

**SECTION 905 -- PROPOSAL (CONTINUED)**

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO. <u>  1  </u>	DATED <u>  1/21/2026  </u>	ADDENDUM NO. <u>      </u>	DATED <u>      </u>
ADDENDUM NO. <u>      </u>	DATED <u>      </u>	ADDENDUM NO. <u>      </u>	DATED <u>      </u>
ADDENDUM NO. <u>      </u>	DATED <u>      </u>	ADDENDUM NO. <u>      </u>	DATED <u>      </u>

Number	Description
1	Revised Table of Contents; Notice to Bidder No. 7624 Replace Notice to Bidder No. 3599; Deleted Notice to Bidder No. 5750; Revised Wage Rates; Amendment EBSx Download Required.

TOTAL ADDENDA:   1    
(Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE \_\_\_\_\_

\_\_\_\_\_  
Contractor

BY \_\_\_\_\_  
Signature

TITLE \_\_\_\_\_

ADDRESS \_\_\_\_\_

CITY, STATE, ZIP \_\_\_\_\_

PHONE \_\_\_\_\_

FAX \_\_\_\_\_

E-MAIL \_\_\_\_\_

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of \_\_\_\_\_ and the names, titles and business addresses of the executives are as follows:

_____	President	Address
_____	Secretary	Address
_____	Treasurer	Address

The following is my (our) itemized proposal.  
 STBG-0013-02(036)/ 109020301000  
 Copiah County(ies)  
 Revised 01/26/2016

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
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**PROJECT: STBG-0013-02(036)/109020301 - Copiah**

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Progress Schedule

(REVISIONS TO THE ABOVE WILL BE INDICATED ON THE SECOND SHEET  
OF SECTION 905 AS ADDENDA)

01/21/2026 09:08 AM

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

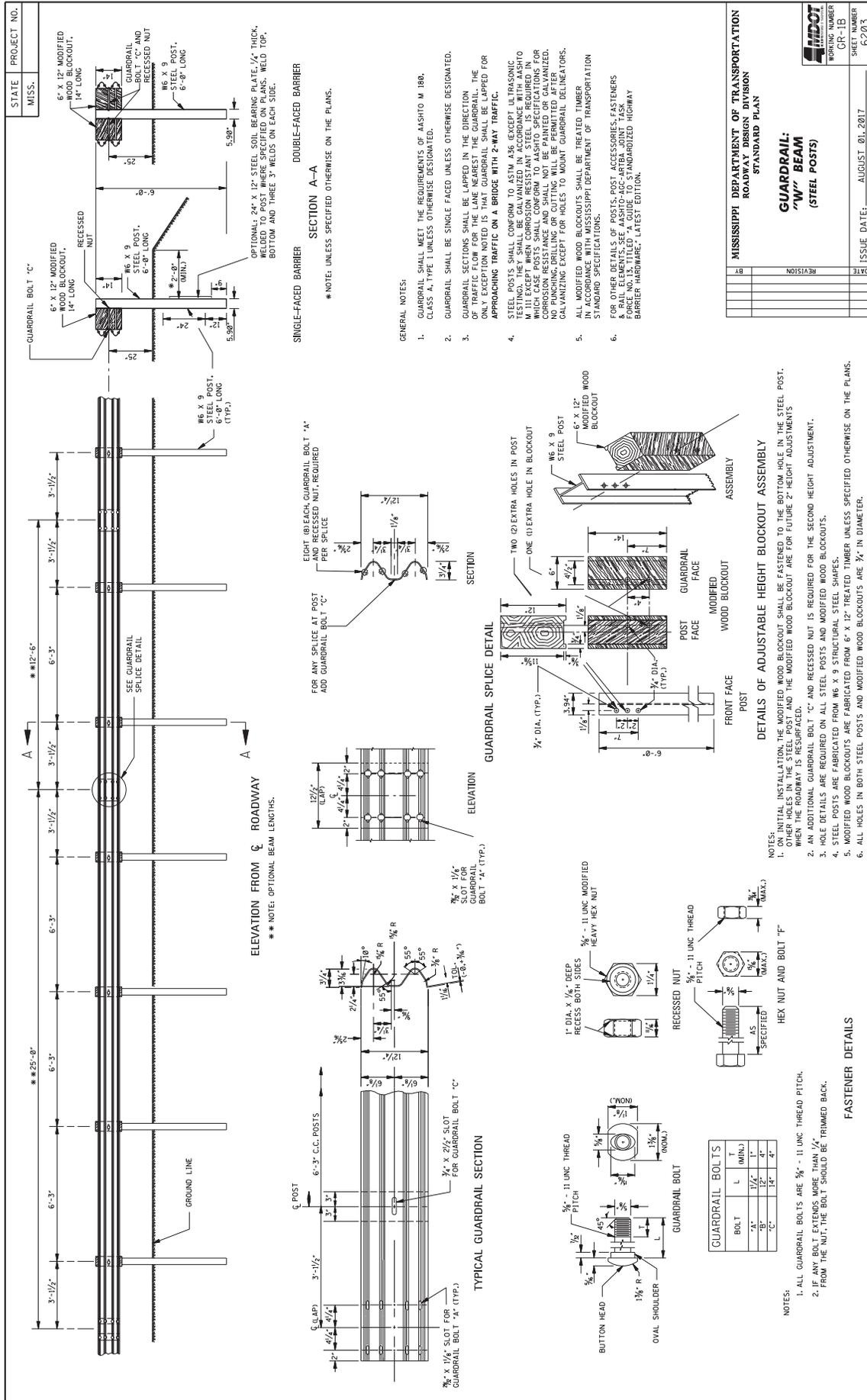
**SUPPLEMENT TO NOTICE TO BIDDERS NO. 7624**

**DATE: 01/20/2026**

After the last drawing on page 33, add the following.

















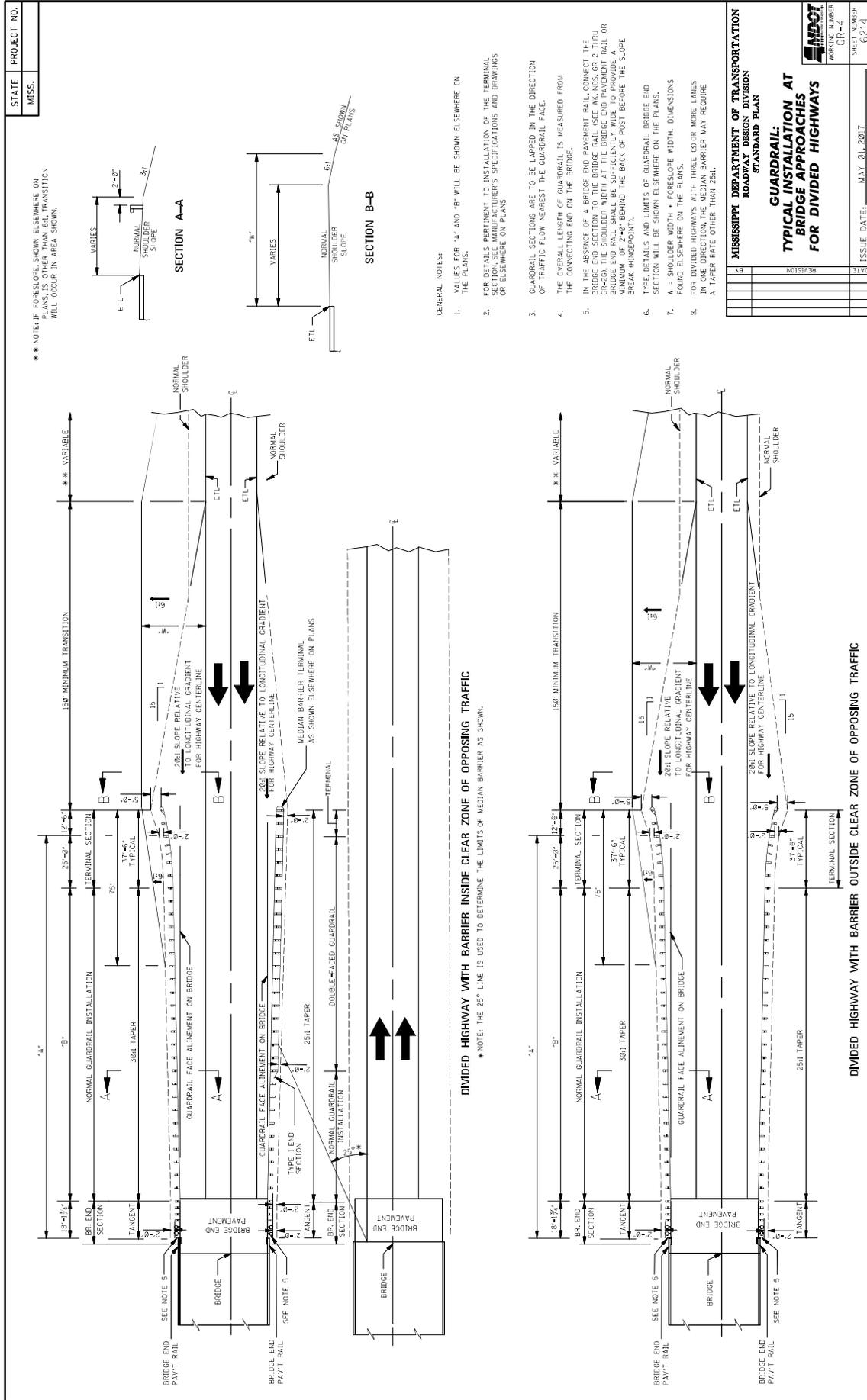












\*\* NOTE: IF FORESLOPE, SHOWN ELSEWHERE ON PLANS, IS OTHER THAN 6:1, TRANSITION WILL OCCUR IN AREA SHOWN.

GENERAL NOTES:

- VALUES FOR 'X' AND 'B' WILL BE SHOWN ELSEWHERE ON THE PLANS.
- FOR DETAILS PERTINENT TO INSTALLATION OF THE TERMINAL SECTION, SEE MANUFACTURER'S SPECIFICATIONS AND DRAWINGS OR ELSEWHERE ON PLANS.
- GUARDRAIL SECTIONS ARE TO BE LAPPED IN THE DIRECTION OF TRAFFIC FLOW NEAREST THE GUARDRAIL FACE.
- THE OVERALL LENGTH OF GUARDRAIL IS MEASURED FROM THE CONNECTING END ON THE BRIDGE.
- IN THE ABSENCE OF A BRIDGE END PAVEMENT RAIL, CONNECT THE BRIDGE END SECTION TO THE BRIDGE RAIL (SEE W.C. NO. 100-2 THRU 100-20). THE SHOULDER WIDTH AT THE BRIDGE END PAVEMENT RAIL OR BRIDGE END SHALL BE SUFFICIENTLY WIDE TO PROVIDE A BREAK HINGEPONT BEHIND THE BACK OF POST BEFORE THE SCOPE.
- TYPE DETAILS AND LIMITS OF GUARDRAIL BRIDGE END SECTION WILL BE SHOWN ELSEWHERE ON THE PLANS.
- 'W' IS SHOULDER WIDTH & FORESLOPE WIDTH, DIMENSIONS FOUND ELSEWHERE ON THE PLANS.
- FOR DIVIDED HIGHWAYS WITH THREE OR MORE LANES IN ONE DIRECTION, THE MEDIAN BARRIER MAY REQUIRE A TAPER RATE OTHER THAN 25:1.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
<b>TYPICAL INSTALLATION AT BRIDGE APPROACHES FOR DIVIDED HIGHWAYS</b>	
DATE	ISSUE DATE: MAY 01, 2017
REVISION	SHEET NUMBER CR-4
	WORKING NUMBER 6214

**DIVIDED HIGHWAY WITH BARRIER INSIDE CLEAR ZONE OF OPPOSING TRAFFIC**

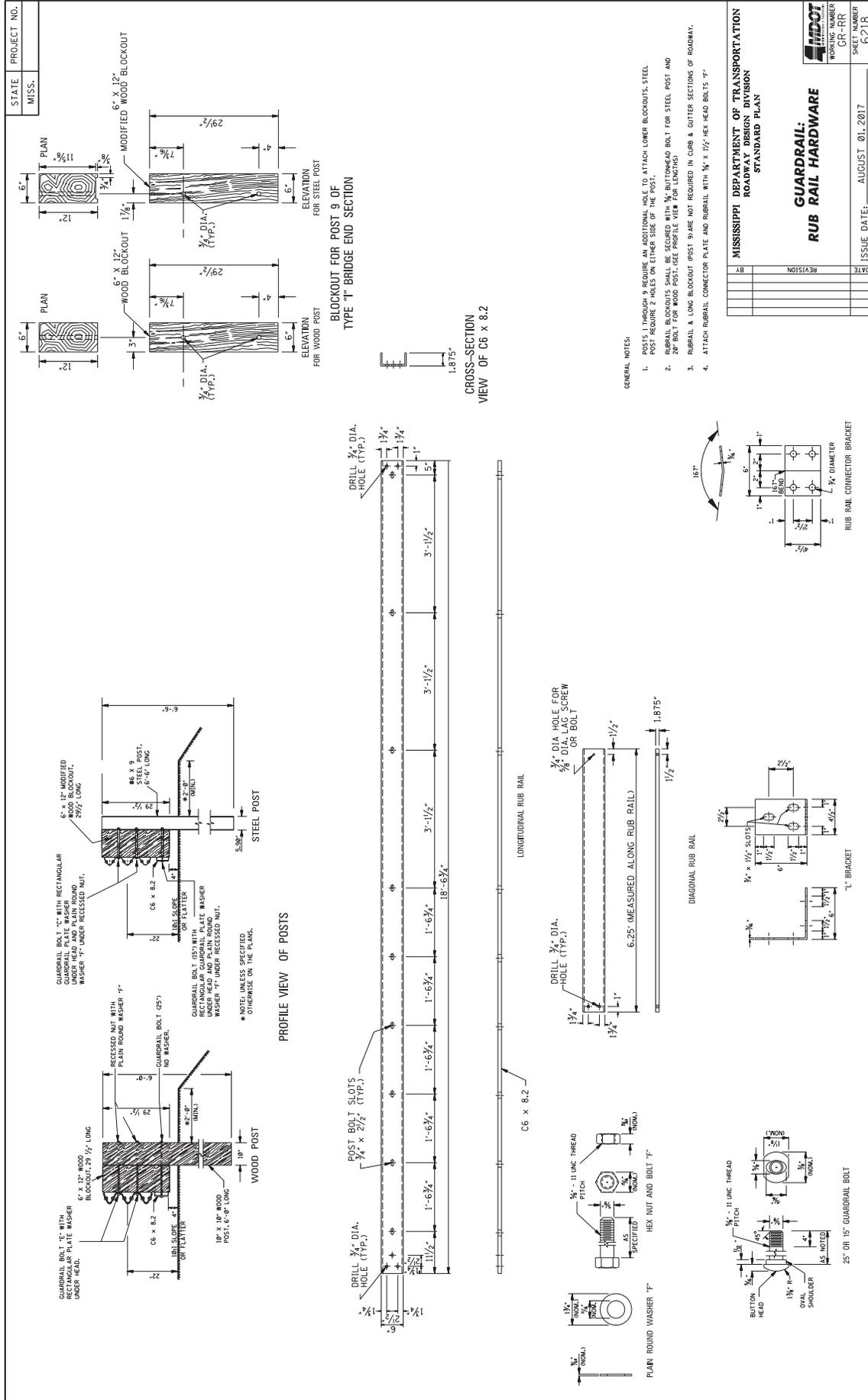
\* NOTE: THE 25° LINE IS USED TO DETERMINE THE LIMITS OF MEDIAN BARRIER AS SHOWN.

