signs, set up night time lane closures (if needed), and traffic handling devices in accordance with the Traffic Control Plan. The cost for this work is to be included in the price bid for pay item 907-618-A: Maintenance of Traffic. All traffic control devices on this project should comply with the latest version of the MUTCD. Fluorescent orange sheeting shall be used on all construction and traffic control signs except for those designated in the standards to be black legend and border on white background.
2. All MDOT owned right-of way along US 51 shall be cleared completely of trees greater than 4 inches in diameter in maturity. Careful consideration should be given to any above-ground utilities within the above-described limits, with clearing activities near those adhering to all applicable safety standards.

Cut trees may either be hauled away, mulched in place, or another method approved by the Engineer. Tree stumps shall be ground flush to the ground line unless directed otherwise by the Engineer. Material may not be mulched in residential areas.

Removal should be done so as not to disturb below-ground root systems that may cause instability or slope failure, potentially damaging the roadway substructure. Any removal method alternatives to grinding mechanisms to remove remaining stumps to ground level should be pre-approved by the District Engineer with a plan to ensure substructure failure is unlikely to occur. If roadside foreslopes are steeper than 3:1 or include other roadway

features that would make tree removal infeasible, risk assessment should be conducted by the Project Engineer and Highway Safety Division.

This work shall be paid for as 201-D001: Random Clearing, per station. Each side of the roadway shall be measured separately.

3. Prior to the overlay, the existing shoulders shall be clipped, and surplus material shall be spread along the edge of the shoulders, fore slopes, or other adjacent areas as directed by the Project Engineer, and will be an absorbed item.
4. The Contractor shall fine mill at the following locations:

<u>ROUTE</u>	<u>LOCATION</u>	<u>LENGTH</u>	<u>REMARKS</u>
US 51	BOP	150'	Tie In
US 51	361+00	1,025'	Bridge End and Hwy. 568 Intersection
US 51	364+63 to 374+59	996'	Bridge End (8.2) to Bridge End (8.4)
US 51	377+18	320'	Bridge End
US 51	EOP	150'	Tie In

**Local Roads**

St. Mary Dr.  
Chatawa Rd.  
Scott Furr Rd.  
SR 568  
Donald Dunn Rd.  
Beacham Hill Rd.

**As Directed**

5. The Contractor shall place ¾" and variable of 9.5-mm, ST, Leveling asphalt at the approximate locations as follows or as directed by the Engineer.

<b>Station</b>	<b>Lane</b>	<b>Length, LF</b>
<b>35+00 – 39+00</b>	<b>Both</b>	<b>400</b>
<b>54+00 – 58+00</b>	<b>Both</b>	<b>400</b>
<b>300+00 – 340+00</b>	<b>Both</b>	<b>4000</b>
<b>392+00 – EOP</b>	<b>Both</b>	<b>4000</b>

The Contractor shall remove existing pavement markers prior to placing asphalt. The cost of removing these pavement markers is to be absorbed in other items bid.

The Contractor shall take due care to maintain a uniform outside edge of pavement, and shall place asphalt to establish an approximate vertical face in order for granular material to be placed directly against the surface and not on a shelf of an underlying course.

6. The Contractor shall place top lifts of asphalt on the roadway left and right of the centerline from BOP to EOP as shown in TS-1. The finished cross-slope is to be 2% in tangent sections

and match the existing super elevation rate in horizontal curves.

Asphalt surface shall be placed on all local roads and driveway aprons (1½" Thickness).

Driveway aprons shall be paved 10' wide or as directed by the Engineer. All local roads shall be paved to the normal right-of-way line or as directed by the Engineer.

Asphalt quantities were estimated as follows:

Mainline	11,153 Tons
<b>Leveling</b>	<b>1,227 Tons</b>
Local Roads	184 Tons
Driveway Pads	343 Tons
Guardrail Pads	270 Tons

Note: The Contractor shall be responsible for traffic control while MDOT personnel conduct density testing on the asphalt. The cost shall be included in the price bid for pay item 907-618-A: Maintenance of Traffic.

7. The Contractor shall perform sawing and sealing of transverse joints in asphalt pavement at the locations indicated on TS-1.
8. The Contractor shall place granular material on the shoulders to raise the existing shoulders to the new grade, bladed, shaped, and compacted to a minimum slope of 4%. Granular material will not be allowed to be placed directly on the top lift of asphalt but must be placed directly on the gravel shoulder by means of a road widener machine approved by the Project Engineer. Light blading or mowing of the shoulders will be required prior to placement of the granular material. Please refer to Subsection 618.03.3 of the Standard Specifications as well as sheet SDTCP-16 for requirements related to pavement edge drop-offs.
9. The Contractor shall place rumble strips in the locations indicated in the typical section on sheet TS-1.
10. The Contractor shall place all permanent pavement markings, including stripe and raised pavement markers, throughout the project as required by the Standard Drawings or as directed by the Engineer.

The Contractor shall be responsible for replacing guardrail delineators throughout the project as required by the Standard Drawings or as directed by the Engineer.

An asphalt taper shall be placed at the temporary joints caused by the milling or overlay in order to provide for the safe movement of traffic. The taper shall be three feet (3') in length per one inch of depth and will be an absorbed item.

Temporary striping shall be required after milling and overlaying operations: Temporary striping shall be placed in the same locations and layout as permanent stripe. All centerline, lane lines, edge lines, and no passing stripes that have been removed during the day's operations shall be replaced

with temporary stripe before work is discontinued for the day or as soon thereafter as weather conditions will permit, except that:

- Replacement of no-passing stripes may be delayed for a period not to exceed three (3) days for a two or three lane road.
- Temporary edge lines on projects requiring shoulders constructed of granular material may be delayed for a period not to exceed three (3) days.

All asphalt and concrete curbs along local roads from BOP to EOP shall be painted (two applications) with white traffic paint and traffic beads as shown on sheet DCIS-1; such costs shall be included in other items bid.

It shall be the responsibility of the Contractor to protect the roadway and all existing structures, such as bridges, culverts, signs, and curbs, from damage occurring as a result of the Contractor's operations. Damages to existing structures caused by the Contractor's operations shall be repaired or replaced at no cost to the Department.

Incidental work such as removing vegetation, shaping and compaction of shoulders, removing excess asphalt material, project clean-up, and other incidental work necessary to complete the project will not be measured for separate payment. Such costs shall be included in the price of other items bid.

It is the Contractor's responsibility to ensure the drainage of surface water from milled areas. Temporary wedges (paper joints) of full lane width asphalt shall be placed by the Contractor immediately after the fine milling process to allow the safe transition of traffic. These wedges shall be maintained in a satisfactory condition by the Contractor until the permanent asphalt is placed, cost to be absorbed.



