

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO.	<u>1</u>	DATED	<u>1/22/2026</u>	ADDENDUM NO.	_____	DATED	_____
ADDENDUM NO.	_____	DATED	_____	ADDENDUM NO.	_____	DATED	_____
ADDENDUM NO.	_____	DATED	_____	ADDENDUM NO.	_____	DATED	_____

Number

Description

- 1 Revised Table of Contents; Deleted Notice to Bidder No. 5750; Added Notice to Bidder Nos. 7518 & 7624; Amendment EBSx Download Required.

TOTAL ADDENDA: 1

(Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

Contractor

BY _____

Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

President

Address

Secretary

Address

Treasurer

Address

The following is my (our) itemized proposal.

SP-0733-00(013)/ 110051301000

Webster County(ies)

Revised 01/26/2016

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION
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PROJECT: SP-0733-00(013)/110051301 - Webster

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Progress Schedule

(REVISIONS TO THE ABOVE WILL BE INDICATED ON THE SECOND SHEET
OF SECTION 905 AS ADDENDA)

01/21/2026 02:50 PM

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 7518

CODE: (SP)

DATE: 01/21/2026

SUBJECT: Standard Drawing Clarification

PROJECT: SP-0733-00(013) / 110051301 – Webster County

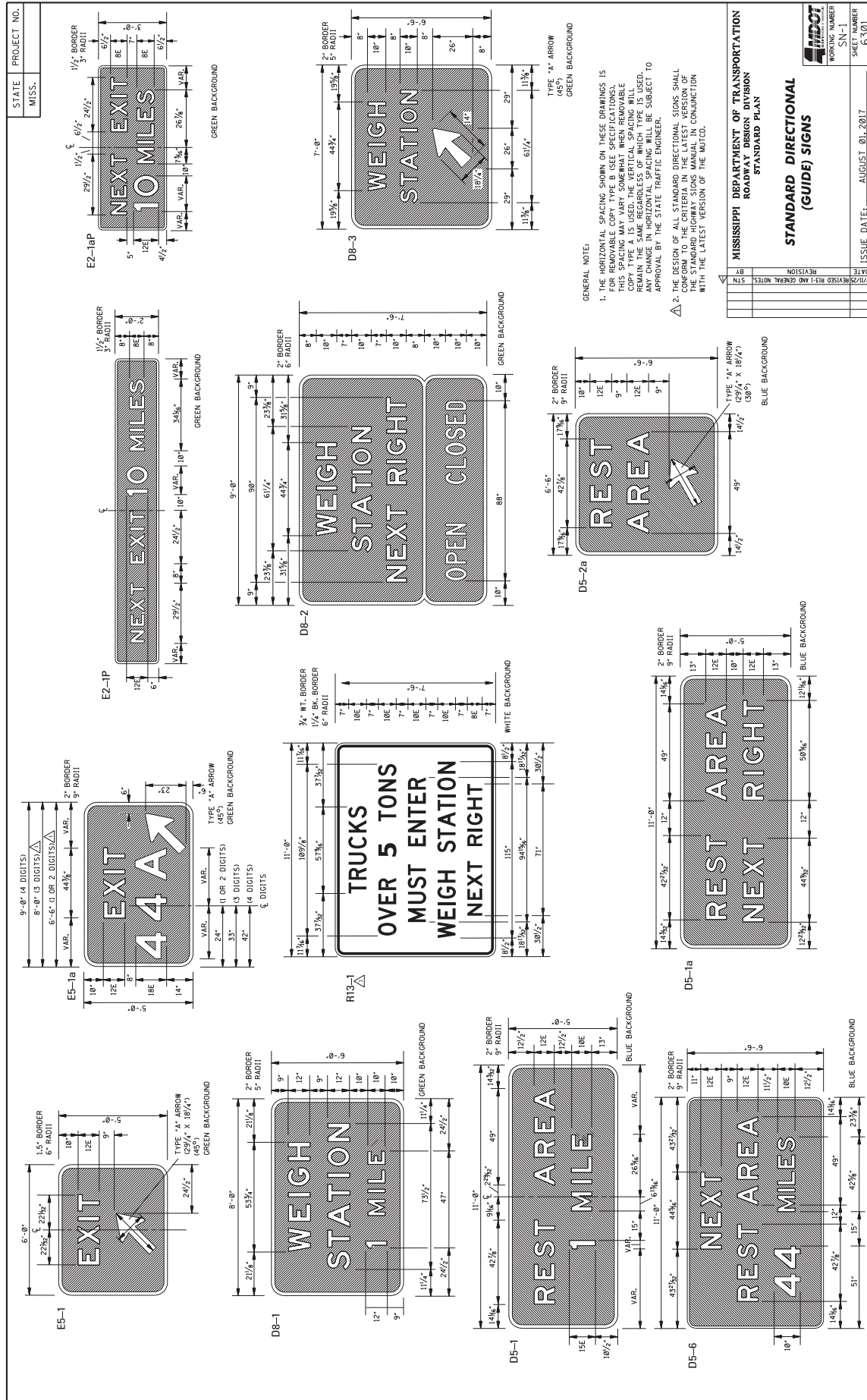
Bidders are hereby advised that in instances where the Contract Plans include previous versions of any drawings included in Notice to Bidders No. 7624, Standard Drawings, and the Supplement to Notice to Bidders No. 7624, the drawings included in Notice to Bidders No. 7624 and the Supplement to Notice to Bidders No. 7624 shall govern.

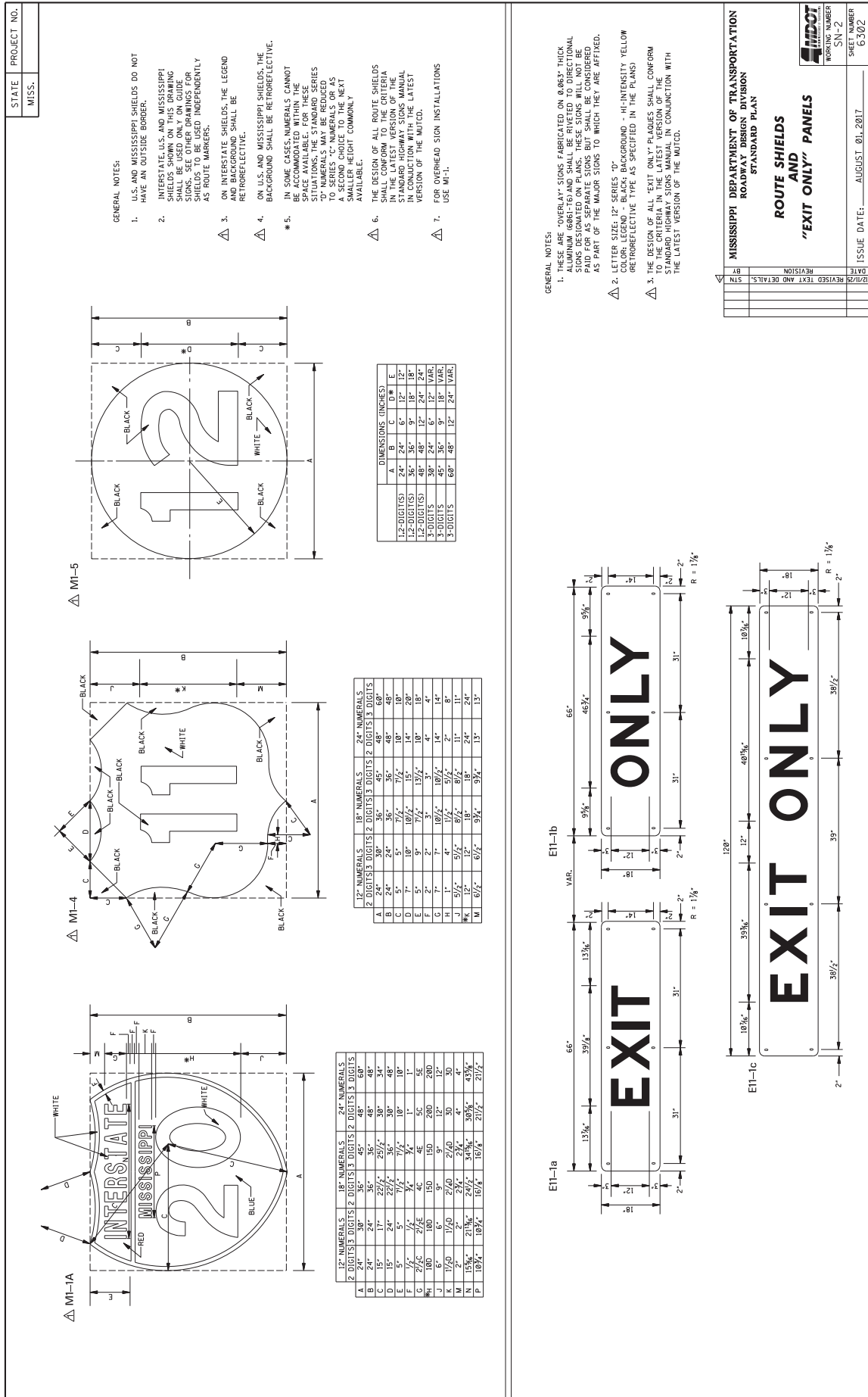
MISSISSIPPI DEPARTMENT OF TRANSPORTATION















































































SUPPLEMENT TO NOTICE TO BIDDERS NO. 7624

DATE: **01/21/2026**

After the last drawing on page 33, add the following.





