

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO.	<u>1</u>	DATED	<u>1/21/2026</u>	ADDENDUM NO.	_____	DATED	_____
ADDENDUM NO.	_____	DATED	_____	ADDENDUM NO.	_____	DATED	_____
ADDENDUM NO.	_____	DATED	_____	ADDENDUM NO.	_____	DATED	_____

Number

Description

- 1 Revised Table of Contents; Notice to Bidder No. 7624 Replace Notice to Bidder No. 3599; Deleted Notice to Bidder No. 5750; Revised Wage Rates; Amendment EBSx Download Required.

TOTAL ADDENDA: 1

(Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

Contractor

BY _____

Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

President

Address

Secretary

Address

Treasurer

Address

The following is my (our) itemized proposal.

STBG-0013-02(036)/ 109020301000

Copiah County(ies)

Revised 01/26/2016

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

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PROJECT: STBG-0013-02(036)/109020301 - Copiah

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Progress Schedule

(REVISIONS TO THE ABOVE WILL BE INDICATED ON THE SECOND SHEET
OF SECTION 905 AS ADDENDA)

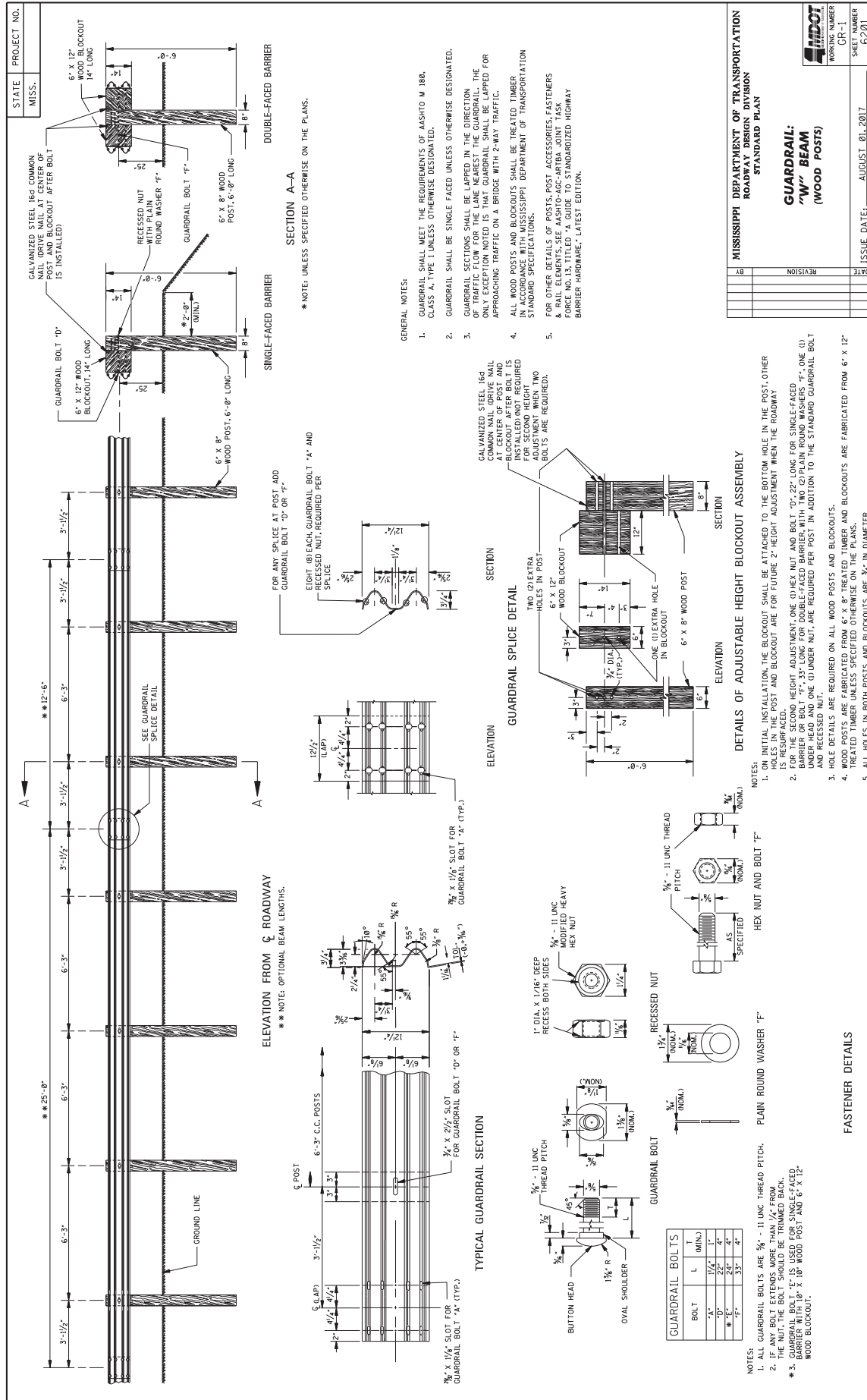
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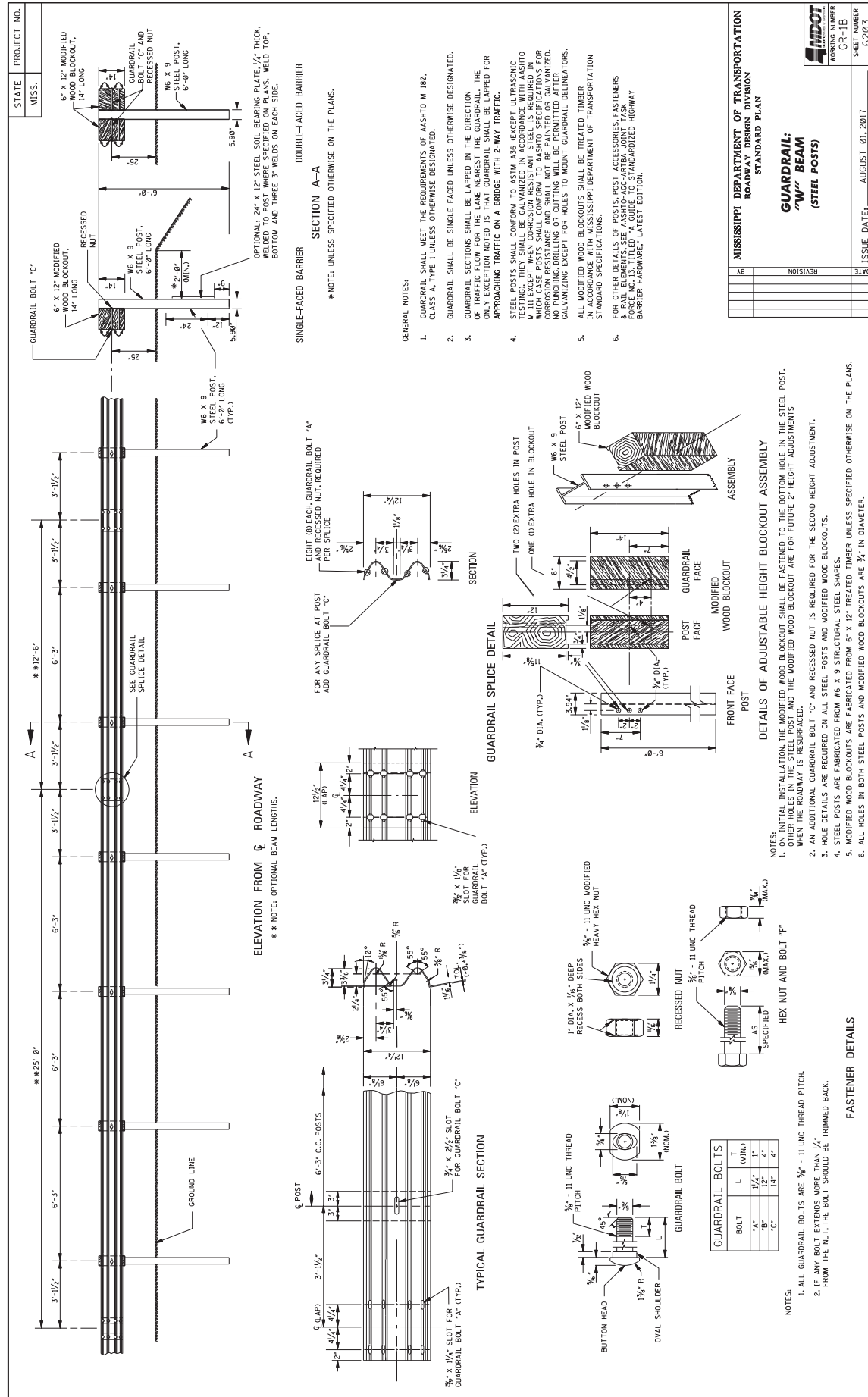
MISSISSIPPI DEPARTMENT OF TRANSPORTATION


SUPPLEMENT TO NOTICE TO BIDDERS NO. 7624

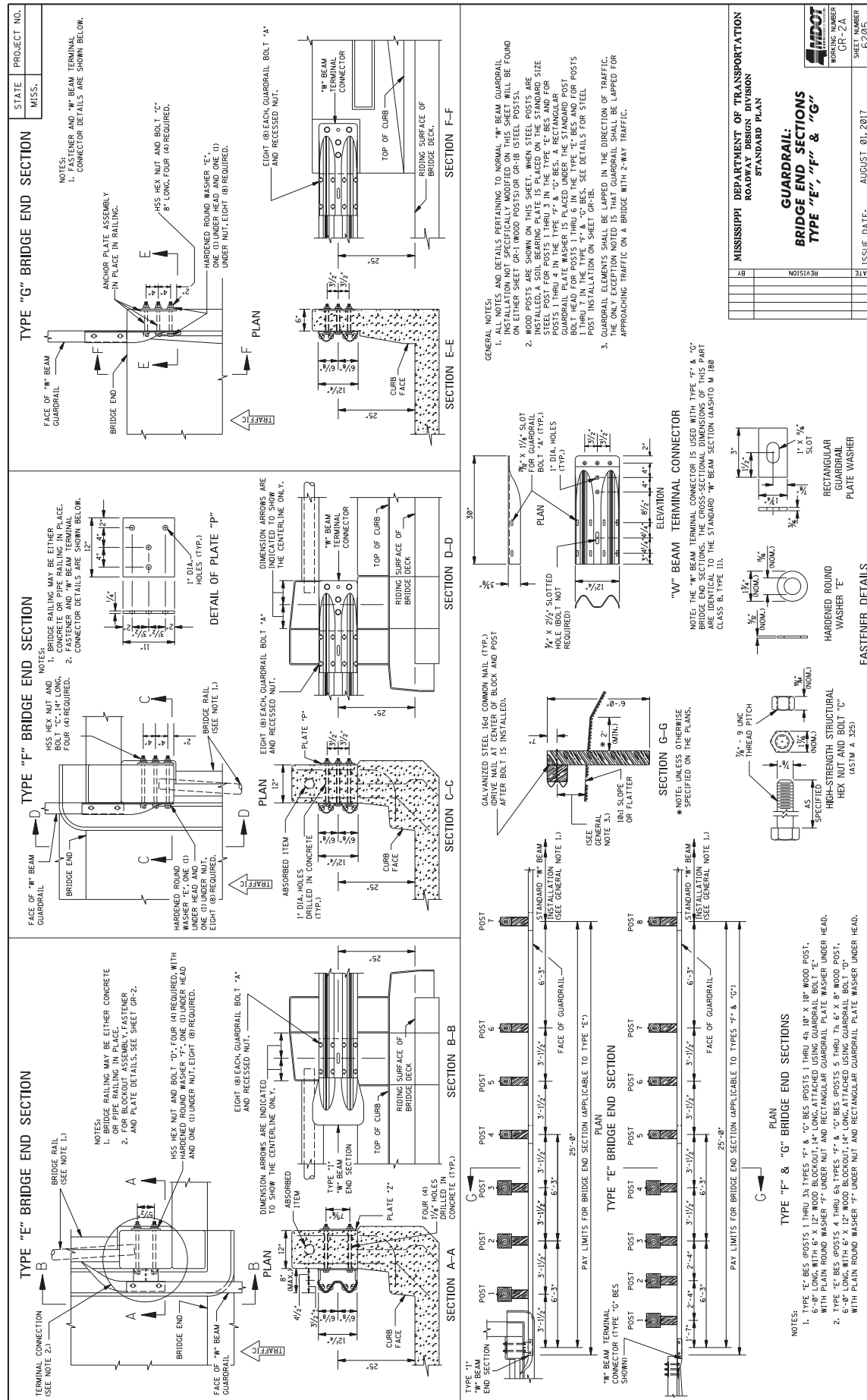
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After the last drawing on page 33, add the following.

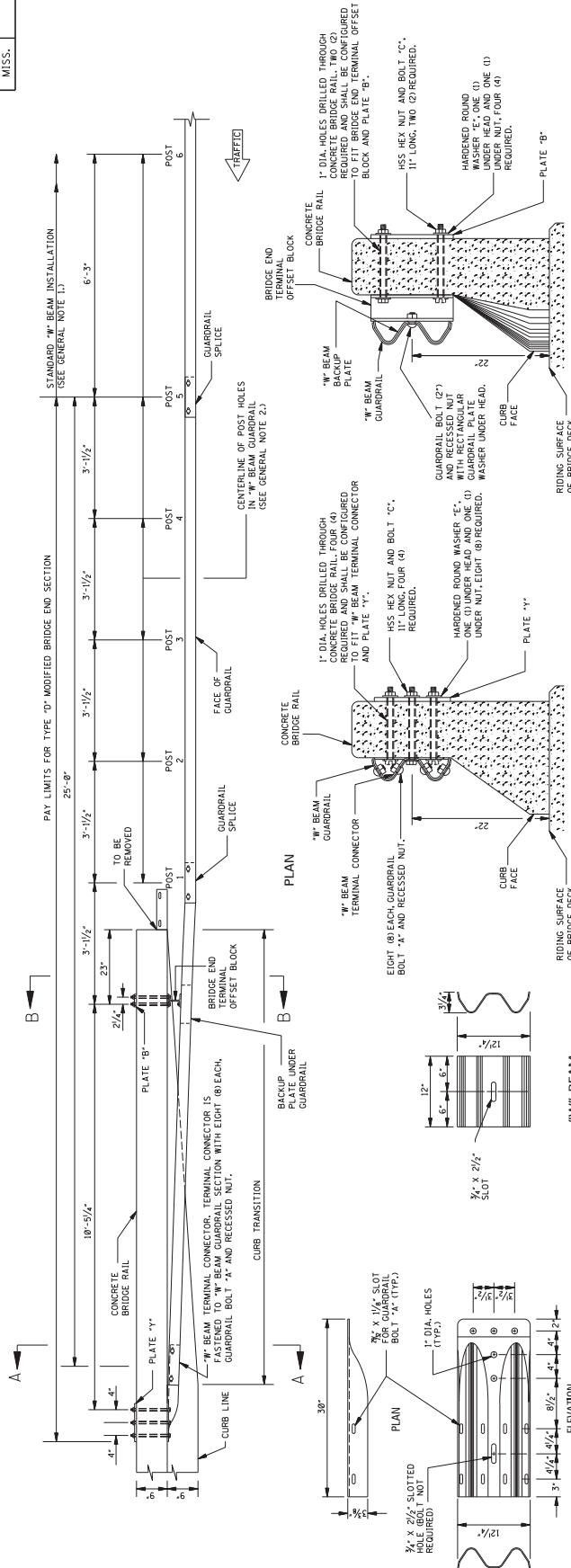




MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	GUARDRAIL: BRIDGE END SECTIONS TYPE "A" & "C"	 MIDOT MISSISSIPPI WORKING DRAWING GP-2 SHEET NO. 6704 DATE: AUGUST 01, 2017
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STATE	PROJECT NO.
MISS.	



SECTION B-B

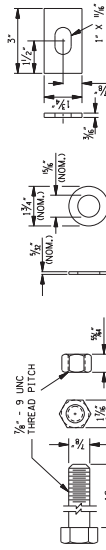
SECTION A-A

"W" BEAM
BACKUP PLATE

"V" BEAM TERMINAL CONNECTOR ELEVATION

NOTE: THE "W" BEAM BACK-UP PLATE IS USED UNDER THE "W" BEAM RAIL SECTION WHERE INDICATED ON THIS SHEET. THE CROSS-SECTION DIMENSIONS OF THIS PART ARE IDENTICAL TO THOSE OF THE STANDARD "W" BEAM RAIL SECTION.

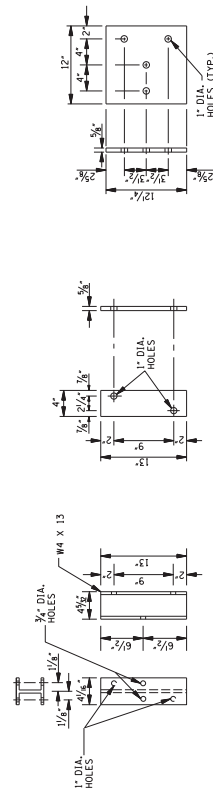
NOTE: THE "W" BEAM TERMINAL CONNECTOR IS USED WITH THE TYPE "O" MODIFIED BRIDGE END SECTION. THE CROSS-SECTIONAL DIMENSIONS OF THIS PART ARE IDENTICAL TO THE STANDARD "W" BEAM SECTION (AASHTO M 180 CLASS B, TYPE II).