MISSISSIPPI DEPARTMENT OF TRANSPORTATION

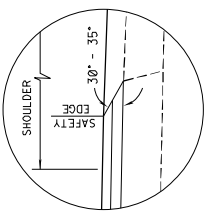
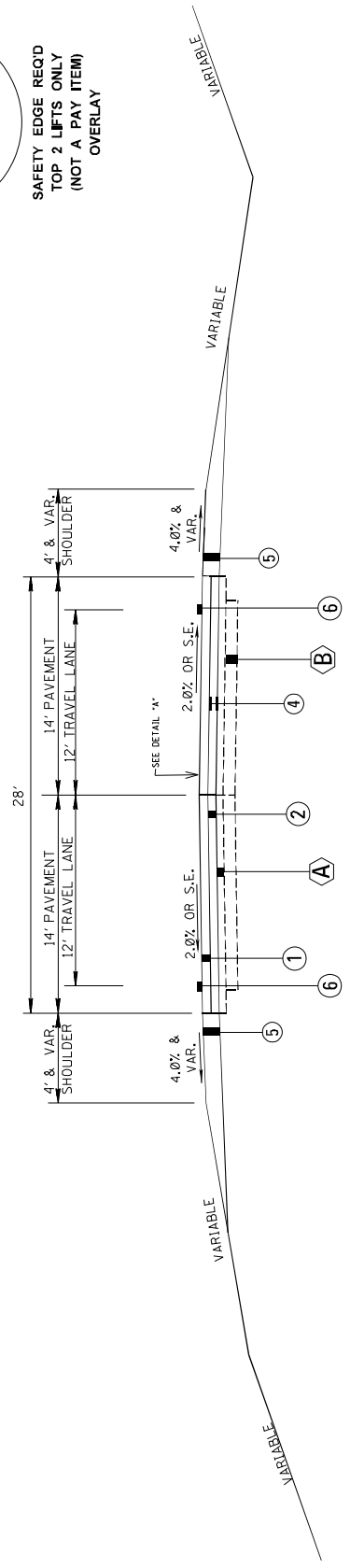
DESIGNED BY:  
CHECKED BY:  
DATE:

FMS CON: 109048/301000  
PROJECT NO.: SP-2175-00(07)-5  
COUNTY: PIKE

Notice to Bidders No. 7156 -- Construction

WKS. NO. 151  
SHEET NO. 1

US 51  
CL



SAFETY EDGE REQ'D  
TOP 2 LIFTS ONLY  
(NOT A PAY ITEM)  
OVERLAY

TYPICAL SECTION

B.O.P. STA. 0+00 - E.O.P. STA. 432+00

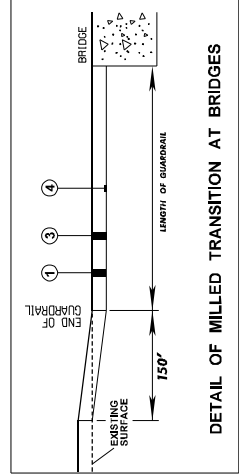
NOT TO SCALE

EXISTING

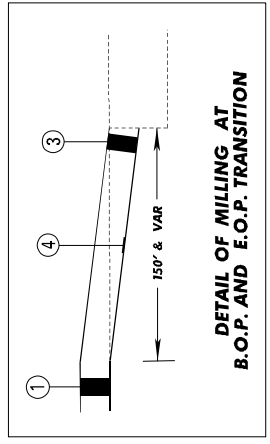
- A BITUMINOUS PAVEMENT IN PLACE
- B GRANULAR SUBGRADE IN PLACE

PROPOSED

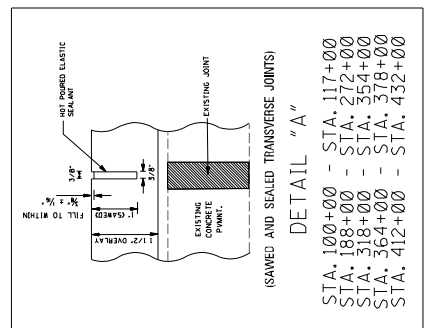
- 1 1.5" & VAR. OF 9.5MM ST ASPHALT REQ'D
- 2 0.75" & VAR. 9.5 MM ST LEVELING WHERE REQ'D
- 3 1.5" & VAR. FINE MILLING WHERE REQ'D
- 4 TACK COAT REQ'D
- 5 1.5" & VAR. OF CL. 6, GR. D GRANULAR MATERIAL REQ'D
- 6 RUMBLE STRIPE REQ'D (SEE SHEET RS-1 DETAIL B 14" PAVEMENT EDGE)



DETAIL OF MILLED TRANSITION AT BRIDGES



DETAIL OF MILLING AT  
B.O.P. AND E.O.P. TRANSITION



DETAIL "A"

(SAWED AND SEALED TRANSVERSE JOINTS)  
STA. 100+00 - STA. 117+00  
STA. 188+00 - STA. 272+00  
STA. 318+00 - STA. 354+00  
STA. 364+00 - STA. 378+00  
STA. 412+00 - STA. 432+00

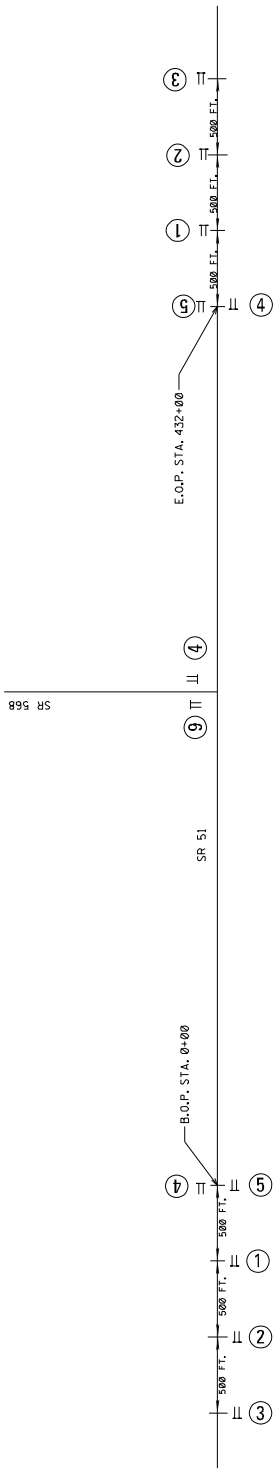


DESIGNED BY:	
DETAILED BY:	
CHECKED BY:	
DATE:	
FMS CON: 109048/301000	
PROJECT NO.: SP-2175-0000	
COUNTY: PIKE	

Notice to Bidders No. 7156	
State of Mississippi	

WORK NO.	109048/301000
SHEET NO.	3

NOTE: W20-1 (48"x48") SHALL BE REQ'D ON ALL LOCAL ROADS.



SIGN LEGEND	
①	W20-1 (48" X 48") 500 FT. AHEAD
②	W20-1 (48" X 48") 500 FT. AHEAD
③	W20-1 (48" X 48") 500 FT. AHEAD
④	W20-1 (48" X 48") 500 FT. AHEAD
⑤	W20-1 (48" X 48") 500 FT. AHEAD
⑥	W20-1 (48" X 48") 500 FT. AHEAD

ADDITIONAL TRAFFIC CONTROL SIGNS REQUIRED:

R4-1 "DO NOT PASS", R4-2 "PASS WITH CARE", AND W14-3 "NO PASSING ZONE" SIGNS ARE REQUIRED IN ACCORDANCE WITH SUB-SECTION 907-618 AND AS SPECIFIED IN THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, IF "NO PASSING ZONES" ARE 1,000' OR MORE. INSTALL ADDITIONAL "DO NOT PASS" CONTROL DEVICES, IF "NO PASSING ZONE" SIGNS ON MAXIMUM SPACING OF 750'.