SIGN NUMBER		09-2	D10-1	D10-2	D10-3	D10-1a	D10-2a	D10-3a	D10-4	D10-5	M1-1a	M1-1a	M1-4	M1-5
SIGN BLACK THICKNESS		0.080"	0.080"	0.080"	0.100"	0.080"	0.100"	0.100"	0.100"	0.100"	0.080"	0.100"	0.080"	MISS.
LETTER & NUMERAL SERIES	INSIDE													
	OUTSIDE													
	SIZE (WIDTH X HEIGHT)	24" X 24"	12" X 24"	12" X 36"	12" X 48"	12" X 36"	12" X 48"	12" X 48"	18" X 54"	18" X 60"	36" X 36"	24" X 24"	24" X 24"	24" X 24"
	COLORS	WHITE BLUE BACKGROUND	WHITE BLUE BACKGROUND	WHITE BLUE BACKGROUND	WHITE BLUE BACKGROUND	WHITE BLUE BACKGROUND	WHITE BLUE BACKGROUND	WHITE BLUE BACKGROUND	WHITE RED BLUE GREEN	WHITE RED BLUE GREEN	WHITE RED BLUE GREEN	WHITE RED BLUE GREEN	WHITE RED BLUE GREEN	WHITE RED BLUE GREEN
	REFLECTORIZATION	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL
LETTER & NUMERAL SERIES	INSIDE													
	OUTSIDE													
	SIZE (WIDTH X HEIGHT)	21" X 15"	24" X 12"	36" X 18"	24" X 12"	24" X 12"	21" X 15"	21" X 15"	21" X 15"	21" X 15"	21" X 15"	21" X 15"	21" X 15"	21" X 15"
	COLORS	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND
	REFLECTORIZATION	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL
LETTER & NUMERAL SERIES	INSIDE													
	OUTSIDE													
	SIZE (WIDTH X HEIGHT)	21" X 15"	24" X 12"	36" X 18"	24" X 12"	24" X 12"	21" X 15"	21" X 15"	21" X 15"	21" X 15"	21" X 15"	21" X 15"	21" X 15"	21" X 15"
	COLORS	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND
	REFLECTORIZATION	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL

GENERAL NOTES:

1. THE DIMENSIONS FOR ALL STANDARD ROADSIDE SIGNS SHALL CONFORM WITH THOSE SHOWN IN THE STANDARD HIGHWAY SIGNS IN CONJUNCTION WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITIONS.

2. ROUTE MARKERS AND SHIELDS SHOWN ON THIS SHEET ARE FOR USE INDEPENDENTLY OF INTERSTATE DIRECTIONAL (GUIDE) SIGNS. SEE OTHER DRAWINGS FOR SHIELDS TO BE USED ON INTERSTATE DIRECTIONAL (GUIDE) SIGNS.

3. THE QUANTITIES LISTED ON THE SUMMARY OF QUANTITIES SHEET FOR THE SIGNS SHOWN ON THIS SHEET WILL BE USED AS THE BASIS FOR FINAL PAYMENT, EXCEPT WHERE SIGNS ARE MODIFIED FROM THAT SHOWN.

4. M-5, IN SOME CASES MATERIALS CANNOT BE ACCOMMODATED WITHIN THE SPACE AVAILABLE FOR THESE SITUATIONS, THE STANDARD SERIES "D" NUMERALS SHALL BE REDUCED TO SERIES "C".

WEST	EAST	SOUTH	NORTH
M3-1A	M3-2A	M3-3A	M3-4A
M3-10A	M3-10B	M3-10C	M3-10D


MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

STANDARD ROADSIDE SIGNS

DATE	REVISION	TEXT	BY	DATE
08/01/2017	1	ISSUE DATE:		
08/01/2017	2	SN - 3		
08/01/2017	3	SHEET NUMBER		
08/01/2017	4	6505		

STN	REVISION	DATE	08/26
01		ISSUE DATE:	AUGUST 01, 2017

STANDARD ROADSIDE SIGNS

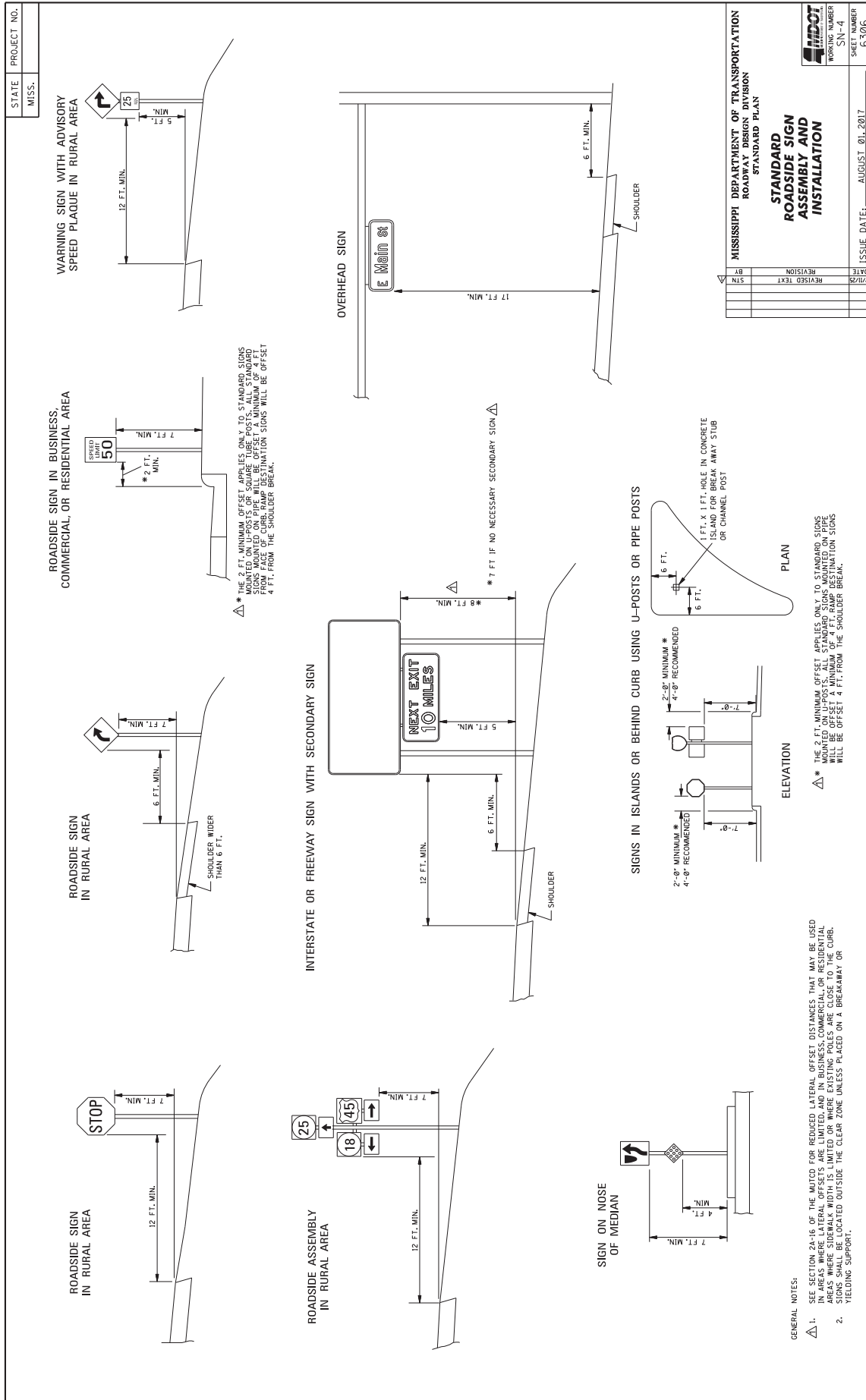


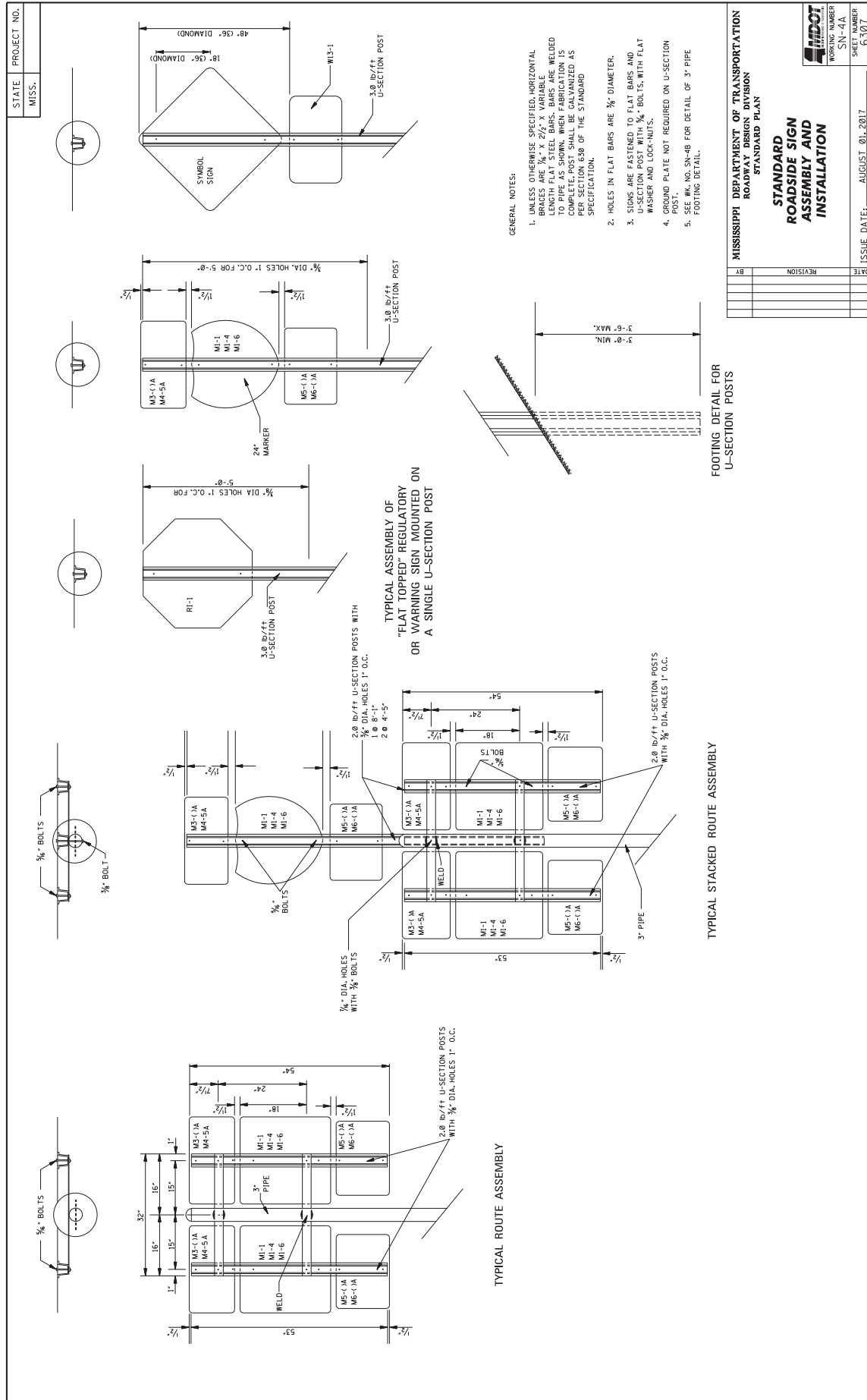
MISSISSIPPI
DEPARTMENT OF TRANSPORTATION
ROADSIDE SIGNS DIVISION
STANDARD PLAN

GENERAL NOTES.