FASTENER DETAILS

GENERAL NOTES:

- RAIL NOTES**
1. ALL NOTES AND DETAILS PERTAINING TO NORMAL "WY BEAM GUARDRAIL SHALL BE OBSERVED AND SPECIFICALLY PROTECTED ON THIS BRIDGE.
 2. THE EXISTING GUARDRAIL IS A TYPE-III GUARDRAIL WITH GR-15 STEEL POSTS FOUND ON EITHER SHEET GR-1 WOOD POSTS OR GR-1B STEEL POSTS.
 2. INSTALLATION DETAILS FOR BRIDGE END SECTION GUARDRAIL POSTS WILL BE FOUND ON EITHER SHEET GR-2C WOOD POSTS OR GR-2D STEEL POSTS).
 3. GUARDRAIL ELEMENTS SHALL BE LAPPED IN THE DIRECTION OF TRAFFIC. THE ONLY EXCEPTION NOTED IS THAT GUARDRAIL SHALL BE LAPPED FOR APPROXimating TRAFFIC ON A BRIDGE WITH 2-WAY TRAFFIC.
 4. THE HEIGHT OF RAIL AT THE BRIDGE END IS 22" AND WILL BE TRANSITIONED TO 25" AT POST 5.



DETAIL OF PLATE "Y"

DETAIL OF PLATE "B"

FRONT FACE SIDE

DETAIL OF BRIDGE END
TERMINAL OFFSET BLOCK

ARTMENT OF TRANSPORTATION
 HIGHWAY DESIGN DIVISION
 STANDARD PLAN

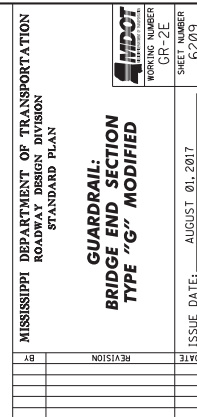
**GUARDRAIL:
BRIDGE END SECTION
TYPE "D" MODIFIED**

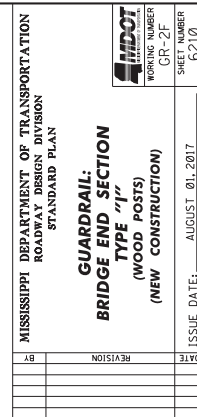
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	WORKING NUMBER GR-2B	SHEET NUMBER 6206
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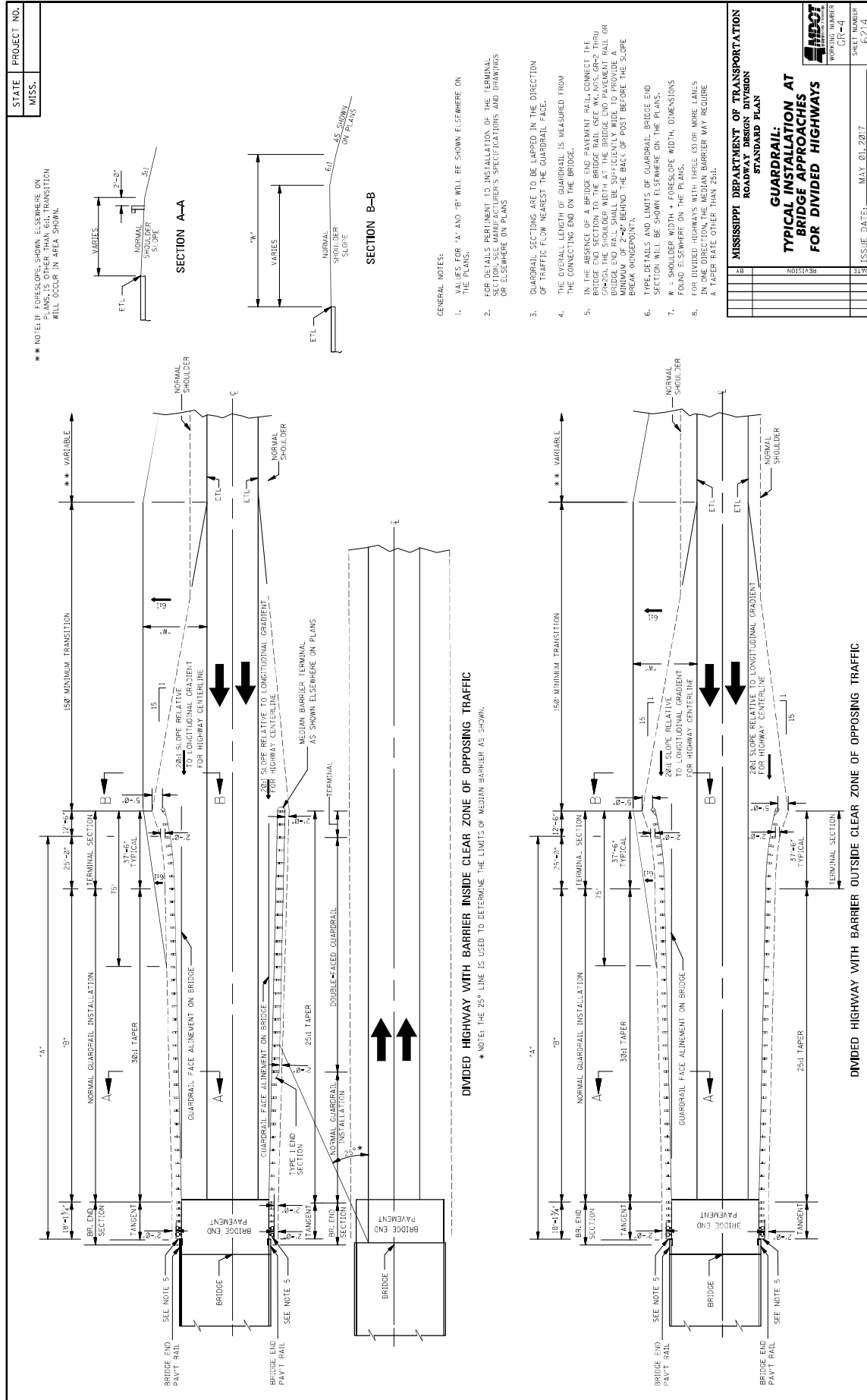
DATE	ISSUE DATE:	AUGUST 01, 2017
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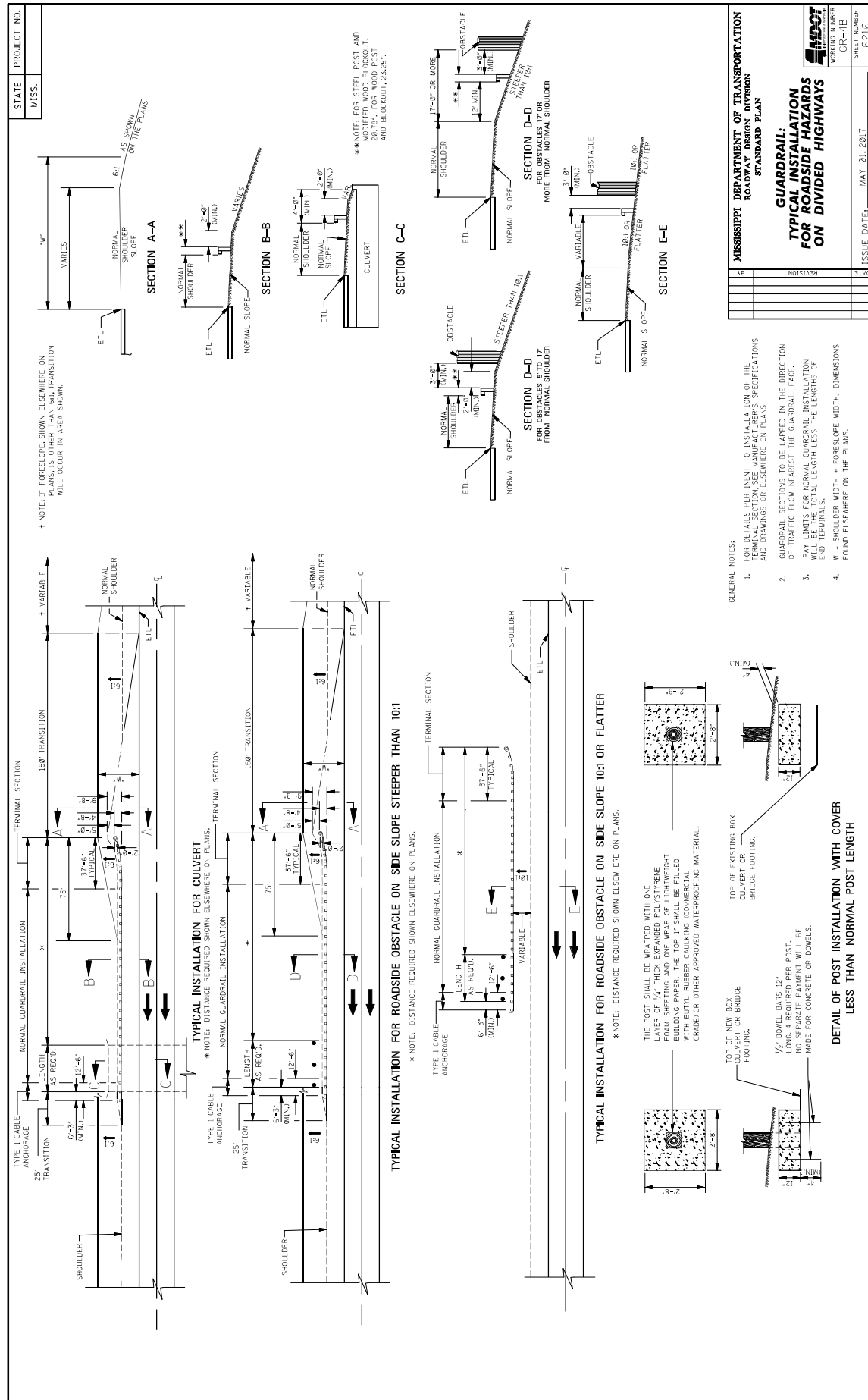
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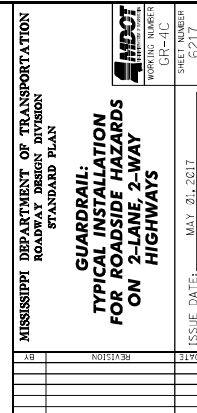


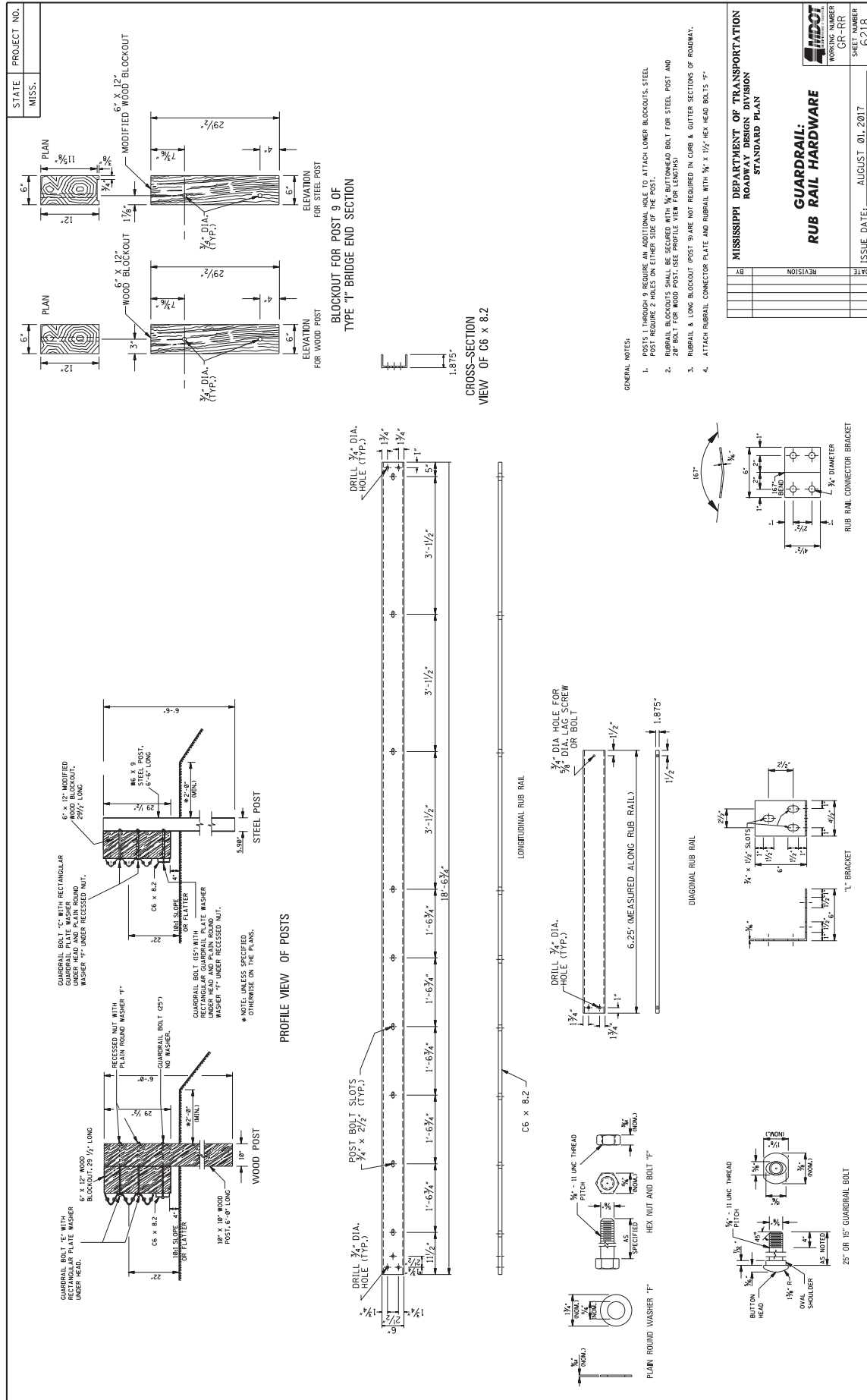


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MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 – NOTICE TO BIDDERS NO. 7624

CODE: (SP)

