

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO.	<u>1</u>	DATED	<u>1/22/2026</u>	ADDENDUM NO.	_____	DATED	_____
ADDENDUM NO.	_____	DATED	_____	ADDENDUM NO.	_____	DATED	_____
ADDENDUM NO.	_____	DATED	_____	ADDENDUM NO.	_____	DATED	_____

Number	Description
1	Revised Table of Contents; Deleted NTB No. 5750; Added NTB No. 7464; Revised Wage Rates; Revised Bid Items; Revised or Added Plan Sheet Nos. 6051, 6351, 6358-6359, 6363 & 6366, 8001-8002 & 8007; Amendment EBSx Download Required.

TOTAL ADDENDA: 1
(Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

Contractor

BY _____
Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

President	Address
Secretary	Address
Treasurer	Address

The following is my (our) itemized proposal.

STBG-0044-01(036)/ 109672301000

Union County(ies)

Revised 01/26/2016

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
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PROJECT: STBG-0044-01(036)/109672301 - Union

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(REVISIONS TO THE ABOVE WILL BE INDICATED ON THE SECOND SHEET
OF SECTION 905 AS ADDENDA)

01/21/2026 04:42 PM

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 7464

CODE: (SP)

DATE: 01/22/2026

SUBJECT: Additional Pay Item

PROJECT: STBG-0044-01(036) / 109672301 -- Union County

Bidders are hereby advised that the following pay item has been added from what is shown on the Summary of Quantities Plan Sheet:

Pay Item 203-EX023 Borrow Excavation, AH, LVM, Class B10 -per Cubic Yard

This pay item has been added to the quantities on the proposal's bid sheet.

"General Decision Number: MS20260118 01/02/2026

Superseded General Decision Number: MS20250118

State: Mississippi

Construction Type: Highway

County: Union County in Mississippi.

HIGHWAY CONSTRUCTION PROJECTS

Modification Number 0 Publication Date 01/02/2026

SUMS2010-041 08/04/2014

CARPENTER (Form Work Only).....\$ 14.13 0.00

CARPENTER, Excludes Form Work....\$ 13.49 0.00

CEMENT MASON/CONCRETE FINISHER...\$ 13.70 0.00

ELECTRICIAN.....\$ 21.80 7.93

HIGHWAY/PARKING LOT STRIPING:

Truck Driver (Line Striping
Truck).....\$ 15.97 0.00

INSTALLER - GUARDRAIL.....\$ 11.51 0.00

IRONWORKER, REINFORCING.....\$ 14.82 0.00

LABORER: Common or General,

Shoveling, Spreading and

LABORATORY GRADE CHECKED: 11/11/07 BY: 11/11/07

Cement/Concrete.....\$ 11.08 0.00

LABORER: Pipelayer.....\$ 10.76 0.00

LABORER: Laborer-Cones/

Setter/Mover/Sweeper.....\$ 10.38 0.00

OPERATOR: Asphalt Spreader.....\$ 16.03 0.00

OPERATOR:

BACKHOE, EXCAVATOR, TRUCKHOE, 1970S \$ 1500

OPERATOR: Boiling Machine 19.11 0.00

OPERATOR: Bulldozer.....	\$ 14.34	0.00
OPERATOR: Concrete Saw.....	\$ 15.68	0.00
OPERATOR: Crane.....	\$ 18.32	0.00
OPERATOR: Distributor.....	\$ 12.59	0.00
OPERATOR: Drill.....	\$ 19.22	0.00
OPERATOR: Grader/Blade.....	\$ 14.50	0.00
OPERATOR: Loader.....	\$ 11.54	0.00
OPERATOR: Mechanic.....	\$ 15.13	0.00
OPERATOR: Milling Machine.....	\$ 15.12	0.00
OPERATOR: Oiler.....	\$ 12.33	0.00
OPERATOR: Paver (Asphalt, Aggregate, and Concrete).....	\$ 14.47	0.00
OPERATOR: Piledriver.....	\$ 15.13	0.00
OPERATOR: Roller (All Types)....	\$ 11.54	0.00
OPERATOR: Scraper.....	\$ 13.15	0.00
OPERATOR: Tractor.....	\$ 11.25	0.00
OPERATOR: Trencher.....	\$ 15.00	0.00
TRUCK DRIVER: Flatbed Truck.....	\$ 13.79	0.00
TRUCK DRIVER: Lowboy Truck.....	\$ 13.30	0.00
TRUCK DRIVER: Mechanic.....	\$ 14.23	0.00
TRUCK DRIVER: Off the Road Truck.....	\$ 12.29	0.00
TRUCK DRIVER: Water Truck.....	\$ 10.58	0.00
TRUCK DRIVER: Dump Truck (All Types).....	\$ 10.97	0.00
TRUCK DRIVER: Semi/Trailer Truck.....	\$ 15.81	0.00

WELDERS - Receive rate prescribed for craft performing operation to which welding is incidental.