1. THE QUANTITIES LISTED ON THE SUMMARY OF QUANTITIES SHEET FOR THE SIGNS SHOWN ON THIS SHEET WILL BE USED AS THE BASIS FOR FINAL PAYMENT. EXCEPT WHERE SIGNS ARE MODIFIED FROM THAT SHOWN.
2. THE SPEED LIMITS REQUIRED ON SIGNS R2-1 AND R2-40 WILL BE SHOWN ON INDIVIDUAL PLAN SHEETS.
3. THE DESIGN OF ALL STANDARD ROADSIDE SIGNS SHALL CONFORM TO THE CRITERIA IN THE LATEST VERSION OF THE STANDARD HIGHWAY SIGNS MANUAL IN CONJUNCTION WITH THE LATEST VERSION OF THE AUTOCAD.

SIGN NUMBER	W1-1L W1-1R	W1-2L W1-2R	W1-3L W1-3R	W1-4L W1-4R	W1-7	W1-8	W1-9	W1-10	W1-11	W1-12	W1-13	W1-14	W1-15	W1-16	W1-17	W1-18	W1-19	W1-20	W1-21	W1-22	W1-23	W1-24	W1-25	W1-26	W1-27	W1-28	W1-29	W1-30	W1-31	W1-32	W1-33	W1-34	W1-35	W1-36	W1-37	W1-38	W1-39	W1-40	W1-41	W1-42	W1-43	W1-44	W1-45	W1-46	W1-47	W1-48	W1-49	W1-50	W1-51	W1-52	W1-53	W1-54	W1-55	W1-56	W1-57	W1-58	W1-59	W1-60	W1-61	W1-62	W1-63	W1-64	W1-65	W1-66	W1-67	W1-68	W1-69	W1-70	W1-71	W1-72	W1-73	W1-74	W1-75	W1-76	W1-77	W1-78	W1-79	W1-80	W1-81	W1-82	W1-83	W1-84	W1-85	W1-86	W1-87	W1-88	W1-89	W1-90	W1-91	W1-92	W1-93	W1-94	W1-95	W1-96	W1-97	W1-98	W1-99	W1-100	W1-101	W1-102	W1-103	W1-104	W1-105	W1-106	W1-107	W1-108	W1-109	W1-110	W1-111	W1-112	W1-113	W1-114	W1-115	W1-116	W1-117	W1-118	W1-119	W1-120	W1-121	W1-122	W1-123	W1-124	W1-125	W1-126	W1-127	W1-128	W1-129	W1-130	W1-131	W1-132	W1-133	W1-134	W1-135	W1-136	W1-137	W1-138	W1-139	W1-140	W1-141	W1-142	W1-143	W1-144	W1-145	W1-146	W1-147	W1-148	W1-149	W1-150	W1-151	W1-152	W1-153	W1-154	W1-155	W1-156	W1-157	W1-158	W1-159	W1-160	W1-161	W1-162	W1-163	W1-164	W1-165	W1-166	W1-167	W1-168	W1-169	W1-170	W1-171	W1-172	W1-173	W1-174	W1-175	W1-176	W1-177	W1-178	W1-179	W1-180	W1-181	W1-182	W1-183	W1-184	W1-185	W1-186	W1-187	W1-188	W1-189	W1-190	W1-191	W1-192	W1-193	W1-194	W1-195	W1-196	W1-197	W1-198	W1-199	W1-200	W1-201	W1-202	W1-203	W1-204	W1-205	W1-206	W1-207	W1-208	W1-209	W1-210	W1-211	W1-212	W1-213	W1-214	W1-215	W1-216	W1-217	W1-218	W1-219	W1-220	W1-221	W1-222	W1-223	W1-224	W1-225	W1-226	W1-227	W1-228	W1-229	W1-230	W1-231	W1-232	W1-233	W1-234	W1-235	W1-236	W1-237	W1-238	W1-239	W1-240	W1-241	W1-242	W1-243	W1-244	W1-245	W1-246	W1-247	W1-248	W1-249	W1-250	W1-251	W1-252	W1-253	W1-254	W1-255	W1-256	W1-257	W1-258	W1-259	W1-260	W1-261	W1-262	W1-263	W1-264	W1-265	W1-266	W1-267	W1-268	W1-269	W1-270	W1-271	W1-272	W1-273	W1-274	W1-275	W1-276	W1-277	W1-278	W1-279	W1-280	W1-281	W1-282	W1-283	W1-284	W1-285	W1-286	W1-287	W1-288	W1-289	W1-290	W1-291	W1-292	W1-293	W1-294	W1-295	W1-296	W1-297	W1-298	W1-299	W1-300	W1-301	W1-302	W1-303	W1-304	W1-305	W1-306	W1-307	W1-308	W1-309	W1-310	W1-311	W1-312	W1-313	W1-314	W1-315	W1-316	W1-317	W1-318	W1-319	W1-320	W1-321	W1-322	W1-323	W1-324	W1-325	W1-326	W1-327	W1-328	W1-329	W1-330	W1-331	W1-332	W1-333	W1-334	W1-335	W1-336	W1-337	W1-338	W1-339	W1-340	W1-341	W1-342	W1-343	W1-344	W1-345	W1-346	W1-347	W1-348	W1-349	W1-350	W1-351	W1-352	W1-353	W1-354	W1-355	W1-356	W1-357	W1-358	W1-359	W1-360	W1-361	W1-362	W1-363	W1-364	W1-365	W1-366	W1-367	W1-368	W1-369	W1-370	W1-371	W1-372	W1-373	W1-374	W1-375	W1-376	W1-377	W1-378	W1-379	W1-380	W1-381	W1-382	W1-383	W1-384	W1-385	W1-386	W1-387	W1-388	W1-389	W1-390	W1-391	W1-392	W1-393	W1-394	W1-395	W1-396	W1-397	W1-398	W1-399	W1-400	W1-401	W1-402	W1-403	W1-404	W1-405	W1-406	W1-407	W1-408	W1-409	W1-410	W1-411	W1-412	W1-413	W1-414	W1-415	W1-416	W1-417	W1-418	W1-419	W1-420	W1-421	W1-422	W1-423	W1-424	W1-425	W1-426	W1-427	W1-428	W1-429	W1-430	W1-431	W1-432	W1-433	W1-434	W1-435	W1-436	W1-437	W1-438	W1-439	W1-440	W1-441	W1-442	W1-443	W1-444	W1-445	W1-446	W1-447	W1-448	W1-449	W1-450	W1-451	W1-452	W1-453	W1-454	W1-455	W1-456	W1-457	W1-458	W1-459	W1-460	W1-461	W1-462	W1-463	W1-464	W1-465	W1-466	W1-467	W1-468	W1-469	W1-470	W1-471	W1-472	W1-473	W1-474	W1-475	W1-476	W1-477	W1-478	W1-479	W1-480	W1-481	W1-482	W1-483	W1-484	W1-485	W1-486	W1-487	W1-488	W1-489	W1-490	W1-491	W1-492	W1-493	W1-494	W1-495	W1-496	W1-497	W1-498	W1-499	W1-500	W1-501	W1-502	W1-503	W1-504	W1-505	W1-506	W1-507	W1-508	W1-509	W1-510	W1-511	W1-512	W1-513	W1-514	W1-515	W1-516	W1-517	W1-518	W1-519	W1-520	W1-521	W1-522	W1-523	W1-524	W1-525	W1-526	W1-527	W1-528	W1-529	W1-530	W1-531	W1-532	W1-533	W1-534	W1-535	W1-536	W1-537	W1-538	W1-539	W1-540	W1-541	W1-542	W1-543	W1-544	W1-545	W1-546	W1-547	W1-548	W1-549	W1-550	W1-551	W1-552	W1-553	W1-554	W1-555	W1-556	W1-557	W1-558	W1-559	W1-560	W1-561	W1-562	W1-563	W1-564	W1-565	W1-566	W1-567	W1-568	W1-569	W1-570	W1-571	W1-572	W1-573	W1-574	W1-575	W1-576	W1-577	W1-578	W1-579	W1-580	W1-581	W1-582	W1-583	W1-584	W1-585	W1-586	W1-587	W1-588	W1-589	W1-590	W1-591	W1-592	W1-593	W1-594	W1-595	W1-596	W1-597	W1-598	W1-599	W1-600	W1-601	W1-602	W1-603	W1-604	W1-605	W1-606	W1-607	W1-608	W1-609	W1-610	W1-611	W1-612	W1-613	W1-614	W1-615	W1-616	W1-617	W1-618	W1-619	W1-620	W1-621	W1-622	W1-623	W1-624	W1-625	W1-626	W1-627	W1-628	W1-629	W1-630	W1-631	W1-632	W1-633	W1-634	W1-635	W1-636	W1-637	W1-638	W1-639	W1-640	W1-641	W1-642	W1-643	W1-644	W1-645	W1-646	W1-647	W1-648	W1-649	W1-650	W1-651	W1-652	W1-653	W1-654	W1-655	W1-656	W1-657	W1-658	W1-659	W1-660	W1-661	W1-662	W1-663	W1-664	W1-665	W1-666	W1-667	W1-668	W1-669	W1-670	W1-671	W1-672	W1-673	W1-674	W1-675	W1-676	W1-677	W1-678	W1-679	W1-680	W1-681	W1-682	W1-683	W1-684	W1-685	W1-686	W1-687	W1-688	W1-689	W1-690	W1-691	W1-692	W1-693	W1-694	W1-695	W1-696	W1-697	W1-698	W1-699	W1-700	W1-701	W1-702	W1-703	W1-704	W1-705	W1-706	W1-707	W1-708	W1-709	W1-710	W1-711	W1-712	W1-713	W1-714	W1-715	W1-716	W1-717	W1-718	W1-719	W1-720	W1-721	W1-722	W1-723	W1-724	W1-725	W1-726	W1-727	W1-728	W1-729	W1-730	W1-731	W1-732	W1-733	W1-734	W1-735	W1-736	W1-737	W1-738	W1-739	W1-740	W1-741	W1-742	W1-743	W1-744	W1-745	W1-746	W1-747	W1-748	W1-749	W1-750	W1-751	W1-752	W1-753	W1-754	W1-755	W1-756	W1-757	W1-758	W1-759	W1-760	W1-761	W1-762	W1-763	W1-764	W1-765	W1-766	W1-767	W1-768	W1-769	W1-770	W1-771	W1-772	W1-773	W1-774	W1-775	W1-776	W1-777	W1-778	W1-779	W1-780	W1-781	W1-782	W1-783	W1-784	W1-785	W1-786	W1-787	W1-788	W1-789	W1-790	W1-791	W1-792	W1-793	W1-794	W1-795	W1-796	W1-797	W1-798	W1-799	W1-800	W1-801	W1-802	W1-803	W1-804	W1-805	W1-806	W1-807	W1-808	W1-809	W1-810	W1-811	W1-812	W1-813	W1-814	W1-815	W1-816	W1-817	W1-818	W1-819	W1-820	W1-821	W1-822	W1-823	W1-824	W1-825	W1-826	W1-827	W1-828	W1-829	W1-830	W1-831	W1-832	W1-833	W1-834	W1-835	W1-836	W1-837	W1-838	W1-839	W1-840	W1-841	W1-842	W1-843	W1-844	W1-845	W1-846	W1-847	W1-848	W1-849	W1-850	W1-851	W1-852	W1-853	W1-854	W1-855	W1-856	W1-857	W1-858	W1-859	W1-860	W1-861	W1-862	W1-863	W1-864	W1-865	W1-866	W1-867	W1-868	W1-869	W1-870	W1-871	W1-872	W1-873	W1-874	W1-875	W1-876	W1-877	W1-878	W1-879	W1-880	W1-881	W1-882	W1-883	W1-884	W1-885	W1-886	W1-887	W1-888	W1-889	W1-890	W1-891	W1-892	W1-893	W1-894	W1-895	W1-896	W1-897	W1-898	W1-899	W1-900	W1-901	W1-902	W1-903	W1-904	W1-905	W1-906	W1-907	W1-908	W1-909	W1-910	W1-911	W1-912	W1-913	W1-914	W1-915	W1-916	W1-917	W1-918	W1-919	W1-920	W1-921	W1-922	W1-923	W1-924	W1-925	W1-926	W1-927	W1-928	W1-929	W1-930	W1-931	W1-932	W1-933	W1-934	W1-935	W1-936	W1-937	W1-938	W1-939	W1-940	W1-941	W1-942	W1-943	W1-944	W1-945	W1-946	W1-947	W1-948	W1-949	W1-950	W1-951	W1-952	W1-953	W1-954	W1-955	W1-956	W1-957	W1-958	W1-959	W1-960	W1-961	W1-962	W1-963	W1-964	W1-965	W1-966	W1-967	W1-968	W1-969	W1-970	W1-971	W1-972	W1-973	W1-974	W1-975	W1-976	W1-977	W1-978	W1-979	W1-980	W1-981	W1-982	W1-983	W1-984	W1-985	W1-986	W1-987	W1-988	W1-989	W1-990	W1-991	W1-992	W1-993	W1-994	W1-995	W1-996	W1-997	W1-998	W1-999	W1-1000	W1-1001	W1-1002	W1-1003	W1-1004	W1-1005	W1-1006	W1-1007	W1-1008	W1-1009	W1-1010	W1-1011	W1-1012	W1-1013	W1-1014	W1-1015	W1-1016	W1-1017	W1-1018	W1-1019	W1-1020	W1-1021	W1-1022	W1-1023	W1-1024	W1-1025	W1-1026	W1-1027	W1-1028	W1-1029	W1-1030	W1-1031	W1-1032	W1-1033	W1-1034	W1-1035	W1-1036	W1-1037	W1-1038	W1-1039	W1-1040	W1-1041	W1-1042	W1-1043	W1-1044	W1-1045	W1-1046	W1-1047	W1-1048	W1-1049	W1-1050	W1-1051	W1-1052	W1-1053	W1-1054	W1-1055	W1-1056	W1-1057	W1-1058	W1-1059	W1-1060	W1-1061	W1-1062	W1-1063	W1-1064	W1-1065	W1-1066	W1-1067	W1-1068	W1-1069	W1-1070	W1-1071	W1-1072	W1-1073	W1-1074	W1-1075	W1-1076	W1-1077	W1-1078	W1-1079	W1-1080	W1-1081	W1-1082	W1-1083	W1-1084	W1-1085	W1-1086	W1-1087	W1-1088	W1-1089	W1-1090	W1-1091	W1-1092	W1-1093	W1-1094	W1-1095	W1-1096	W1-1097	W1-1098	W1-1099	W1-1100	W1-1101	W1-1102	W1-1103	W1-1104	W1-1105	W1-1106	W1-1107	W1-1108	W1-1109	W1-1110	W1-1111	W1-1112	W
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STATE MISS.