**DIVIDED HIGHWAY WITH BARRIER OUTSIDE CLEAR ZONE OF OPPOSING TRAFFIC**

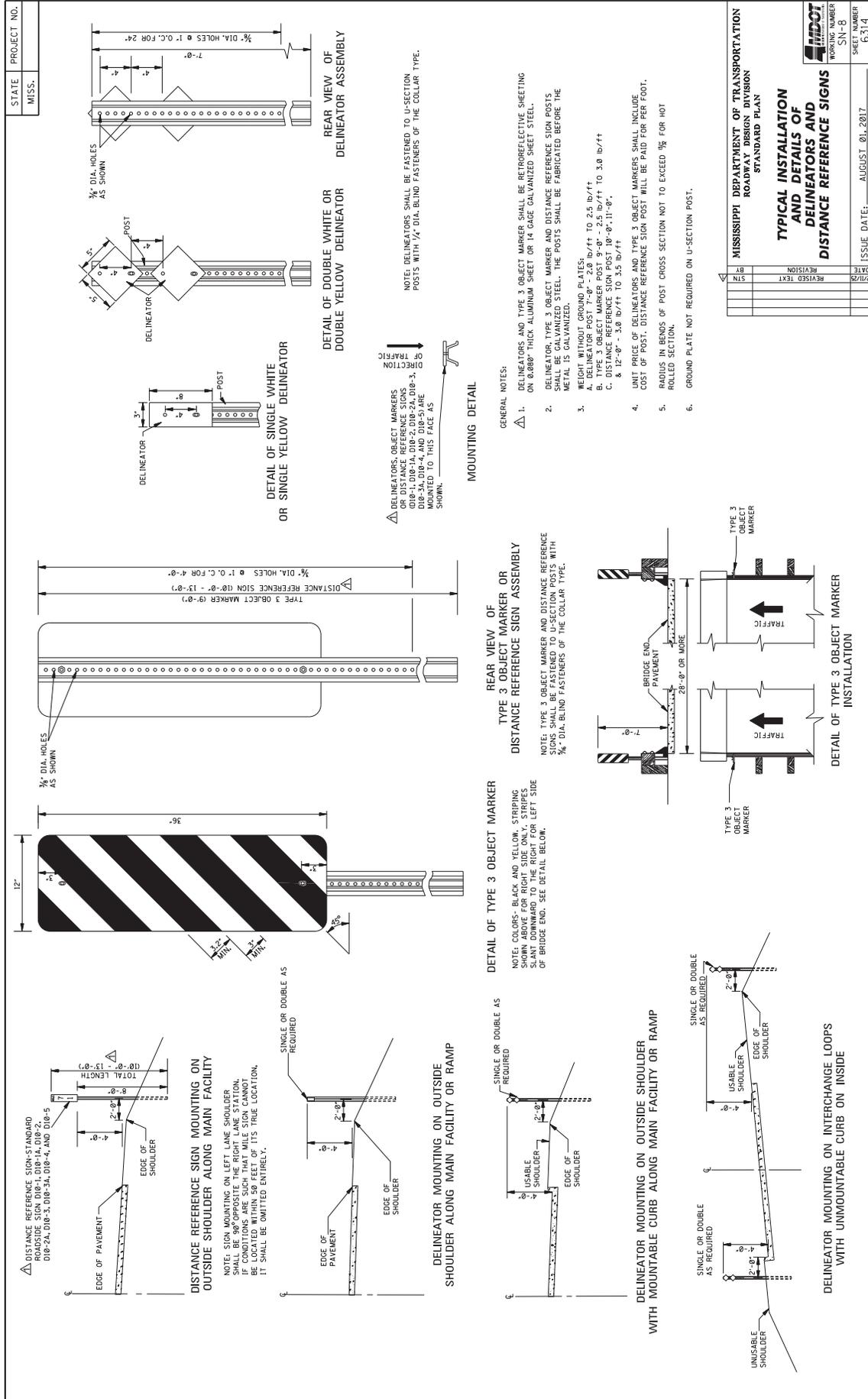












**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 – NOTICE TO BIDDERS NO. 7624**

**CODE: (SP)**

**DATE: 01/20/2026**

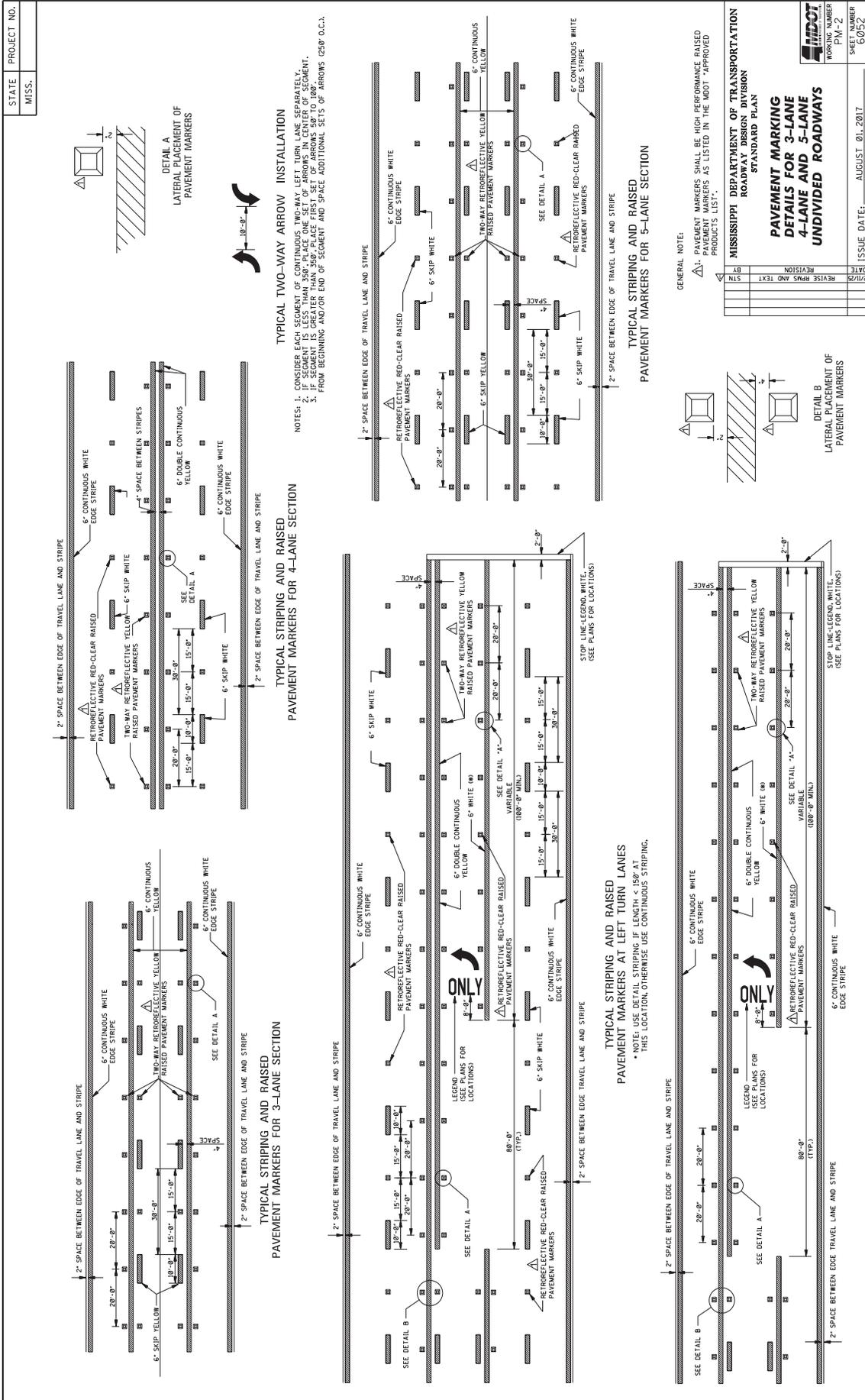
**SUBJECT: Standard Drawings**

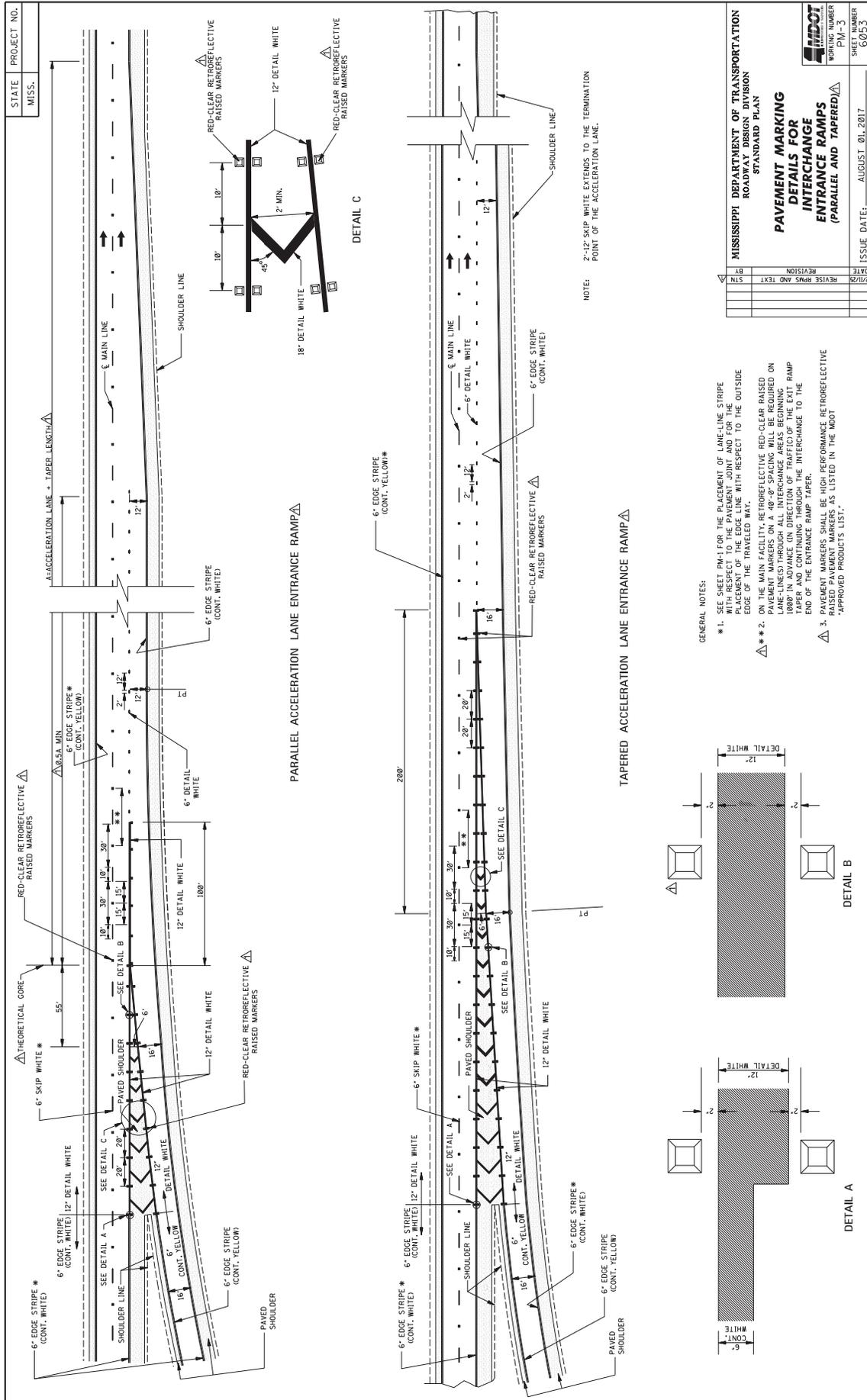
Standard Drawings attached hereto shall govern appropriate items of required work.