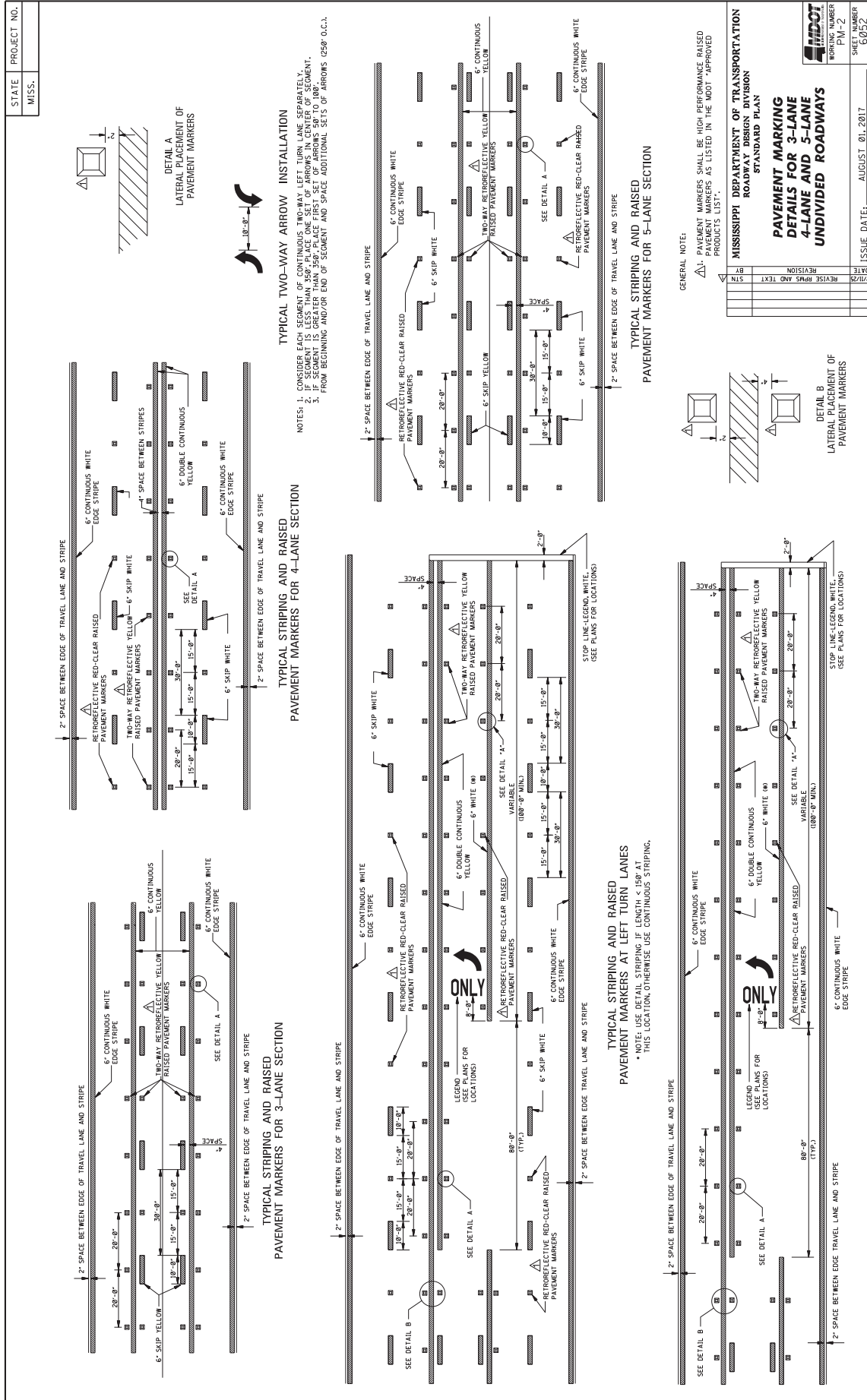
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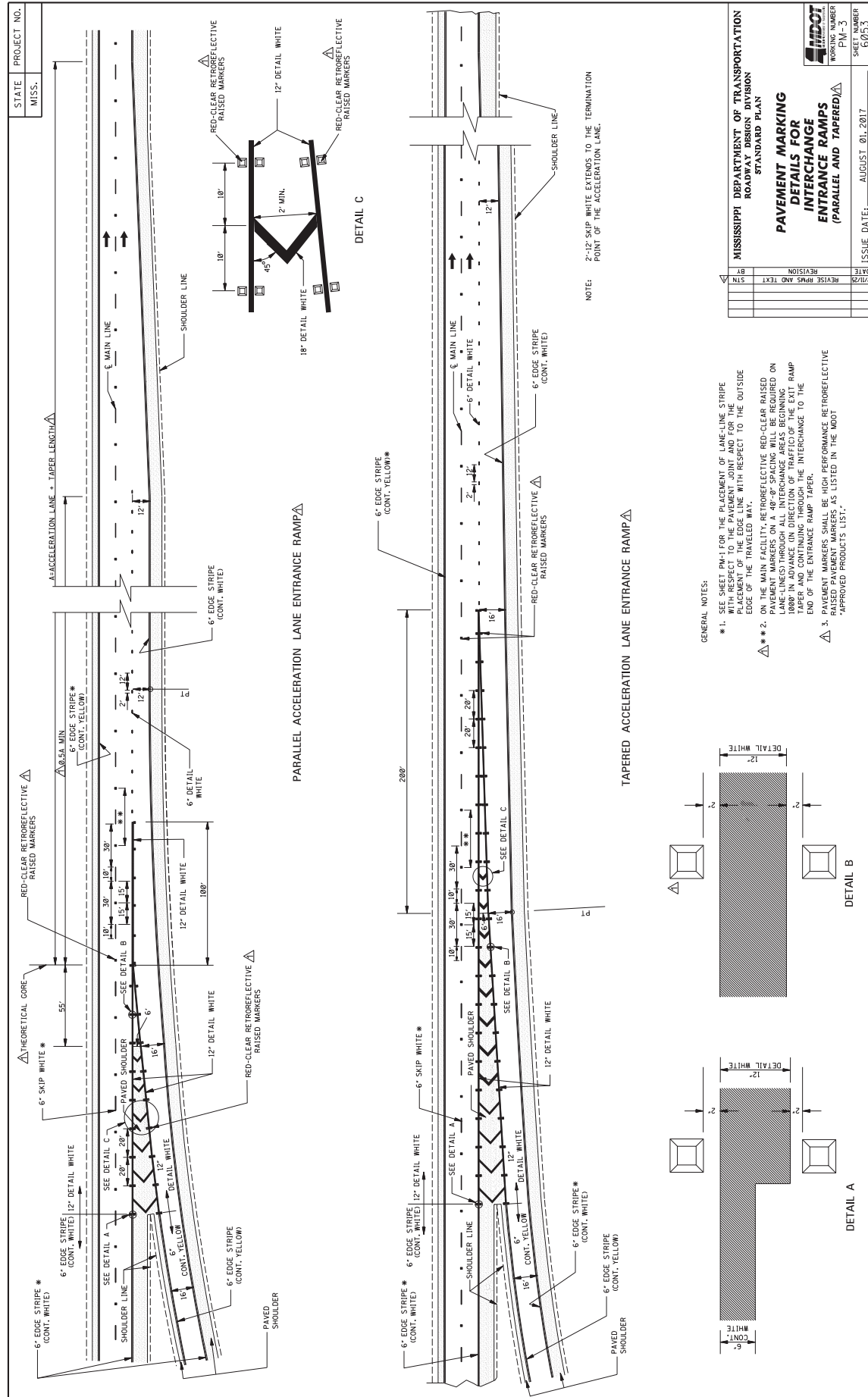
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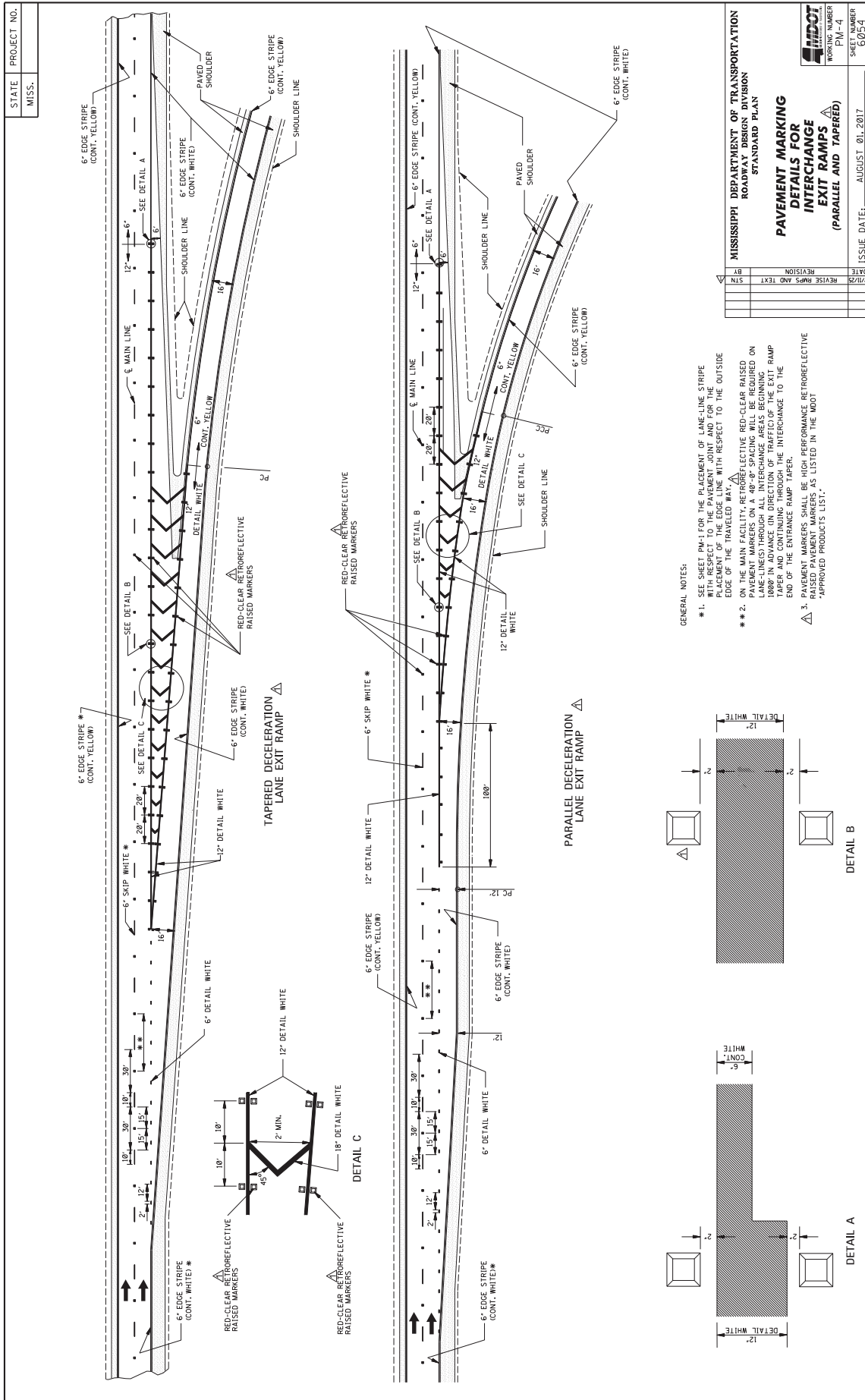
Standard Drawings attached hereto shall govern appropriate items of required work.

Larger copies of Standard Drawings may be purchased from:

MDOT Plans Print Shop
MDOT Shop Complex, Building C, Room 114
2567 North West Street
P.O. Box 1850
Jackson, MS 39215-1850
Telephone: (601) 359-7460
or FAX: (601) 359-7461
or e-mail: plans@mdot.state.ms.us








DATE	BY	REVISION	STN	TEXT
08/20/17		REVISE RUMPS AND TEXT		

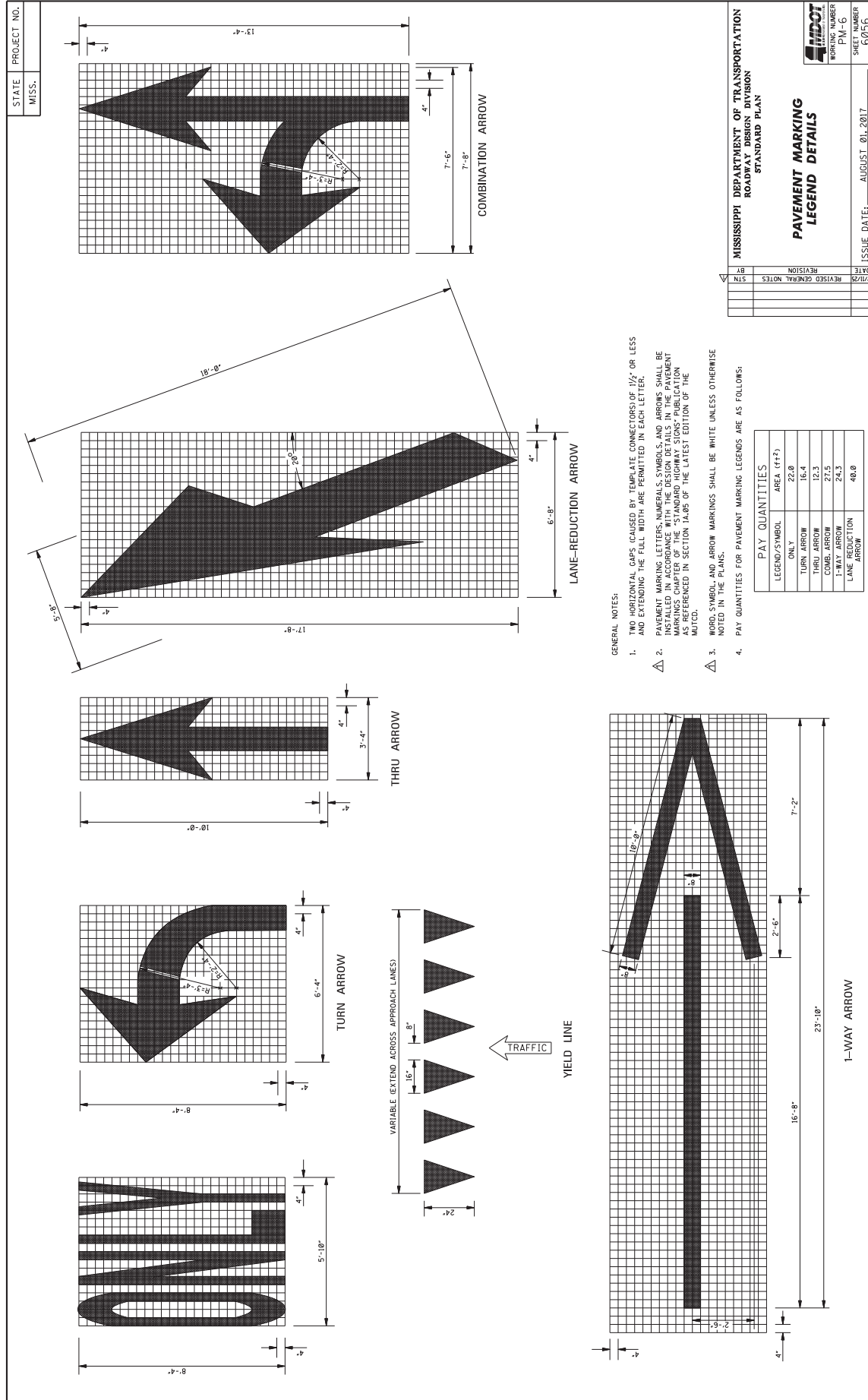
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN
**PAVEMENT MARKING
DETAILS FOR
INTERCHANGE
EXIT RAMP
(PARALLEL AND TAPERED)**

WORKING NUMBER
PM-4

SHEET NUMBER
6034

ISSUE DATE: AUGUST 01, 2017

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
1B 1A 1C 1D 1E 1F 1G 1H 1I 1J 1K 1L 1M 1N 1O 1P 1Q 1R 1S 1T 1U 1V 1W 1X 1Y 1Z 1AA 1AB 1AC 1AD 1AE 1AF 1AG 1AH 1AI 1AJ 1AK 1AL 1AM 1AN 1AO 1AP 1AQ 1AR 1AS 1AT 1AU 1AV 1AW 1AX 1AY 1AZ 1BA 1BB 1BC 1BD 1BE 1BF 1BG 1BH 1BI 1BJ 1BK 1BL 1BM 1BN 1BO 1BP 1BQ 1BR 1BS 1BT 1BU 1BV 1BW 1BX 1BY 1BZ 1CA 1CB 1CC 1CD 1CE 1CF 1CG 1CH 1CI 1CJ 1CK 1CL 1CM 1CN 1CO 1CP 1CQ 1CR 1CS 1CT 1CU 1CV 1CW 1CX 1CY 1CZ 1DA 1DB 1DC 1DD 1DE 1DF 1DG 1DH 1DI 1DJ 1DK 1DL 1DM 1DN 1DO 1DP 1DQ 1DR 1DS 1DT 1DU 1DV 1DW 1DX 1DY 1DZ 1EA 1EB 1EC 1ED 1EE 1EF 1EG 1EH 1EI 1EJ 1EK 1EL 1EM 1EN 1EO 1EP 1EQ 1ER 1ES 1ET 1EU 1EV 1EW 1EX 1EY 1EZ 1FA 1FB 1FC 1FD 1FE 1FF 1FG 1FH 1FI 1FJ 1FK 1FL 1FM 1FN 1FO 1FP 1FQ 1FR 1FS 1FT 1FU 1FV 1FW 1FX 1FY 1FZ 1GA 1GB 1GC 1GD 1GE 1GF 1GG 1GH 1GI 1GJ 1GK 1GL 1GM 1GN 1GO 1GP 1GQ 1GR 1GS 1GT 1GU 1GV 1GW 1GX 1GY 1GZ 1HA 1HB 1HC 1HD 1HE 1HF 1HG 1HH 1HI 1HJ 1HK 1HL 1HM 1HN 1HO 1HP 1HQ 1HR 1HS 1HT 1HU 1HV 1HW 1HX 1HY 1HZ 1IA 1IB 1IC 1ID 1IE 1IF 1IG 1IH 1II 1IJ 1IK 1IL 1IM 1IN 1IO 1IP 1IQ 1IR 1IS 1IT 1IU 1IV 1IW 1IX 1IY 1IZ 1JA 1JB 1JC 1JD 1JE 1JF 1JG 1JH 1JI 1JJ 1JK 1JL 1JM 1JN 1JO 1JP 1JQ 1JR 1JS 1JT 1JU 1JV 1JW 1JX 1JY 1JZ 1KA 1KB 1KC 1KD 1KE 1KF 1KG 1KH 1KI 1KJ 1KK 1KL 1KM 1KN 1KO 1KP 1KQ 1KR 1KS 1KT 1KU 1KV 1KW 1KX 1KY 1KZ 1LA 1LB 1LC 1LD 1LE 1LF 1LG 1LH 1LI 1LJ 1LK 1LL 1LM 1LN 1LO 1LP 1LQ 1LR 1LS 1LT 1LU 1LV 1LW 1LX 1LY 1LZ 1MA 1MB 1MC 1MD 1ME 1MF 1MG 1MH 1MI 1MJ 1MK 1ML 1MM 1MN 1MO 1MP 1MQ 1MR 1MS 1MT 1MU 1MV 1MW 1MX 1MY 1MZ 1NA 1NB 1NC 1ND 1NE 1NF 1NG 1NH 1NI 1NJ 1NK 1NL 1NM 1NN 1NO 1NP 1NQ 1NR 1NS 1NT 1NU 1NV 1NW 1NX 1NY 1NZ 1OA 1OB 1OC 1OD 1OE 1OF 1OG 1OH 1OI 1OJ 1OK 1OL 1OM 1ON 1OO 1OP 1OQ 1OR 1OS 1OT 1OU 1OV 1OW 1OX 1OY 1OZ 1PA 1PB 1PC 1PD 1PE 1PF 1PG 1PH 1PI 1PJ 1PK 1PL 1PM 1PN 1PO 1PP 1PQ 1PR 1PS 1PT 1PU 1PV 1PW 1PX 1PY 1PZ 1QA 1QB 1QC 1QD 1QE 1QF 1QG 1QH 1QI 1QJ 1QK 1QL 1QM 1QN 1QO 1QP 1QQ 1QR 1QS 1QT 1QU 1QV 1QW 1QX 1QY 1QZ 1RA 1RB 1RC 1RD 1RE 1RF 1RG 1RH 1RI 1RJ 1RK 1RL 1RM 1RN 1RO 1RP 1RQ 1RR 1RS 1RT 1RU 1RV 1RW 1RX 1RY 1RZ 1SA 1SB 1SC 1SD 1SE 1SF 1SG 1SH 1SI 1SJ 1SK 1SL 1SM 1SN 1SO 1SP 1SQ 1SR 1SS 1ST 1SU 1SV 1SW 1SX 1SY 1SZ 1TA 1TB 1TC 1TD 1TE 1TF 1TG 1TH 1TI 1TJ 1TK 1TL 1TM 1TN 1TO 1TP 1TQ 1TR 1TS 1TT 1TU 1TV 1TW 1TX 1TY 1TZ 1UA 1UB 1UC 1UD 1UE 1UF 1UG 1UH 1UI 1UJ 1UK 1UL 1UM 1UN 1UO 1UP 1UQ 1UR 	