PROJECT NO.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**TYPICAL INSTALLATION
OF GROUND MOUNTED
DIRECTIONAL SIGNS**

DATE: AUGUST 01, 2017
SHEET NUMBER 6-309

ALIGNMENT OF SIGNS FOR TANGENT SECTIONS

ALIGNMENT OF SIGN FOR CURVED SECTION

ALIGNMENT OF SIGNS FOR TANGENT SECTIONS

ALIGNMENT OF SIGN FOR CURVED SECTION

SPECIAL INSTALLATION OF A GROUND MOUNTED DIRECTIONAL SIGN WITH STANDARD ROADSIDE SIGNS MOUNTED BELOW

TYPICAL INSTALLATION OF A CUT OR FILL SECTION

TYPICAL INSTALLATION OF GROUND MOUNTED CORE SIGN (G-2 OR G-3)

TYPICAL INSTALLATION IN A FILL SECTION PROTECTED BY GUARDRAIL

GENERAL NOTES:

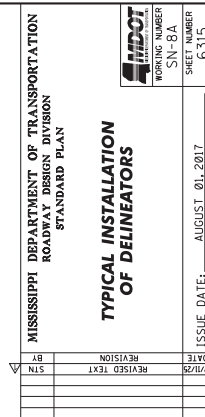
- * 1. 7'-0" MINIMUM FROM GROUND TO BOTTOM EDGE OF LOWEST SIGN (SHORTEST POST).