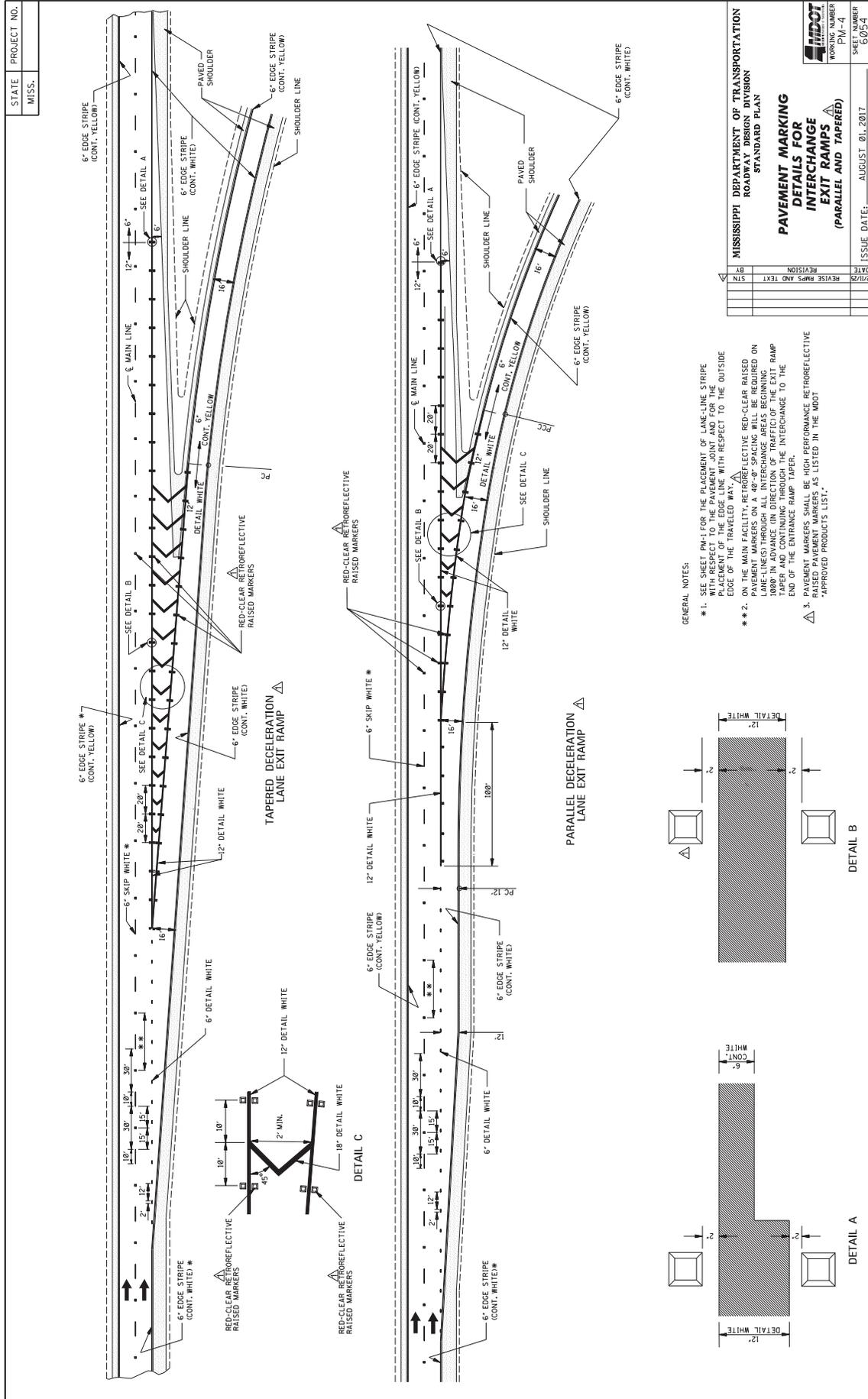
Larger copies of Standard Drawings may be purchased from:

MDOT Plans Print Shop  
MDOT Shop Complex, Building C, Room 114  
2567 North West Street  
P.O. Box 1850  
Jackson, MS 39215-1850  
Telephone: (601) 359-7460  
or FAX: (601) 359-7461  
or e-mail: [plans@mdot.state.ms.us](mailto:plans@mdot.state.ms.us)

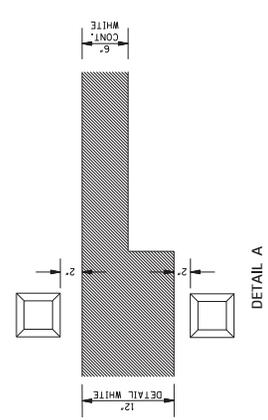
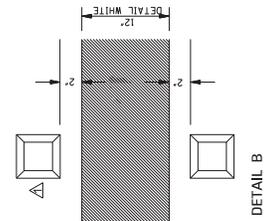








- GENERAL NOTES:
- \* 1. SEE SHEET PM-1 FOR THE PLACEMENT OF LANE-LINE STRIPE WITH RESPECT TO THE PAVEMENT JOINT AND FOR THE PLACEMENT OF THE EDGE LINE WITH RESPECT TO THE OUTSIDE EDGE OF THE PAVED SHOULDER.
  - \*\* 2. FOR THE TAPERED DECELERATION LANE EXIT RAMP, RED-CLEAR RAISED PAVEMENT MARKERS ON K-40-90 SPACING WILL BE REQUIRED ON LANE-LINES THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE IN DIRECTION OF TRAFFIC OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
  - △ 3. PAVED PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RETROREFLECTIVE \*APPROVED PRODUCTS LIST\*.

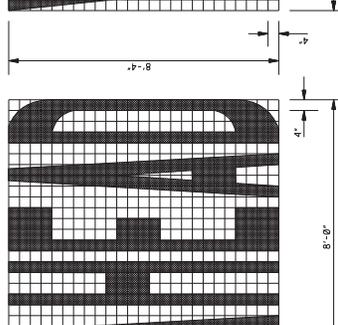
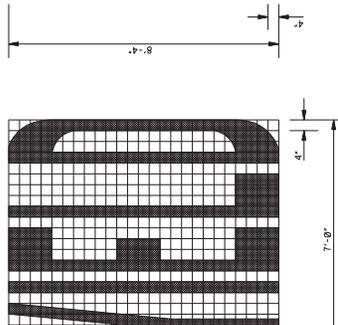
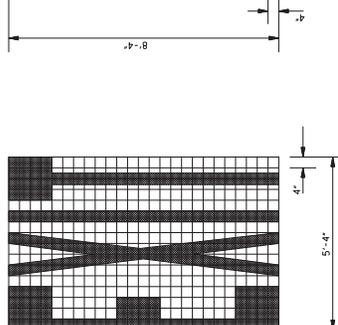
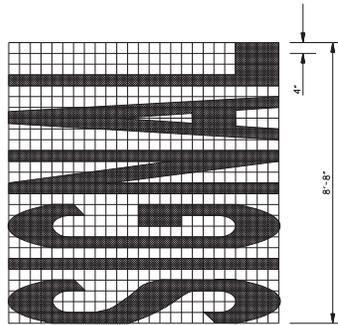
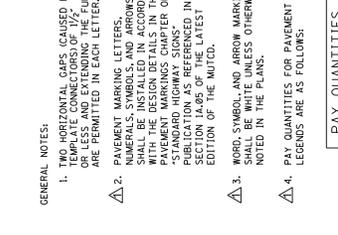
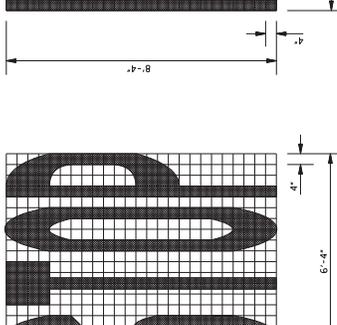
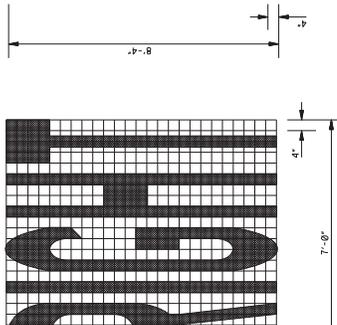
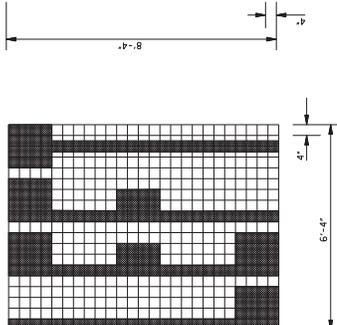
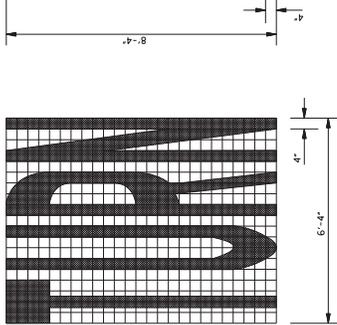
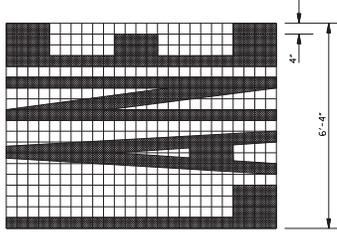


STATE PROJECT NO.		MISS.	
DATE		REVISION	
BY	DATE	BY	DATE
DRAWN		CHECKED	
SCALE		SHEET NUMBER	
		PM-4	
		SHEET NUMBER	
		6034	
		ISSUE DATE: AUGUST 01, 2017	

MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
 ROADWAY DESIGN DIVISION  
 STANDARD PLAN  
**PAVEMENT MARKING  
 DETAILS FOR  
 INTERCHANGE  
 EXIT RAMP  
 (PARALLEL AND TAPERED)**



STATE PROJECT NO.  
MISS.



GENERAL NOTES:

1. TWO HORIZONTAL GAPS (CAUSED BY UNAVAILABLE LETTERS) SHALL BE PERMITTED IN EACH LETTER. UNLESS OTHERWISE NOTED IN THE TABLES, ALL LETTERS AND ARROWS SHALL BE INSTALLED IN ACCORDANCE WITH THE REQUIREMENTS OF THE "STANDARD HIGHWAY SIGNS" PUBLICATION AS REFERENCED IN SECTION 103.04 OF THE LATEST EDITION OF THE MUTCD.
2. NUMERALS, SYMBOLS, AND ARROWS SHALL BE WHITE, UNLESS OTHERWISE NOTED IN THE TABLES.
3. WORD, SYMBOL, AND ARROW MARKINGS SHALL BE WHITE, UNLESS OTHERWISE NOTED IN THE TABLES.
4. PAY QUANTITIES FOR PAVEMENT MARKING LEGENDS ARE AS FOLLOWS:

LEGEND	AREA (FT <sup>2</sup> )
STOP	24.6
RIGHT	28.6
LEFT	19.5
TURN	27.3
LANE	25.2
AHEAD	32.3
YIELD	26.8
EXIT	18.5
SIGNAL	32.5
SCHOOL	35.5

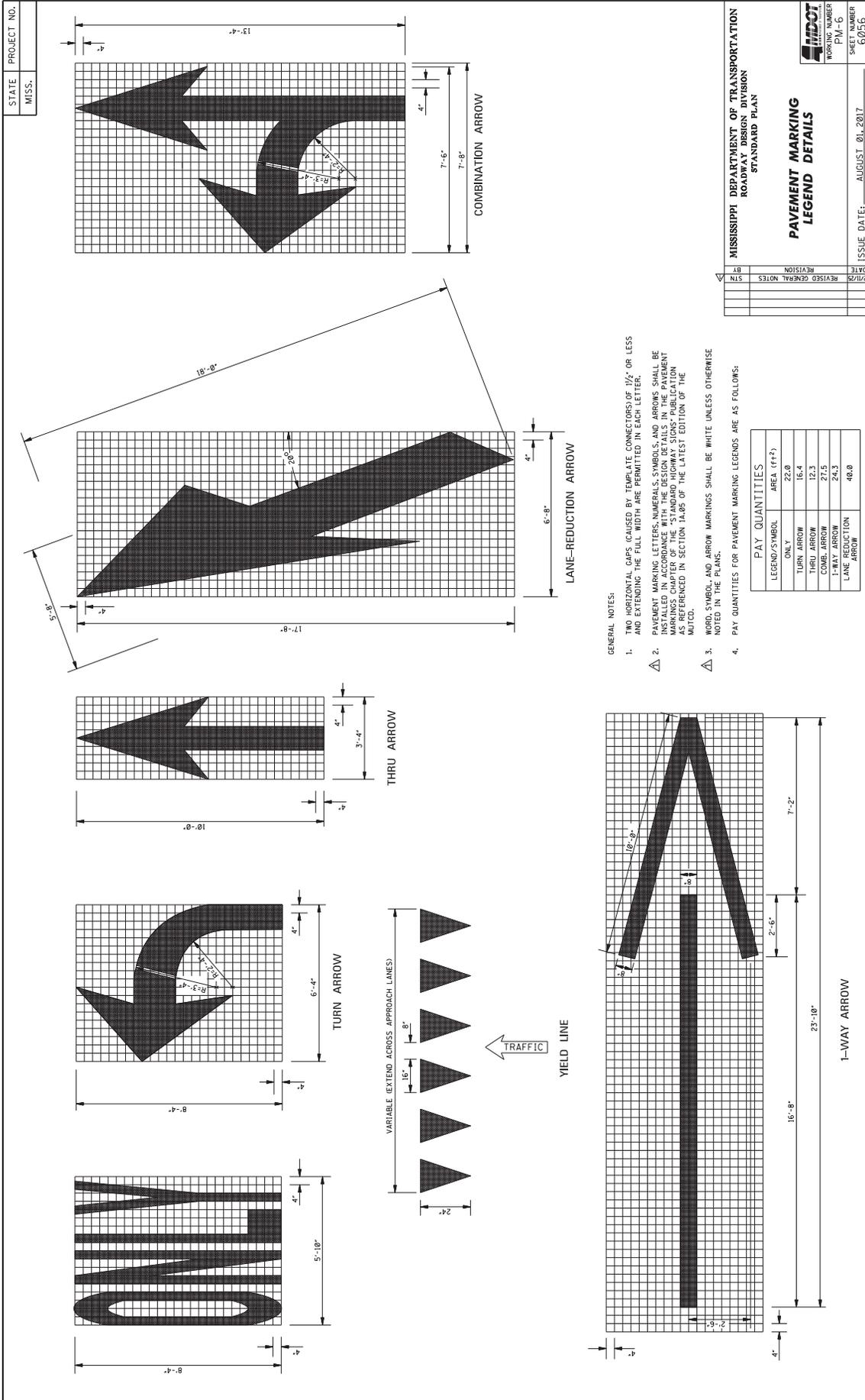
MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
ROADWAY DESIGN DIVISION  
STANDARD PLAN