ALL TRAFFIC CONTROL ITEMS SHALL BE INCLUDED IN BID FOR PAY ITEM 618-0001 MAINTENANCE OF TRAFFIC. FLUORESCENT ORANGE SHEETING SHALL BE USED ON ALL CONSTRUCTION AND TRAFFIC CONTROL SIGNS EXCEPT FOR THOSE DESIGNATED ON THE PLANS TO BE BLACK LEGEND AND BORDER ON WHITE BACKGROUND.



DESIGNED BY: \_\_\_\_\_  
CHECKED BY: \_\_\_\_\_  
DATE: \_\_\_\_\_

PROJECT NO.: SP-2175-04017  
COUNTY: PIKE  
FMS CON: 109048/301000

TRAFFIC CONTROL SIGNS  
-- 95% QUANTITIES --  
Notice to Bidders

WFO: TC00-1  
SHEET NO. 4

SIGNS REQUIRED (CONT'D)					SIGNS REQUIRED (CONT'D)				
SIGN NO.	SIZE	UNIT AREA REQ'D SQ. FT.	QUANTITY REQ'D	REMARKS	SIGN NO.	SIZE	UNIT AREA REQ'D SQ. FT.	QUANTITY REQ'D	REMARKS
W8-7	48" X 48"	16.00	1	LOOSE GRAVEL	W1-1L	48" X 48"	16.00	1	STOP
W8-9	48" X 48"	16.00	1	LOW SHOULDER	W1-1R	48" X 48"	16.00	1	STOP
W8-11	36" X 36"	9.00	1	UNEVEN LANES	W1-2L	48" X 48"	16.00	1	YIELD
W8-12	48" X 48"	16.00	1	NO CENTER STRIPE	W1-2R	48" X 48"	16.00	1	YIELD
W10-1	36" DIA.	7.67	1		W1-3L	48" X 48"	16.00	1	3-WAY, 4-WAY, ETC.
W10-1	48" DIA.	12.56	1		W1-3R	48" X 48"	16.00	1	3-WAY, 4-WAY, ETC.
W13-1	24" X 24"	4.00	1	XX MPH	W1-6L	48" X 48"	16.00	1	SPEED LIMIT
W14-3	36" X 48" X 48"	5.56	53	NO PASSING ZONE	W1-6R	48" X 48"	16.00	1	SPEED LIMIT
W14-3	48" X 44" X 64"	9.89	1	XXX FEET	W1-7L	48" X 48"	16.00	1	DO NOT ENTER
W16-2	24" X 18"	3.00	1	BRIDGE MAY ICE IN COLD WEATHER	W1-7R	48" X 48"	16.00	1	DO NOT ENTER
W19-2	48" X 48"	16.00	1	ADVANCE ROAD WORK	W1-8L	48" X 48"	16.00	1	WRONG WAY
W20-1	36" X 36"	9.00	1		W1-8R	48" X 48"	16.00	1	WRONG WAY
W20-2	48" X 48"	16.00	1	ADVANCE DETOUR	W3-3	48" X 48"	16.00	1	ONE WAY
W20-3	48" X 48"	16.00	1	ADVANCE ROAD CLOSED	W3-5	48" X 48"	16.00	1	ONE WAY
W20-4	48" X 48"	16.00	1	ADVANCE ROAD CLOSED	W3-6	48" X 48"	16.00	1	ONE WAY
W20-4b	48" X 48"	16.00	1	ADVANCE ROAD CLOSED	W3-7	48" X 48"	16.00	1	ONE WAY
W20-5L	48" X 48"	16.00	1	ADVANCE ROAD CLOSED	W3-8	48" X 48"	16.00	1	ONE WAY
W20-5R	48" X 48"	16.00	1	ADVANCE ROAD CLOSED	W3-9	48" X 48"	16.00	1	ONE WAY
W20-7a	48" X 48"	16.00	1	ADVANCE ROAD CLOSED	W3-10	48" X 48"	16.00	1	ONE WAY
W21-1	36" X 36"	9.00	1	WORKERS	W3-11	48" X 48"	16.00	1	ONE WAY
W21-1a	36" X 36"	9.00	1	WORKERS	W3-12	48" X 48"	16.00	1	ONE WAY
W21-2	36" X 36"	9.00	1	WORKERS	W3-13	48" X 48"	16.00	1	ONE WAY
W21-3	48" X 48"	16.00	1	WORKERS	W3-14	48" X 48"	16.00	1	ONE WAY
W21-5	48" X 48"	16.00	1	WORKERS	W3-15	48" X 48"	16.00	1	ONE WAY
W21-6	36" X 36"	16.00	1	WORKERS	W3-16	48" X 48"	16.00	1	ONE WAY
W24-1L	48" X 48"	16.00	1	WORKERS	W3-17	48" X 48"	16.00	1	ONE WAY
W24-1R	48" X 48"	16.00	1	WORKERS	W3-18	48" X 48"	16.00	1	ONE WAY
W24-1aL	48" X 48"	16.00	1	WORKERS	W3-19	48" X 48"	16.00	1	ONE WAY
W24-1aR	48" X 48"	16.00	1	WORKERS	W3-20	48" X 48"	16.00	1	ONE WAY
W24-1bL	48" X 48"	16.00	1	WORKERS	W3-21	48" X 48"	16.00	1	ONE WAY
W24-1bR	48" X 48"	16.00	1	WORKERS	W3-22	48" X 48"	16.00	1	ONE WAY
VP-4L	12" X 36"	3.00	1	WORKERS	W3-23	48" X 48"	16.00	1	ONE WAY
VP-4R	12" X 36"	3.00	1	WORKERS	W3-24	48" X 48"	16.00	1	ONE WAY
OM-3L	12" X 36"	3.00	1	WORKERS	W3-25	48" X 48"	16.00	1	ONE WAY
OM-3R	12" X 36"	3.00	1	WORKERS	W3-26	48" X 48"	16.00	1	ONE WAY
TOTAL SIGN AREA LESS THAN 10 SQ. FT.					W3-27	48" X 48"	16.00	1	ONE WAY
TOTAL SIGN AREA GREATER THAN 10 SQ. FT.					W3-28	48" X 48"	16.00	1	ONE WAY