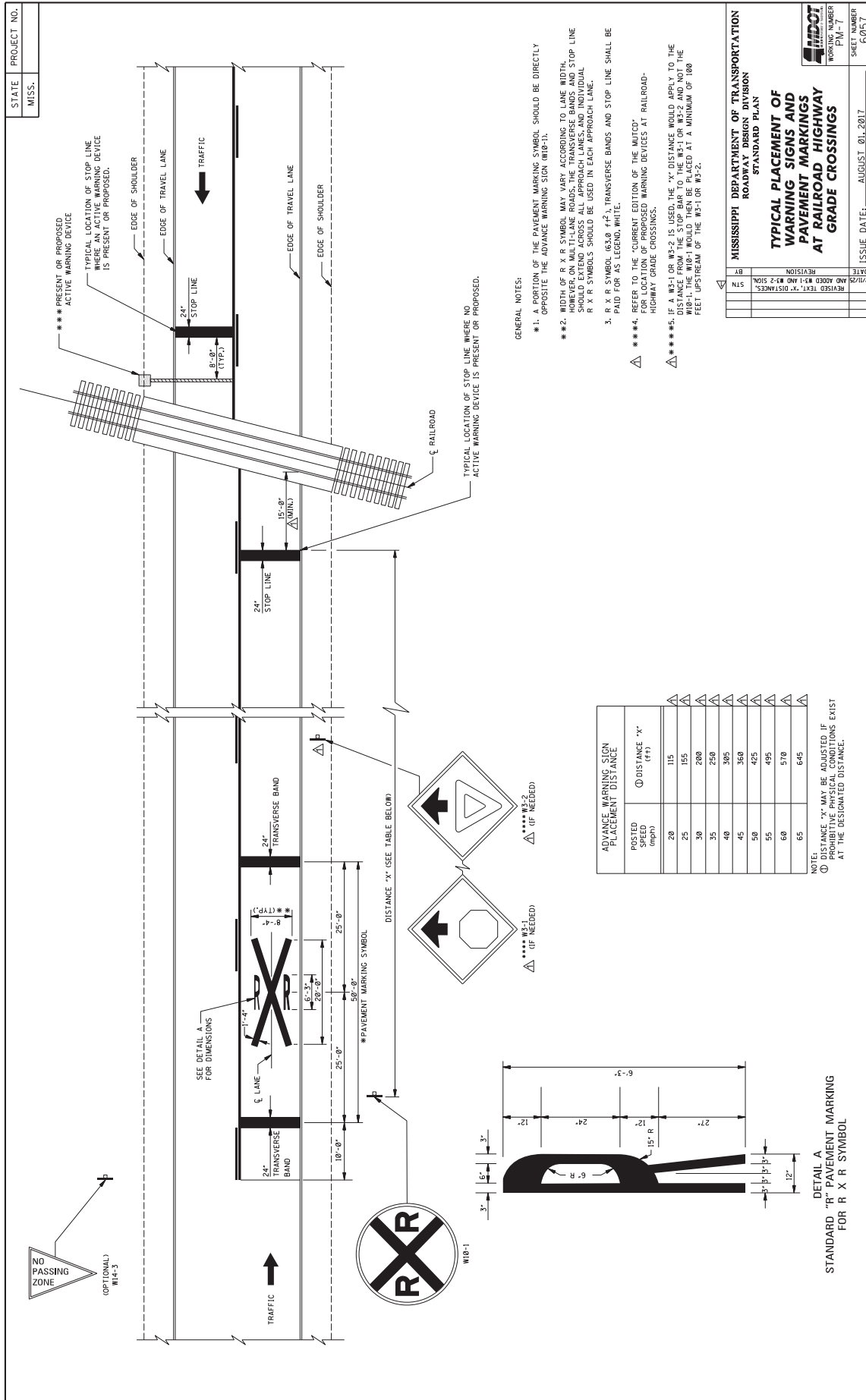
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

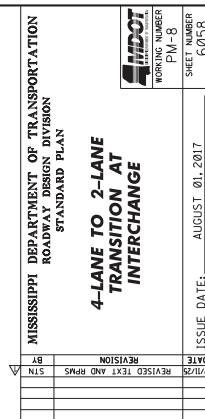
**PAVEMENT MARKING
LEGEND DETAILS**

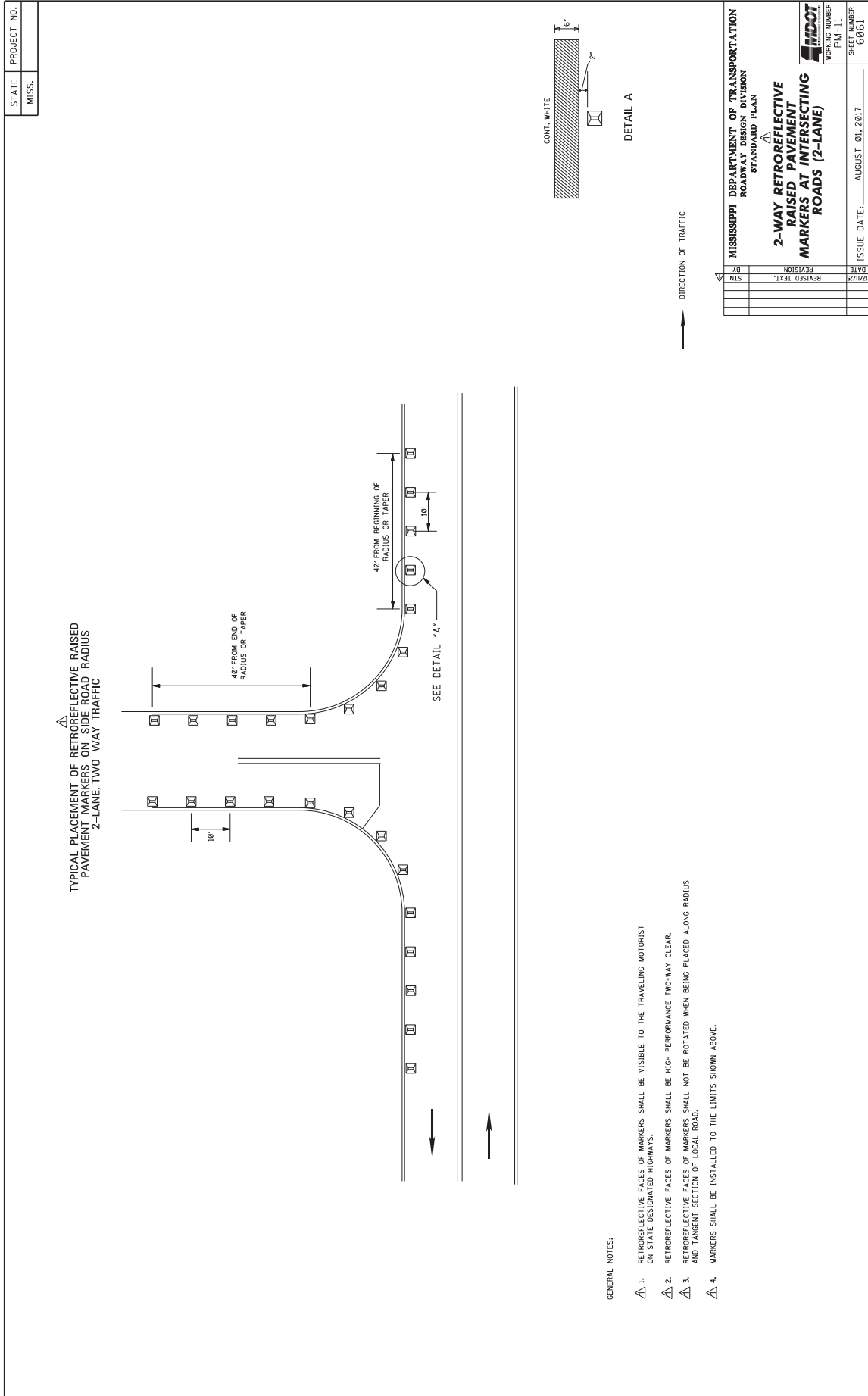
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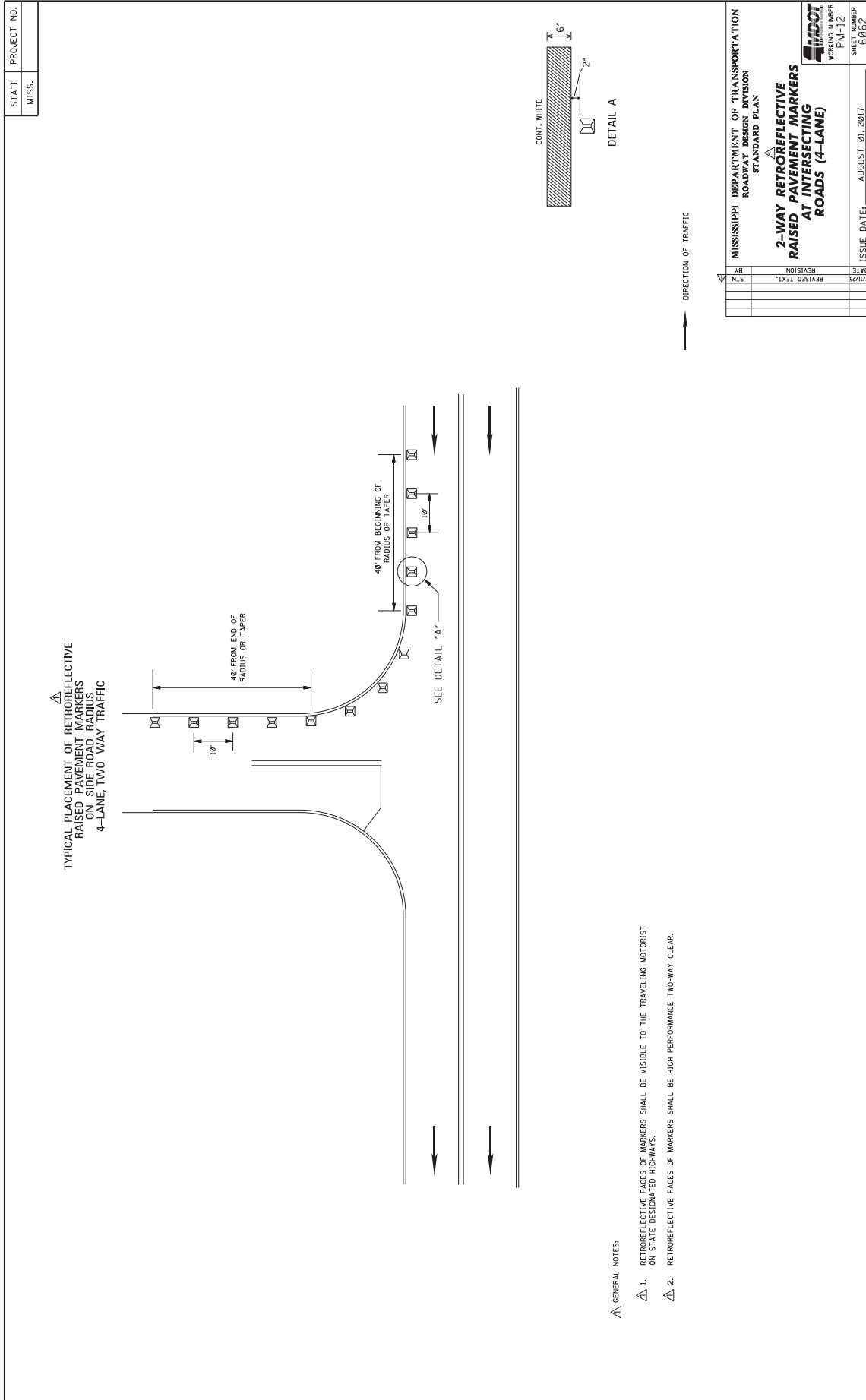
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SHEET NUMBER: 6036