**PAVEMENT MARKING  
LEGEND DETAILS**

ISSUE DATE: AUGUST 01, 2017

WORKING NUMBER: PM-5  
SHEET NUMBER: 6035

DATE	REVISION	NOTES

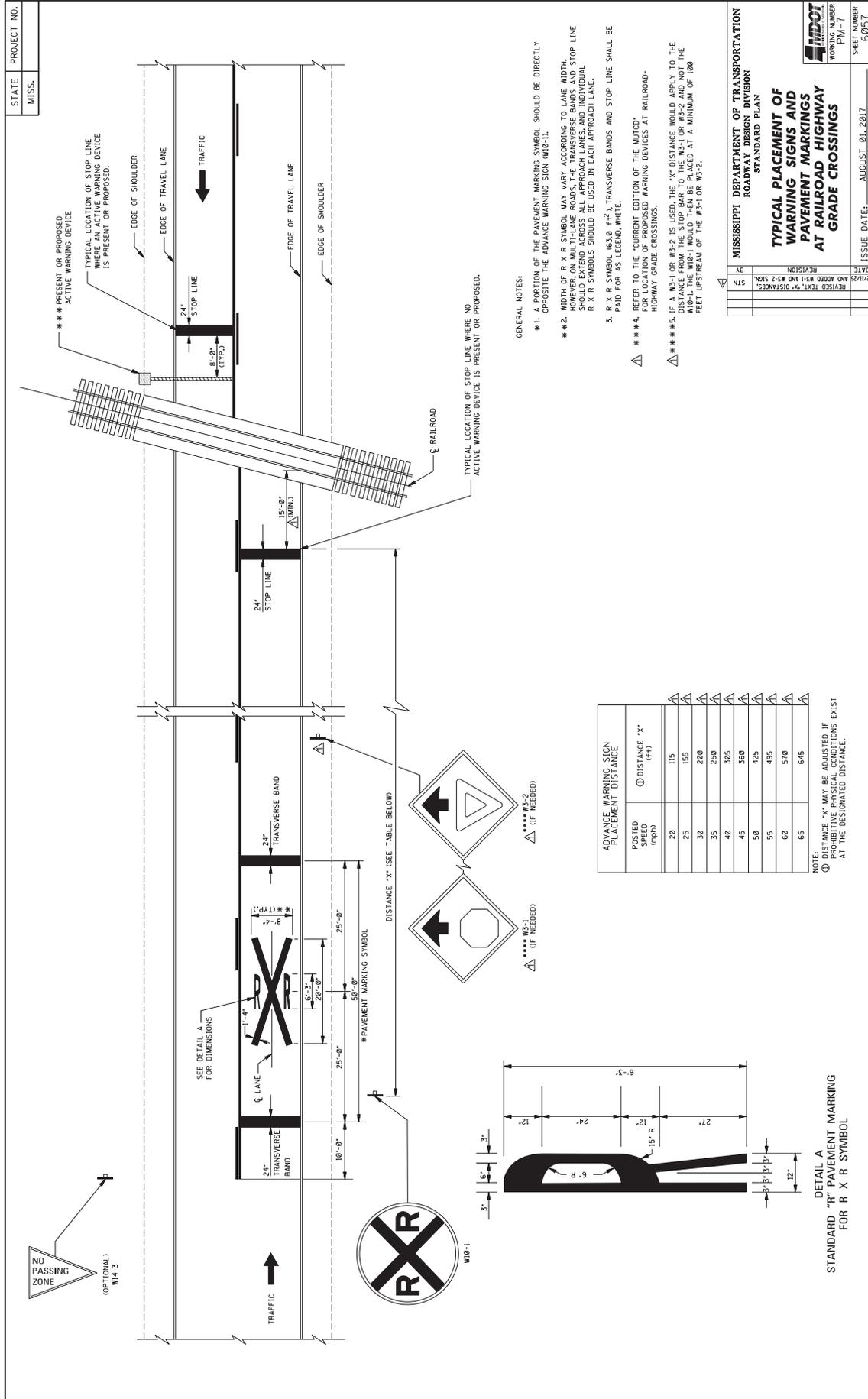


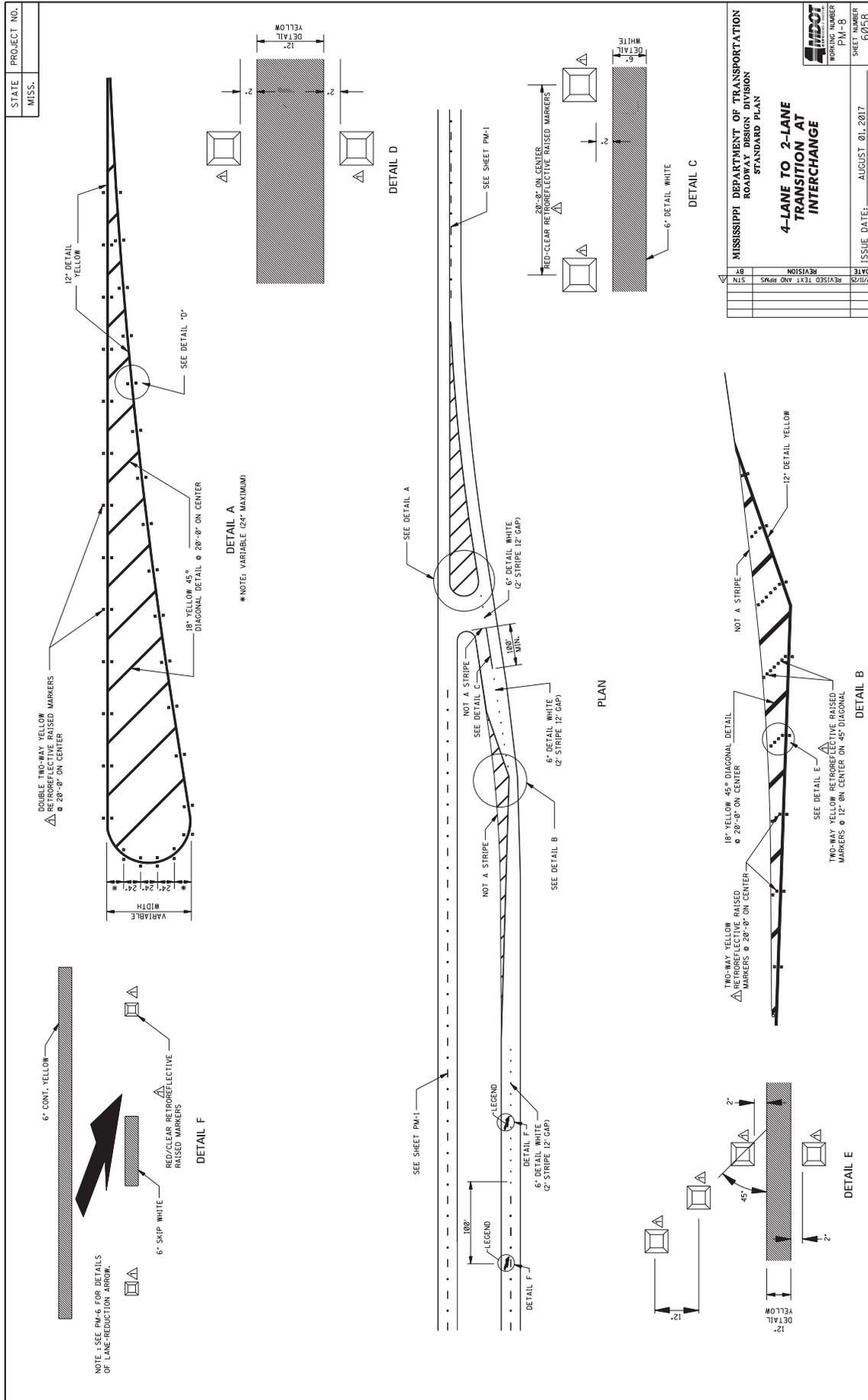
**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**  
ROADWAY DESIGN DIVISION  
STANDARD PLAN

**PAVEMENT MARKING  
LEGEND DETAILS**

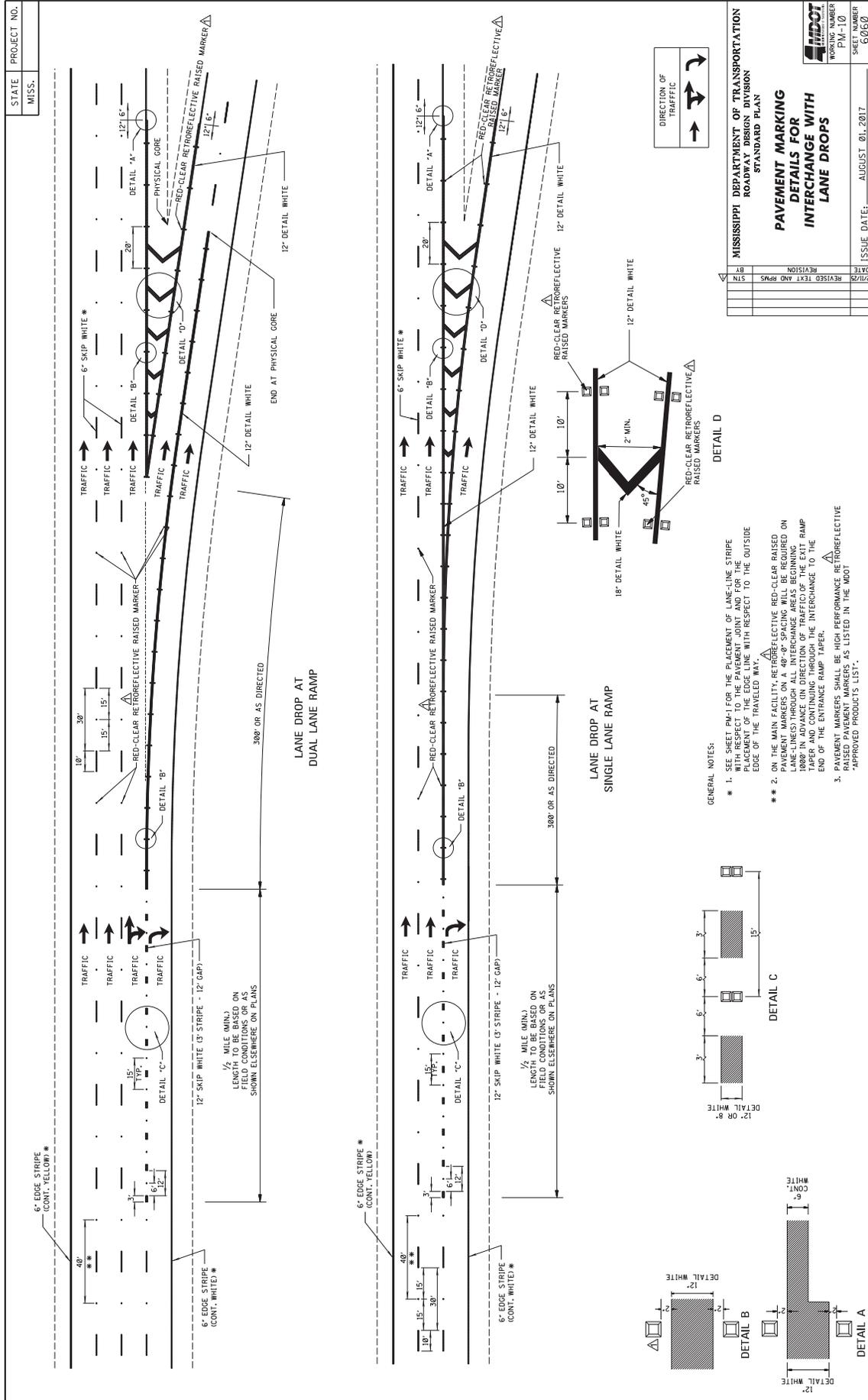
ISSUE DATE: AUGUST 01, 2017  
SHEET NUMBER: 6056

DATE	REVISION	GENERAL NOTES	BY	CHKD



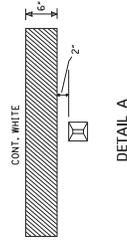
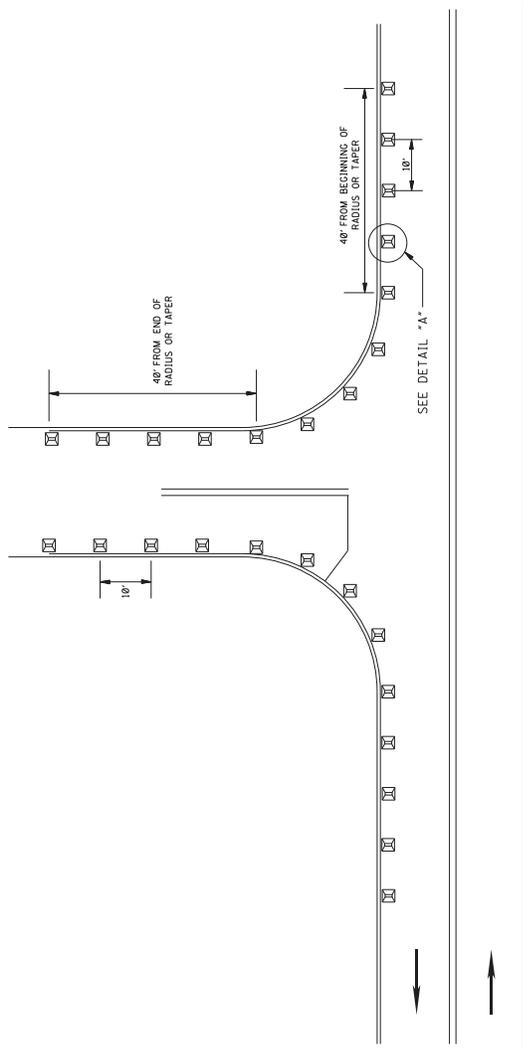






STATE	PROJECT NO.
MISS.	

△ TYPICAL PLACEMENT OF RETROREFLECTIVE RAISED PAVEMENT MARKERS ON SIDE ROAD RADIUS  
2-LANE, TWO WAY TRAFFIC



GENERAL NOTES:

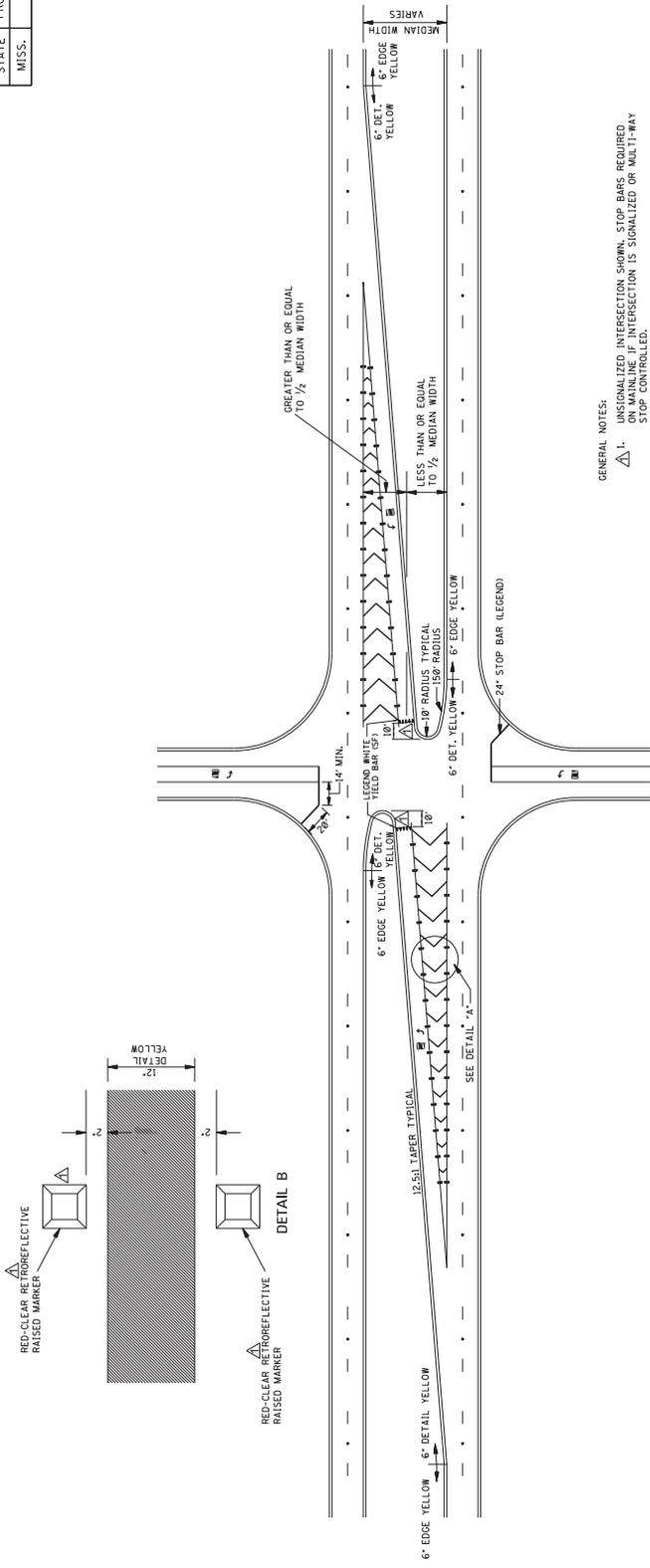
- △ 1. RETROREFLECTIVE FACES OF MARKERS SHALL BE VISIBLE TO THE TRAVELING MOTORIST ON STATE DESIGNATED HIGHWAYS.
- △ 2. RETROREFLECTIVE FACES OF MARKERS SHALL BE HIGH PERFORMANCE TWO-WAY CLEAR.
- △ 3. RETROREFLECTIVE FACES OF MARKERS SHALL NOT BE ROTATED WHEN BEING PLACED ALONG RADIUS AND TANGENT SECTION OF LOCAL ROAD.
- △ 4. MARKERS SHALL BE INSTALLED TO THE LIMITS SHOWN ABOVE.