SIGNS REQUIRED (CONT'D)					SIGNS REQUIRED (CONT'D)				
SIGN NO.	SIZE	UNIT AREA REQ'D SQ. FT.	QUANTITY REQ'D	REMARKS	SIGN NO.	SIZE	UNIT AREA REQ'D SQ. FT.	QUANTITY REQ'D	REMARKS
W1-1L	48" X 48"	16.00	1	STOP	W1-1L	48" X 48"	16.00	1	STOP
W1-1R	48" X 48"	16.00	1	STOP	W1-1R	48" X 48"	16.00	1	STOP
W1-2L	48" X 48"	16.00	1	YIELD	W1-2L	48" X 48"	16.00	1	YIELD
W1-2R	48" X 48"	16.00	1	YIELD	W1-2R	48" X 48"	16.00	1	YIELD
W1-3L	48" X 48"	16.00	1	3-WAY, 4-WAY, ETC.	W1-3L	48" X 48"	16.00	1	3-WAY, 4-WAY, ETC.
W1-3R	48" X 48"	16.00	1	3-WAY, 4-WAY, ETC.	W1-3R	48" X 48"	16.00	1	3-WAY, 4-WAY, ETC.
W1-6L	48" X 48"	16.00	1	SPEED LIMIT	W1-6L	48" X 48"	16.00	1	SPEED LIMIT
W1-6R	48" X 48"	16.00	1	SPEED LIMIT	W1-6R	48" X 48"	16.00	1	SPEED LIMIT
W1-7L	48" X 48"	16.00	1	DO NOT ENTER	W1-7L	48" X 48"	16.00	1	DO NOT ENTER
W1-7R	48" X 48"	16.00	1	DO NOT ENTER	W1-7R	48" X 48"	16.00	1	DO NOT ENTER
W1-8L	48" X 48"	16.00	1	WRONG WAY	W1-8L	48" X 48"	16.00	1	WRONG WAY
W1-8R	48" X 48"	16.00	1	WRONG WAY	W1-8R	48" X 48"	16.00	1	WRONG WAY
W3-3	48" X 48"	16.00	1	ONE WAY	W3-3	48" X 48"	16.00	1	ONE WAY
W3-5	48" X 48"	16.00	1	ONE WAY	W3-5	48" X 48"	16.00	1	ONE WAY
W3-6	48" X 48"	16.00	1	ONE WAY	W3-6	48" X 48"	16.00	1	ONE WAY
W3-7	48" X 48"	16.00	1	ONE WAY	W3-7	48" X 48"	16.00	1	ONE WAY
W3-8	48" X 48"	16.00	1	ONE WAY	W3-8	48" X 48"	16.00	1	ONE WAY
W3-9	48" X 48"	16.00	1	ONE WAY	W3-9	48" X 48"	16.00	1	ONE WAY
W3-10	48" X 48"	16.00	1	ONE WAY	W3-10	48" X 48"	16.00	1	ONE WAY
W3-11	48" X 48"	16.00	1	ONE WAY	W3-11	48" X 48"	16.00	1	ONE WAY
W3-12	48" X 48"	16.00	1	ONE WAY	W3-12	48" X 48"	16.00	1	ONE WAY
W3-13	48" X 48"	16.00	1	ONE WAY	W3-13	48" X 48"	16.00	1	ONE WAY
W3-14	48" X 48"	16.00	1	ONE WAY	W3-14	48" X 48"	16.00	1	ONE WAY
W3-15	48" X 48"	16.00	1	ONE WAY	W3-15	48" X 48"	16.00	1	ONE WAY
W3-16	48" X 48"	16.00	1	ONE WAY	W3-16	48" X 48"	16.00	1	ONE WAY
W3-17	48" X 48"	16.00	1	ONE WAY	W3-17	48" X 48"	16.00	1	ONE WAY
W3-18	48" X 48"	16.00	1	ONE WAY	W3-18	48" X 48"	16.00	1	ONE WAY
W3-19	48" X 48"	16.00	1	ONE WAY	W3-19	48" X 48"	16.00	1	ONE WAY
W3-20	48" X 48"	16.00	1	ONE WAY	W3-20	48" X 48"	16.00	1	ONE WAY
W3-21	48" X 48"	16.00	1	ONE WAY	W3-21	48" X 48"	16.00	1	ONE WAY
W3-22	48" X 48"	16.00	1	ONE WAY	W3-22	48" X 48"	16.00	1	ONE WAY
W3-23	48" X 48"	16.00	1	ONE WAY	W3-23	48" X 48"	16.00	1	ONE WAY
W3-24	48" X 48"	16.00	1	ONE WAY	W3-24	48" X 48"	16.00	1	ONE WAY
W3-25	48" X 48"	16.00	1	ONE WAY	W3-25	48" X 48"	16.00	1	ONE WAY
W3-26	48" X 48"	16.00	1	ONE WAY	W3-26	48" X 48"	16.00	1	ONE WAY
W3-27	48" X 48"	16.00	1	ONE WAY	W3-27	48" X 48"	16.00	1	ONE WAY
W3-28	48" X 48"	16.00	1	ONE WAY	W3-28	48" X 48"	16.00	1	ONE WAY

NOTES

- 1 STANDARD
- 2 SPECIAL (USE WHERE WARRANTED)
- 3 INTERSTATE ROUTE MARKER
- 4 STATE ROUTE MARKER
- 5 BLACK STRIPES ON YELLOW BACKGROUND
- 6 INTERSTATE USE ONLY
- 7 TOP OF SIGN - BLACK LETTERING ON ORANGE BACKGROUND, BOTTOM OF SIGN - BLACK LETTERING ON WHITE BACKGROUND
- 8 THE BACKGROUND OF ALL WARNING SIGNS ("W" SERIES) EXCEPT W10-1 SHALL BE ORANGE. THE W10-1 BACKGROUND SHALL BE YELLOW IN ALL CASES.
- 9 COLORS OF CARDINAL DIRECTIONAL MARKERS AND DIRECTIONAL ROUTE MARKERS.

SIGNS REQUIRED					SIGNS REQUIRED						
SIGN NO.	SIZE	UNIT AREA REQ'D SQ. FT.	QUANTITY REQ'D	TOTAL SIGN AREA SQ. FT.	REMARKS	SIGN NO.	SIZE	UNIT AREA REQ'D SQ. FT.	QUANTITY REQ'D	TOTAL SIGN AREA SQ. FT.	REMARKS
G20 - 1	60" X 24"	10.00	2	20	ROAD WORK NEXT XX MILES	G20 - 2	48" X 24"	8.00	3	24	END ROAD WORK PILOT CAR FOLLOW ME
G20 - 4	36" X 18"	4.50									1 OR 2 DIGIT
M1 - 1	24" X 24"	4.00									3 DIGIT
M1 - 1	30" X 24"	5.00									1 OR 2 DIGIT
M1 - 4	24" X 24"	4.00									3 DIGIT
M1 - 4	30" X 24"	5.00									1 OR 2 DIGIT
M1 - 5	24" X 24"	4.00									3 DIGIT
M1 - 5	30" X 24"	5.00									3 DIGIT
M3 - 1	24" X 12"	2.00									NORTH - 1 OR 2 DIGIT RATE MARKER
M3 - 1	30" X 13"	3.13									DIGIT RATE MARKER
M3 - 2	24" X 12"	2.00									EAST - 1 OR 2 DIGIT RATE MARKER
M3 - 2	30" X 13"	3.13									DIGIT RATE MARKER
M3 - 3	24" X 12"	2.00									DIGIT RATE MARKER
M3 - 3	30" X 13"	3.13									DIGIT RATE MARKER
M3 - 4	24" X 12"	2.00									WEST - 1 OR 2 DIGIT RATE MARKER
M3 - 4	30" X 13"	3.13									DIGIT RATE MARKER
M4 - 8	24" X 12"	2.00									DIGIT RATE MARKER
M4 - 8	30" X 13"	3.13									DIGIT RATE MARKER
M4 - 9	48" X 36"	12.00									DETOUR ↑
M4 - 9L	48" X 36"	12.00									DETOUR ↔
M4 - 9BL	48" X 36"	12.00									DETOUR ↵
M4 - 9SL	48" X 36"	12.00									DETOUR ↘
M4 - 9BSL	48" X 36"	12.00									DETOUR ↙
M4 - 9R	48" X 36"	12.00									DETOUR ↗
M4 - 9BR	48" X 36"	12.00									DETOUR ↖
M4 - 9SR	48" X 36"	12.00									DETOUR ↗
M4 - 9BSR	48" X 36"	12.00									DETOUR ↖
M4 - 10L	48" X 18"	6.00									DETOUR □
M4 - 10R	48" X 18"	6.00									DETOUR ▢
M4 - 5	24" X 12"	2.00									TO
M5 - 1L	21" X 13"	2.19									↗
M5 - 1R	21" X 13"	2.19									↖
M5 - 2L	21" X 15"	2.19									↘
M5 - 2R	21" X 15"	2.19									↙
M6 - 1L	21" X 13"	2.19									↖
M6 - 1R	21" X 13"	2.19									↗
M6 - 2L	21" X 15"	2.19									↖
M6 - 2R	21" X 15"	2.19									↗
M6 - 3	21" X 13"	2.19									↑