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Note: Executive Order (EO) 13706, Establishing Paid Sick Leave for Federal Contractors applies to all contracts subject to the Davis-Bacon Act for which the contract is awarded (and any solicitation was issued) on or after January 1, 2017. If this contract is covered by the EO, the contractor must provide employees with 1 hour of paid sick leave for every 30 hours they work, up to 56 hours of paid sick leave each year. Employees must be permitted to use paid sick leave for their own illness, injury or other health-related needs, including

preventive care; to assist a family member (or person who is like family to the employee) who is ill, injured, or has other health-related needs, including preventive care; or for reasons resulting from, or to assist a family member (or person who is like family to the employee) who is a victim of, domestic violence, sexual assault, or stalking. Additional information on contractor requirements and worker protections under the EO is available at <https://www.dol.gov/agencies/whd/government-contracts>.

Note: Executive Order 13658 generally applies to contracts subject to the Davis-Bacon Act that were awarded on or between January 1, 2015 and January 29, 2022, and that have not been renewed or extended on or after January 30, 2022. Executive Order 13658 does not apply to contracts subject only to the Davis-Bacon Related Acts regardless of when they were awarded. If a contract is subject to Executive Order 13658, the contractor must pay all covered workers at least \$13.30 per hour (or the applicable wage rate listed on this wage determination, if it is higher) for all hours spent performing on the contract in 2025. The applicable Executive Order minimum wage rate will be adjusted annually. Additional information on contractor requirements and worker protections under Executive Order 13658 is available at www.dol.gov/whd/govcontracts.

Unlisted classifications needed for work not included within the scope of the classifications listed may be added after award only as provided in the labor standards contract clauses (29CFR 5.5 (a) (1) (iii)).

The body of each wage determination lists the classifications and wage rates that have been found to be prevailing for the type(s) of construction and geographic area covered by the wage determination. The classifications are listed in alphabetical order under rate identifiers indicating whether the particular rate is a union rate (current union negotiated rate), a survey rate, a weighted union average rate, a state adopted rate, or a supplemental classification rate.

Union Rate Identifiers

A four-letter identifier beginning with characters other than ""SU"", ""UAVG"", ?SA?, or ?SC? denotes that a union rate was prevailing for that classification in the survey. Example: PLUM0198-005 07/01/2024. PLUM is an identifier of the union whose collectively bargained rate prevailed in the survey for this classification, which in this example would be Plumbers. 0198 indicates the local union number or district council number where applicable, i.e., Plumbers Local 0198. The next number, 005 in the example, is an internal number used in processing the wage determination. The date, 07/01/2024 in the example, is the effective date of the most current negotiated rate.

Union prevailing wage rates are updated to reflect all changes over time that are reported to WHD in the rates in the collective bargaining agreement (CBA) governing the classification.

Union Average Rate Identifiers

The UAVG identifier indicates that no single rate prevailed for those classifications, but that 100% of the data reported for the classifications reflected union rates. EXAMPLE:

UAVG-OH-0010 01/01/2024. UAVG indicates that the rate is a weighted union average rate. OH indicates the State of Ohio. The next number, 0010 in the example, is an internal number used in producing the wage determination. The date, 01/01/2024 in the example, indicates the date the wage determination was updated to reflect the most current union average rate.

A UAVG rate will be updated once a year, usually in January, to reflect a weighted average of the current rates in the collective bargaining agreements on which the rate is based.

Survey Rate Identifiers

The ""SU"" identifier indicates that either a single non-union rate prevailed (as defined in 29 CFR 1.2) for this classification in the survey or that the rate was derived by computing a weighted average rate based on all the rates reported in the survey for that classification. As a weighted average rate includes all rates reported in the survey, it may include both union and non-union rates. Example: SUFL2022-007 6/27/2024. SU indicates the rate is a single non-union prevailing rate or a weighted average of survey data for that classification. FL indicates the State of Florida. 2022 is the year of the survey on which these classifications and rates are based. The next number, 007 in the example, is an internal number used in producing the wage determination. The date, 6/27/2024 in the example, indicates the survey completion date for the classifications and rates under that identifier.

?SU? wage rates typically remain in effect until a new survey is conducted. However, the Wage and Hour Division (WHD) has the discretion to update such rates under 29 CFR 1.6(c)(1).