↑ DIRECTION OF TRAFFIC

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
<b>2-WAY RETROREFLECTIVE RAISED PAVEMENT MARKERS AT INTERSECTING ROADS (2-LANE)</b>	
DATE	ISSUE DATE: AUGUST 01, 2017
BY	SHEET NUMBER PW-11
REVISION	WORKING NUMBER 6061
STN.	

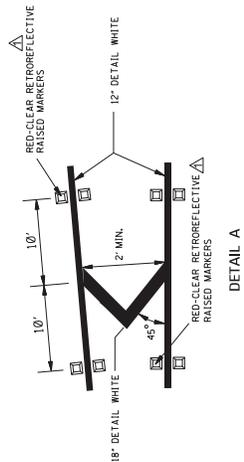
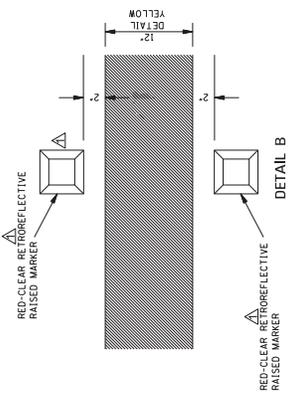


STATE	PROJECT NO.
MISS.	



GENERAL NOTES:

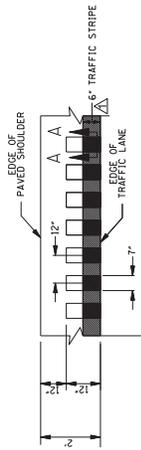
1. UNIGNALIZED INTERSECTION SHOWN. STOP BARS REQUIRED AT UNIGNALIZED INTERSECTION IS SIGNALIZED ON MULTILANE STOP CONTROLLED.
2. SEE M.S. NOS. PM-1, PM-3 and PM-6 FOR OTHER DETAILS.
3. TAPER LENGTH VARIES WITH MEDIAN WIDTH.
4. TAPERED OFFSET LEFT TURN LANE SHOWN. PARALLEL OFFSET LEFT TURN LANE PAVEMENT MARKINGS ARE SIMILAR.
5. 3-LANE SIDE ROAD SHOWN. SEE PERMANENT PAVEMENT MARKING DETAIL SHEETS FOR SIDE ROAD PAVEMENT MARKING DETAILS.
6. SEE PM-12 FOR INTERSECTING ROAD RAISED PAVEMENT MARKER PLACEMENT.



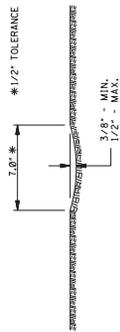
MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
<b>OFFSET LEFT TURN LANES</b>	
DATE	ISSUE DATE: AUGUST 01, 2017
BY	
REVISION	
REVISED TEXT AND RAMP	
ADDED TEXT AND RAMP	
SYN	
WORKING NUMBER	PM-13
SHEET NUMBER	6063

STATE	PROJECT NO.
MISS.	

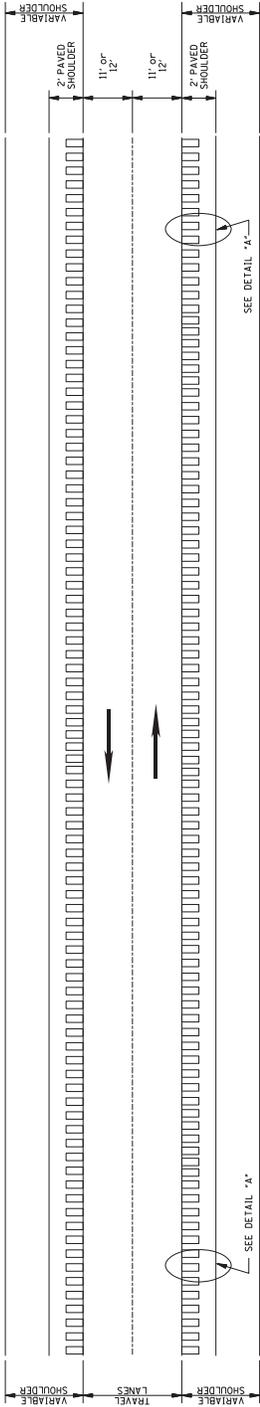
- GENERAL NOTES
- GROUND-IN RUMBLE STRIPES SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT.
  - GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO ALL PAVED SHOULDERS ON ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER.
  - COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS.
  - GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO:
    - MAINLINE
    - INTERSECTING ROADWAY IF OVERLAID OR RECONSTRUCTED BEYOND NORMAL MAINLINE R.O.M.
    - ANY ROADWAY WITH EXISTING RUMBLE STRIPES PRIOR TO CONSTRUCTION.
  - OMIT GROUND-IN RUMBLE STRIPES WHERE TRAFFIC LANE IS LESS THAN 11 FEET WIDE.



DETAIL "A"



SECTION "A-A"



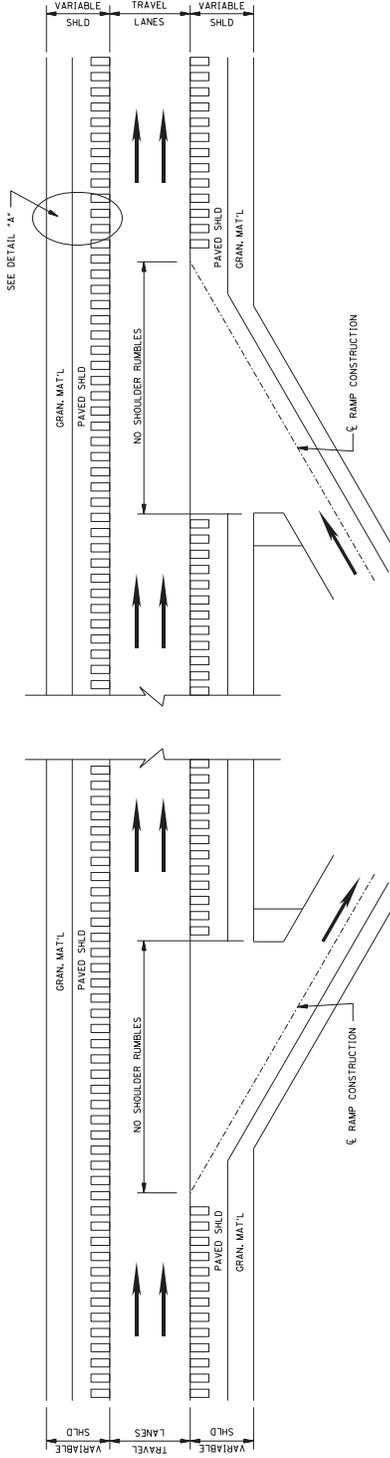
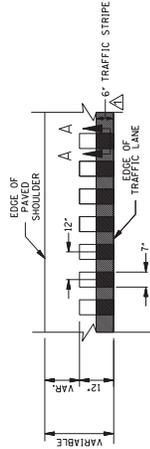
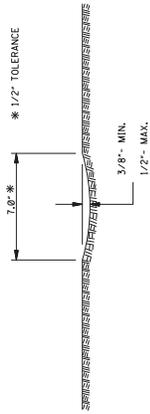
PLAN  
NOT TO SCALE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
<b>RUMBLE STRIPES 2-LANE HIGHWAYS (ASPHALT LANES, 2-FT ASPHALT SHOULDERS)</b>	
DATE	ISSUE DATE: AUGUST 01, 2017
BY	WORKING NUMBER 6064
REVISION	SHEET NUMBER RS-1
REVISED TEXT AND STRIPE DETAIL	PROJECT NUMBER 7624

STATE	PROJECT NO.
MISS.	

GENERAL NOTES

1. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT.
2. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO ALL PAVED SHOULDERS ON ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER.
3. COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS
4. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO:
  - A. MAINLINE
  - B. INTERSECTING ROADWAY IF OVERLAID OR RECONSTRUCTED BEYOND NORMAL MAINLINE R.O.M.
  - C. ANY ROADWAY WITH EXISTING RUMBLE STRIPES PRIOR TO CONSTRUCTION.



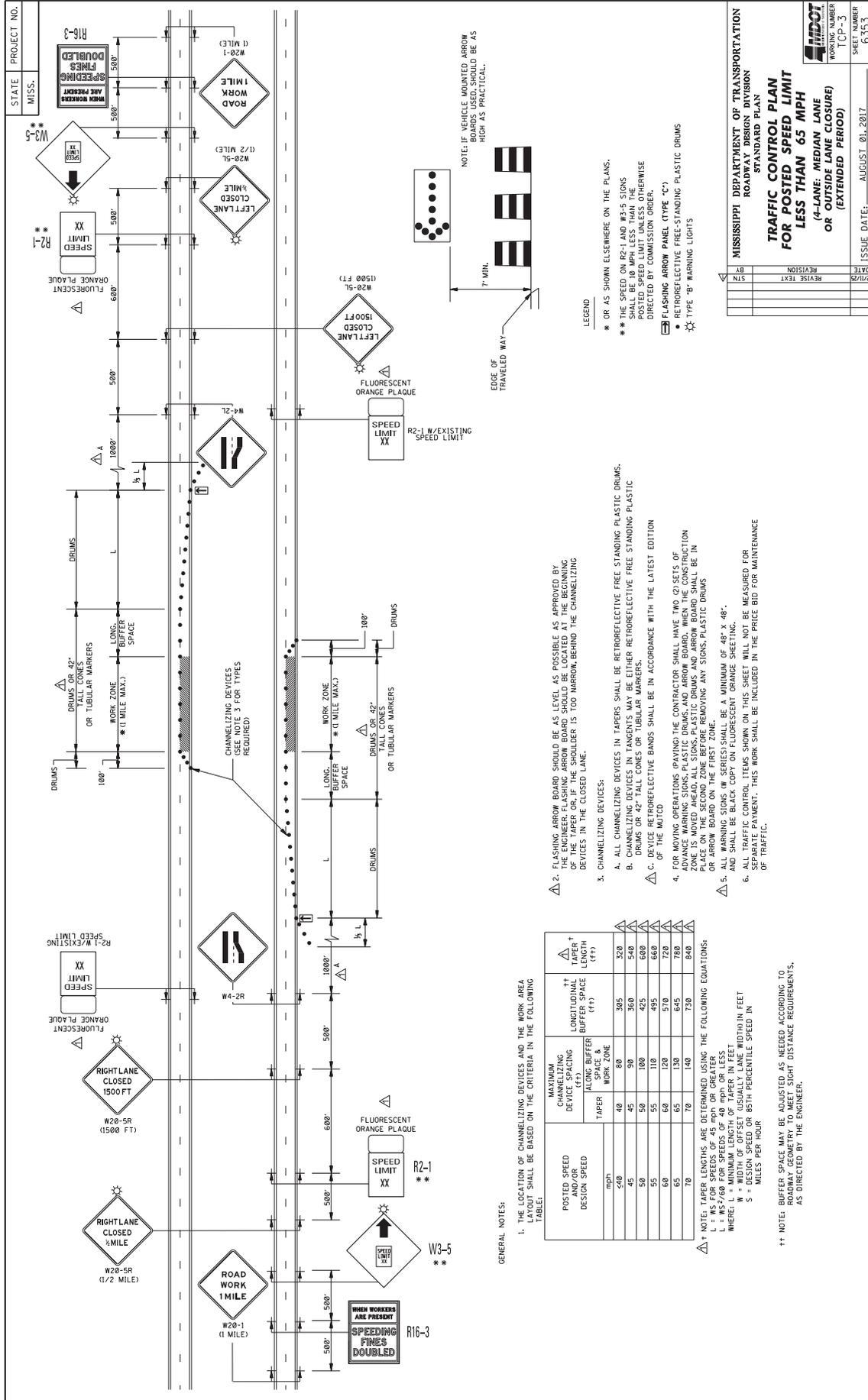
NOT TO SCALE  
DETAILS OF  
RUMBLE STRIPS