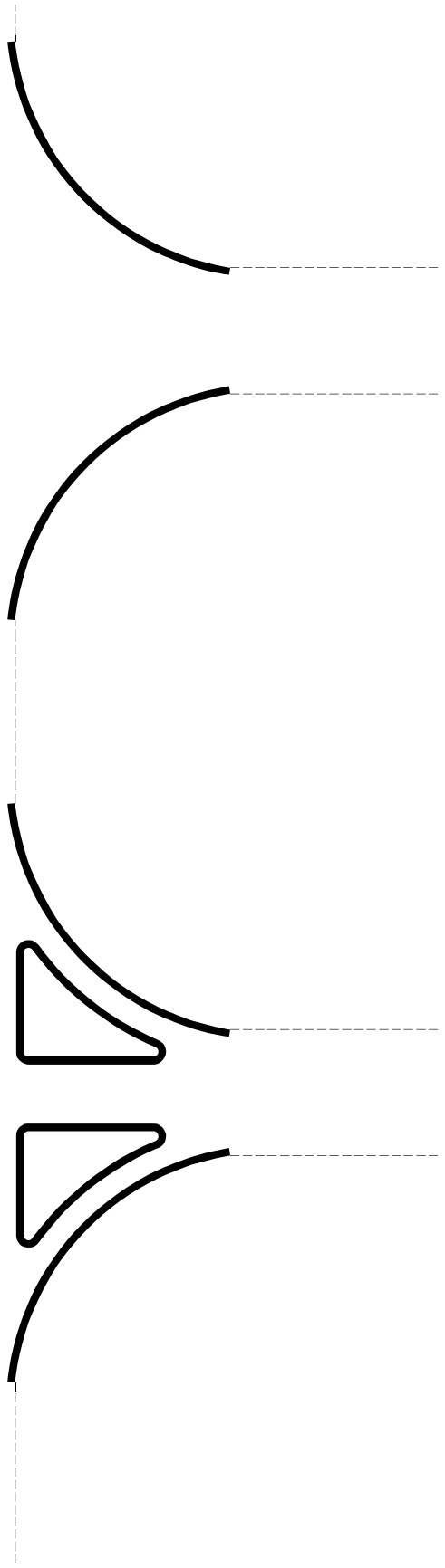
DESIGNED BY:	
DETAILED BY:	
CHECKED BY:	
DATE:	

FMS CON: 109048/301000
PROJECT NO.: SP-2175-00(000)-8
COUNTY: PIKE

Notice to Bidders No. 7156 - Cont.	
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WKS NO. 1
DISSES
SHEET NO. 5

\*\* ALL ASPHALT AND CONCRETE CURBS ALONG RAMPS, LOCAL ROADS, ETC. FROM B.O.P. TO E.O.P. SHALL BE PAINTED (TWO APPLICATIONS) WITH WHITE TRAFFIC PAINT AND TRAFFIC BEADS; COST TO BE ABSORBED IN OTHER PAY ITEMS.

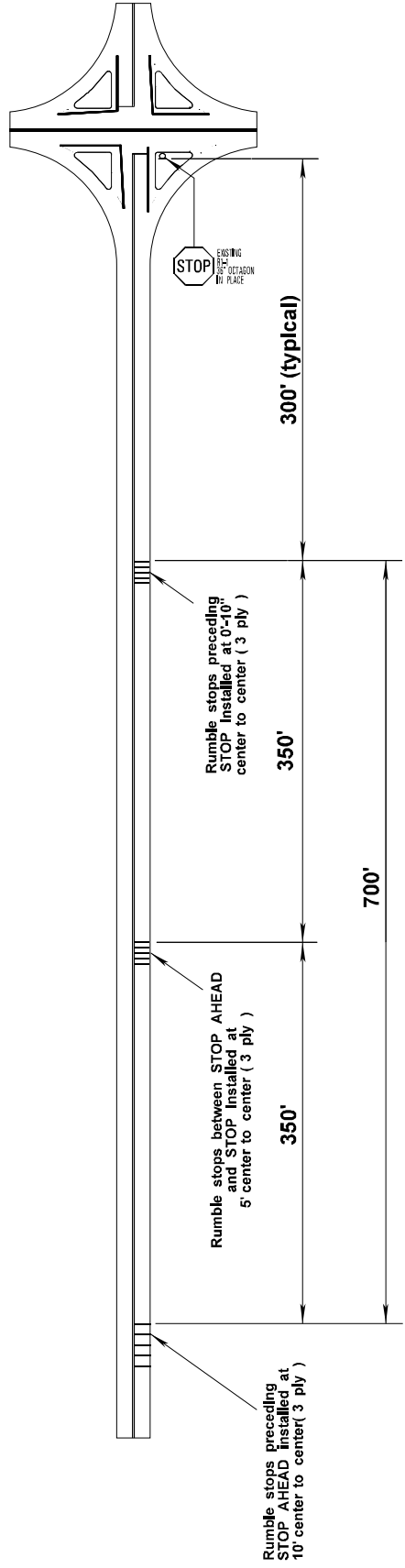




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DETAILED BY:	
CHECKED BY:	
DATE:	
PROJECT NO.: SP-2175-0000	
FMS CON: 109048/301000	
COUNTY: PIKE	

Notice to Bidders No. 7156 -- Cont

WKS NO.	SDS-1
SHEET NO.	6



- NOTES:
- 1. Install rumble stop bars as shown :
  - 2. 1 set of rumble stops approx. 250' from STOP AHEAD
  - 3. 1 set of rumble stops approx. 300' from STOP
  - 4. 1 set of rumble stops approx. Halfway between first and last set
  - 5. Rumble stops to be 6" thermoplastic ( 120 mil/each ply, 360 mil total)
  - 6. 5 rumble stops per set minimum
  - 7. Installation may vary due to terrain
  - 8. Signs should be 48" for channelized Intersection, 36" for non-channelized Intersection