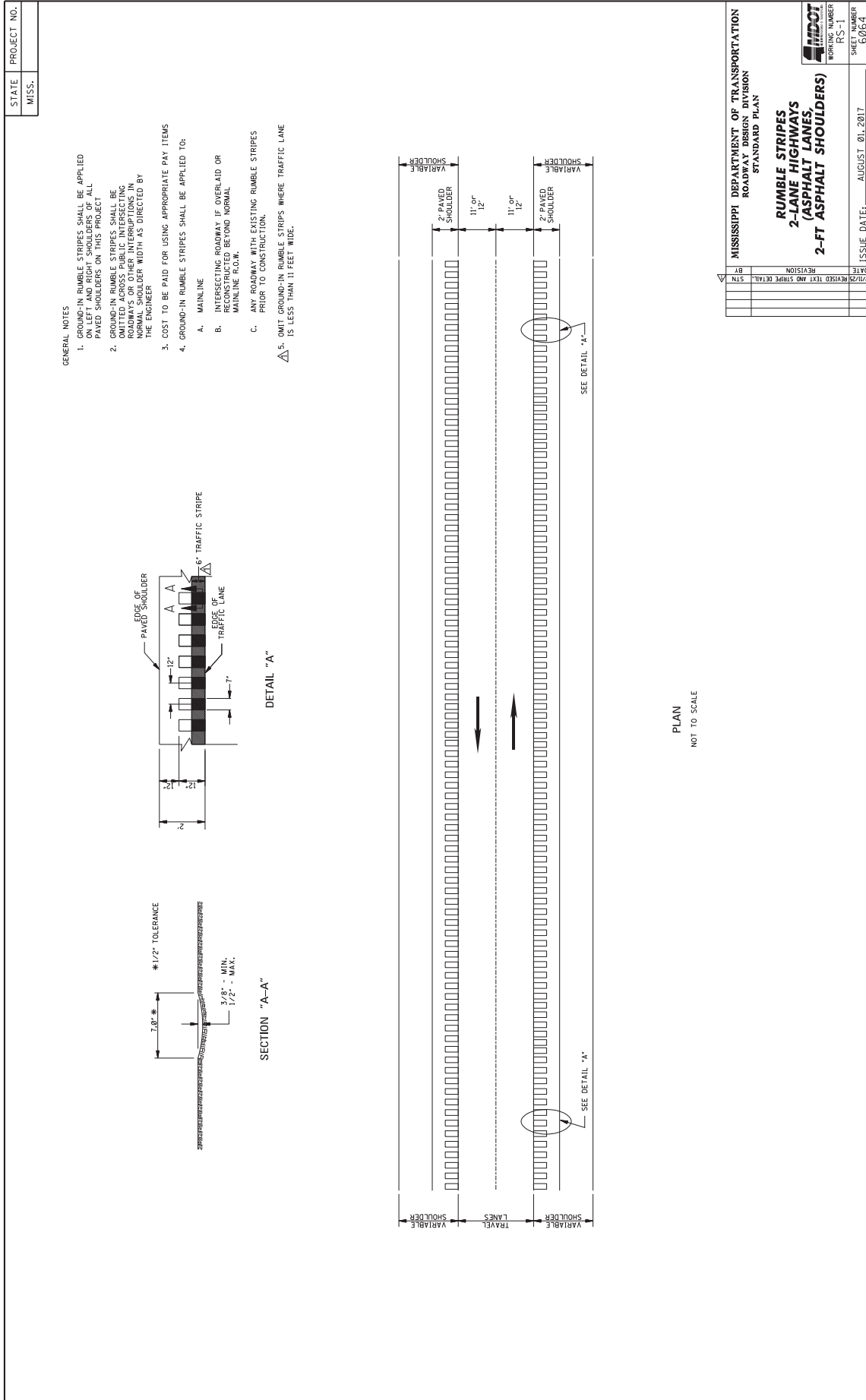


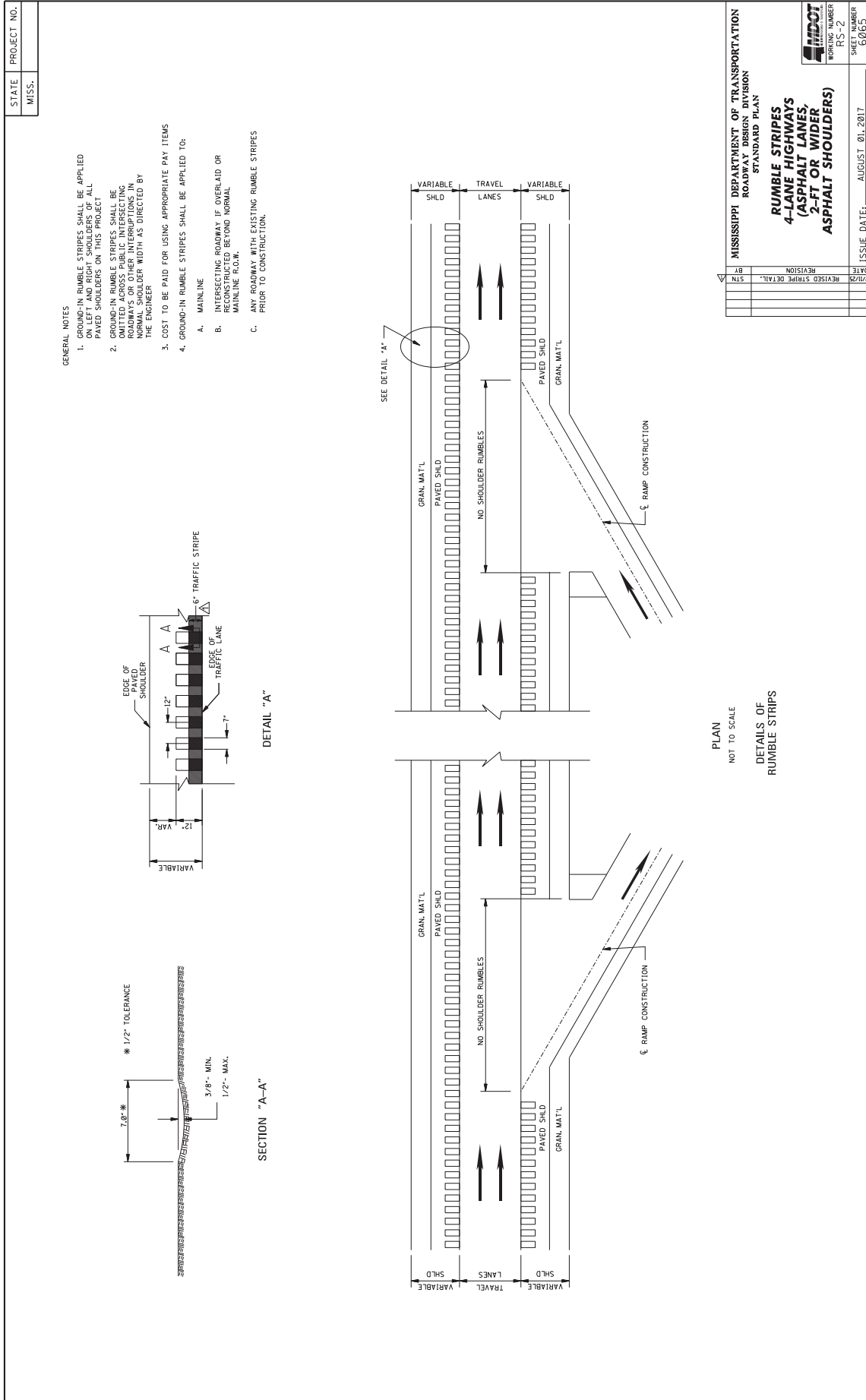


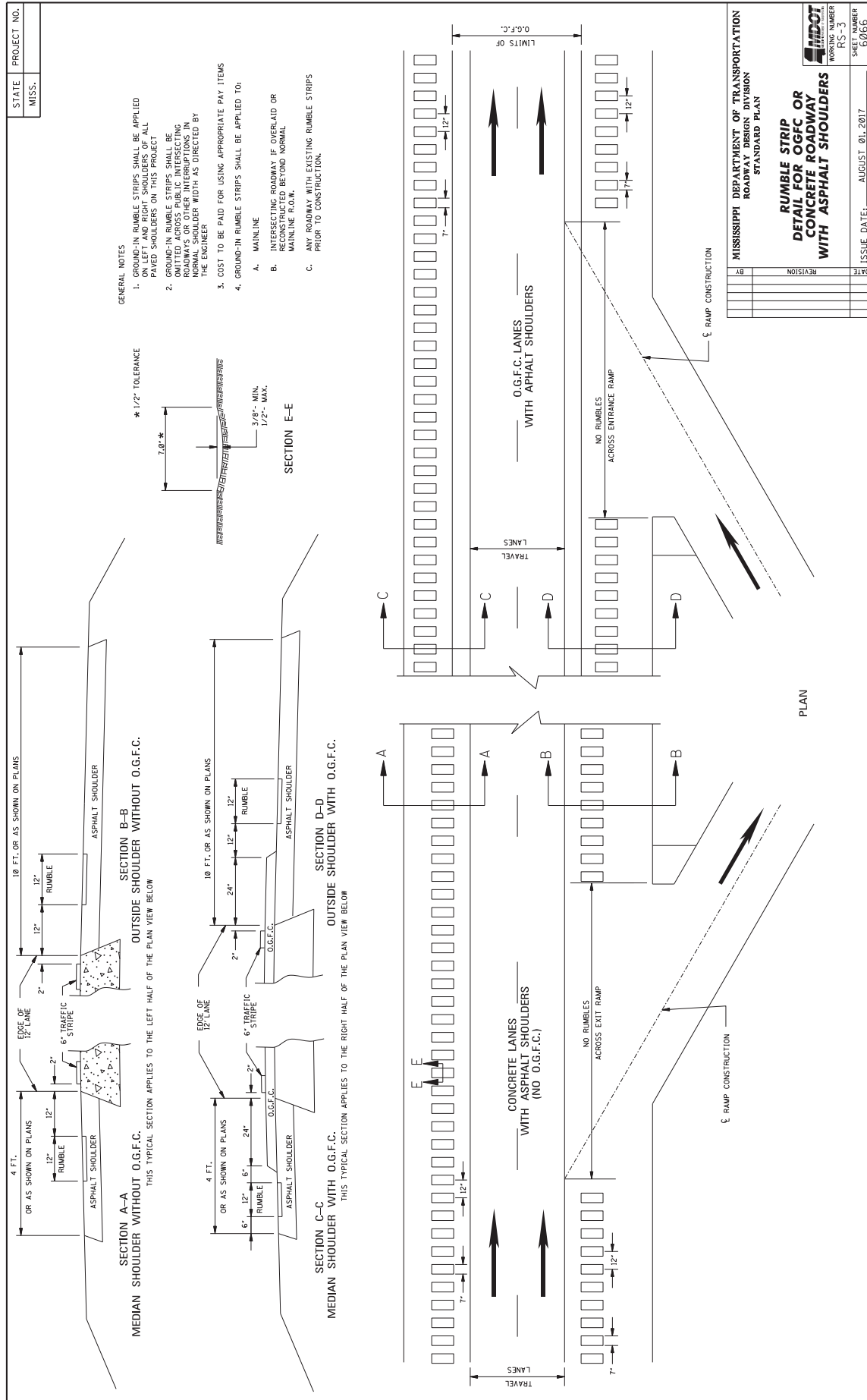


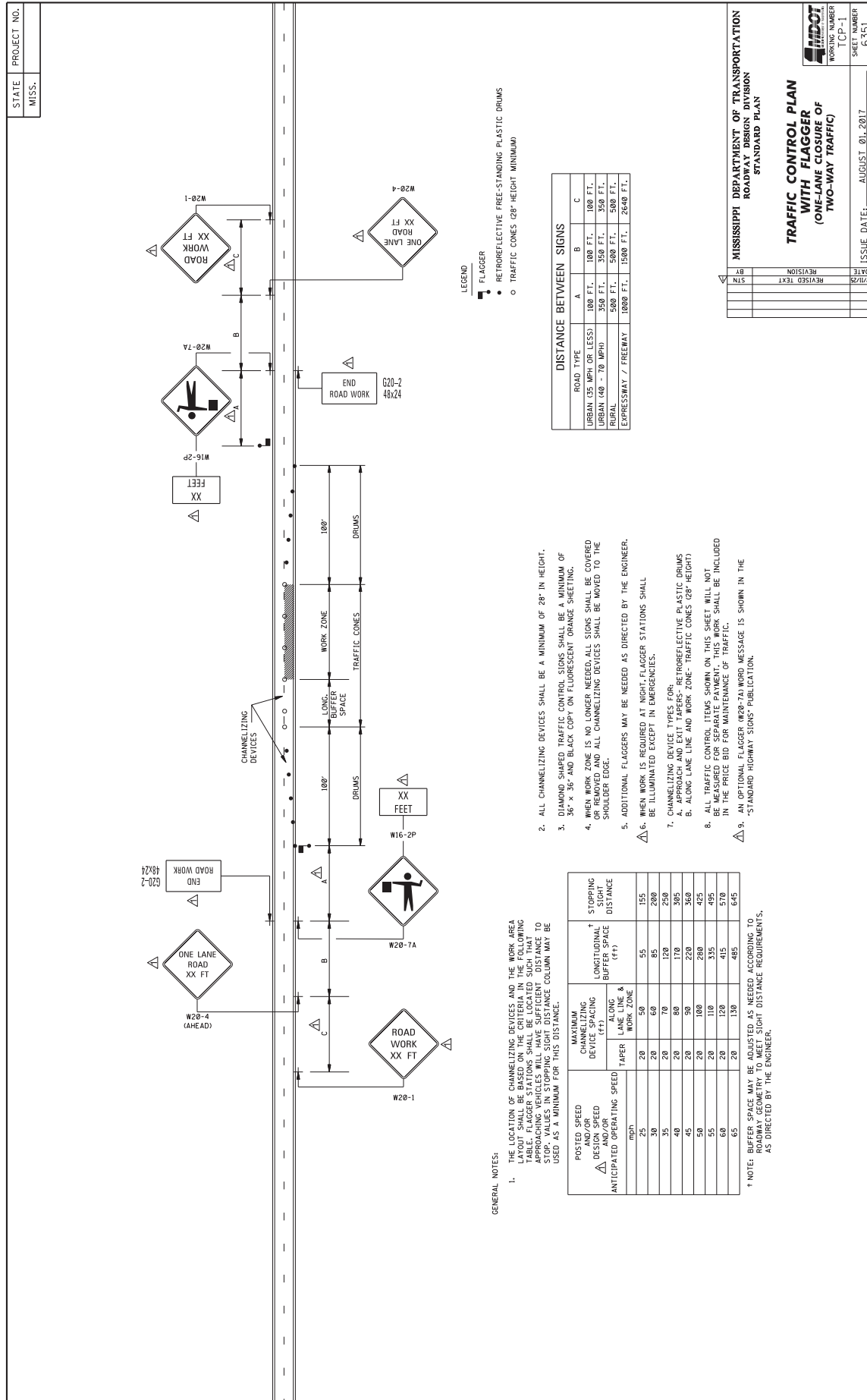


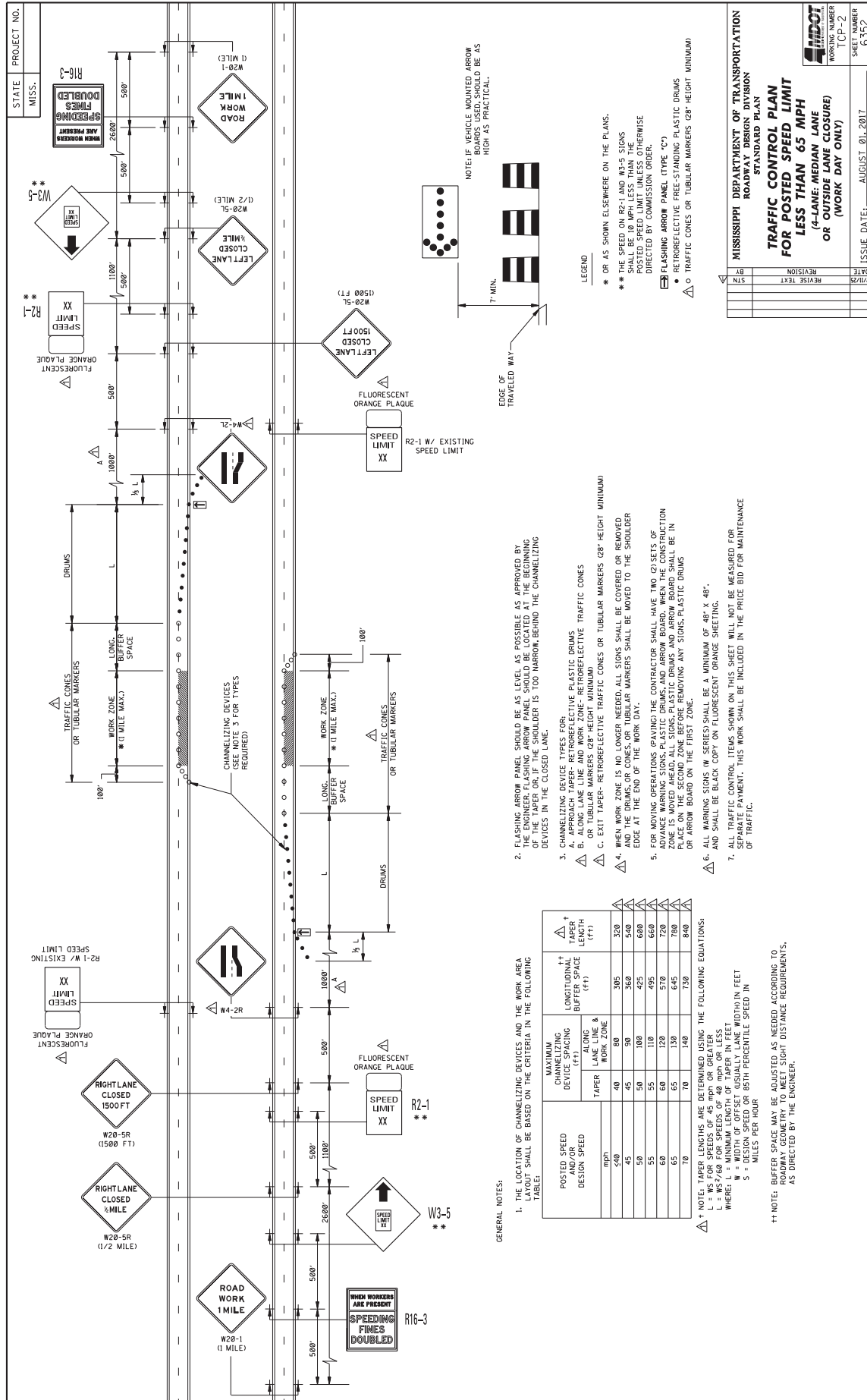
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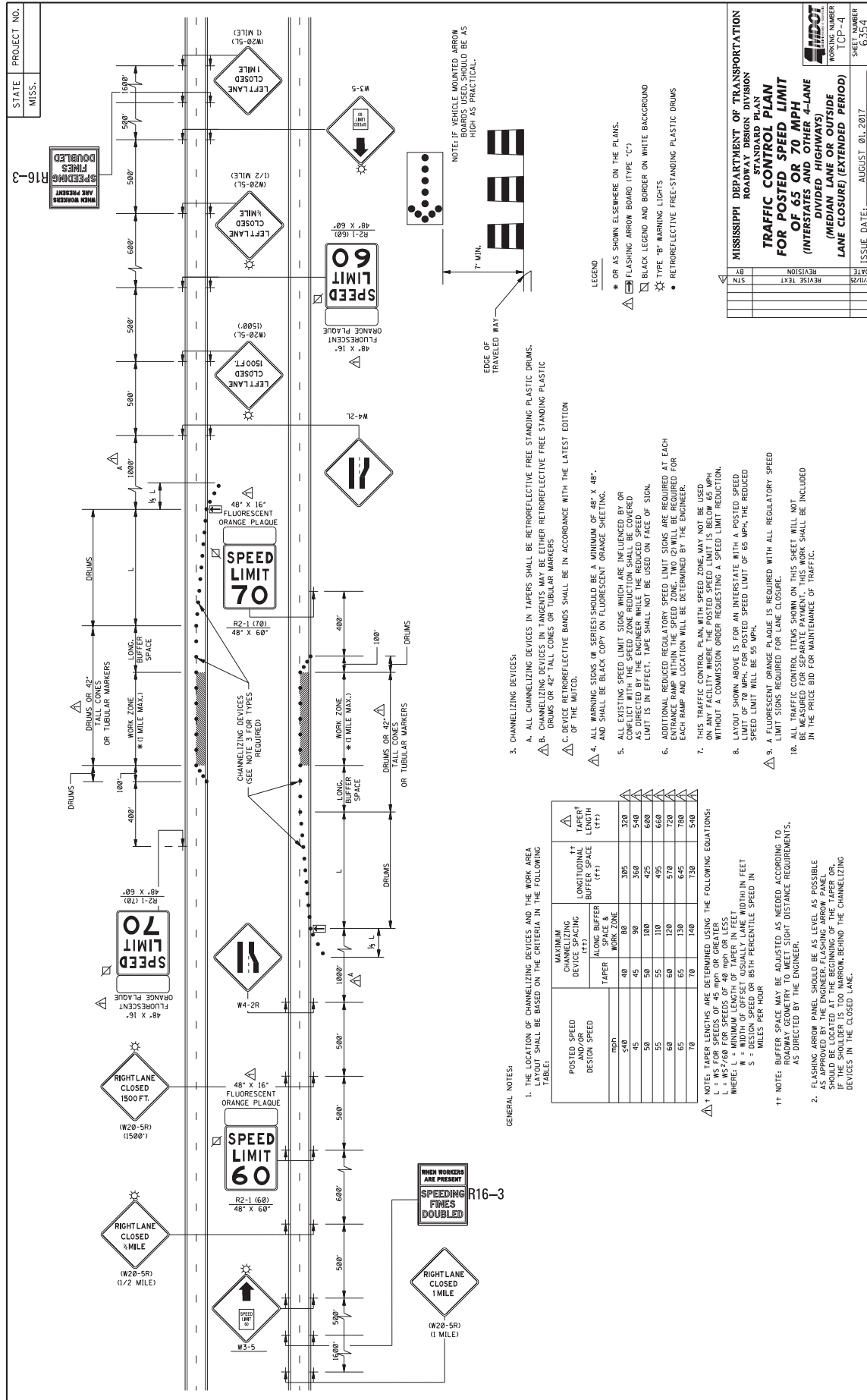