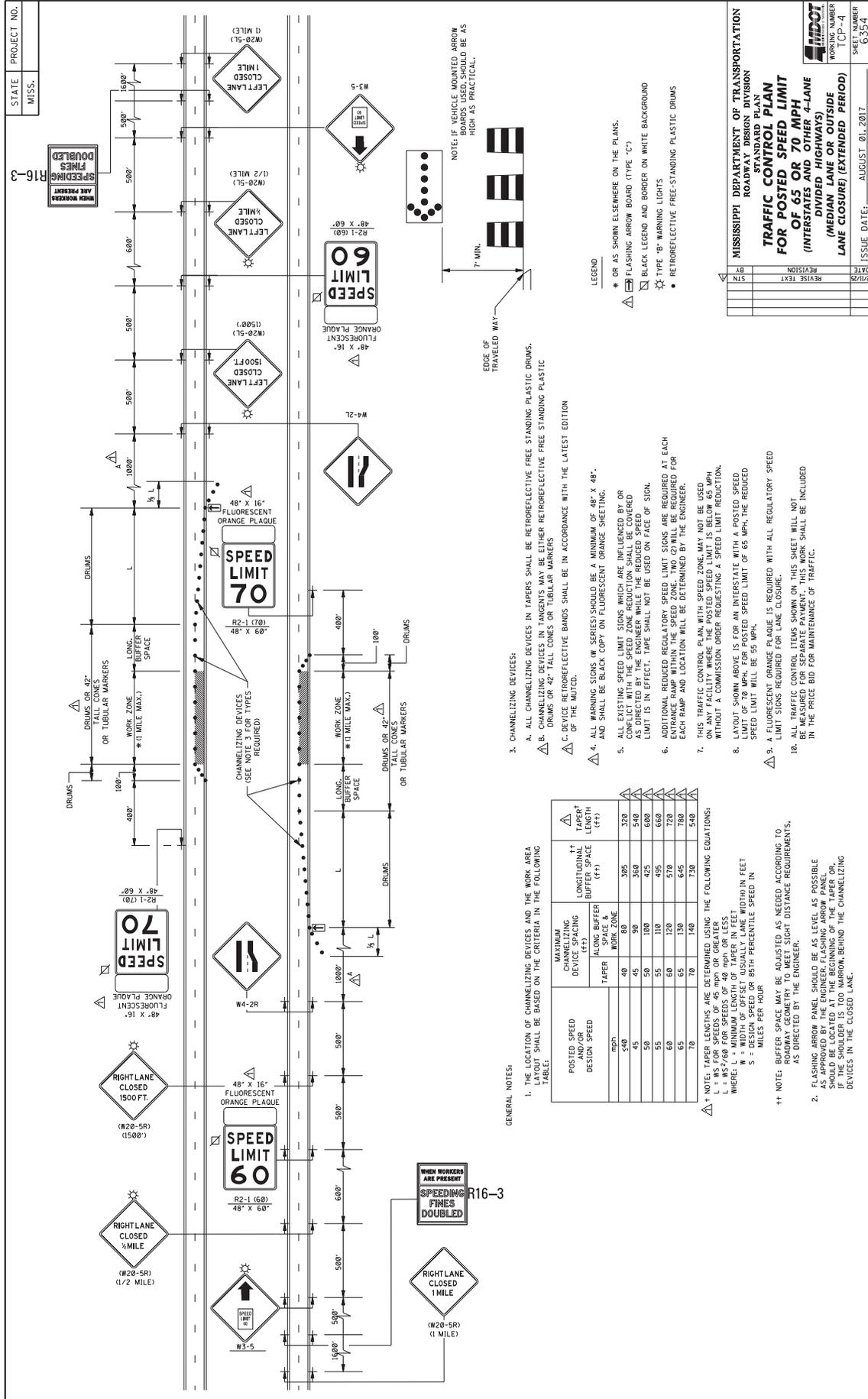
MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
<b>RUMBLE STRIPES 4-LANE HIGHWAYS (ASPHALT LANES, 2-FT OR WIDER, ASPHALT SHOULDERS)</b>	
WORKING NUMBER RS-2	SHEET NUMBER 0005
ISSUE DATE: AUGUST 01, 2017	
DATE	REVISION
	REVISED STRIPS DETAIL

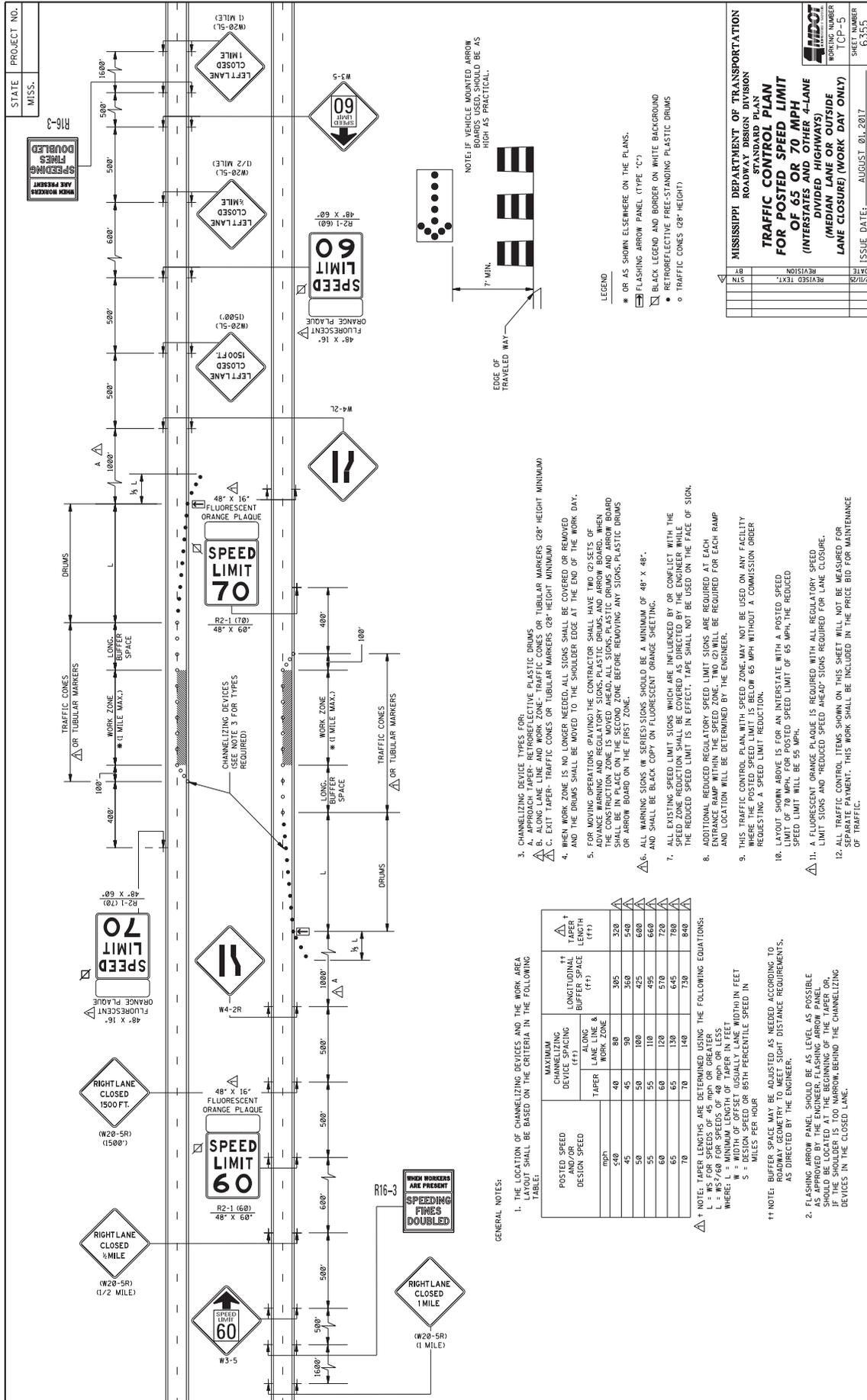




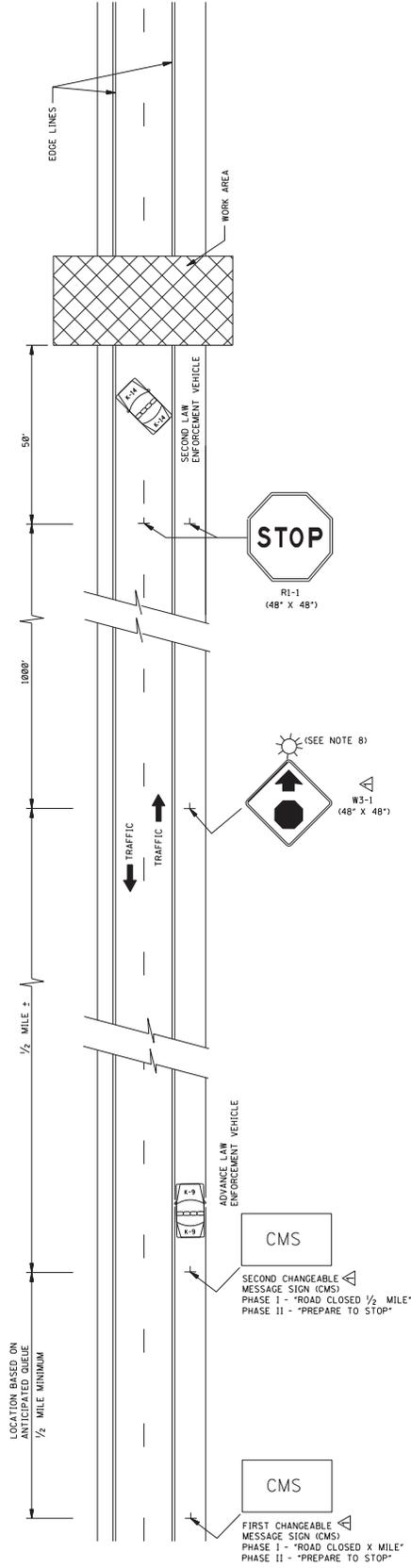








STATE	PROJECT NO.
MISS.	



GENERAL NOTES:

1. THIS TYPE OF HIGHWAY CLOSURE SHOULD BE USED ONLY WHEN THE DURATION OF CLOSURE WILL NOT EXCEED 30 MINUTES. AFTER THE HIGHWAY CLOSURE BEGINS, THE ADVANCE LAW ENFORCEMENT VEHICLE SHOULD ELAPSE BEFORE ANOTHER 30 MINUTES SHOULD ELAPSE BEFORE ANOTHER ADVANCE LAW ENFORCEMENT VEHICLE WITH THE APPROVAL OF THE ENGINEER.
2. AT LEAST TWO LAW ENFORCEMENT OFFICERS AND ONE ADVANCE LAW ENFORCEMENT VEHICLE ON EACH APPROACH TO THE CLOSURE. EACH LAW ENFORCEMENT VEHICLE SHOULD HAVE A PROTRUDING OPERATING FLASHING BLUE LIGHT ON LIGHT BAR.
3. RESTRICTIONS ON ROAD CLOSURES ARE SPECIFIED IN THE CONTRACT DOCUMENT.
4. THE ADVANCE LAW ENFORCEMENT VEHICLE SHOULD BE EQUIPPED WITH A SIGN AS SHOWN BY THE QUEUING OF STOPPED VEHICLES.
5. IF QUEUE EXCEEDS THE FIRST CHANGEABLE MESSAGE SIGN (CMS) AT ANY TIME DURING A CLOSURE, THE TRAFFIC CONTROL PLAN SHOULD BE ADJUSTED AS NECESSARY, WITH APPROVAL OF THE ENGINEER.
6. TRAFFIC CONTROL FOR THE CLOSURE SHOULD BE ACCOMPLISHED IN THE FOLLOWING ORDER:
  - A. FIRST CHANGEABLE MESSAGE SIGN (CMS)
  - B. SECOND CHANGEABLE MESSAGE SIGN (CMS)
  - C. ADVANCE LAW ENFORCEMENT VEHICLE, LIGHTS ON
  - D. "W3-1 48" X 48" (ORANGE/BLACK TYPE XI RETROREFLECTIVE SHEETING) SIGNS ERECTED.
  - E. "R1-1 48" X 48" SIGNS ERECTED OR INSTALLED ON TEMPORARY STANDS TO STOP TRAFFIC. THE ORDER OF ERECTION SHOULD BE IN THE FOLLOWING ORDER: RIGHT SHOULDER THEN CENTER, AND FLASHERS ON.
  - F. SECOND LAW ENFORCEMENT VEHICLE, LIGHTS ON.
7. TRAFFIC CONTROL SHOULD BE REMOVED IN THE FOLLOWING ORDER:
  - A. WITH TRAFFIC STOPPED REMOVE THE "R1-1 48" X 48" SIGNS TOWARD THE RIGHT SHOULDER THEN SIGN ON THE RIGHT SHOULDER. SECOND LAW ENFORCEMENT VEHICLE LEADS TRAFFIC THROUGH WORK AREA.
  - B. AFTER ALL STOPPED VEHICLES HAVE STARTED MOVING, THE "W3-1 48" X 48" SIGNS SHALL BE REMOVED. THESE SIGNS SHOULD BE COVERED IF RE-USE IS IMMINENT.
  - C. AFTER ALL VEHICLES HAVE RESUMED APPROXIMATELY NORMAL SPEED, THE CHANGEABLE MESSAGE SIGNS SHALL BE TURNED OFF.
8. UNILLUMINATED SECTIONS OF HIGHWAYS SHOULD NOT BE CLOSED DURING HOURS OF DARKNESS EXCEPT FOR EMERGENCY REPAIRS. THE ADVANCE LAW ENFORCEMENT VEHICLE SHOULD BE CLOSING HOURS OF DARKNESS. A TYPE B HIGH INTENSITY FLASHING BARRICADE WARNING LIGHT SHALL BE USED ON EACH W3-1 SIGN.
9. IF AN ENTRANCE RAMP IS LOCATED BETWEEN THE CLOSURE AND THE RAMP SHOULDER, AND W3-1 SIGNS SHALL ALSO BE ERECTED ON THE RAMP SHOULDER.
10. THE ABOVE DURATION WILL APPLY TO EACH APPROACH TO THE CLOSURE.
11. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINLINE CONSTRUCTION INCLUDING SECURING LAW ENFORCEMENT SERVICES.

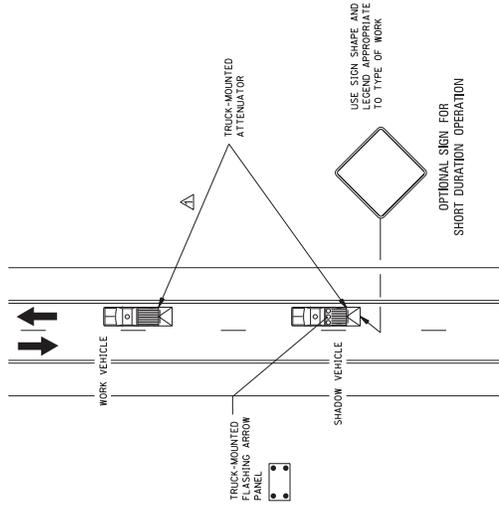
MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
<b>SHORT DURATION CLOSING OF TWO-LANE TWO-WAY HIGHWAYS</b>	
DATE	ISSUE DATE: AUGUST 01, 2017
REVISION	SHEET NUMBER TCP-6
REVISED TEXT	WORKING NUMBER 6356
BY	
BY	





STATE	PROJECT NO.
MISS.	