DESIGNED BY: \_\_\_\_\_

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DATE: \_\_\_\_\_

PROJECT NO.: SP-2175-0001 -

FMS CON: 109048/301000

COUNTY: PIKE

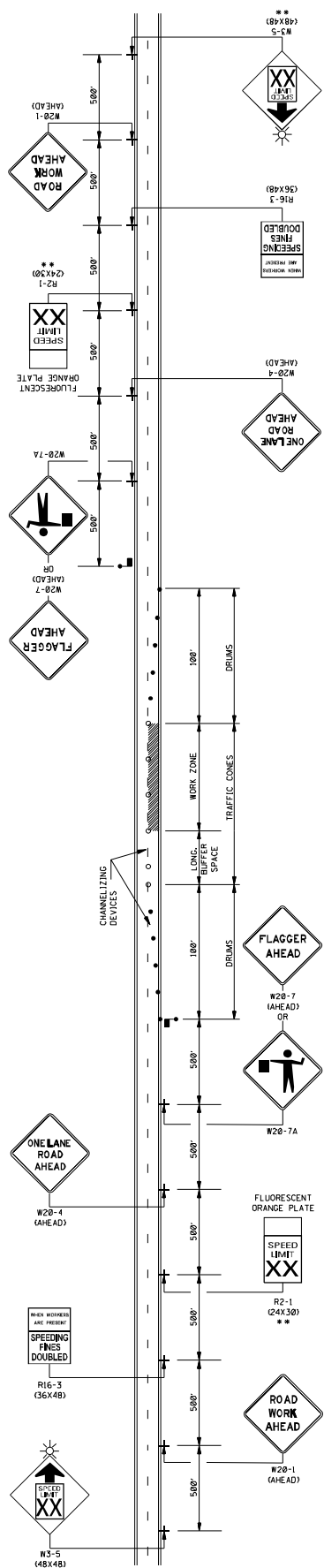
TRAFFIC CONTROL PLAN WITH FLAGGER

ONE LANE ROAD WITH FLAGGER

W-100-1

SDTSP-1

SHEET NO. 7



- LEGEND
- FLAGGER
  - RETROREFLECTIVE FREE-STANDING PLASTIC DRUMS
  - TRAFFIC CONES (8" HEIGHT MINIMUM)
  - THE SPEED ON R2-1 AND W3-5 SIGNS SHALL BE THE SPEED LIMIT FOR THE ROAD.
  - UNLESS OTHERWISE DIRECTED BY COMMISSION ORDER,

DISTANCE BETWEEN SIGNS				
ROAD TYPE	A	B	C	
URBAN (25 MPH OR LESS)	100 FT.	100 FT.	100 FT.	
URBAN (35 - 45 MPH)	150 FT.	150 FT.	150 FT.	
RURAL (50 - 70 MPH)	200 FT.	200 FT.	200 FT.	
EXPRESSWAY / FREEWAY	1000 FT.	1500 FT.	2640 FT.	

- GENERAL NOTES:
- THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA SHALL BE DETERMINED BY THE ENGINEER. THE FOLLOWING TABLE, FLAGGER STATIONS SHALL BE LOCATED SUCH THAT APPROACHING VEHICLES WILL HAVE SUFFICIENT DISTANCE TO STOP AND EXIT THE WORK AREA. THE STOPPING DISTANCE COLUMN MAY BE USED AS A MINIMUM FOR THIS DISTANCE.
  - ALL CHANNELIZING DEVICES SHALL BE A MINIMUM OF 28" IN HEIGHT.
  - DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 36" X 36" AND BLACK COPY ON FLUORESCENT ORANGE SHEETING.
  - WHEN WORK ZONE IS NO LONGER NEEDED, ALL SIGNS SHALL BE COVERED OR REMOVED. ALL CHANNELIZING DEVICES SHALL BE MOVED TO THE SHOULDER EDGE.
  - ADDITIONAL FLAGGERS MAY BE NEEDED AS DIRECTED BY THE ENGINEER.
  - WHEN WORK IS REQUIRED AT NIGHT, FLAGGER STATIONS SHALL BE ILLUMINATED.
  - CHANNELIZING DEVICE TYPES FOR:
    - A. APPROACH AND EXIT TAPERS- RETROREFLECTIVE PLASTIC DRUMS
    - B. ALONG LANE LINE AND WORK ZONE- TRAFFIC CONES (8" HEIGHT)
  - ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

POSTED SPEED (mph)	MAXIMUM DEVICE SPACING (ft)		LONGITUDINAL <sup>†</sup> BUFFER SPACE (ft)	STOPPING DISTANCE
	LANE LINE & TAPER	WORK ZONE		
25	20	50	55	155
30	20	60	85	200
35	20	70	120	250
40	20	80	170	305
45	20	90	220	360
50	20	100	280	425
55	20	110	335	495
60	20	120	415	570
65	20	130	485	645

<sup>†</sup> NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO STOPPING DISTANCE REQUIREMENTS FOR THE SPEED LIMIT AS DIRECTED BY THE ENGINEER.



DESIGNED BY: \_\_\_\_\_

CHECKED BY: \_\_\_\_\_

DATE: \_\_\_\_\_

PROJECT NO.: SP-215-00100

COUNTY: PIKE

FMS CON: 109048/301000

TRAFFIC CONTROL DETAIL

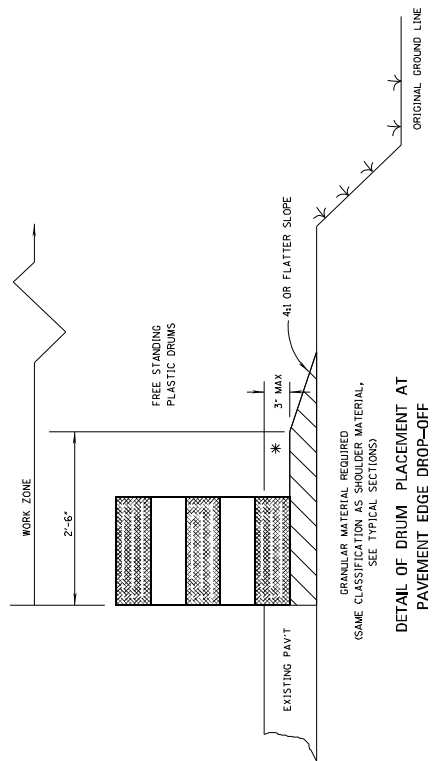
SHOULDER CLOSURE

NOTICE TO TRAVEL

W21-5

SDT02-16

SHEET NO. 8



NOTES:

\* A. PAVEMENT EDGE DROP-OFF

- IF LESS THAN TWO AND ONE QUARTER (2.25) INCHES-NO PROTECTION REQUIRED. PLACE A SHOULDER WORK SIGN (W21-5) 500 FEET IN ADVANCE OF WORK ZONE SHOULDER AND A LOW SHOULDER SIGN (WB-9) AT THE BEGINNING AND THROUGHOUT THE WORK ZONE @ (1 MILE ±0.0, 1).
- TWO AND ONE QUARTER TO THREE INCHES-PLACE DRUMS, VERTICAL PANELS OR BARRICADES EVERY 100 FEET ON TANGENT SECTIONS FOR SPEEDS OF 50 MILES PER HOUR OR GREATER. CONES MAY BE USED IN PLACE OF DRUMS, PANELS, AND BARRICADES DURING DAYLIGHT HOURS. FOR TANGENT SECTIONS WITH SPEEDS LESS THAN 50 MILES PER HOUR AND FOR CURVES, DEVICES SHOULD BE PLACED EVERY 50 FEET. SPACING FOR TAPERS SHOULD BE IN ACCORDANCE WITH THE MULTI-CO. 17 / 3 L, WHERE L IS THE TAPER LENGTH IN FEET.
- GREATER THAN THREE (3) INCHES-POSITIVE SEPARATION OR WEDGE WITH 4:1 OR FLATTER SLOPE NEEDED. IF THERE IS EIGHT (8) FEET OR MORE DISTANCE BETWEEN THE EDGE OF TRAVEL LANE AND DROP-OFF, THEN DRUMS, PANELS OR BARRICADES MAY BE USED.
- FOR TEMPORARY CONDITIONS, DROP-OFFS GREATER THAN THREE (3) INCHES MAY BE PROTECTED WITH DRUMS, VERTICAL PANELS OR BARRICADES FOR SHORT DISTANCES DURING DAYLIGHT HOURS WHILE WORK IS BEING DONE IN THE DROP-OFF AREA.
- LESSER TREATMENTS THAN THOSE DESCRIBED ABOVE MAY BE CONSIDERED FOR LOW-VOLUME LOCAL STREETS.

B. DRUM SPACING

- TANGENTS = 2 X S
- TAPERS = L / 3
- WHERE L = S X W
- L = TAPER LENGTH IN FEET
- S = SPEED IN MPH (POSTED OR 85 PERCENTILE)
- W = WIDTH OF OFFSET IN FEET

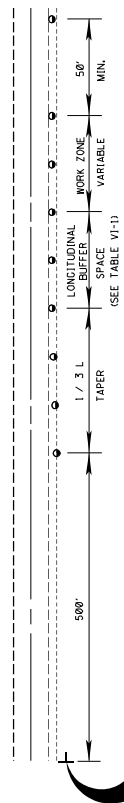
C. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.

TABLE VI-1: GUIDELINES FOR LENGTH OF LONGITUDINAL BUFFER SPACE

POSTED SPEED (MPH)	LENGTH (FEET)
20	25
25	30
30	35
35	40
40	45
45	50
50	55
55	60
60	65

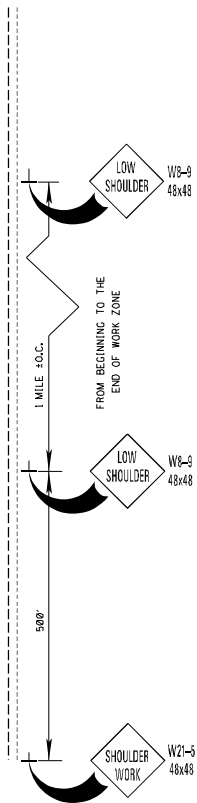
\*\* POSTED SPEED, OFF-PEAK 85 PERCENTILE SPEED  
\*\* BUFFER SPACE, 100 FEET MINIMUM, OR THE ANTICIPATED  
OPERATING SPEED IN MPH.

PLASTIC DRUMS  
(SEE NOTE FOR SPACING)



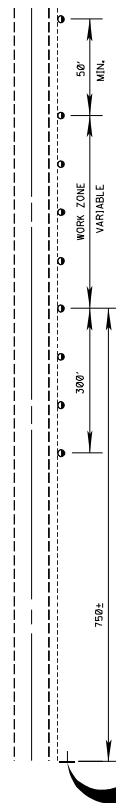
TYPICAL SHOULDER CLOSURE

- TO BE USED WITH EIGHT (8) FOOT OR GREATER WIDTH IMPROVED SHOULDER.
- TO BE USED WHEN CONSTRUCTION VEHICLES (EQUIPMENT) ENCROACHES ON OR WITHIN TWO (2) FEET OF THE SHOULDER BREAK.



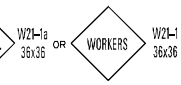
TYPICAL SHOULDER WORK #1  
(SEE NOTE A-1 THIS SHEET)

PLASTIC DRUMS  
(SEE NOTE FOR SPACING)



TYPICAL SHOULDER WORK #2

NOTE:  
WORK OUTSIDE TWO (2) FOOT AND WITHIN TEN (10) FEET OF THE SHOULDER BREAK MAY BE PROTECTED BY  
PLACING DRUMS ALONG THE SHOULDER EDGE, 300 FEET PRIOR TO AND 50 FEET BEYOND THE WORK AREA, OR  
SEE NOTE A-3 THIS SHEET.



	URBAN AREA (FT - IN)	RURAL AREA (FT - IN)
TANGENT SECTIONS	40' - 0"	80' - 0"
HORIZONTAL CURVES	40' - 0"	40' - 0"
INTERCHANGE LIMITS	40' - 0"	40' - 0"