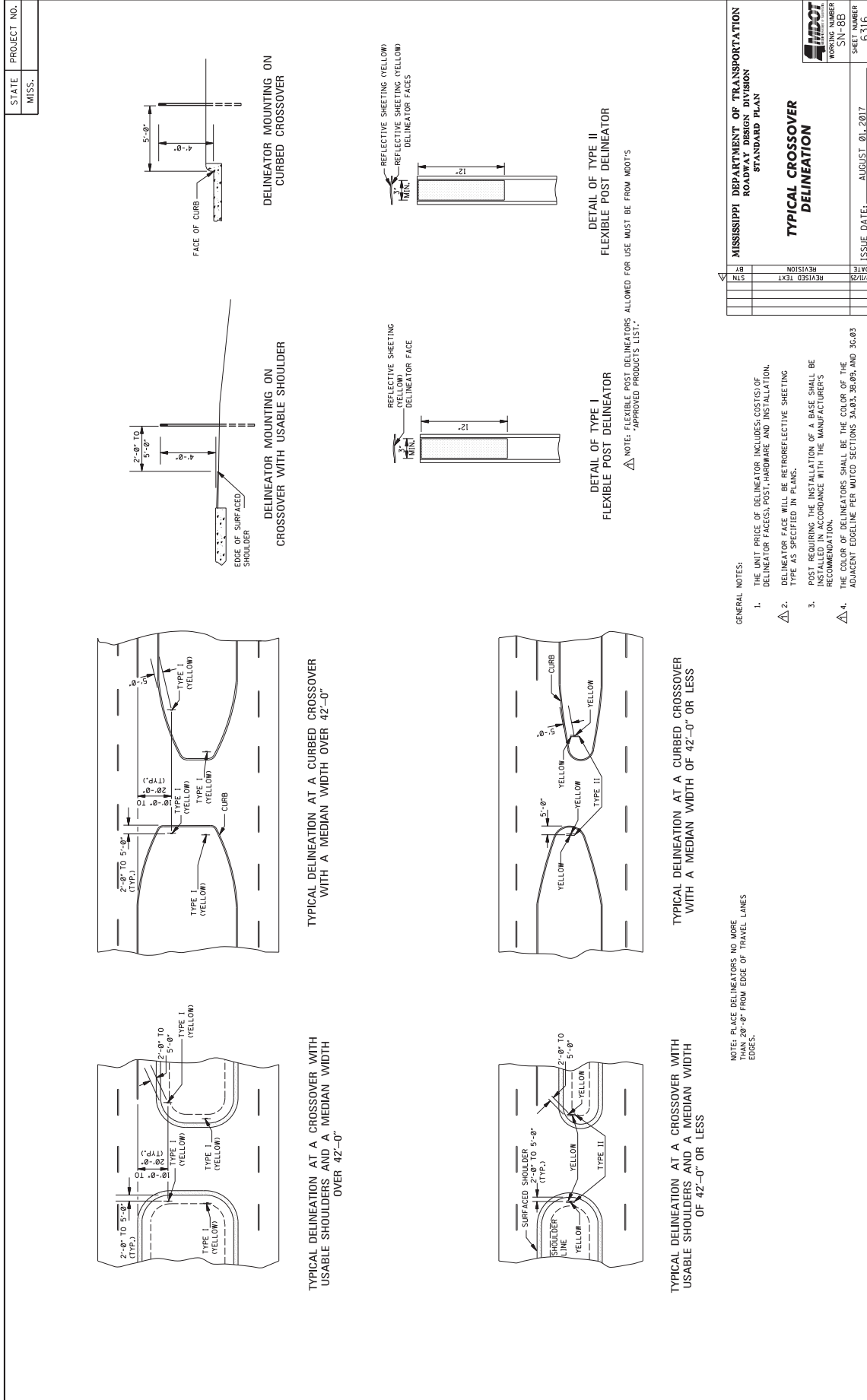
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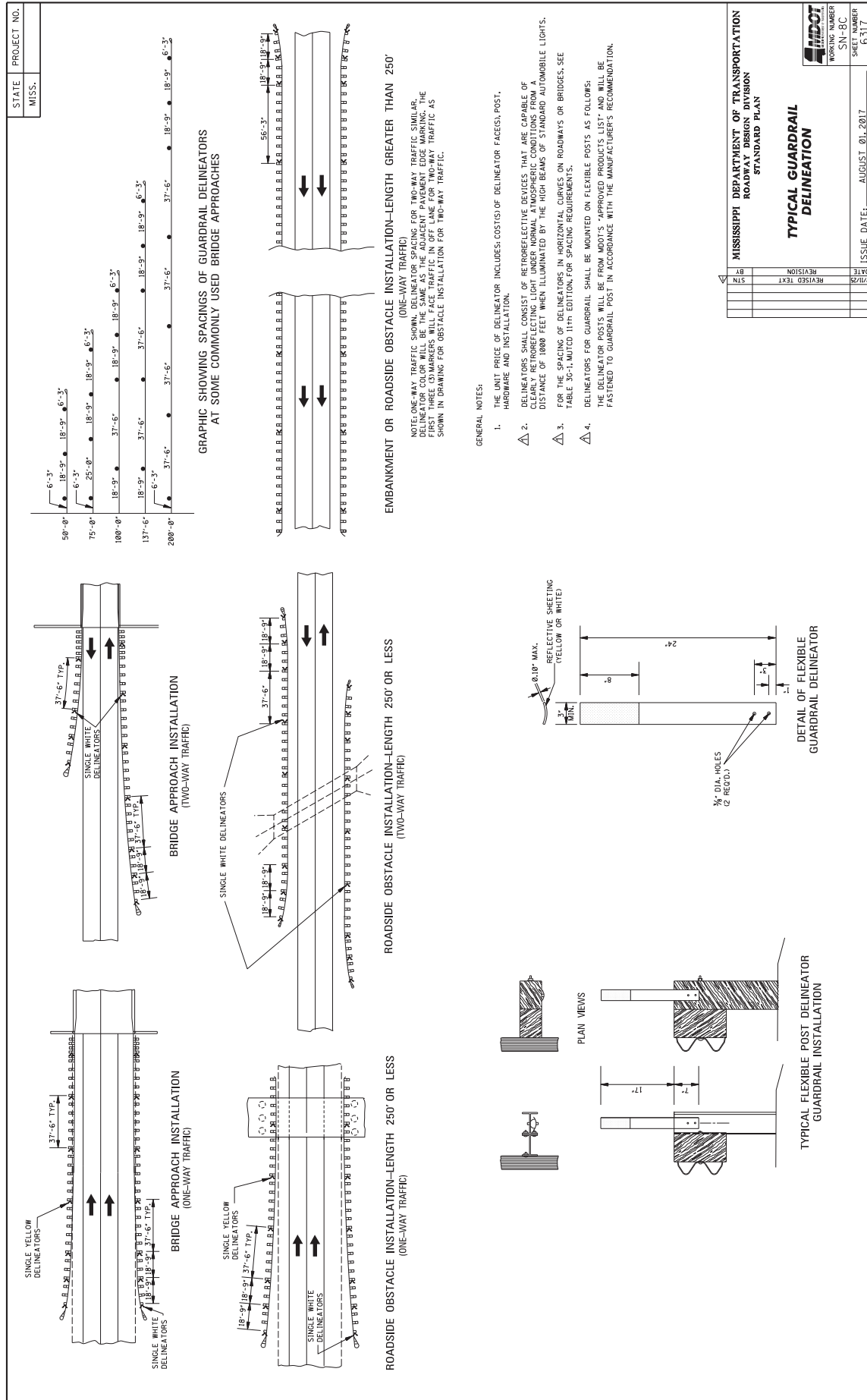
- * 1. 7'-0" MINIMUM FROM GROUND TO BOTTOM EDGE OF LOWEST SIGN (SHORTEST POST).

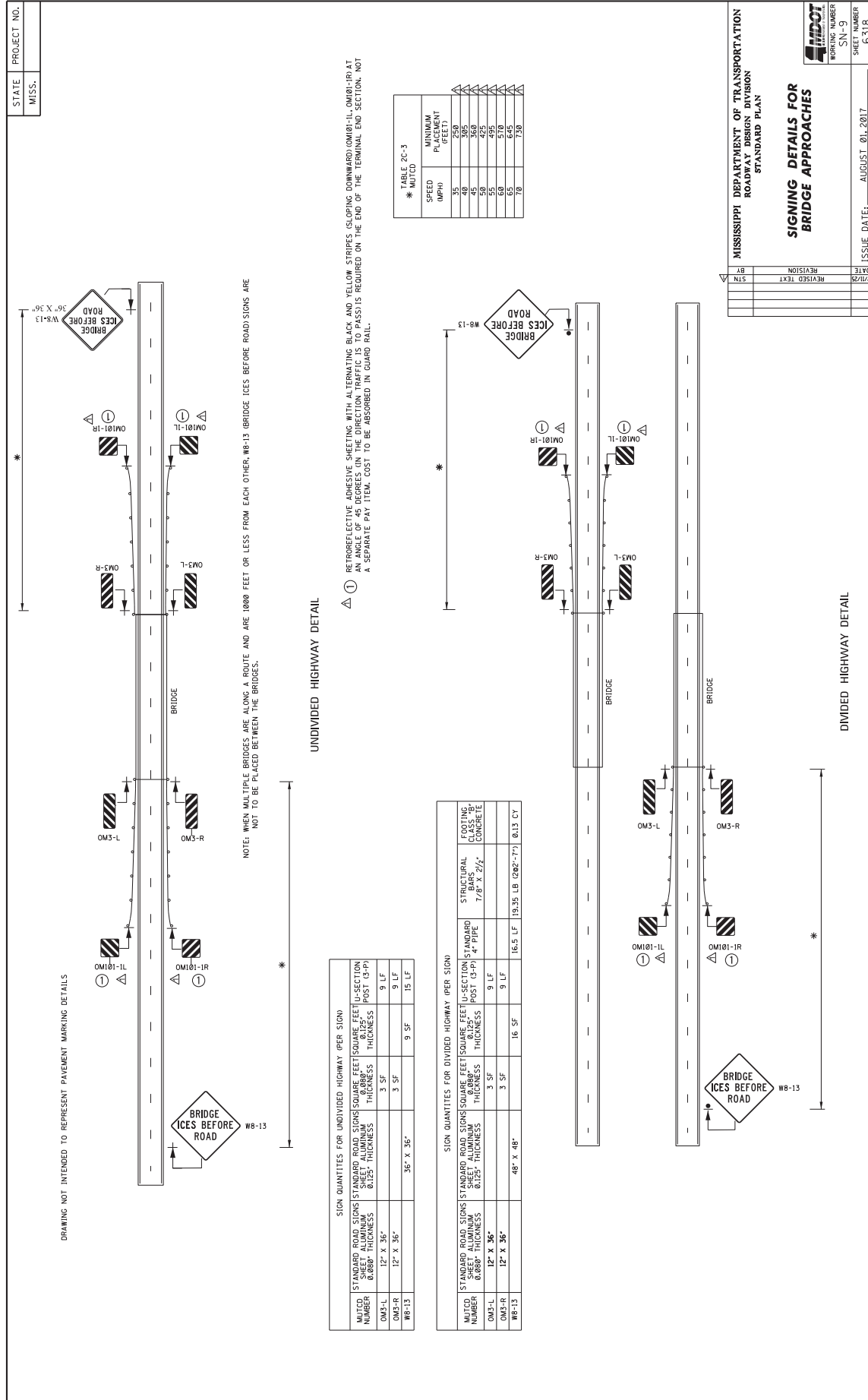
 MDOT Michigan Department of Transportation	WORKING NUMBER SN-6	SHEET NUMBER 6310
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	WORKING NUMBER SN-6A	SHEET NUMBER 6311
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MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 – NOTICE TO BIDDERS NO. 7624

CODE: (SP)