MOBILE OPERATIONS ON TWO-LANE ROAD

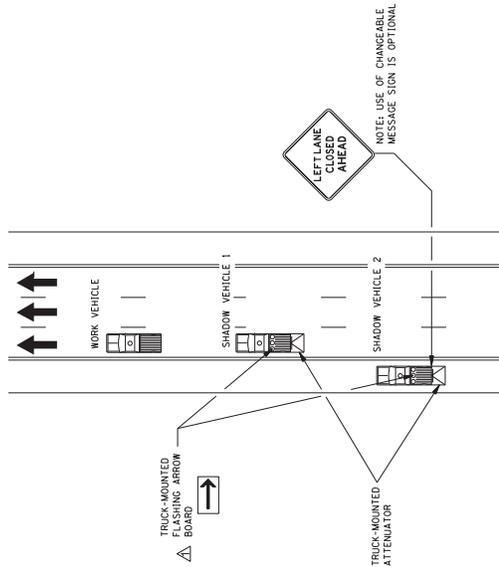


MOBILE OPERATIONS ON TWO-LANE ROAD

NOTES FOR TWO-LANE OPERATION:

- WHERE PRACTICAL AND WHEN NEEDED, THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS. IF THIS CAN NOT BE DONE FREQUENTLY AS AN ALTERNATIVE, A "DO NOT PASS" SIGN MAY BE PLACED ON THE REAR OF THE VEHICLE BLOCKING THE LANE.
- THE DISTANCE BETWEEN THE WORK AND SHADOW VEHICLES MAY VARY ACCORDING TO TERRAIN, PAINT DRYING TIME, AND OTHER FACTORS. SHADOW VEHICLES ARE USED TO WARN TRAFFIC OF THE OPERATION AHEAD. WHENEVER ADEQUATE SPACE IS AVAILABLE, SHADOW VEHICLES SHOULD BE POSITIONED TO MAINTAIN THE MINIMUM DISTANCE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. THE SHADOW VEHICLE SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- ADDITIONAL SHADOW VEHICLES TO WARN AND REDUCE THE SPEED OF ONCOMING OR OPPOSING TRAFFIC MAY BE USED. POLICE PATROL CARS MAY BE USED FOR THIS PURPOSE.
- A TRUCK-MOUNTED ATTENUATOR (TMA) SHOULD BE USED ON THE SHADOW VEHICLE AND MAY BE USED ON THE WORK VEHICLE.
- THE WORK VEHICLE SHALL BE EQUIPPED WITH BEACONS, AND THE SHADOW VEHICLE SHALL BE EQUIPPED WITH BEACONS. TRUCK-MOUNTED FLASHING ARROW LIGHTS MOUNTED ON THE REAR, ADJACENT TO THE SIGN, SHADOW AND WORK VEHICLES SHALL DISPLAY FLASHING OR ROTATING BEACONS BOTH FORWARD AND TO THE REAR.
- VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBTSCURED BY EQUIPMENT ON SIGNPOSTS. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
- ARROW BOARD TO BE USED IN CAUTION MODE.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

MOBILE OPERATIONS ON MULTILANE ROAD

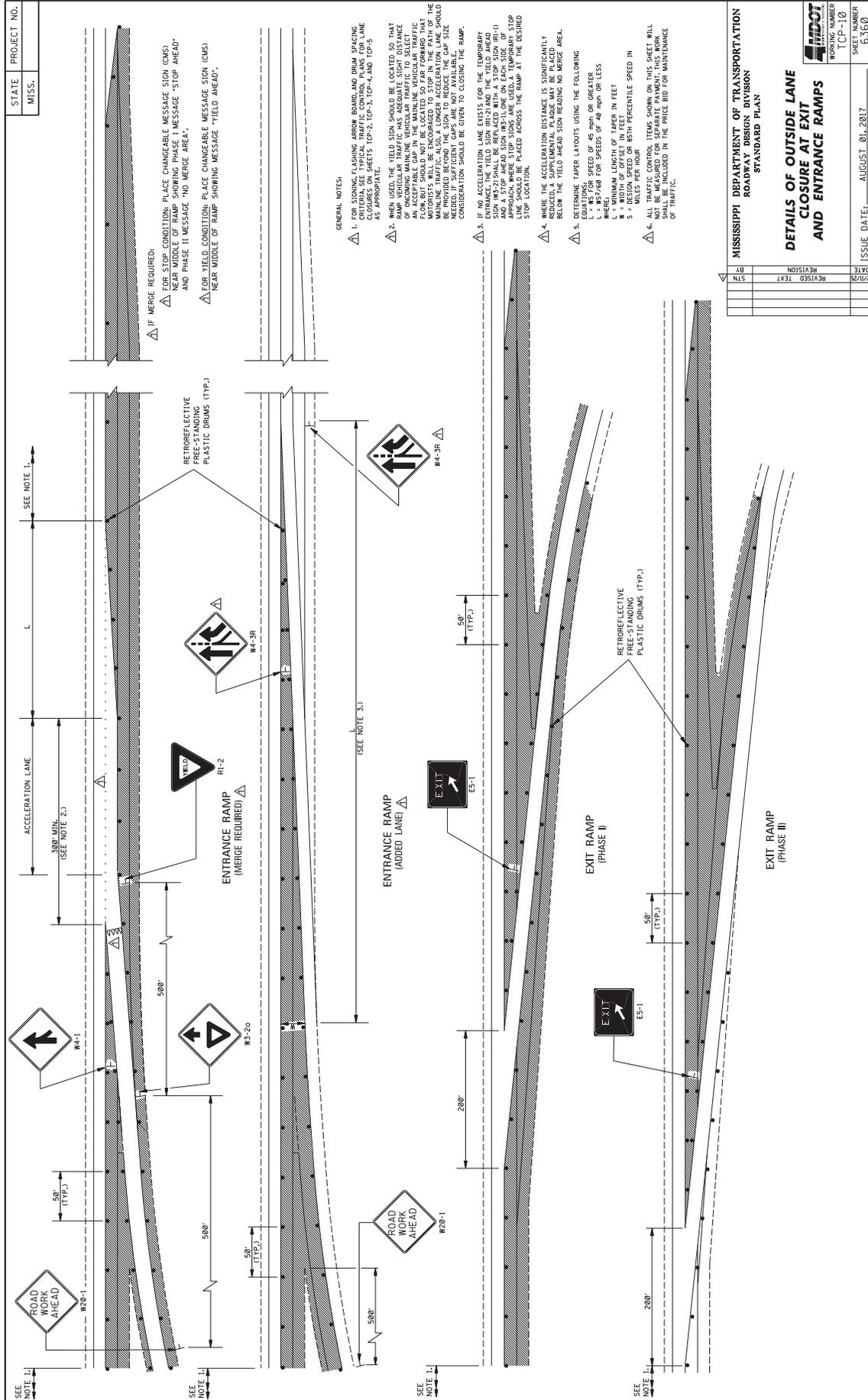


MOBILE OPERATIONS ON MULTILANE ROAD

NOTES FOR MULTILANE LANE OPERATION:

- SHADOW AND WORK VEHICLES SHALL DISPLAY HIGH-INTENSITY FLASHING LIGHTS. A TRUCK-MOUNTED FLASHING ARROW BOARD SHALL BE USED WHEN A FREEWAY LANE IS CLOSED. WHEN MORE THAN ONE FREEWAY LANE IS CLOSED, A SEPARATE ARROW BOARD SHALL BE USED FOR EACH CLOSED LANE.
- SHADOW VEHICLE 2 SHOULD BE EQUIPPED WITH AN ARROW BOARD AND TRUCK-MOUNTED ATTENUATOR (TMA). APPROPRIATE LANE CLOSURE SIGN SHOULD BE PLACED ON SHADOW VEHICLE 2 SO AS NOT TO OBTSCURE THE ARROW BOARD.
- SHADOW VEHICLE 1 SHOULD BE EQUIPPED WITH AN ARROW BOARD AND TRUCK-MOUNTED ATTENUATOR (TMA).
- SHADOW VEHICLE 2 SHOULD TRAVEL AT A VARYING DISTANCE FROM THE WORK OPERATION SO AS TO PROVIDE ADEQUATE SIGHT DISTANCE FOR TRAFFIC APPROACHING FROM THE REAR.
- THE SPACING BETWEEN THE WORK VEHICLES AND THE SHADOW VEHICLES, AND BETWEEN EACH SHADOW VEHICLE, SHOULD BE MINIMIZED TO DETER ROAD USERS FROM DRIVING IN BETWEEN.
- ON HIGH-SPEED ROADWAYS, A THIRD SHADOW VEHICLE (NOT SHOWN) MAY BE USED WITH SHADOW VEHICLE 1 IN THE CLOSED LANE, SHADOW VEHICLE 2 STRADDLING THE EDGE LINE, AND SHADOW VEHICLE 3 ON THE SHOULDER.
- WHERE ADEQUATE SHOULDER WIDTH IS NOT AVAILABLE, SHADOW VEHICLE 3 MAY ALSO STRADDLE THE EDGE LINE.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
- ON HIGH-SPEED ROADWAYS, A THIRD SHADOW VEHICLE (NOT SHOWN) MAY BE USED WITH SHADOW VEHICLE 1 IN THE CLOSED LANE, SHADOW VEHICLE 2 STRADDLING THE EDGE LINE, AND SHADOW VEHICLE 3 ON THE SHOULDER.
- ARROW BOARD SHALL BE AS A MINIMUM TYPE B, 60" x 36" IN ACCORDANCE WITH THE CRITERIA PRESENTED IN THE MUTCD.
- WORK SHOULD NORMALLY BE DONE DURING OFF-PEAK HOURS.

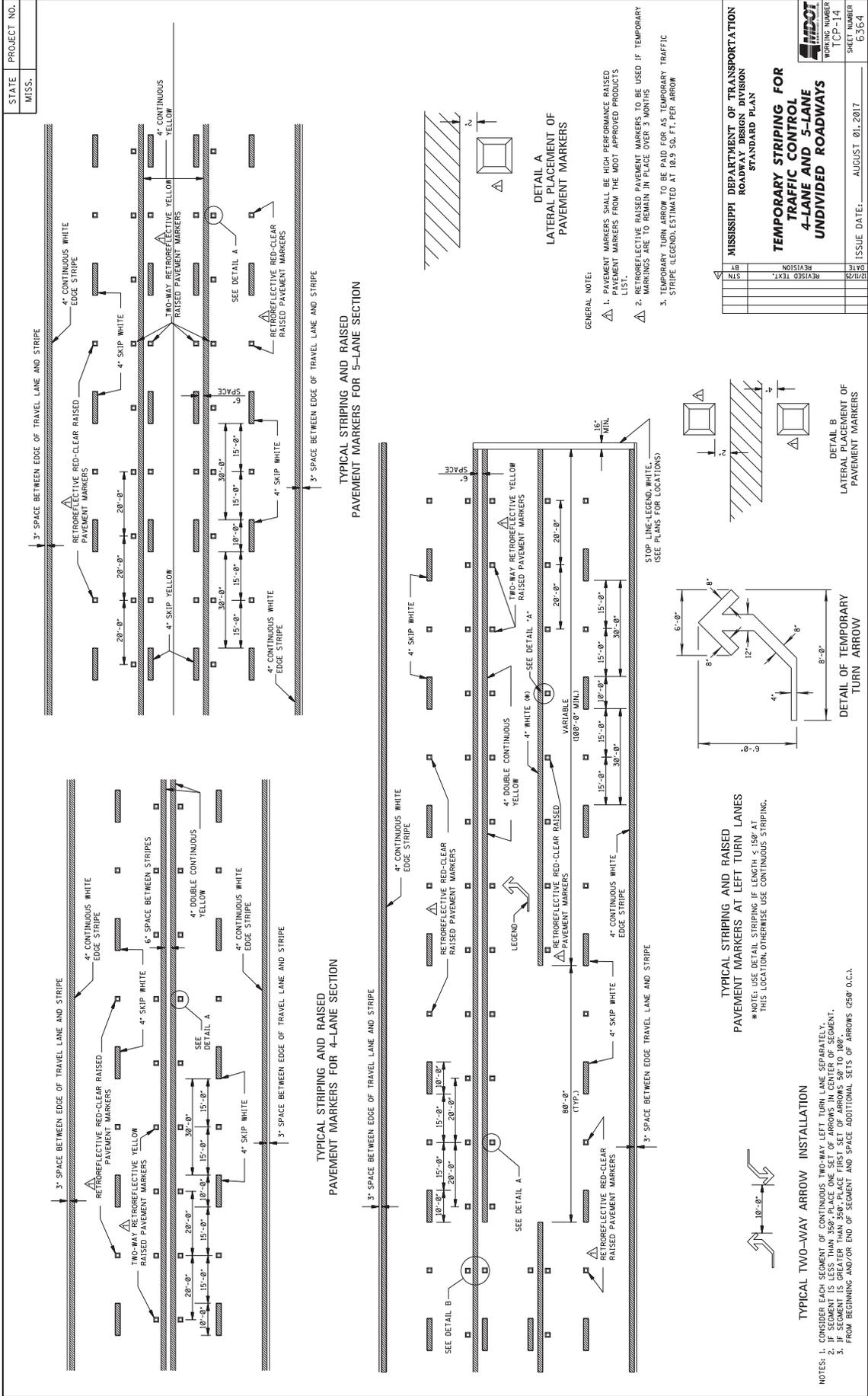
MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
<b>TRAFFIC CONTROL PLAN</b>	
<b>MOBILE OPERATIONS</b>	
<b>MULTILANE ROADS</b>	
<b>TWO-LANE ROADS</b>	
DATE	ISSUE DATE: AUGUST 01, 2017
BY	
REVISION	
TEXT AND DETAILS	
REVISED	
WORKING NUMBER	TCP-9
SHEET NUMBER	6359