State Adopted Rate Identifiers

The ""SA"" identifier indicates that the classifications and prevailing wage rates set by a state (or local) government were adopted under 29 C.F.R 1.3(g)-(h). Example: SAME2023-007 01/03/2024. SA reflects that the rates are state adopted. ME refers to the State of Maine. 2023 is the year during which the state completed the survey on which the listed classifications and rates are based. The next number, 007 in the example, is an internal number used in producing the wage determination. The date, 01/03/2024 in the example, reflects the date on which the classifications and rates under the ?SA? identifier took effect under state law in the state from which the rates were adopted.

WAGE DETERMINATION APPEALS PROCESS

1) Has there been an initial decision in the matter? This can be:

- a) a survey underlying a wage determination
- b) an existing published wage determination
- c) an initial WHD letter setting forth a position on a wage determination matter
- d) an initial conformance (additional classification and rate) determination

On survey related matters, initial contact, including requests for summaries of surveys, should be directed to the WHD Branch of Wage Surveys. Requests can be submitted via email to davisbaconinfo@dol.gov or by mail to:

Branch of Wage Surveys
Wage and Hour Division
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

Regarding any other wage determination matter such as conformance decisions, requests for initial decisions should be directed to the WHD Branch of Construction Wage Determinations. Requests can be submitted via email to BCWD-Office@dol.gov or by mail to:

Branch of Construction Wage Determinations
Wage and Hour Division
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

2) If an initial decision has been issued, then any interested party (those affected by the action) that disagrees with the decision can request review and reconsideration from the Wage and Hour Administrator (See 29 CFR Part 1.8 and 29 CFR Part 7). Requests for review and reconsideration can be submitted via email to dba.reconsideration@dol.gov or by mail to:

Wage and Hour Administrator
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

The request should be accompanied by a full statement of the interested party's position and any information (wage payment data, project description, area practice material, etc.) that the requestor considers relevant to the issue.

3) If the decision of the Administrator is not favorable, an interested party may appeal directly to the Administrative Review Board (formerly the Wage Appeals Board). Write to:

Administrative Review Board
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210.

END OF GENERAL DECISION

"

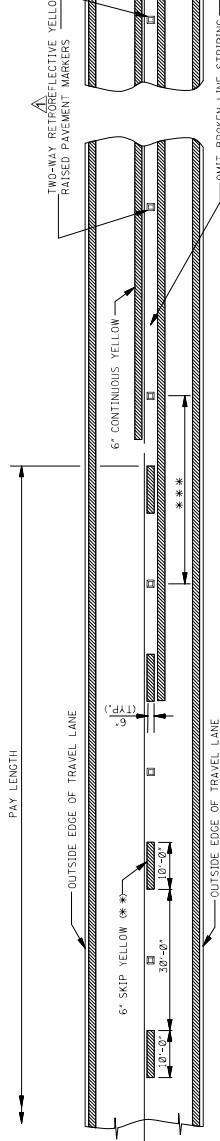
Bridge Repair on SR 30 over Tallahatchie River (Bridge No. 19.0), known as Federal Aid Project No. STBG-0044-01(036) / 109672301 in Union County.