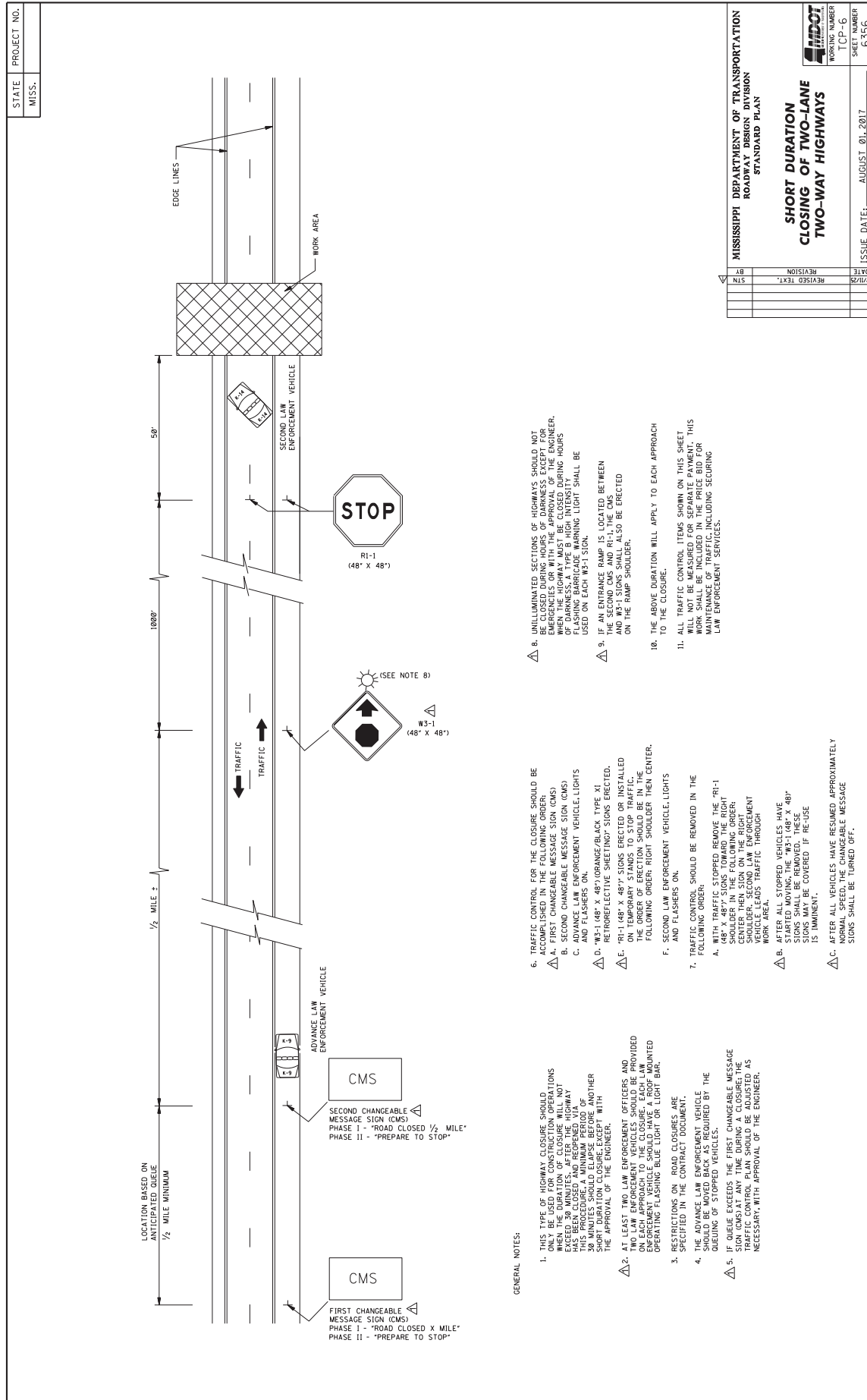


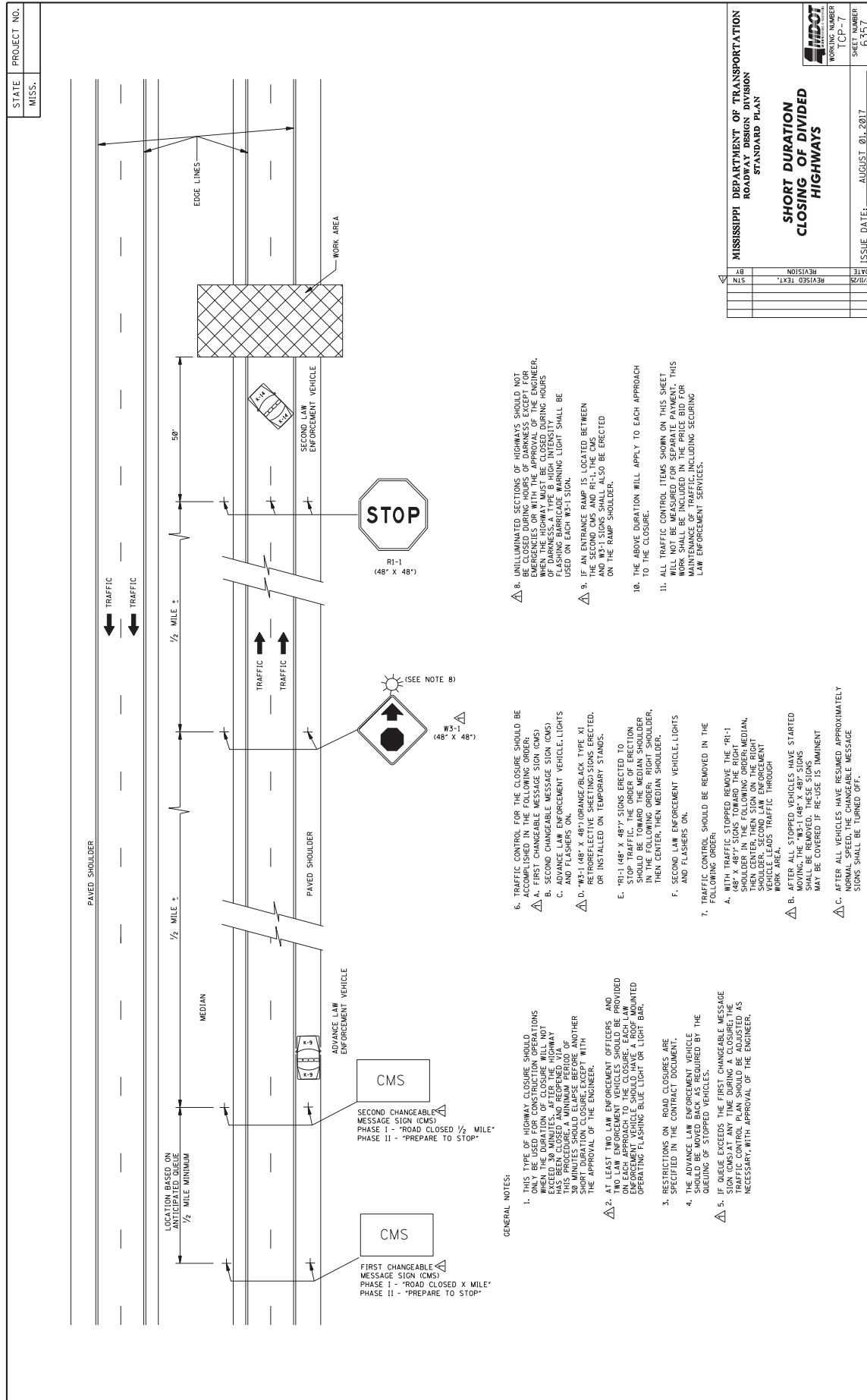












WING BARRICADES

1. WING BARRICADES ARE TYPE III BARRICADES ERRECTED ON THE SHOULDER OF A TWO-LANE ROADWAY OR THE SHOULDER OF A FOUR-LANE ROADWAY. THE BARRICADES MAY BE USED AS A MOUNTING FOR THE ADVANCE WARNING SIGNS OR FLASHERS.

2. WING BARRICADES SHOULD BE USED:

- IN ADVANCE OF A CONSTRUCTION PROJECT EVEN WHEN NO TRAFFIC DIVERSION IS REQUIRED.
- IN ADVANCE OF ALL BRIDGE OR CULVERT WIDENING OPERATIONS.

BARRICADE CHARACTERISTICS

	I	II	III
WIDTH OF RAIL **	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.
LENGTH OF RAIL **	24" MIN. / 36" MAX.	24" MIN. / 96" MAX.	48" MIN. / 96" MAX.
WIDTH OF STRIPE *	6"	6"	6"
HEIGHT	36" MIN.	36" MIN.	60" MIN.
NUMBER OF RETROREFLECTORIZED RAIL FACES	2 ONE EACH DIRECTION	4 (TWO EACH DIRECTION)	3 IF FACING TRAFFIC IN ONE DIRECTION 6 IF FACING TRAFFIC IN TWO DIRECTIONS

* 1. FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.

** 2. BARRICADES INTENDED FOR USE ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH SPEED ROADWAYS, SHALL HAVE A MINIMUM OF 270 IN. OF REFLECTIVE AREA FACING TRAFFIC.

STANDARD BARRICADES

1. THE RETROREFLECTIVE MARKING FOR BARRICADE RAILS SHALL BE ORANGE AND WHITE SLOPING DOWNWARD AT AN ANGLE OF 30° TO THE HORIZONTAL, STARTING FROM THE TOP PASS.

2. RAIL STRIPE SHOULD BE 6 INCHES EXCEPT THAT 4-INCH WIDE STRIPES MAY BE USED IF RAIL LENGTHS ARE LESS THAN 36 INCHES.

3. DO NOT PLACE SANDBAGS OR OTHER DEVICES ON BARRICADE RAILS TO PROVIDE MASS SANDBAGS; HOWEVER, THEY MAY BE PLACED AS BALLAST TO THE LOWER PARTS OF THE FRAME OR THE STAYS TO BARRICADES.

4. FOR ADDITIONAL INFORMATION OR DETAILS, SEE MUTCD, LATEST EDITION.

5. BARRICADES ARE CLASSIFIED BY FHWA AS CATEGORY II WORK ZONE DEVICES.

6. A LIST OF CRASHWORTHY BARRICADES AND OTHER CRASHWORTHY DEVICES: <http://highways.dot.gov/safety/fhwa/reduce-crash-severity>

7. WHERE ROAD USERS INCLUDE PEDESTRIANS, THE PROVISION OF SUPPLEMENTAL AUDIBLE INFORMATION OR DETECTABLE BARRIERS OR BARRICADES SHOULD BE PROVIDED FOR PEOPLE WITH VISION DISABILITIES.

8. BARRICADE RAIL SUPPORTS SHOULD NOT PROJECT INTO PEDESTRIAN CIRCULATION ROUTES MORE THAN 4 INCHES FOR THE SUPPORTS LOCATED BETWEEN 27 INCHES TO 88 INCHES ABOVE THE EXISTING SURFACE.

RETRORFLECTIVE CHEVRON ALIGNMENT SIGN DETAIL (WT-8)

1. A CHEVRON SIGN CONSISTS OF A BLACK CHEVRON TYPE MARKING ON AN ORANGE BACKGROUND AND SHALL POINT IN THE DIRECTION OF TRAFFIC FLOW.

2. THE CHEVRON SIGN SHALL BE MOUNTED ON CRASHWORTHY SUPPORT.

3. CHEVRON SIGNS MAY BE USED TO SUPPLEMENT OTHER STANDARD DEVICES WHERE ONE OR MORE LANES ARE CLOSED FOR CONSTRUCTION OR MAINTENANCE. THEY SHOULD BE PLACED APPROXIMATELY 2'-0" BEHIND THE LANE TRANSITION STRIPE.

4. CHEVRON SIGNS SHALL BE INSTALLED AT A MINIMUM HEIGHT OF 4 FEET MEASURED VERTICALLY FROM THE BOTTOM OF THE SIGN TO THE ELEVATION OF THE NEAR SIDE OF THE TRAVELWAY.

BARRICADE CLOSING A ROAD

TYPE 3 OBJECT MARKER (OM3-R)

1. TYPE 3 OBJECT MARKERS SHALL BE USED AT ALL EXPOSED BRIDGE ABUTMENTS AND AT OTHER LOCATIONS AS DEEMED NECESSARY BY THE ENGINEER.

2. THE OM3-R IS SHOWN. THE OM3-L IS SIMILAR EXCEPT THE STRIPES SLOPE DOWNWARD FROM THE UPPER LEFT SIDE TO THE LOWER RIGHT SIDE AND SHALL BE PLACED ON THE LEFT SIDE OF THE OBJECT.

3. USE OF OBJECT MARKERS SHALL BE IN LINE WITH THE INNER YELLOW SLOPING DOWNWARD TOWARD THE SIDE ON WHICH TRAFFIC IS THE PASS.

TYPE 3 OBJECT MARKER (OM3-R)

1. TYPE 3 OBJECT MARKERS SHALL BE USED AT ALL EXPOSED BRIDGE ABUTMENTS AND AT OTHER LOCATIONS AS DEEMED NECESSARY BY THE ENGINEER.

2. THE OM3-R IS SHOWN. THE OM3-L IS SIMILAR EXCEPT THE STRIPES SLOPE DOWNWARD FROM THE UPPER LEFT SIDE TO THE LOWER RIGHT SIDE AND SHALL BE PLACED ON THE LEFT SIDE OF THE OBJECT.

3. USE OF OBJECT MARKERS SHALL BE IN LINE WITH THE INNER YELLOW SLOPING DOWNWARD TOWARD THE SIDE ON WHICH TRAFFIC IS THE PASS.

PLASTIC CONE DETAIL (28" - 36")

1. PLASTIC DRUMS, TUBULAR MARKERS OR PLASTIC CONES SHALL BE ON END AND USED AS AN EXPEDIENT METHOD FOR TRAFFIC CHANNELIZATION. THE COLOR AND MARKING OF THE DEVICE USED SHALL BE CONSISTENT WITH MARKING STANDARDS FOR BARRICADE. THE PREDOMINANT CIRCUMFERENTIAL WHITE STRIPES AS SHOWN ON THE DETAILS.

2. DRUMS, TUBULAR MARKERS OR PLASTIC CONES SHOULD NEVER BE PLACED IN THE ROADWAY WITHOUT WARNING SIGNS.

3. WHERE PRACTICAL, PLASTIC DRUMS, TUBULAR MARKERS OR PLASTIC CONES SHOULD BE PLACED NO CLOSER THAN 3'-0" FROM THE EDGE OF TRAVELED LANE.

4. BALLAST SHALL NOT BE PLACED ON THE TOP OF THE DRUM.

PLASTIC CONE DETAIL (36" OR GREATER)

1. PLASTIC DRUMS, TUBULAR MARKERS OR PLASTIC CONES SHALL BE ON END AND USED AS AN EXPEDIENT METHOD FOR TRAFFIC CHANNELIZATION. THE COLOR AND MARKING OF THE DEVICE USED SHALL BE CONSISTENT WITH MARKING STANDARDS FOR BARRICADE. THE PREDOMINANT CIRCUMFERENTIAL WHITE STRIPES AS SHOWN ON THE DETAILS.