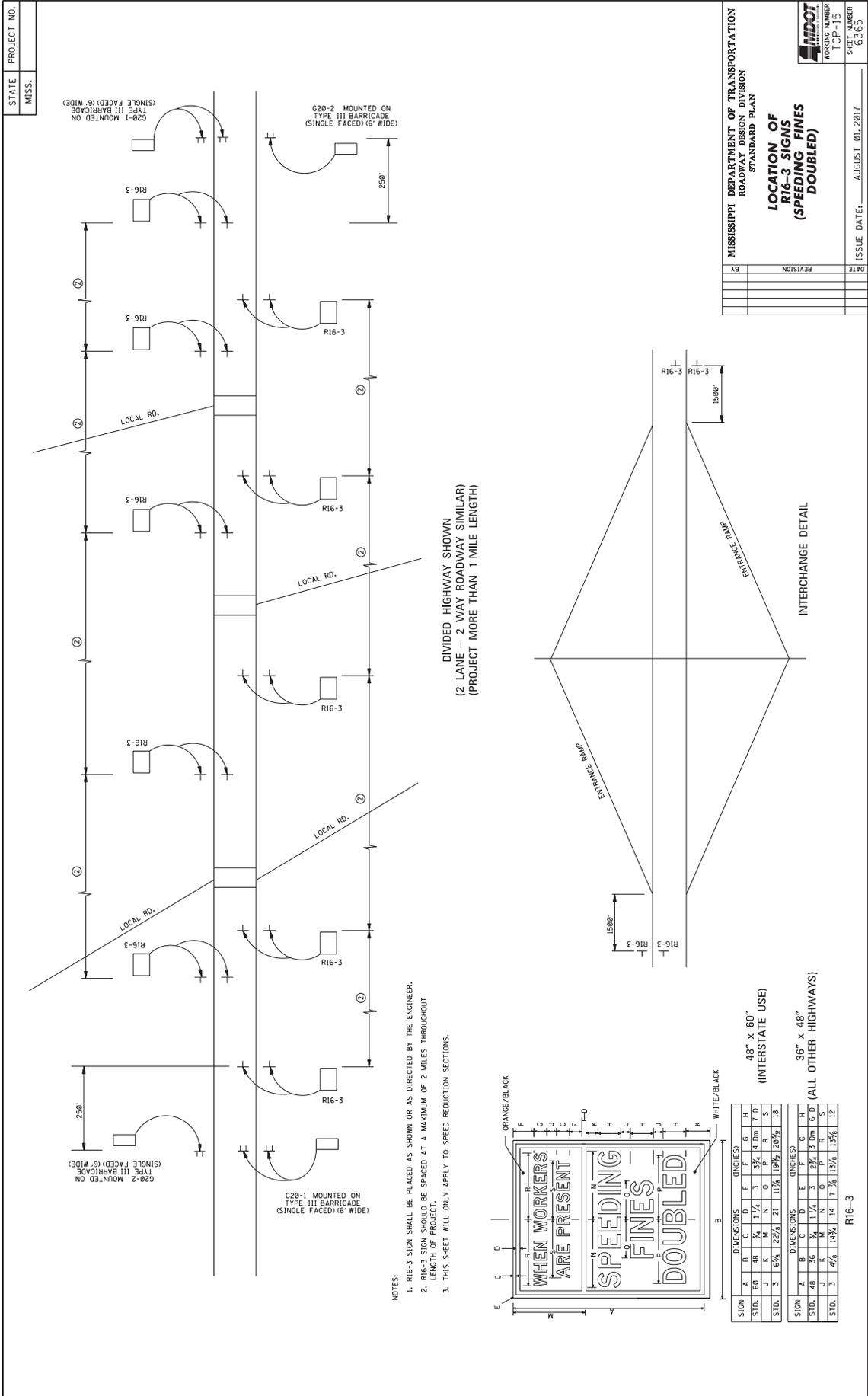










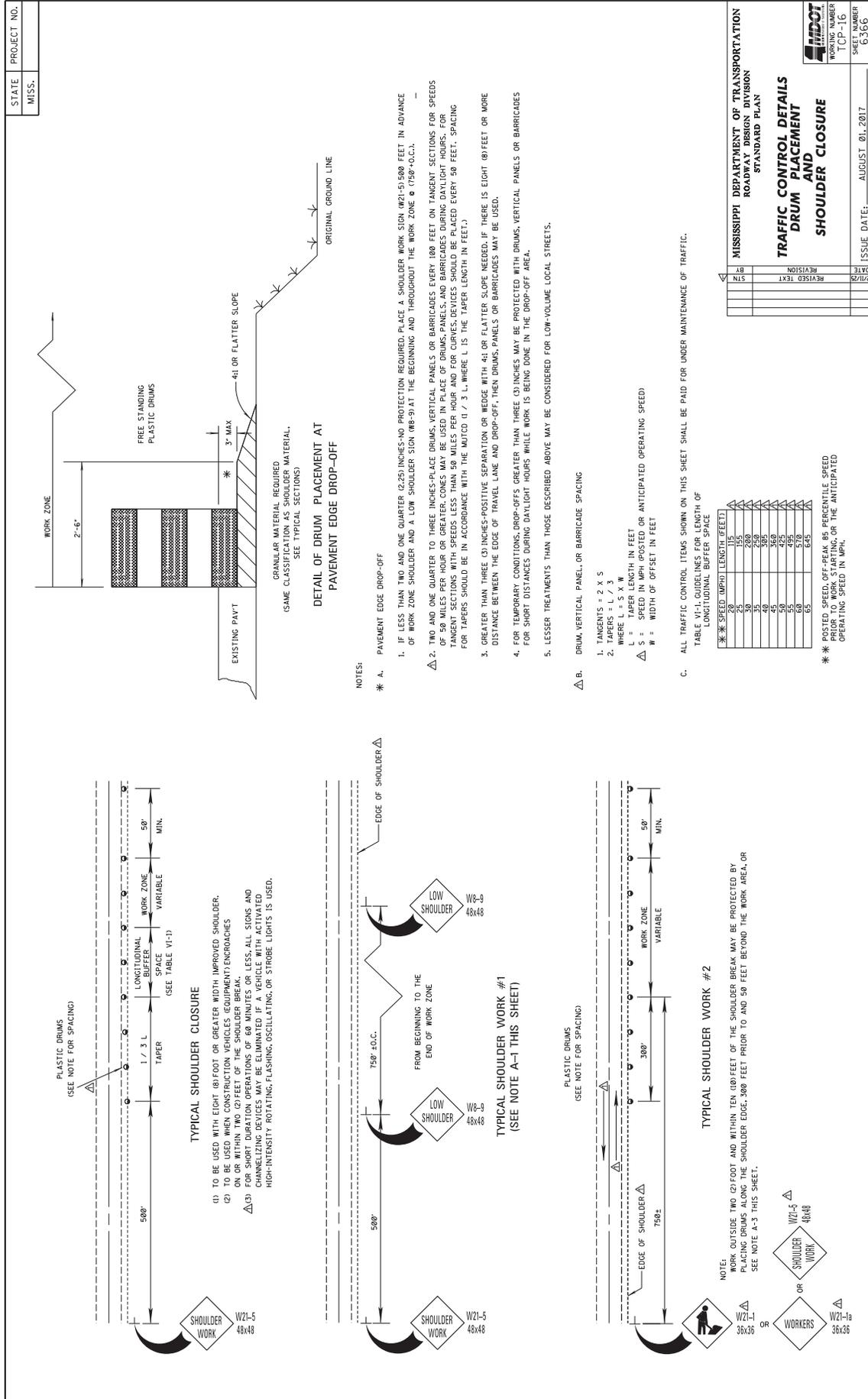


STATE PROJECT NO.  
MISS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
ROADWAY DESIGN DIVISION  
STANDARD PLAN  
**LOCATION OF  
R16-3 SIGNS  
(SPEEDING FINES  
DOUBLED)**

ISSUE DATE: AUGUST 01, 2017  
SHEET NUMBER  
TCP-15  
G-363

- NOTES:
- R16-3 SIGN SHALL BE PLACED AS SHOWN OR AS DIRECTED BY THE ENGINEER.
  - R16-3 SIGN SHOULD BE SPACED AT A MAXIMUM OF 2 MILES THROUGHOUT LENGTH OF PROJECT.
  - THIS SHEET WILL ONLY APPLY TO SPEED REDUCTION SECTIONS.





Backhoe/Excavator/Trackhoe.....	\$ 14.69	0.00
OPERATOR: Bobcat/Skid Steer/Skid Loader.....	\$ 11.64	0.00
OPERATOR: Broom/Sweeper.....	\$ 10.14	0.00
OPERATOR: Bulldozer.....	\$ 15.25	0.00
OPERATOR: Concrete Saw.....	\$ 16.50	0.00
OPERATOR: Crane.....	\$ 19.22	0.00
OPERATOR: Distributor.....	\$ 11.17	0.00
OPERATOR: Grader/Blade.....	\$ 11.67	0.00
OPERATOR: Grinding/Grooving Machine.....	\$ 16.86	0.00
OPERATOR: Loader.....	\$ 16.75	0.00
OPERATOR: Mechanic.....	\$ 19.16	0.00
OPERATOR: Milling Machine.....	\$ 14.68	0.00
OPERATOR: Mixer.....	\$ 14.25	0.00
OPERATOR: Oiler.....	\$ 12.35	0.00
OPERATOR: Paver (Asphalt, Aggregate, and Concrete).....	\$ 11.40	0.00
OPERATOR: Roller (All Types)....	\$ 11.29	0.00
OPERATOR: Scraper.....	\$ 12.25	0.00
OPERATOR: Tractor.....	\$ 11.22	0.00
TRUCK DRIVER: Flatbed Truck.....	\$ 14.06	0.00
TRUCK DRIVER: Lowboy Truck.....	\$ 11.96	0.00
TRUCK DRIVER: Mechanic.....	\$ 13.00	0.00
TRUCK DRIVER: Water Truck.....	\$ 19.00	0.00
TRUCK DRIVER: Dump Truck (All Types).....	\$ 12.66	0.00
TRUCK DRIVER: Semi/Trailer Truck.....	\$ 14.60	0.00

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WELDERS - Receive rate prescribed for craft performing operation to which welding is incidental.

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Note: Executive Order (EO) 13706, Establishing Paid Sick Leave for Federal Contractors applies to all contracts subject to the Davis-Bacon Act for which the contract is awarded (and any solicitation was issued) on or after January 1, 2017. If this contract is covered by the EO, the contractor must provide employees with 1 hour of paid sick leave for every 30 hours

they work, up to 56 hours of paid sick leave each year. Employees must be permitted to use paid sick leave for their own illness, injury or other health-related needs, including preventive care; to assist a family member (or person who is like family to the employee) who is ill, injured, or has other health-related needs, including preventive care; or for reasons resulting from, or to assist a family member (or person who is like family to the employee) who is a victim of, domestic violence, sexual assault, or stalking. Additional information on contractor requirements and worker protections under the EO is available at <https://www.dol.gov/agencies/whd/government-contracts>.

Note: Executive Order 13658 generally applies to contracts subject to the Davis-Bacon Act that were awarded on or between January 1, 2015 and January 29, 2022, and that have not been renewed or extended on or after January 30, 2022. Executive Order 13658 does not apply to contracts subject only to the Davis-Bacon Related Acts regardless of when they were awarded. If a contract is subject to Executive Order 13658, the contractor must pay all covered workers at least \$13.30 per hour (or the applicable wage rate listed on this wage determination, if it is higher) for all hours spent performing on the contract in 2025. The applicable Executive Order minimum wage rate will be adjusted annually. Additional information on contractor requirements and worker protections under Executive Order 13658 is available at [www.dol.gov/whd/govcontracts](http://www.dol.gov/whd/govcontracts).

Unlisted classifications needed for work not included within the scope of the classifications listed may be added after award only as provided in the labor standards contract clauses (29CFR 5.5 (a) (1) (iii)).

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The body of each wage determination lists the classifications and wage rates that have been found to be prevailing for the type(s) of construction and geographic area covered by the wage determination. The classifications are listed in alphabetical order under rate identifiers indicating whether the particular rate is a union rate (current union negotiated rate), a survey rate, a weighted union average rate, a state adopted rate, or a supplemental classification rate.

#### Union Rate Identifiers

A four-letter identifier beginning with characters other than ""SU"", ""UAVG"", ?SA?, or ?SC? denotes that a union rate was prevailing for that classification in the survey. Example: PLUM0198-005 07/01/2024. PLUM is an identifier of the union whose collectively bargained rate prevailed in the survey for this classification, which in this example would be Plumbers. 0198 indicates the local union number or district council number where applicable, i.e., Plumbers Local 0198. The next number, 005 in the example, is an internal number used in processing the wage determination. The date, 07/01/2024 in the example, is the effective date of the most current negotiated rate.

Union prevailing wage rates are updated to reflect all changes over time that are reported to WHD in the rates in the collective bargaining agreement (CBA) governing the classification.

## Union Average Rate Identifiers

The UAVG identifier indicates that no single rate prevailed for those classifications, but that 100% of the data reported for the classifications reflected union rates. EXAMPLE:

UAVG-OH-0010 01/01/2024. UAVG indicates that the rate is a weighted union average rate. OH indicates the State of Ohio. The next number, 0010 in the example, is an internal number used in producing the wage determination. The date, 01/01/2024 in the example, indicates the date the wage determination was updated to reflect the most current union average rate.

A UAVG rate will be updated once a year, usually in January, to reflect a weighted average of the current rates in the collective bargaining agreements on which the rate is based.

## Survey Rate Identifiers

The ""SU"" identifier indicates that either a single non-union rate prevailed (as defined in 29 CFR 1.2) for this classification in the survey or that the rate was derived by computing a weighted average rate based on all the rates reported in the survey for that classification. As a weighted average rate includes all rates reported in the survey, it may include both union and non-union rates. Example: SUFL2022-007 6/27/2024. SU indicates the rate is a single non-union prevailing rate or a weighted average of survey data for that classification. FL indicates the State of Florida. 2022 is the year of the survey on which these classifications and rates are based. The next number, 007 in the example, is an internal number used in producing the wage determination. The date, 6/27/2024 in the example, indicates the survey completion date for the classifications and rates under that identifier.

?SU? wage rates typically remain in effect until a new survey is conducted. However, the Wage and Hour Division (WHD) has the discretion to update such rates under 29 CFR 1.6(c)(1).

## State Adopted Rate Identifiers

The ""SA"" identifier indicates that the classifications and prevailing wage rates set by a state (or local) government were adopted under 29 C.F.R 1.3(g)-(h). Example: SAME2023-007 01/03/2024. SA reflects that the rates are state adopted. ME refers to the State of Maine. 2023 is the year during which the state completed the survey on which the listed classifications and rates are based. The next number, 007 in the example, is an internal number used in producing the wage determination. The date, 01/03/2024 in the example, reflects the date on which the classifications and rates under the ?SA? identifier took effect under state law in the state from which the rates were adopted.

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## WAGE DETERMINATION APPEALS PROCESS

1) Has there been an initial decision in the matter? This can be:

- a) a survey underlying a wage determination
- b) an existing published wage determination
- c) an initial WHD letter setting forth a position on a wage determination matter

d) an initial conformance (additional classification and rate) determination

On survey related matters, initial contact, including requests for summaries of surveys, should be directed to the WHD Branch of Wage Surveys. Requests can be submitted via email to [davisbaconinfo@dol.gov](mailto:davisbaconinfo@dol.gov) or by mail to:

Branch of Wage Surveys  
Wage and Hour Division  
U.S. Department of Labor  
200 Constitution Avenue, N.W.  
Washington, DC 20210

Regarding any other wage determination matter such as conformance decisions, requests for initial decisions should be directed to the WHD Branch of Construction Wage Determinations. Requests can be submitted via email to [BCWD-Office@dol.gov](mailto:BCWD-Office@dol.gov) or by mail to:

Branch of Construction Wage Determinations  
Wage and Hour Division  
U.S. Department of Labor  
200 Constitution Avenue, N.W.  
Washington, DC 20210

2) If an initial decision has been issued, then any interested party (those affected by the action) that disagrees with the decision can request review and reconsideration from the Wage and Hour Administrator (See 29 CFR Part 1.8 and 29 CFR Part 7). Requests for review and reconsideration can be submitted via email to [dba.reconsideration@dol.gov](mailto:dba.reconsideration@dol.gov) or by mail to:

Wage and Hour Administrator  
U.S. Department of Labor  
200 Constitution Avenue, N.W.  
Washington, DC 20210

The request should be accompanied by a full statement of the interested party's position and any information (wage payment data, project description, area practice material, etc.) that the requestor considers relevant to the issue.

3) If the decision of the Administrator is not favorable, an interested party may appeal directly to the Administrative Review Board (formerly the Wage Appeals Board). Write to:

Administrative Review Board  
U.S. Department of Labor  
200 Constitution Avenue, N.W.  
Washington, DC 20210.

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END OF GENERAL DECISION

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