Line No.	Item Code	Adj Code	Quantity	Units	Description [Fixed Unit Price]
Roadway Items					
0010	237-A002		200	Linear Feet	Wattles, 20"
0012	203-EX023	(E)	2,100	Cubic Yard	Borrow Excavation, AH, LVM, Class B10
0020	406-D001		267	Square Yard	Fine Milling of Bituminous Pavement, All Depths
0030	619-A1002		3,800	Linear Feet	Temporary Traffic Stripe, Continuous White
0040	619-A1007		4,080	Linear Feet	Temporary Traffic Stripe, Continuous White, Type 1 or 2 Tape
0050	619-A2002		450	Linear Feet	Temporary Traffic Stripe, Continuous Yellow
0060	619-A4001		1,900	Linear Feet	Temporary Traffic Stripe, Skip Yellow
0070	619-A6008		144	Linear Feet	Temporary Traffic Stripe, Legend, Type 1 or 2 Tape
0080	619-D1001		38	Square Feet	Standard Roadside Construction Signs, Less than 10 Square Feet
0090	619-D2001		152	Square Feet	Standard Roadside Construction Signs, 10 Square Feet or More
0100	619-F1001		1,500	Linear Feet	Concrete Median Barrier, Precast
0110	619-F2001		1,500	Linear Feet	Remove and Reset Concrete Median Barrier, Precast
0120	619-G4001		24	Linear Feet	Barricades, Type III, Double Faced
0130	619-G5001		20	Each	Free Standing Plastic Drums
0140	619-G7001		4	Each	Warning Lights, Type "B"
0150	619-J1002		2	Each	Impact Attenuator, 45 MPH
0160	619-J2002		2	Each	Impact Attenuator, 45 MPH, Replacement Package
0170	619-J3001		2	Each	Remove and Reset Impact Attenuator
0180	620-A001		1	Lump Sum	Mobilization
0190	907-234-A001		250	Linear Feet	Temporary Silt Fence
0200	907-234-F001		150	Linear Feet	Turbidity Barrier
0210	907-403-A015	(BA1)	22	Ton	9.5-mm, ST, Asphalt Pavement
0220	907-618-A001		1	Lump Sum	Maintenance of Traffic
0230	907-619-E3001		2	Each	Changeable Message Sign
0240	907-619-H2001		2	Each	Traffic Signal, Portable, Type 1
0250	907-626-B005		3,800	Linear Feet	6" Thermoplastic Traffic Stripe, Continuous White
0260	907-626-D006		1,900	Linear Feet	6" Thermoplastic Traffic Stripe, Skip Yellow
0270	907-626-E006		450	Linear Feet	6" Thermoplastic Traffic Stripe, Continuous Yellow
0280	907-627-L001		48	Each	Two-Way Yellow Reflective High Performance Raised Markers
Bridge Items					
0290	815-A009	(S)	1,922	Ton	Loose Riprap, Size 500
0300	815-E001	(S)	1,268	Square Yard	Geotextile under Riprap
0310	907-202-B001		5,931	Square Yard	Removal of Bridge Deck, Hydrodemolition

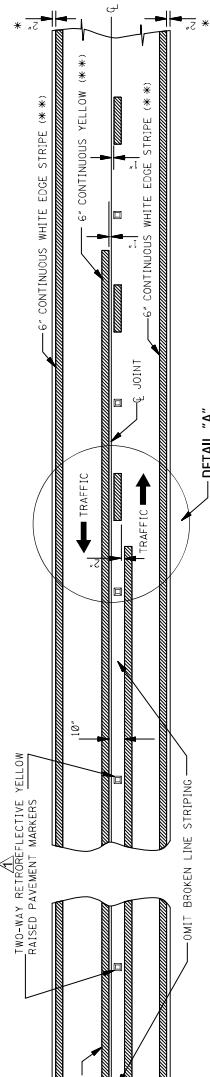
Line No.	Item Code	Adj Code	Quantity	Units	Description [Fixed Unit Price]
0320	907-420-A001		2,000	Pounds	Undersealing
0330	907-804-O001	(S)	330	Cubic Yard	Bridge Deck Overlay Concrete
0340	907-808-A002	(S)	548	Linear Feet	Joint Repair
0350	907-823-A002		235	Linear Feet	Preformed Joint Seal, Type II
0360	907-823-A003		39	Linear Feet	Preformed Joint Seal, Type III
0370	907-823-B002		470	Linear Feet	Saw Cut, Type II
0380	907-823-B003		78	Linear Feet	Saw Cut, Type III
0390	907-824-A003		6	Square Feet	General Epoxy Repair
0400	907-824-C001		5	Each	Cap Cleaning
0410	907-824-PP003		912	Square Feet	Bridge Repair, Removal of Bridge Deck, Per Plans
0420	907-824-PP004		1	Lump Sum	Bridge Repair, Bank Reshaping, Per Plans
0430	907-824-PP006		21	Each	Bridge Repair, Extend Pile Encasements, Per Plans
0440	907-824-PP008		2,925	Linear Feet	Bridge Repair, Class 2 Spray Finish, Per Plans
0450	907-824-PP008		79	Linear Feet	Bridge Repair, Endwall Repair, Per Plans

STATE PROJECT NO.

MISS.

ADDENDUM

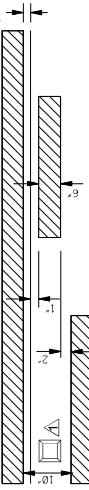
TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)



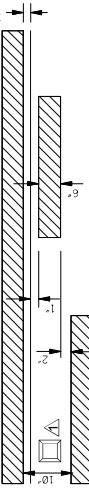
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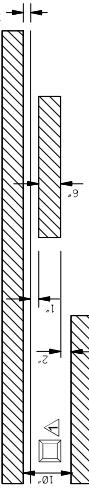
NOTE: THE CRITERIA FOR NO-PASSING ZONES
CAN BE FOUND IN THE MDT ROADWAY
DESIGN MANUAL, SECTION 11-101.