2. DRUMS, TUBULAR MARKERS OR PLASTIC CONES SHOULD NEVER BE PLACED IN THE ROADWAY WITHOUT WARNING SIGNS.

3. WHERE PRACTICAL, PLASTIC DRUMS, TUBULAR MARKERS OR PLASTIC CONES SHOULD BE PLACED NO CLOSER THAN 3'-0" FROM THE EDGE OF TRAVELED LANE.

4. BALLAST SHALL NOT BE PLACED ON THE TOP OF THE DRUM.

TUBULAR MARKER DETAIL

1. PLASTIC DRUMS, TUBULAR MARKERS OR PLASTIC CONES SHALL BE ON END AND USED AS AN EXPEDIENT METHOD FOR TRAFFIC CHANNELIZATION. THE COLOR AND MARKING OF THE DEVICE USED SHALL BE CONSISTENT WITH MARKING STANDARDS FOR BARRICADE. THE PREDOMINANT CIRCUMFERENTIAL WHITE STRIPES AS SHOWN ON THE DETAILS.

2. DRUMS, TUBULAR MARKERS OR PLASTIC CONES SHOULD NEVER BE PLACED IN THE ROADWAY WITHOUT WARNING SIGNS.

3. WHERE PRACTICAL, PLASTIC DRUMS, TUBULAR MARKERS OR PLASTIC CONES SHOULD BE PLACED NO CLOSER THAN 3'-0" FROM THE EDGE OF TRAVELED LANE.

4. BALLAST SHALL NOT BE PLACED ON THE TOP OF THE DRUM.

PLASTIC CONE DETAIL (28" - 36")

1. PLASTIC DRUMS, TUBULAR MARKERS OR PLASTIC CONES SHALL BE ON END AND USED AS AN EXPEDIENT METHOD FOR TRAFFIC CHANNELIZATION. THE COLOR AND MARKING OF THE DEVICE USED SHALL BE CONSISTENT WITH MARKING STANDARDS FOR BARRICADE. THE PREDOMINANT CIRCUMFERENTIAL WHITE STRIPES AS SHOWN ON THE DETAILS.

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3. WHERE PRACTICAL, PLASTIC DRUMS, TUBULAR MARKERS OR PLASTIC CONES SHOULD BE PLACED NO CLOSER THAN 3'-0" FROM THE EDGE OF TRAVELED LANE.

4. BALLAST SHALL NOT BE PLACED ON THE TOP OF THE DRUM.

STATE | PROJECT NO.
MISS. |

MOBILE OPERATIONS ON MULTILANE ROAD

TRUCK MOUNTED FLASHING ARROW BOARD

WORK VEHICLE

SHADOW VEHICLE 1

SHADOW VEHICLE 2

TRUCK MOUNTED ATTENUATOR

NOTE: USE OF CHANGEABLE MESSAGE SIGN IS OPTIONAL

LEFT LANE CLOSED AHEAD

MOBILE OPERATIONS ON TWO-LANE ROAD

WORK VEHICLE

SHADOW VEHICLE

TRUCK MOUNTED FLASHING ARROW PANEL

TRUCK MOUNTED ATTENUATOR

USE SIGN SHADE AND LEGEND APPROPRIATE TO TYPE OF WORK

OPTIONAL SIGN FOR SHORT DURATION OPERATION

MOBILE OPERATIONS ON TWO-LANE ROAD

WORK VEHICLE

SHADOW VEHICLE

TRUCK MOUNTED FLASHING ARROW PANEL

TRUCK MOUNTED ATTENUATOR

USE SIGN SHADE AND LEGEND APPROPRIATE TO TYPE OF WORK

OPTIONAL SIGN FOR SHORT DURATION OPERATION

NOTES FOR MULTILANE LANE OPERATION:

- SHADOW AND WORK VEHICLES SHALL DISPLAY HIGH-INTENSITY FLASHING RED LIGHTS. A TRUCK MOUNTED ATTENUATOR (TMA) OR ARROW BOARD SHALL BE USED WHEN A FREEWAY LANE IS CLOSED. WHEN MORE THAN ONE FREEWAY LANE IS CLOSED, A SEPARATE ARROW BOARD SHALL BE USED FOR EACH CLOSED LANE.
- SHADOW VEHICLE 2 SHOULD BE EQUIPPED WITH AN ARROW BOARD. WHEN MORE THAN ONE FREEWAY LANE IS CLOSED, APPROPRIATE LANE CLOSURE SIGN SHOULD BE PLACED ON SHADOW VEHICLE 2 SO AS NOT TO OBSCURE THE ARROW BOARD.
- SHADOW VEHICLE 1 SHOULD BE EQUIPPED WITH AN ARROW BOARD AND TRUCK-MOUNTED ATTENUATOR (TMA).
- SHADOW VEHICLE 2 SHOULD TRAVEL AT A VARYING DISTANCE FROM THE WORK OPERATION SO AS TO PROVIDE ADEQUATE SIGHT DISTANCE FOR TRAFFIC APPROACHING FROM THE REAR.
- THE SPACING BETWEEN THE WORK VEHICLES AND THE SHADOW VEHICLES, AND BETWEEN EACH SHADOW VEHICLE, SHOULD BE MINIMIZED TO DETERMINE ROAD USERS FROM DRIVING IN BETWEEN.
- ON HIGH-SPEED ROADWAYS, A THIRD SHADOW VEHICLE (NOT SHOWN) MAY BE USED WITH SHADOW VEHICLE 1 IN THE CLOSED LANE, SHADOW VEHICLE 2 STRADDLING THE EDGE LINE, AND SHADOW VEHICLE 3 ON THE SHOULDER.
- WHERE ADEQUATE SHOULDER WIDTH IS NOT AVAILABLE, SHADOW VEHICLE 3 MAY ALSO STRADDLE THE EDGE LINE.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

NOTES FOR TWO-LANE OPERATION:

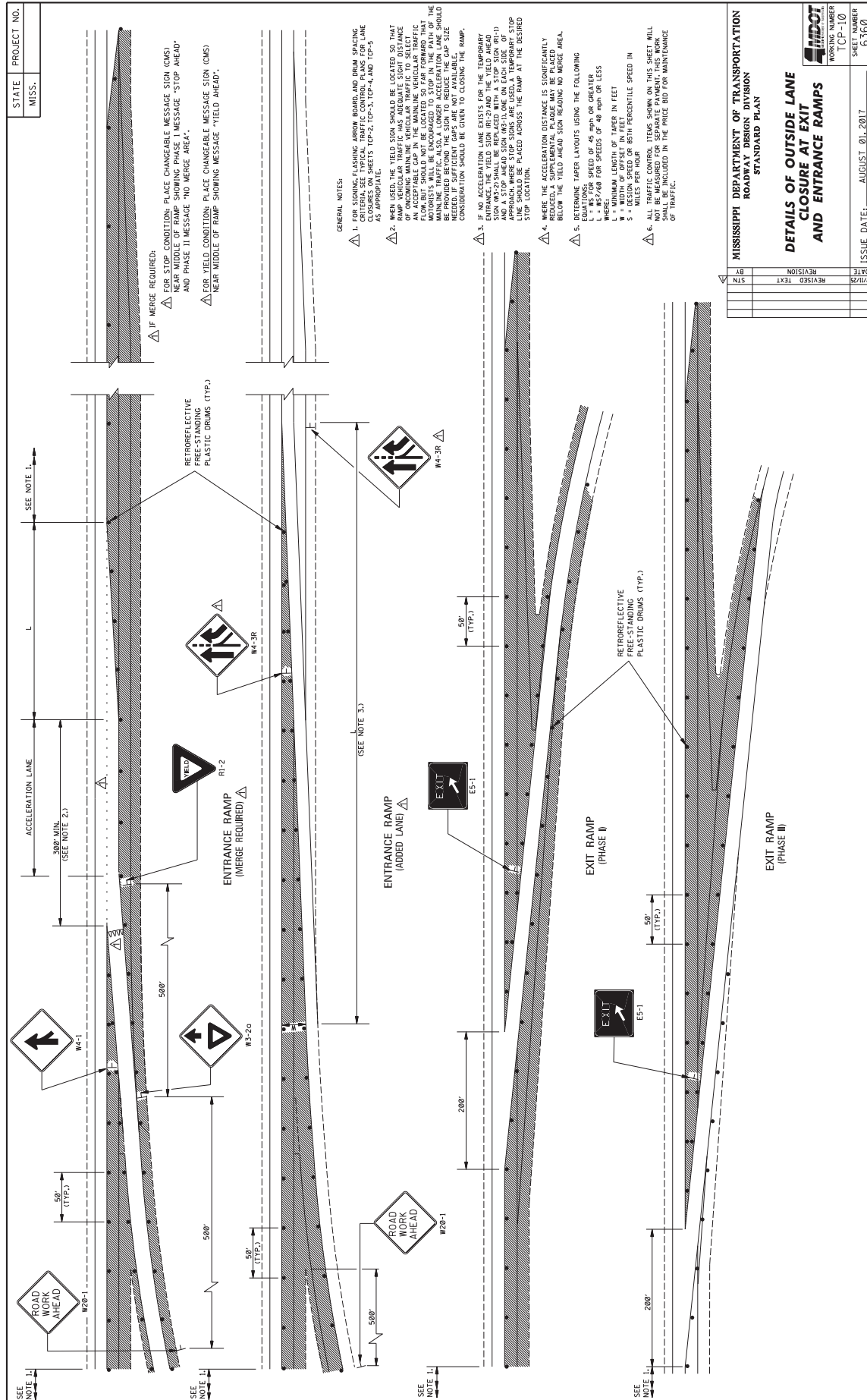
- WHERE PRACTICAL AND WHEN NEEDED, THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS. IF THIS CAN NOT BE DONE FREQUENTLY, AS AN ALTERNATIVE, A "DO NOT PASS" SIGN MAY BE PLACED ON THE REAR OF THE VEHICLE BLOCKING THE LANE.
- THE DISTANCE BETWEEN THE WORK AND SHADOW VEHICLES MAY VARY ACCORDING TO TERRAIN, PAINT DRYING TIME, AND OTHER FACTORS. SHADOW VEHICLES ARE USED TO WARN TRAFFIC OF THE OPERATION AHEAD. WHENEVER ADEQUATE SIGHT DISTANCE IS NOT AVAILABLE, SHADOW VEHICLES SHOULD BE USED TO MAINTAIN THE MINIMUM DISTANCE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. THE SHADOW VEHICLE SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- ADDITIONAL SHADOW VEHICLES TO WARN AND REDUCE THE SPEED OF ONCOMING OR OPPOSING TRAFFIC MAY BE USED. POLICE PATROL CARS MAY BE USED FOR THIS PURPOSE.
- A TRUCK-MOUNTED ATTENUATOR (TMA) SHOULD BE USED ON THE SHADOW VEHICLE AND MAY BE USED ON THE WORK VEHICLE.
- THE WORK VEHICLE SHALL BE EQUIPPED WITH BEACONS, AND THE SHADOW VEHICLES SHALL BE EQUIPPED WITH BEACONS. BEACONS SHOULD BE MOUNTED ON THE REAR, ADJACENT TO THE SIGN, SHADOW AND WORK VEHICLES SHALL DISPLAY FLASHING OR ROTATING BEACONS BOTH FORWARD AND TO THE REAR.
- VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBSCURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
- ARROW BOARD TO BE USED IN CAUTION MODE.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

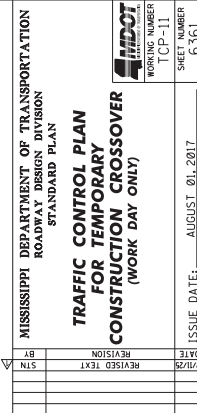
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