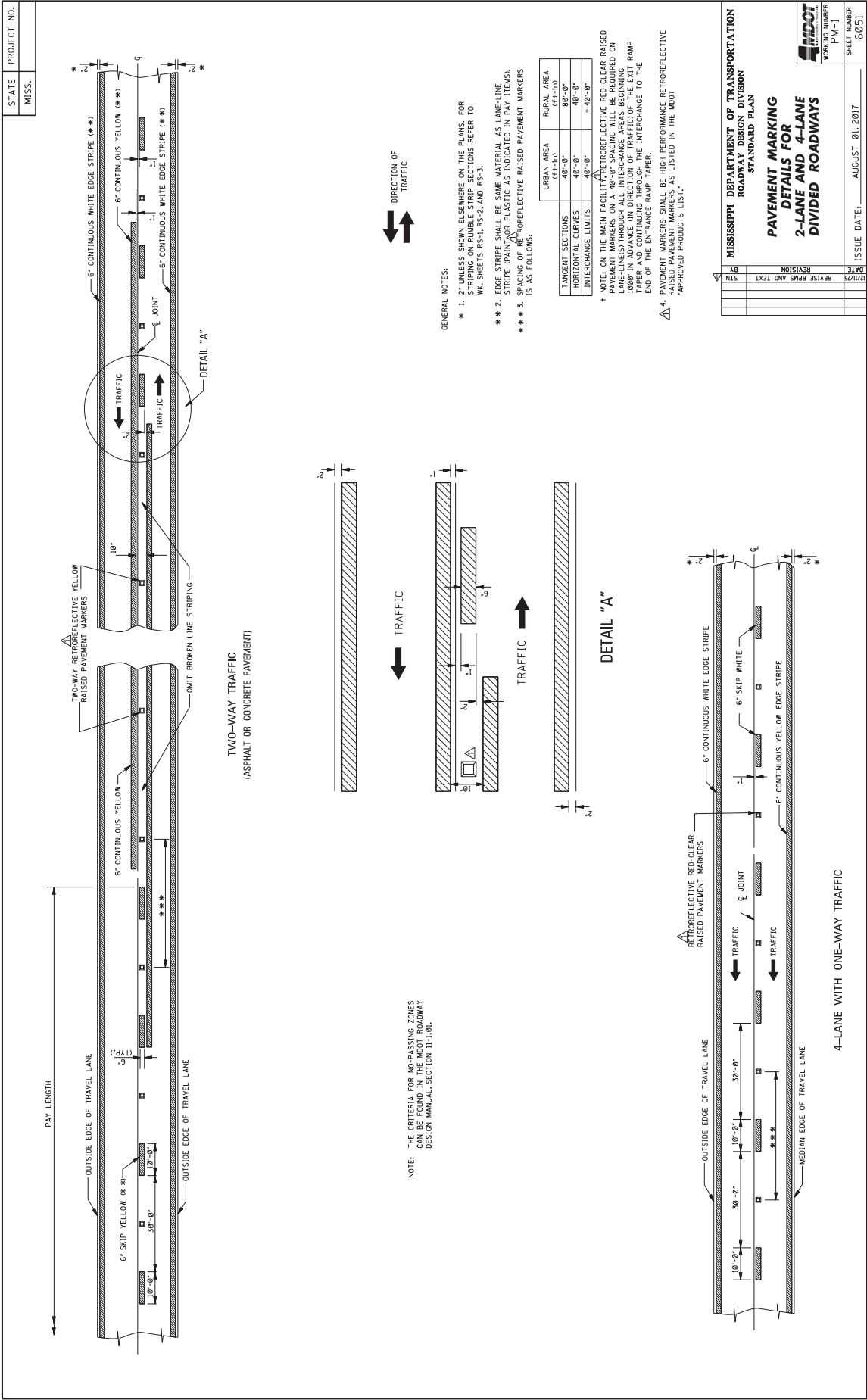
DATE: 01/20/2026

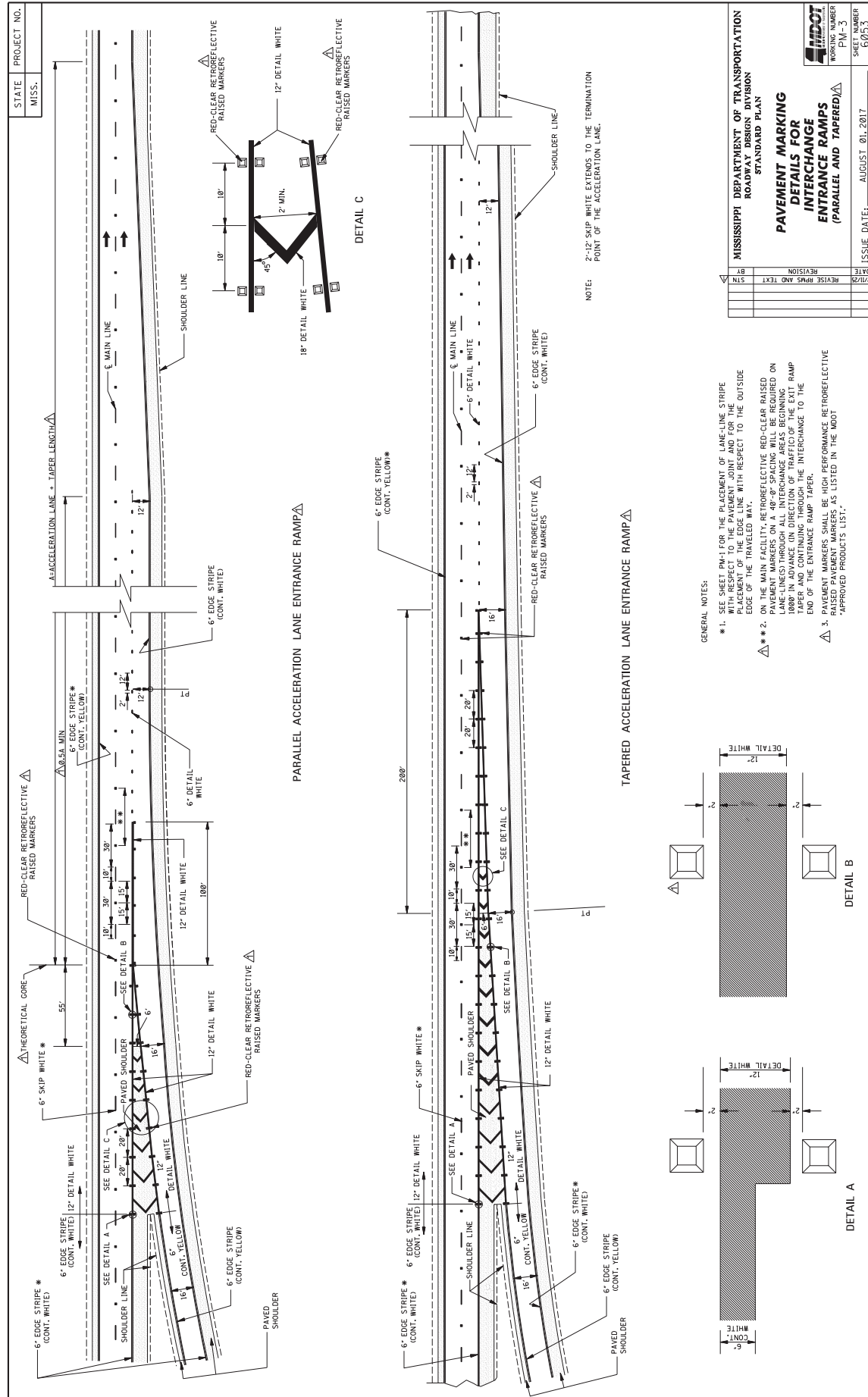
SUBJECT: Standard Drawings

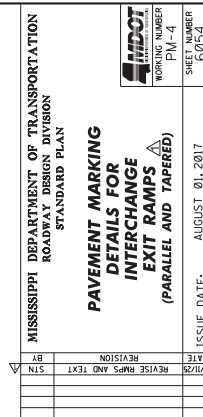
Standard Drawings attached hereto shall govern appropriate items of required work.


Larger copies of Standard Drawings may be purchased from:

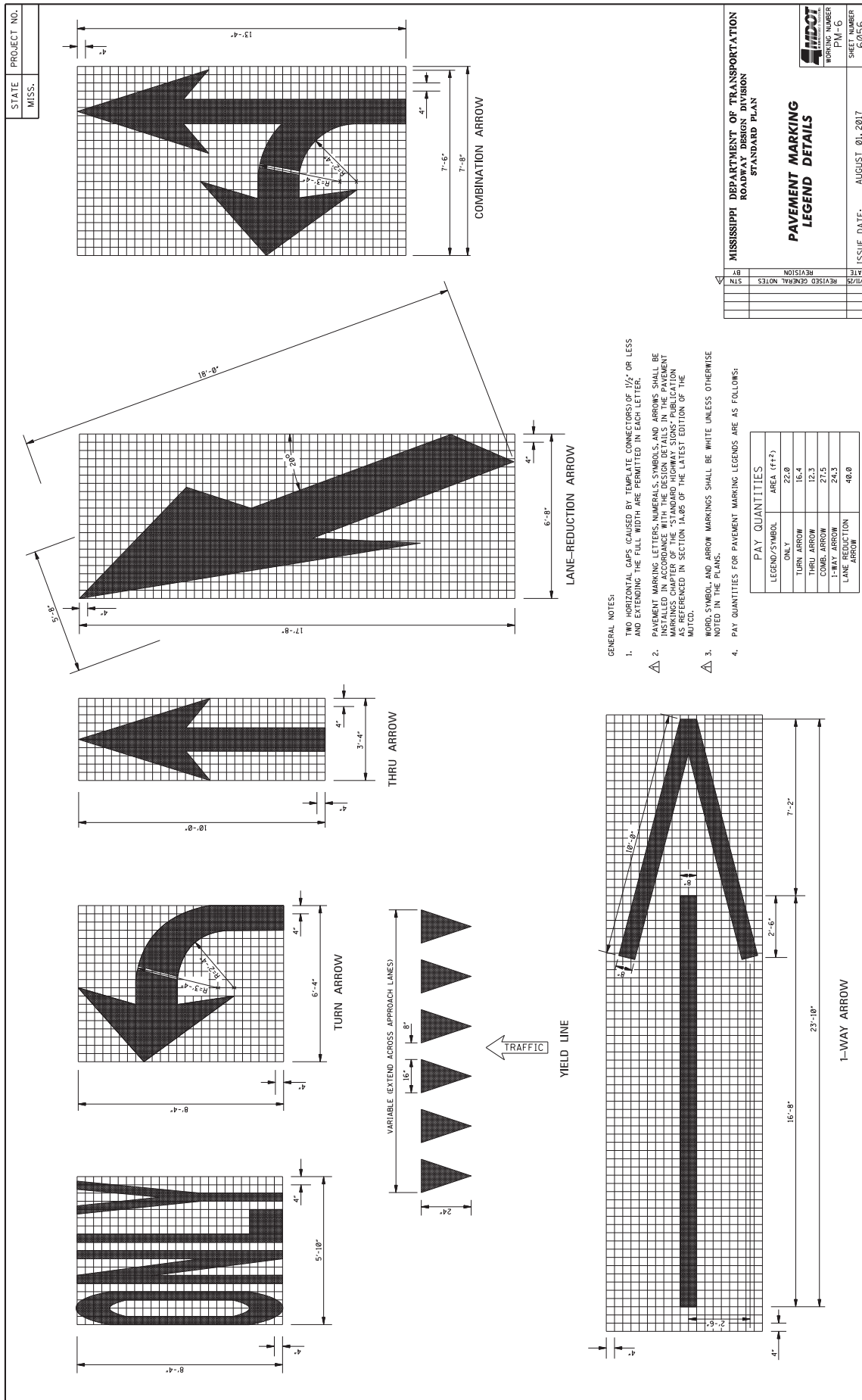
MDOT Plans Print Shop
MDOT Shop Complex, Building C, Room 114
2567 North West Street
P.O. Box 1850
Jackson, MS 39215-1850
Telephone: (601) 359-7460
or FAX: (601) 359-7461
or e-mail: plans@mdot.state.ms.us







MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	 WORKING NUMBER PM-5 SHEET NO. 6055	ISSUE DATE: AUGUST 01, 2017 DATE:
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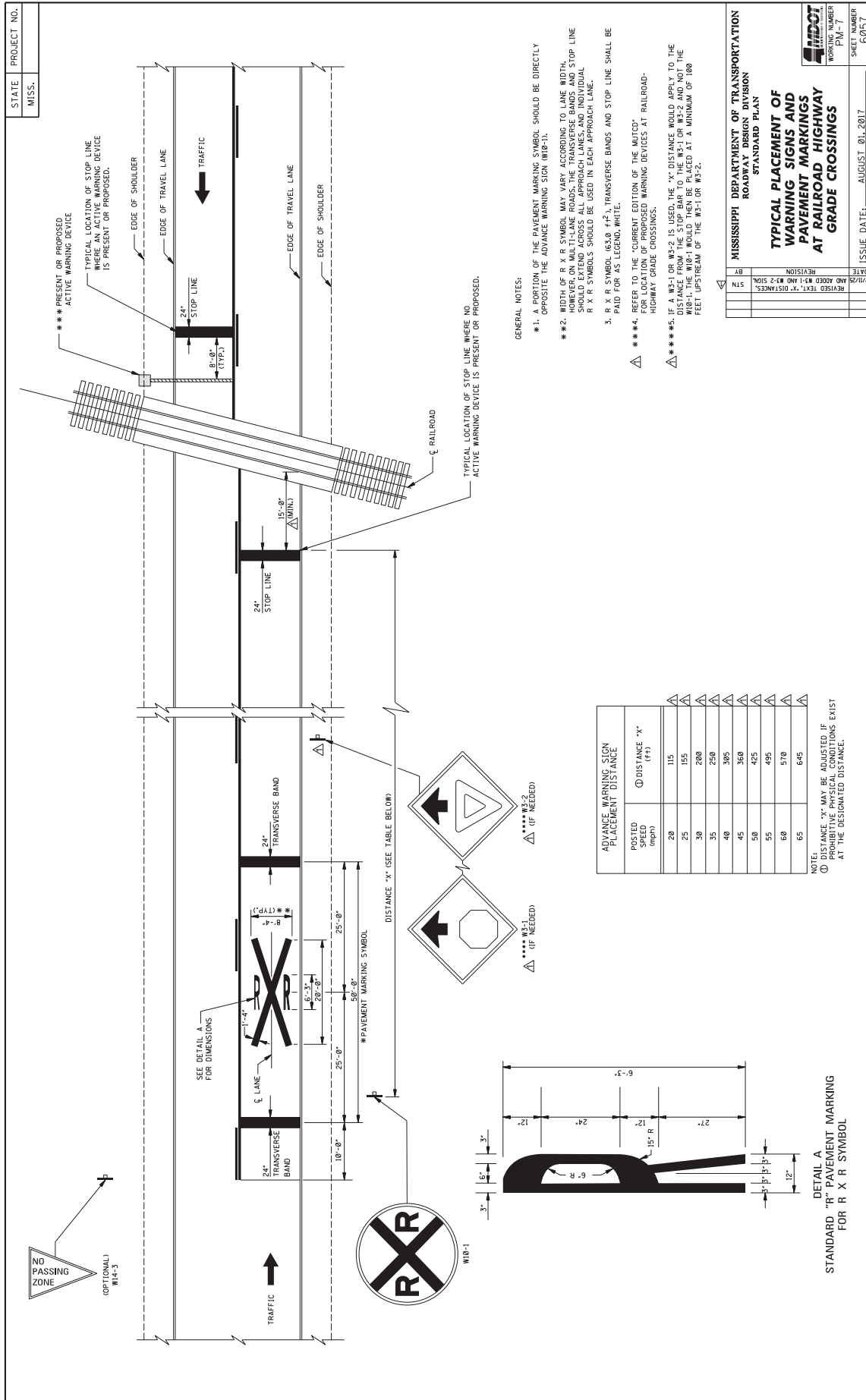
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN


**PAVEMENT MARKING
LEGEND DETAILS**

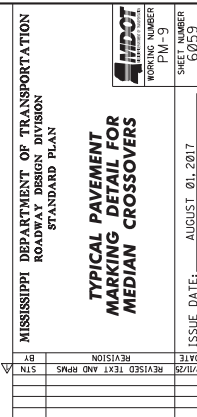
ISSUE DATE: AUGUST 01, 2017

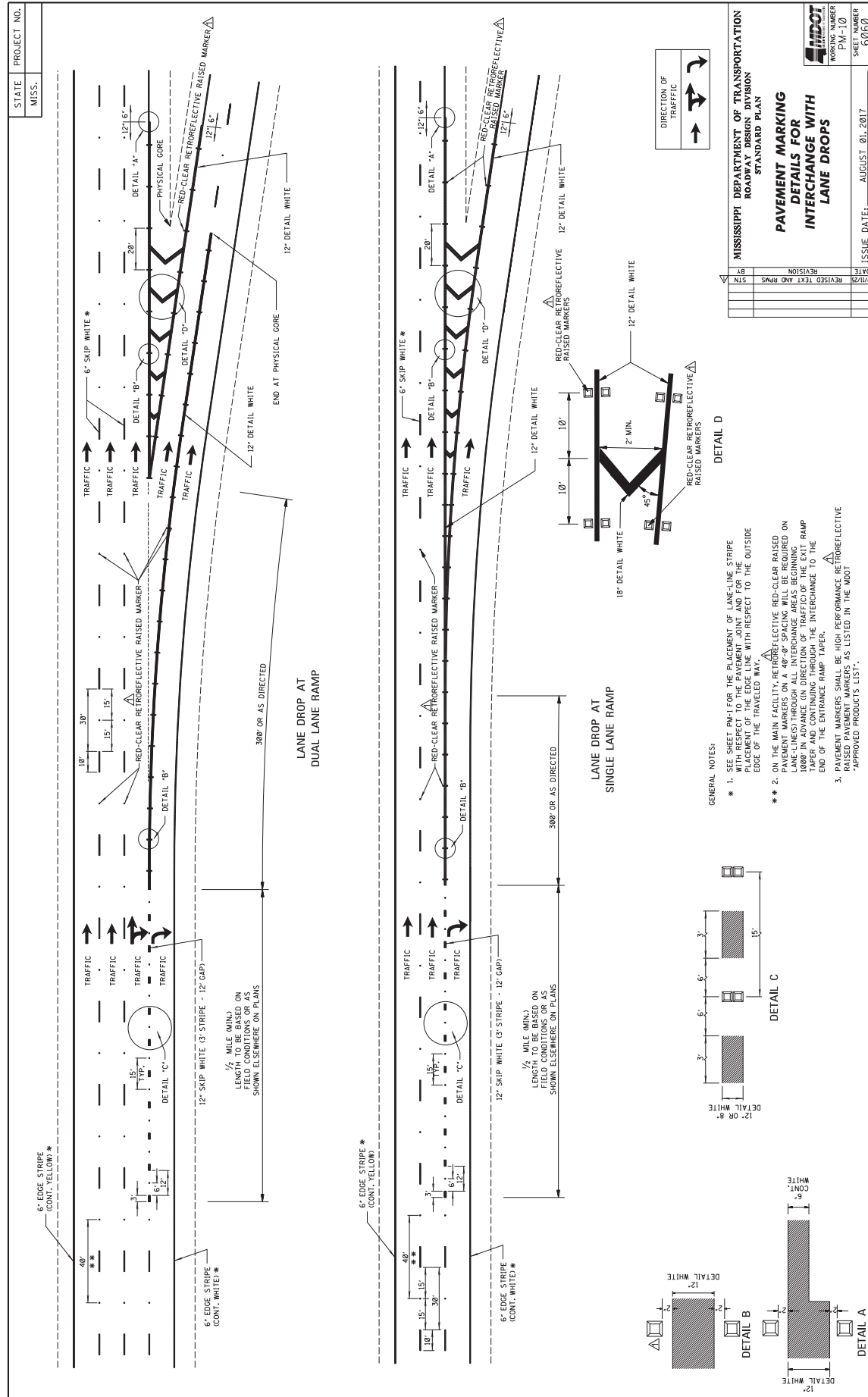
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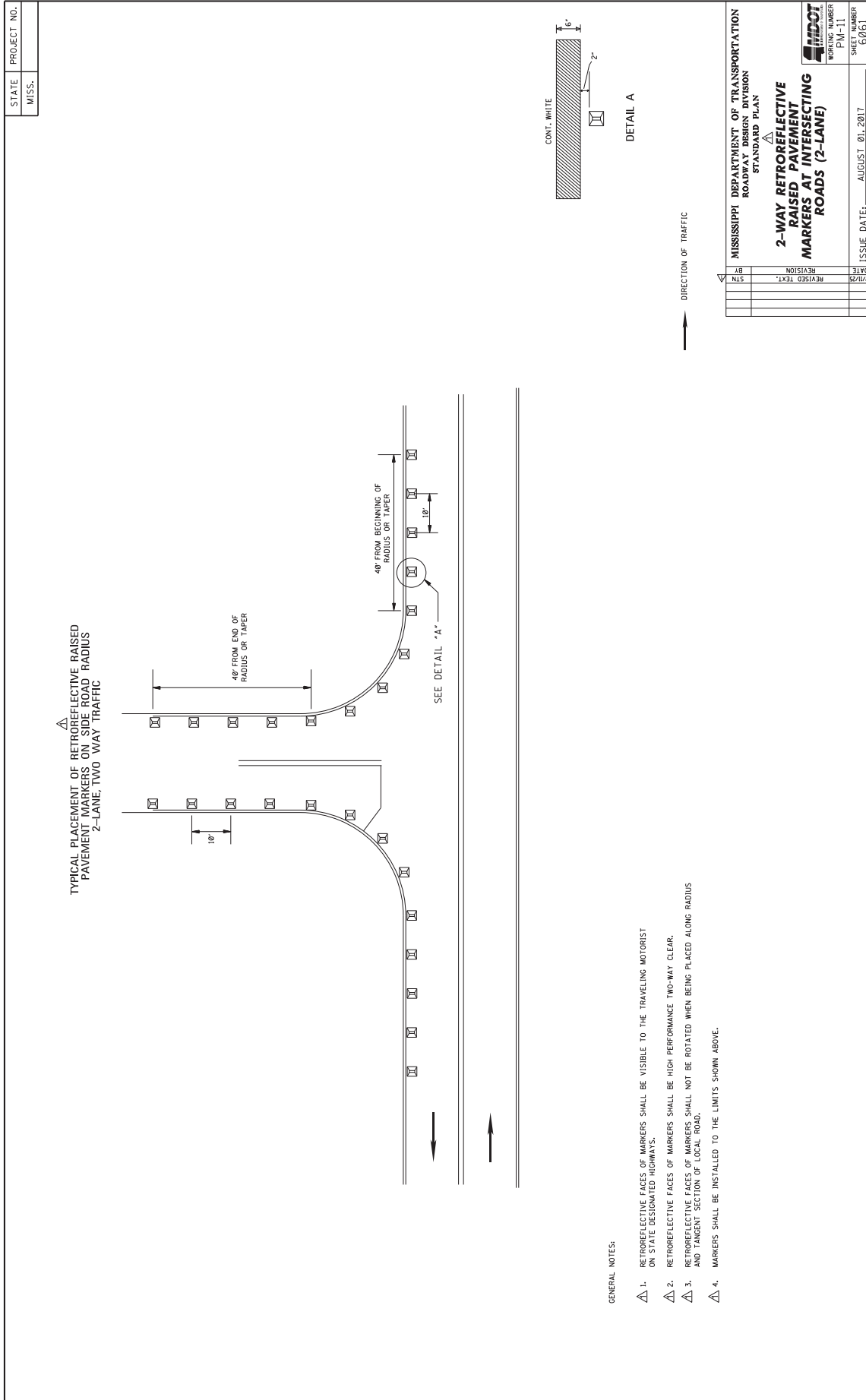
SHEET NUMBER: 6036