TRAFFIC



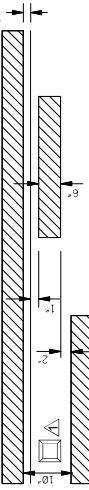
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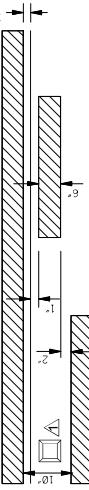
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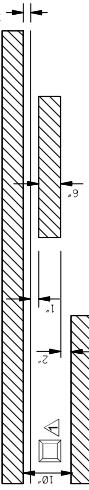
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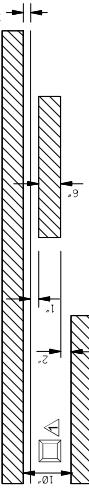
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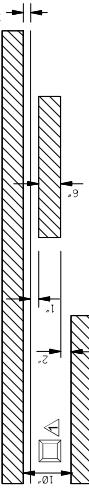
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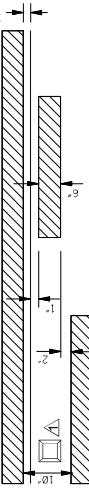
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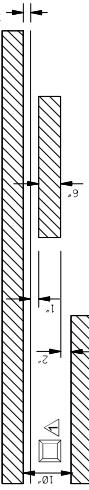
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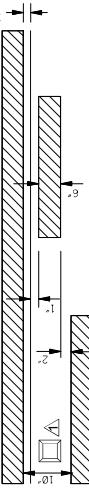
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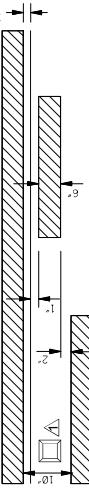
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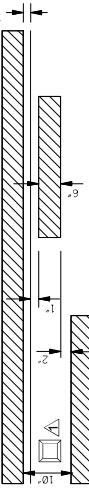
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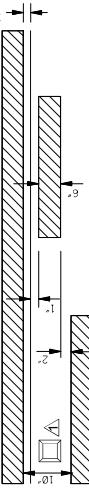
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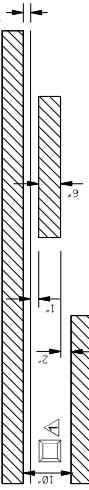
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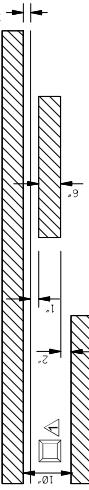
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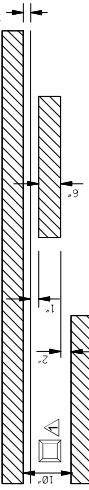
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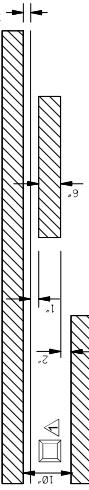
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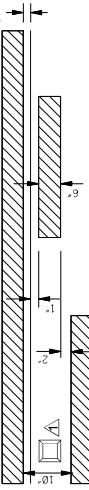
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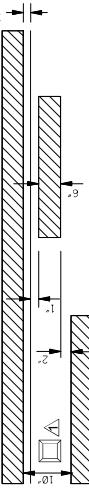
DIRECTION OF TRAFFIC



DETAIL "A"



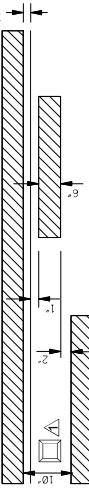
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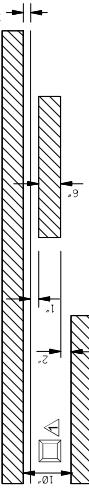
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DETAIL "A"