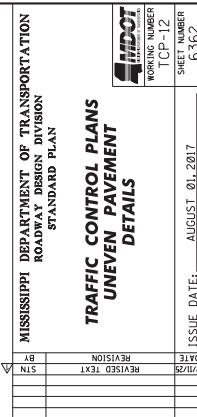
TRAFFIC CONTROL PLAN
MOBILE OPERATIONS
MULTILANE ROADS
TWO-LANE ROADS

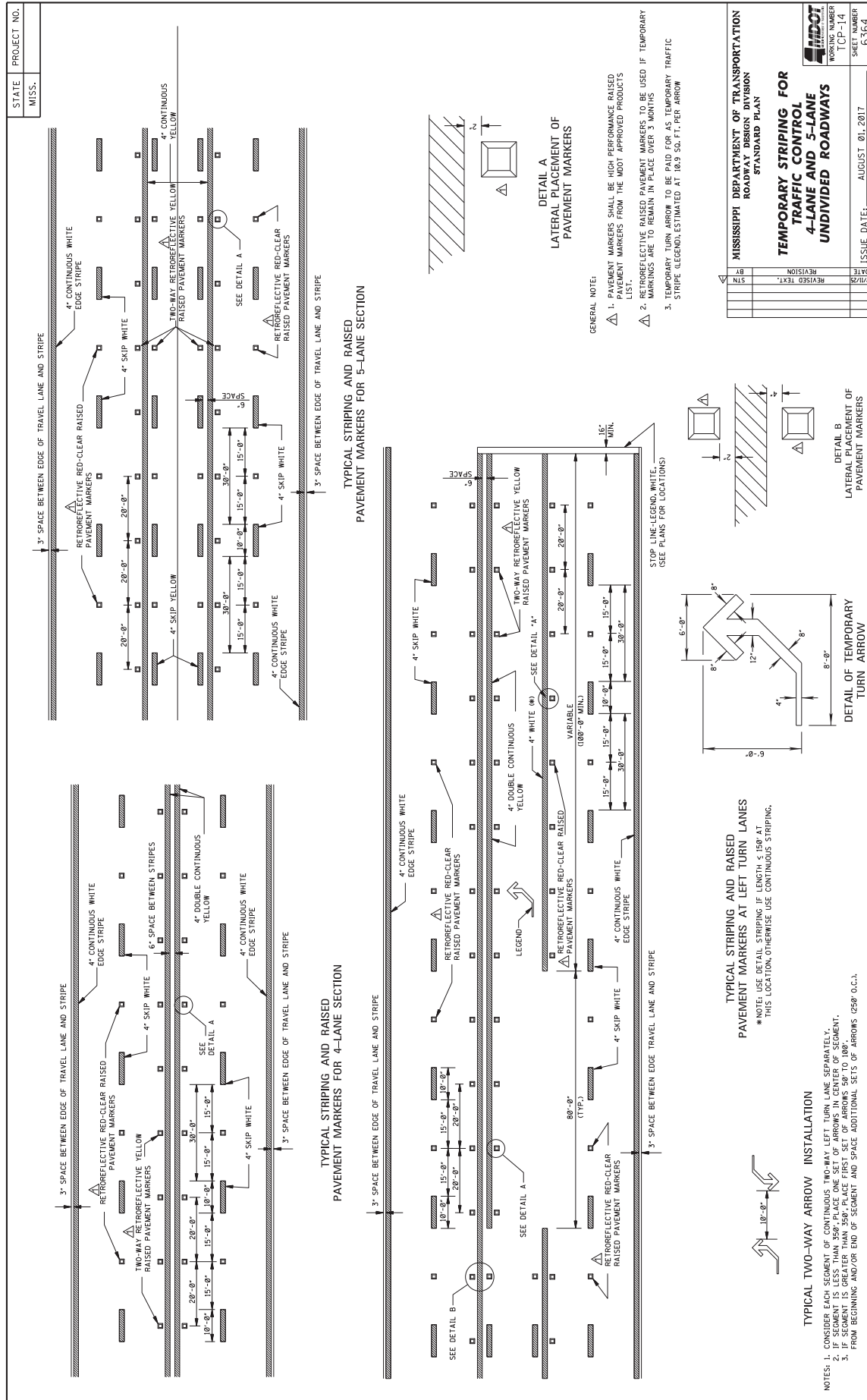
DATE	REVISION	BY

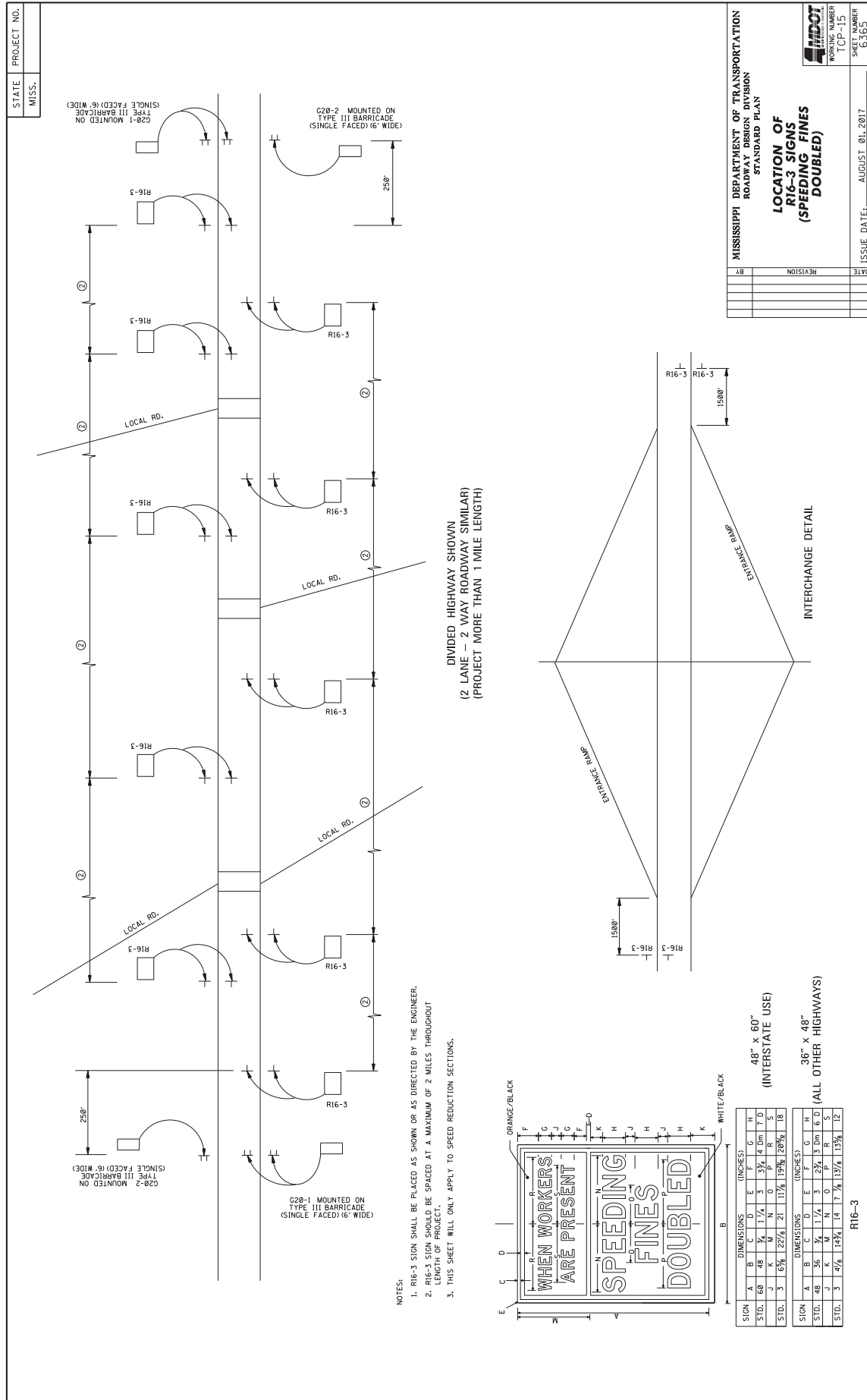
ISSUE DATE: AUGUST 01, 2017
SHEET NUMBER: TCP-9
6359











MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
ROADWAY DESIGN DIVISION	
STANDARD PLAN	
LOCATION OF R16-3 SIGNS (SPEEDING FINES DOUBLED)	
WORKING NUMBER	1CP-15
SHEET NUMBER	6365
DATE	AUGUST 01, 2017
REVISION	
BY	

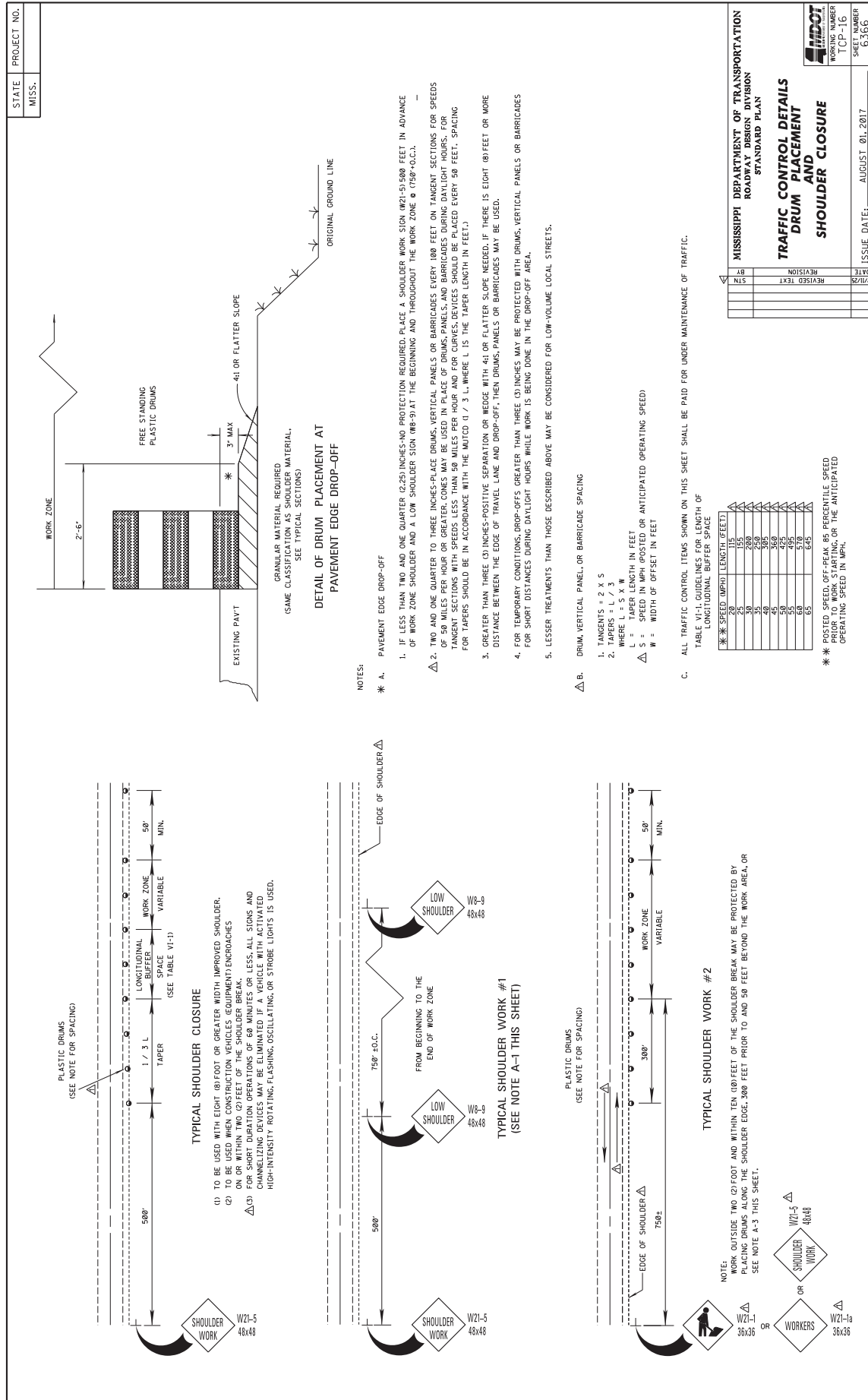


TABLE VI-1 GUIDELINES FOR LENGTH OF LONGITUDINAL BUFFER SPACE

POSTED SPEED (MPH)	LENGTH (FEET)
25	135
30	165
35	200
40	235
45	265
50	300
55	335
60	370
65	405

* POSTED SPEED, OFF-PEAK 85 PERCENTILE SPEED PRIOR TO WORK STARTING, OR THE ANTICIPATED OPERATING SPEED IN MPH.

"General Decision Number: MS20260129 01/02/2026

Superseded General Decision Number: MS20250129

State: Mississippi

Construction Type: Highway

County: Copiah County in Mississippi.

HIGHWAY CONSTRUCTION PROJECTS

Modification Number Publication Date
0 01/02/2026

ELEC0480-010 01/01/2025

	Rates	Fringes
TRAFFIC SIGNALIZATION		
Electrician.....	\$ 30.35	11.39

SUMS2010-052 08/04/2014		

	Rates	Fringes
CARPENTER (Form Work Only).....	\$ 13.28	0.00
CEMENT MASON/CONCRETE FINISHER...	\$ 15.01	0.00
ELECTRICIAN.....	\$ 24.04	5.87
HIGHWAY/PARKING LOT STRIPING:		
Truck Driver (Line Striping		
Truck).....	\$ 12.05	0.00
INSTALLER - GUARDRAIL.....	\$ 12.07	0.00
INSTALLER - SIGN.....	\$ 12.38	0.00
IRONWORKER, REINFORCING.....	\$ 15.47	0.00
LABORER: Common or General,		
Including Asphalt Raking,		
Shoveling, Spreading.....	\$ 10.06	0.00
LABORER: Flagger.....	\$ 10.40	0.00
LABORER: Grade Checker.....	\$ 9.94	0.00
LABORER: Luteman.....	\$ 12.88	0.00
LABORER: Mason Tender -		
Cement/Concrete.....	\$ 11.27	0.00
LABORER: Pipelayer.....	\$ 13.44	0.00
LABORER: Laborer-Cones/		
Barricades/Barrels -		
Setter/Mover/Sweeper.....	\$ 8.25	0.00
OPERATOR: Asphalt Spreader.....	\$ 14.71	0.00
OPERATOR:		

Backhoe/Excavator/Trackhoe.....	\$ 14.69	0.00
OPERATOR: Bobcat/Skid		
Steer/Skid Loader.....	\$ 11.64	0.00
OPERATOR: Broom/Sweeper.....	\$ 10.14	0.00
OPERATOR: Bulldozer.....	\$ 15.25	0.00
OPERATOR: Concrete Saw.....	\$ 16.50	0.00
OPERATOR: Crane.....	\$ 19.22	0.00
OPERATOR: Distributor.....	\$ 11.17	0.00
OPERATOR: Grader/Blade.....	\$ 11.67	0.00
OPERATOR: Grinding/Grooving		
Machine.....	\$ 16.86	0.00
OPERATOR: Loader.....	\$ 16.75	0.00
OPERATOR: Mechanic.....	\$ 19.16	0.00
OPERATOR: Milling Machine.....	\$ 14.68	0.00
OPERATOR: Mixer.....	\$ 14.25	0.00
OPERATOR: Oiler.....	\$ 12.35	0.00
OPERATOR: Paver (Asphalt,		
Aggregate, and Concrete).....	\$ 11.40	0.00
OPERATOR: Roller (All Types)....	\$ 11.29	0.00
OPERATOR: Scraper.....	\$ 12.25	0.00
OPERATOR: Tractor.....	\$ 11.22	0.00
TRUCK DRIVER: Flatbed Truck.....	\$ 14.06	0.00
TRUCK DRIVER: Lowboy Truck.....	\$ 11.96	0.00
TRUCK DRIVER: Mechanic.....	\$ 13.00	0.00
TRUCK DRIVER: Water Truck.....	\$ 19.00	0.00
TRUCK DRIVER: Dump Truck (All		
Types).....	\$ 12.66	0.00
TRUCK DRIVER: Semi/Trailer		
Truck.....	\$ 14.60	0.00

WELDERS - Receive rate prescribed for craft performing operation to which welding is incidental.

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Note: Executive Order (EO) 13706, Establishing Paid Sick Leave for Federal Contractors applies to all contracts subject to the Davis-Bacon Act for which the contract is awarded (and any solicitation was issued) on or after January 1, 2017. If this contract is covered by the EO, the contractor must provide employees with 1 hour of paid sick leave for every 30 hours

they work, up to 56 hours of paid sick leave each year. Employees must be permitted to use paid sick leave for their own illness, injury or other health-related needs, including preventive care; to assist a family member (or person who is like family to the employee) who is ill, injured, or has other health-related needs, including preventive care; or for reasons resulting from, or to assist a family member (or person who is like family to the employee) who is a victim of, domestic violence, sexual assault, or stalking. Additional information on contractor requirements and worker protections under the EO is available at <https://www.dol.gov/agencies/whd/government-contracts>.

Note: Executive Order 13658 generally applies to contracts subject to the Davis-Bacon Act that were awarded on or between January 1, 2015 and January 29, 2022, and that have not been renewed or extended on or after January 30, 2022. Executive Order 13658 does not apply to contracts subject only to the Davis-Bacon Related Acts regardless of when they were awarded. If a contract is subject to Executive Order 13658, the contractor must pay all covered workers at least \$13.30 per hour (or the applicable wage rate listed on this wage determination, if it is higher) for all hours spent performing on the contract in 2025. The applicable Executive Order minimum wage rate will be adjusted annually. Additional information on contractor requirements and worker protections under Executive Order 13658 is available at www.dol.gov/whd/govcontracts.

Unlisted classifications needed for work not included within the scope of the classifications listed may be added after award only as provided in the labor standards contract clauses (29CFR 5.5 (a) (1) (iii)).

The body of each wage determination lists the classifications and wage rates that have been found to be prevailing for the type(s) of construction and geographic area covered by the wage determination. The classifications are listed in alphabetical order under rate identifiers indicating whether the particular rate is a union rate (current union negotiated rate), a survey rate, a weighted union average rate, a state adopted rate, or a supplemental classification rate.

Union Rate Identifiers

A four-letter identifier beginning with characters other than ""SU"", ""UAVG"", ?SA?, or ?SC? denotes that a union rate was prevailing for that classification in the survey. Example: PLUM0198-005 07/01/2024. PLUM is an identifier of the union whose collectively bargained rate prevailed in the survey for this classification, which in this example would be Plumbers. 0198 indicates the local union number or district council number where applicable, i.e., Plumbers Local 0198. The next number, 005 in the example, is an internal number used in processing the wage determination. The date, 07/01/2024 in the example, is the effective date of the most current negotiated rate.

Union prevailing wage rates are updated to reflect all changes over time that are reported to WHD in the rates in the collective bargaining agreement (CBA) governing the classification.

Union Average Rate Identifiers

The UAVG identifier indicates that no single rate prevailed for those classifications, but that 100% of the data reported for the classifications reflected union rates. EXAMPLE:

UAVG-OH-0010 01/01/2024. UAVG indicates that the rate is a weighted union average rate. OH indicates the State of Ohio. The next number, 0010 in the example, is an internal number used in producing the wage determination. The date, 01/01/2024 in the example, indicates the date the wage determination was updated to reflect the most current union average rate.

A UAVG rate will be updated once a year, usually in January, to reflect a weighted average of the current rates in the collective bargaining agreements on which the rate is based.

Survey Rate Identifiers

The ""SU"" identifier indicates that either a single non-union rate prevailed (as defined in 29 CFR 1.2) for this classification in the survey or that the rate was derived by computing a weighted average rate based on all the rates reported in the survey for that classification. As a weighted average rate includes all rates reported in the survey, it may include both union and non-union rates. Example: SUFL2022-007 6/27/2024. SU indicates the rate is a single non-union prevailing rate or a weighted average of survey data for that classification. FL indicates the State of Florida. 2022 is the year of the survey on which these classifications and rates are based. The next number, 007 in the example, is an internal number used in producing the wage determination. The date, 6/27/2024 in the example, indicates the survey completion date for the classifications and rates under that identifier.

?SU? wage rates typically remain in effect until a new survey is conducted. However, the Wage and Hour Division (WHD) has the discretion to update such rates under 29 CFR 1.6(c)(1).

State Adopted Rate Identifiers

The ""SA"" identifier indicates that the classifications and prevailing wage rates set by a state (or local) government were adopted under 29 C.F.R 1.3(g)-(h). Example: SAME2023-007 01/03/2024. SA reflects that the rates are state adopted. ME refers to the State of Maine. 2023 is the year during which the state completed the survey on which the listed classifications and rates are based. The next number, 007 in the example, is an internal number used in producing the wage determination. The date, 01/03/2024 in the example, reflects the date on which the classifications and rates under the ?SA? identifier took effect under state law in the state from which the rates were adopted.

WAGE DETERMINATION APPEALS PROCESS

1) Has there been an initial decision in the matter? This can be:

- a) a survey underlying a wage determination
- b) an existing published wage determination
- c) an initial WHD letter setting forth a position on a wage determination matter

d) an initial conformance (additional classification and rate) determination

On survey related matters, initial contact, including requests for summaries of surveys, should be directed to the WHD Branch of Wage Surveys. Requests can be submitted via email to davisbaconinfo@dol.gov or by mail to:

Branch of Wage Surveys
Wage and Hour Division
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

Regarding any other wage determination matter such as conformance decisions, requests for initial decisions should be directed to the WHD Branch of Construction Wage Determinations. Requests can be submitted via email to BCWD-Office@dol.gov or by mail to:

Branch of Construction Wage Determinations
Wage and Hour Division
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

2) If an initial decision has been issued, then any interested party (those affected by the action) that disagrees with the decision can request review and reconsideration from the Wage and Hour Administrator (See 29 CFR Part 1.8 and 29 CFR Part 7). Requests for review and reconsideration can be submitted via email to dba.reconsideration@dol.gov or by mail to:

Wage and Hour Administrator
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

The request should be accompanied by a full statement of the interested party's position and any information (wage payment data, project description, area practice material, etc.) that the requestor considers relevant to the issue.

3) If the decision of the Administrator is not favorable, an interested party may appeal directly to the Administrative Review Board (formerly the Wage Appeals Board). Write to:

Administrative Review Board
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210.

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END OF GENERAL DECISION

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