MISSISSIPPI DEPARTMENT OF TRANSPORTATION

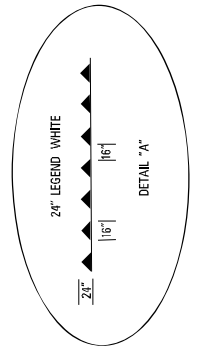
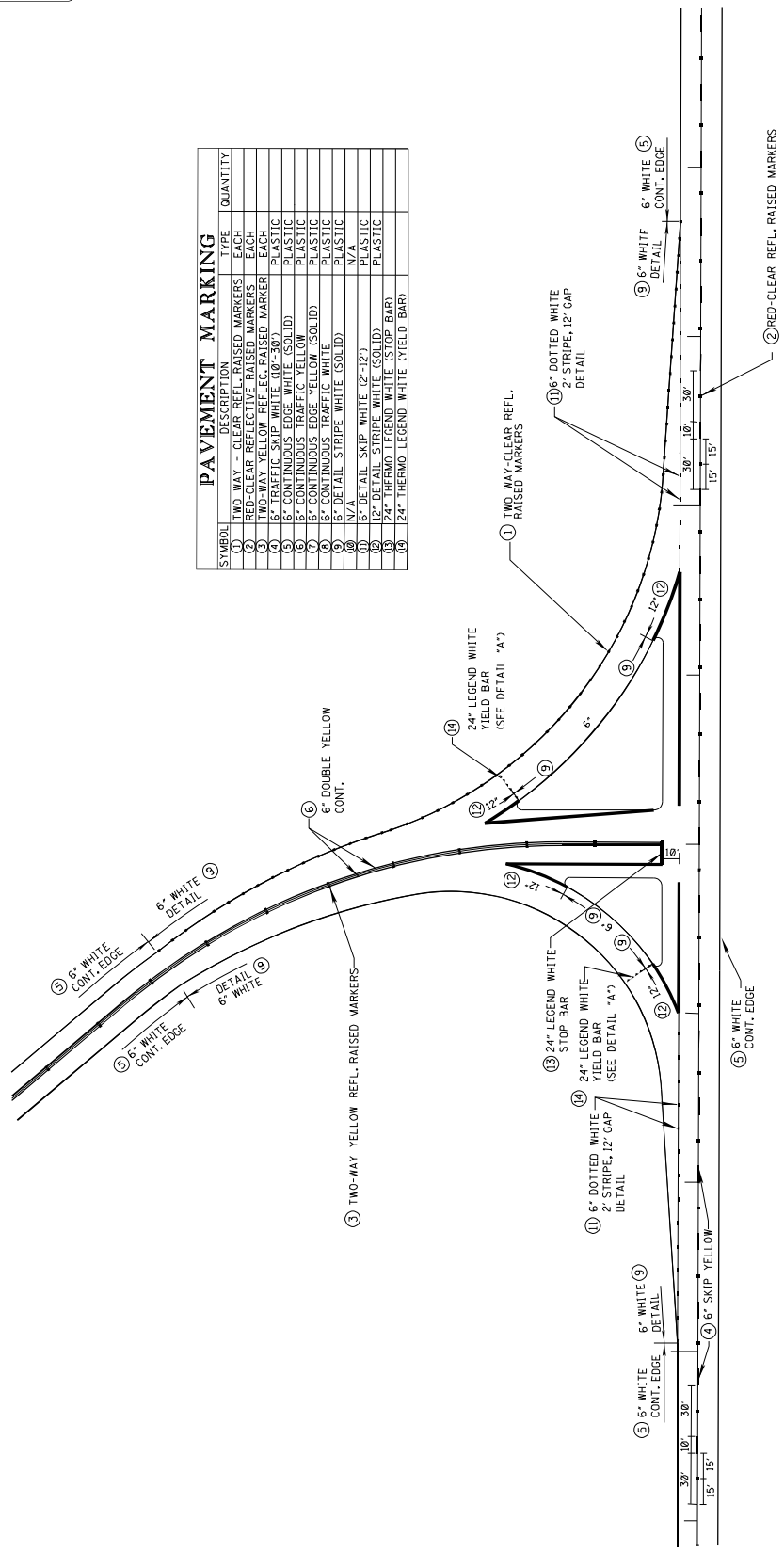
DESIGNED BY:  
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DATE:

FMS CON: 109048/301000  
PROJECT NO.: SP-2175-0001  
COUNTY: PIKE

DETAIL OF STRIPING  
2-LANE HIGHWAY  
CHANCELLOR BLVD. 4156 - Cont.

W. MO. PIKE  
SHEET NO. 10

PAVEMENT MARKING			
SYMBOL	DESCRIPTION	TYPE	QUANTITY
1	TWO-WAY - CLEAR REFLECTIVE RAISED MARKERS	EACH	
2	RED-CLEAR REFLECTIVE RAISED MARKERS	EACH	
3	TWO-WAY YELLOW REFLECTIVE RAISED MARKERS	EACH	
4	6" TRAFFIC SKIP WHITE (10'-30')	PLASTIC	
5	6" CONTINUOUS TRAFFIC WHITE (SOLID)	PLASTIC	
6	6" CONTINUOUS TRAFFIC YELLOW (SOLID)	PLASTIC	
7	6" CONTINUOUS TRAFFIC WHITE (SOLID)	PLASTIC	
8	6" DETAIL STRIPE WHITE (SOLID)	PLASTIC	
9	N/A	N/A	
10	6" DETAIL SKIP WHITE (2'-12')	PLASTIC	
11	12" DETAIL STRIPE WHITE (SOLID)	PLASTIC	
12	24" LEGEND WHITE YIELD BAR (SOLID)	PLASTIC	
13	24" THERMO LEGEND WHITE (SOLID BAR)	PLASTIC	
14	24" THERMO LEGEND WHITE (YIELD BAR)	PLASTIC	





MDOT  
MICHIGAN DEPARTMENT OF TRANSPORTATION

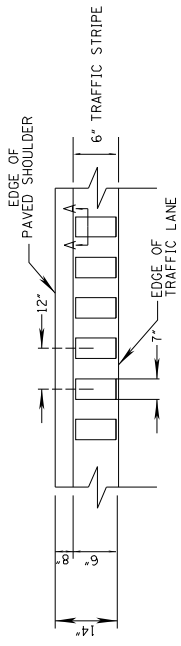
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DATE:	

FMS CON: 109048/301000
PROJECT NO.: SP-2175-00(01)
COUNTY: PIKE

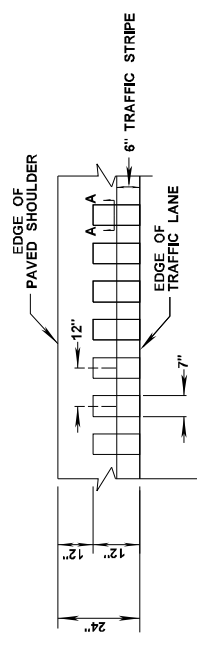
Notice to Bidders  
RUMBLE STRIPS  
(GROUND IN)

WKS. NO. 101
SHEET NO. 11

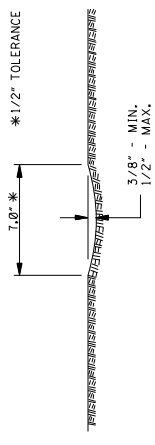
- GENERAL NOTES
1. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT
  2. GROUND-IN RUMBLE STRIPES SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
  3. COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS
  4. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO MAINLINE ONLY



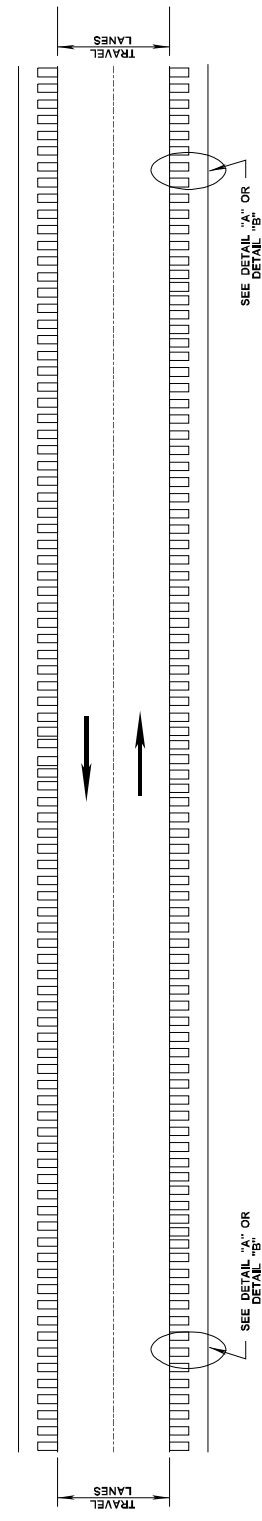
DETAIL "A"  
12' PAVEMENT EDGE



DETAIL "B"  
14' PAVEMENT EDGE



SECTION "A-A"



PLAN  
NOT TO SCALE