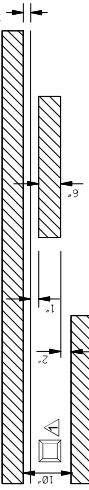
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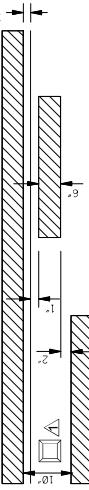
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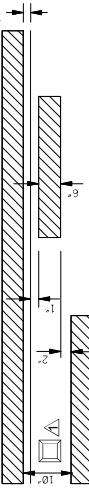
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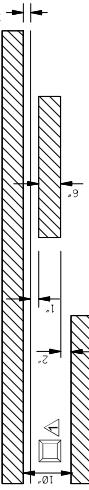
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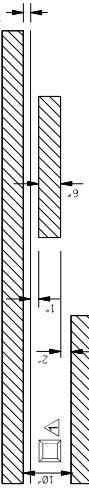
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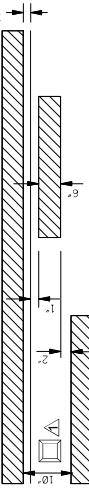
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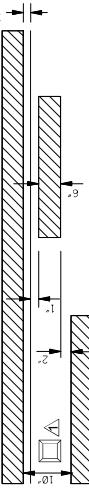
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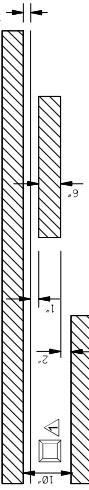
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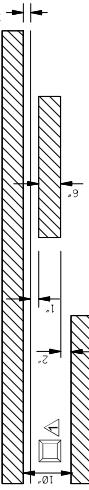
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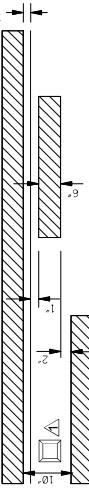
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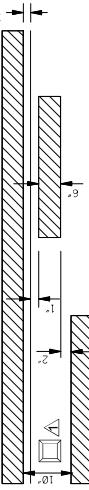
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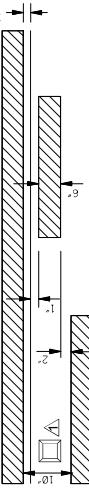
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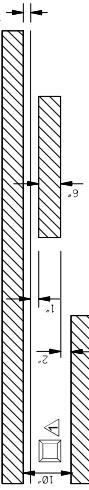
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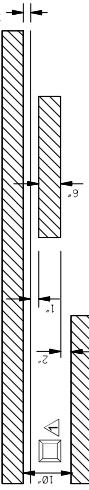
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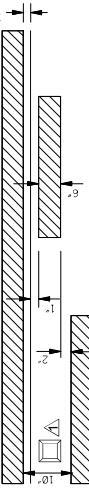
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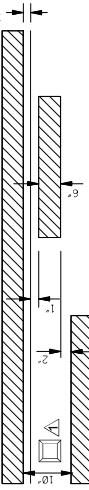
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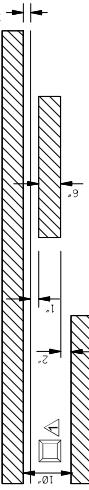
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DIRECTION OF TRAFFIC

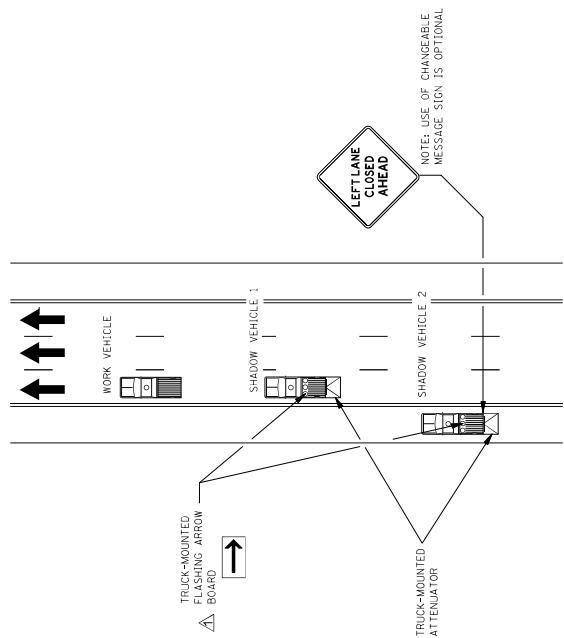


DETAIL "A"



DIRECTION OF TRAFFIC

MOBILE OPERATIONS ON MULTILANE ROAD

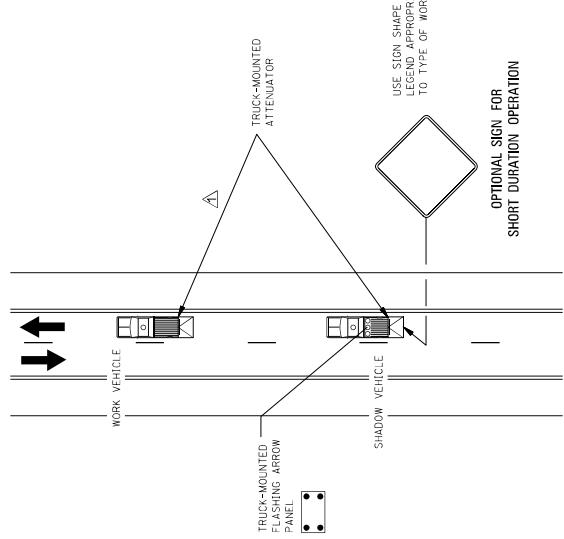


MOBILE OPERATIONS ON MULTILANE ROAD

NOTES FOR MULTILANE LANE OPERATION:

- △ 1. SHADOW AND WORK VEHICLES SHALL DISPLAY HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS. AN ARROW BOARD SHALL BE USED WHEN A FREEWAY LANE IS CLOSED. WHEN MORE THAN ONE FREEWAY LANE IS CLOSED, A SEPARATE ARROW BOARD SHALL BE USED FOR EACH CLOSED LANE.
- △ 2. SHADOW VEHICLE 2 SHOULD BE EQUIPPED WITH AN ARROW BOARD AND A TRUCK-MOUNTED ATTENUATOR (TMA). AN APPROPRIATE TRUCK-MOUNTED ATTENUATOR (TMA) SHOULD BE PLACED ON SHADOW VEHICLE 2 SO AS NOT TO OBSCURE THE ARROW BOARD.
- △ 3. SHADOW VEHICLE 1 SHOULD BE EQUIPPED WITH AN ARROW BOARD AND TRUCK-MOUNTED ATTENUATOR (TMA).
4. SHADOW VEHICLE 2 SHOULD TRAVEL AT A VARYING DISTANCE FROM THE WORK OPERATION SO AS TO PROVIDE ADEQUATE SIGHT DISTANCE FOR TRAFFIC APPROACHING FROM THE REAR.
- △ 5. THE SPACING BETWEEN THE WORK VEHICLES AND THE SHADOW VEHICLES, AND BETWEEN THE SHADOW VEHICLES, SHOULD BE MAXIMIZED TO DEER ROAD USERS FROM DRIVING IN BETWEEN.
- △ 6. ON HIGH-SPEED ROADWAYS, A THIRD SHADOW VEHICLE NOT SHOWN MAY BE USED WITH SHADOW VEHICLE 2 IN THE CLOSED LANE, SHADOW VEHICLE 2 STRADDLING THE EDGE LINE, AND SHADOW VEHICLE 3 ON THE SHOULDER.
- △ 7. ARROW BOARD SHALL BE AS A MINIMUM TYPE B, $60^\circ \times 30^\circ$ IN ACCORDANCE WITH THE CRITERIA PRESENTED IN THE MUTCD.
8. WORK SHOULD NORMALLY BE DONE DURING OFF-PEAK HOURS.

MOBILE OPERATIONS ON TWO-LANE ROAD



MOBILE OPERATIONS ON TWO-LANE ROAD

NOTES FOR TWO-LANE OPERATION:

1. WHERE PRACTICAL AND WHEN NEEDED, THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS. IF THIS CAN NOT BE DONE FREQUENTLY, AS AN ALTERNATIVE, A 'DO NOT PASS' SIGN MAY BE PLACED ON THE REAR OF THE VEHICLE BLOCKING THE LANE.
2. THE DISTANCE BETWEEN THE WORK AND SHADOW VEHICLES MAY VARY ACCORDING TO TERRAIN, AND DRYING TIME, AND OTHER FACTORS. SHADOW VEHICLES ARE REQUIRED TO MAINTAIN A DISTANCE EQUAL TO THE REAR-TO-SHADOW VEHICLE DISTANCE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. THE SHADOW VEHICLE SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
3. ADDITIONAL SHADOW VEHICLES TO WARN AND REDUCE THE SPEED OF ONCOMING TRAFFIC MAY BE USED. POLICE PATROL CARS MAY BE USED FOR THIS PURPOSE.
4. A TRUCK-MOUNTED ATTENUATOR (TMA) SHOULD BE USED ON THE SHADOW VEHICLE.
5. THE WORK VEHICLE SHALL BE EQUIPPED WITH BEACONS, AND THE SHADOW VEHICLES SHALL DISPLAY FLASHING OR ROTATING BEACONS BOTH FORWARD AND TO THE REAR.
6. VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48° ABOVE THE PAVEMENT, AND SHALL NOT BE OBSCURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
7. ARROW BOARD TO BE USED IN CAUTION MODE.
8. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

DATE	REVISED TEXT AND DETAILS	BY
12/11/12	STN	STN
MISSISSIPPI DEPARTMENT OF TRANSPORTATION		
ROADWAY DESIGN DIVISION		
TRAFFIC CONTROL PLAN		
MOBILE OPERATIONS		
MULTILANE ROADS		
AND		
TWO-LANE ROADS		
MDOT		
WORKING NUMBER		
TCP-9		
SHEET NUMBER		
6359		

STATE MISS.	PROJECT NO.
ADDENDUM	
12/17/12	REVISED TEXT
STN	BY
MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
TEMPORARY STRIPING FOR TRAFFIC CONTROL 2-LANE AND 4-LANE DIVIDED HIGHWAYS	
MDOT WORKING NUMBER TC-13	
SHEET NUMBER 6363	
ISSUE DATE: AUGUST 01, 2017	

GENERAL NOTES:

* 1. * UNLESS SHOWN ELSEWHERE ON THE PLANS.

* 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR TAPE AS INDICATED IN PAY ITEMS).

△ 3. RETROREFLECTIVE RAISED PAVEMENT MARKERS TO BE USED IF TEMPORARY MARKINGS ARE TO REMAIN IN PLACE OVER 3 MONTHS

△ 4. SPACING OF RETROREFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

TANGENT SECTIONS ($ft + in$)	URBAN AREA $40'-0"$	RURAL AREA $40'-0"$
HORIZONTAL CURVES $40'-0"$	$40'-0"$	$40'-0"$
INTERCHANGE LIMITS $40'-0"$	$40'-0"$	$40'-0"$

△ 5. RETROREFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RETROREFLECTIVE PRODUCTS LISTED.

DETAIL "A"

2-LANE WITH ONE-WAY TRAFFIC

4-LANE WITH ONE-WAY TRAFFIC

**TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)**

DETAIL "A"

DIRECTION OF TRAFFIC

DIRECTION OF TRAFFIC

**ADDENDUM**

DETAILED INDEX (BRIDGE)	PROJECT NO.: STBG-004-01(036)	DESIGNED BY: ALEX HAWKINS	DATE: 2025-11-12
SR. 30 OVER TALLAHATCHIE RIVER BRIDGE AT STA. 61+58.83	1. OF 7	80021	COUNTY: UNION
ESTIMATED QUANTITIES AND GENERAL NOTES	2. OF 7	80022	CHECKED BY: BARRETT GERMOND
HYDRODEMOLITION DETAILS	3. OF 7	80023	DR. OF STRUCTURES, ASST. STATE BRIDGE ENGINEER - SCOTT WESTERFIELD, P.E.
PLAN OF HYDRODEMOLITION	4. OF 7	80024	DR. OF STRUCTURES, ASST. STATE BRIDGE ENGINEER - MICHAEL DEW, P.E.
END WALL REPAIR DETAILS	5. OF 7	80025	DET. AT STA. 61+58.83
JOINT REPAIR DETAILS	6. OF 7	80026	DES. DR. OF STRUCTURES, ASST. STATE BRIDGE ENGINEER - SCOTT WESTERFIELD, P.E.
PILE REPAIR DETAILS	7. OF 7	80027	DETAILLED INDEX (BRIDGE)
RUBBAP BULKHEAD DETAILS	8. OF 8	80028	DET. DR. OF STRUCTURES, ASST. STATE BRIDGE ENGINEER - MICHAEL DEW, P.E.
INFORMATION PLANS	80029 - 8223	Wk. No. 8001

DESCRIPTION OF SHEETS			
DETAILED INDEX (BRIDGE)			
SR. 30 OVER TALLAHATCHIE RIVER BRIDGE AT STA. 61+58.83			
ESTIMATED QUANTITIES AND GENERAL NOTES			
HYDRODEMOLITION DETAILS			
PLAN OF HYDRODEMOLITION			
END WALL REPAIR DETAILS			
JOINT REPAIR DETAILS			
PILE REPAIR DETAILS			
RUBBAP BULKHEAD DETAILS			
INFORMATION PLANS			

BRIDGE DIVISION			
REVISIONS			
DATE	SHEET NO.	DESCRIPTION	BY
01/20/2026	80022	Revised quantities and repair locations	AWH
01/20/2026	80027	Revised encasement locations, details, and notes	AWH



ADDENDUM

BRIDGE AT STA. 6+58.83

PILE REPAIR DETAILS

DESIGNED BY: ALIX
DETAILED BY: ALEX
PROJECT NO.: STBG-004-A01(036)
COUNTY: UNION
DATE: 2025-11-12
DEP. OF STRUCTURES, STATE BRIDGE ENGINEER - SCOTT WESTERFIELD, P.E.
CHECKED BY: BARRETT
DEP. OF STRUCTURES, STATE BRIDGE ENGINEER - SCOTT WESTERFIELD, P.E.
REV. NO. 6
SHEET NO. 8007