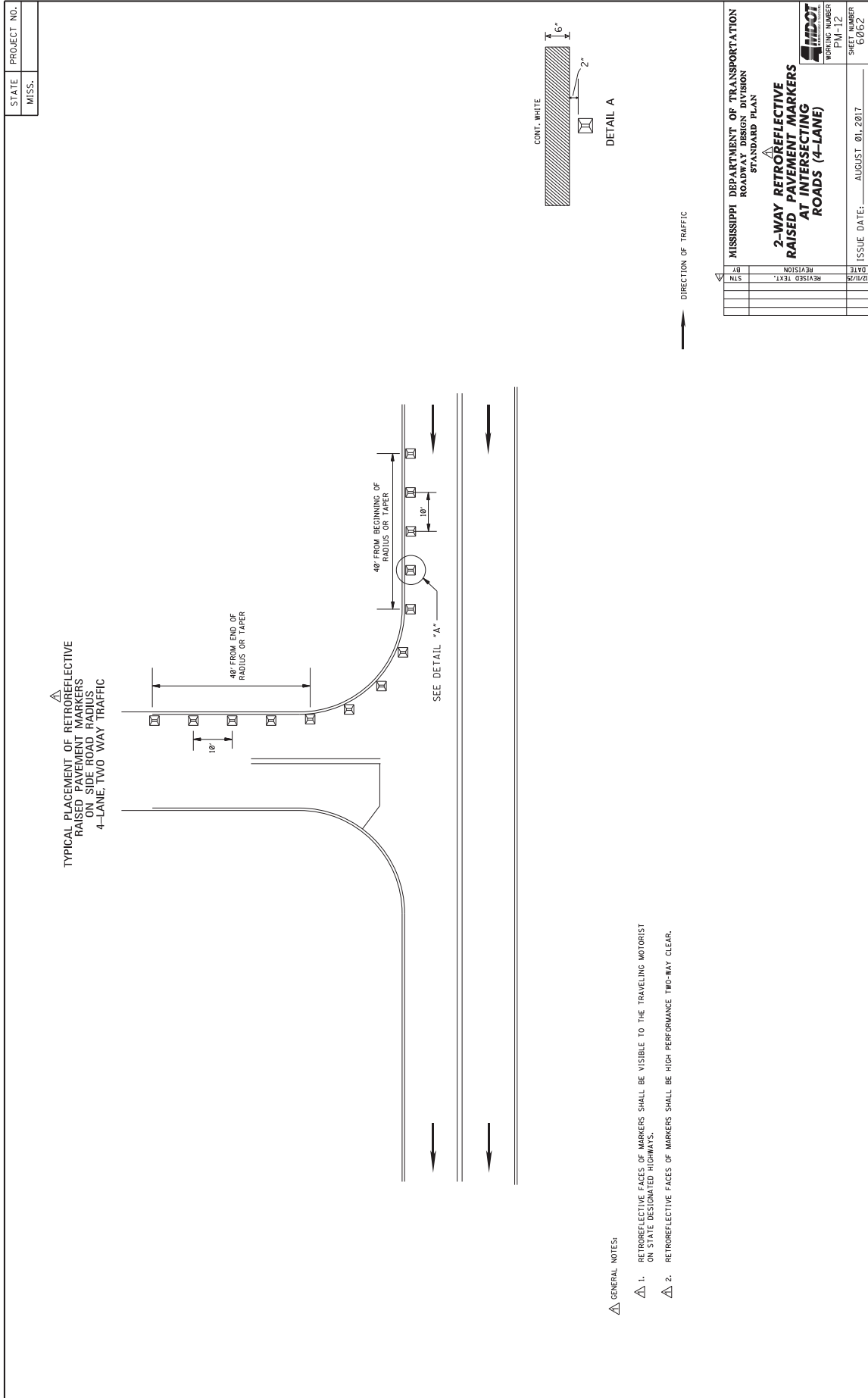


MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	 WORKING IN PART PLAN-B 6-058		ISSUE DATE: AUGUST 01, 2017
BR	REVISION REVISED TEXT AND DIMS		DATE
✓	4-LANE TO 2-LANE TRANSITION AT INTERCHANGE		11/17/07

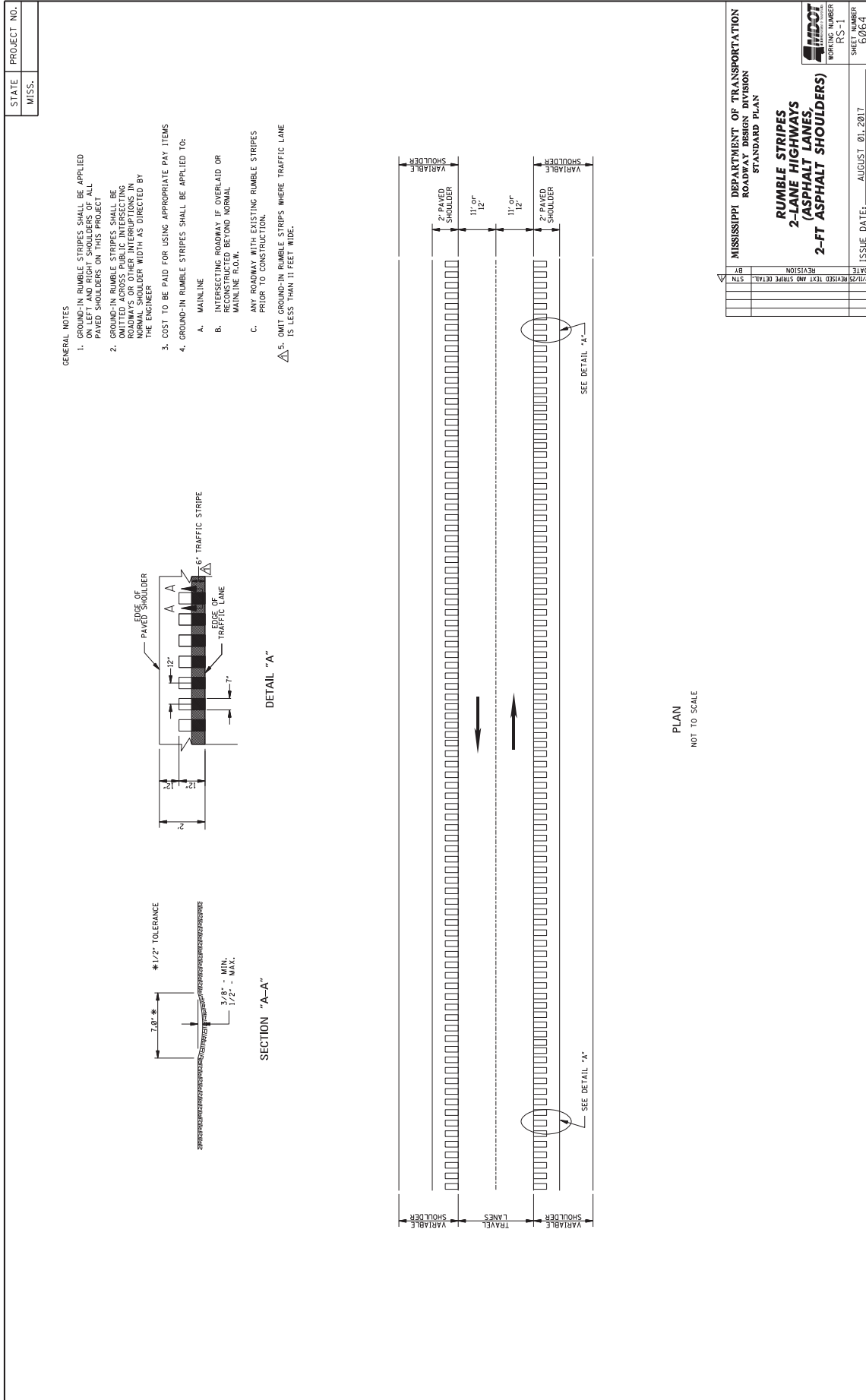


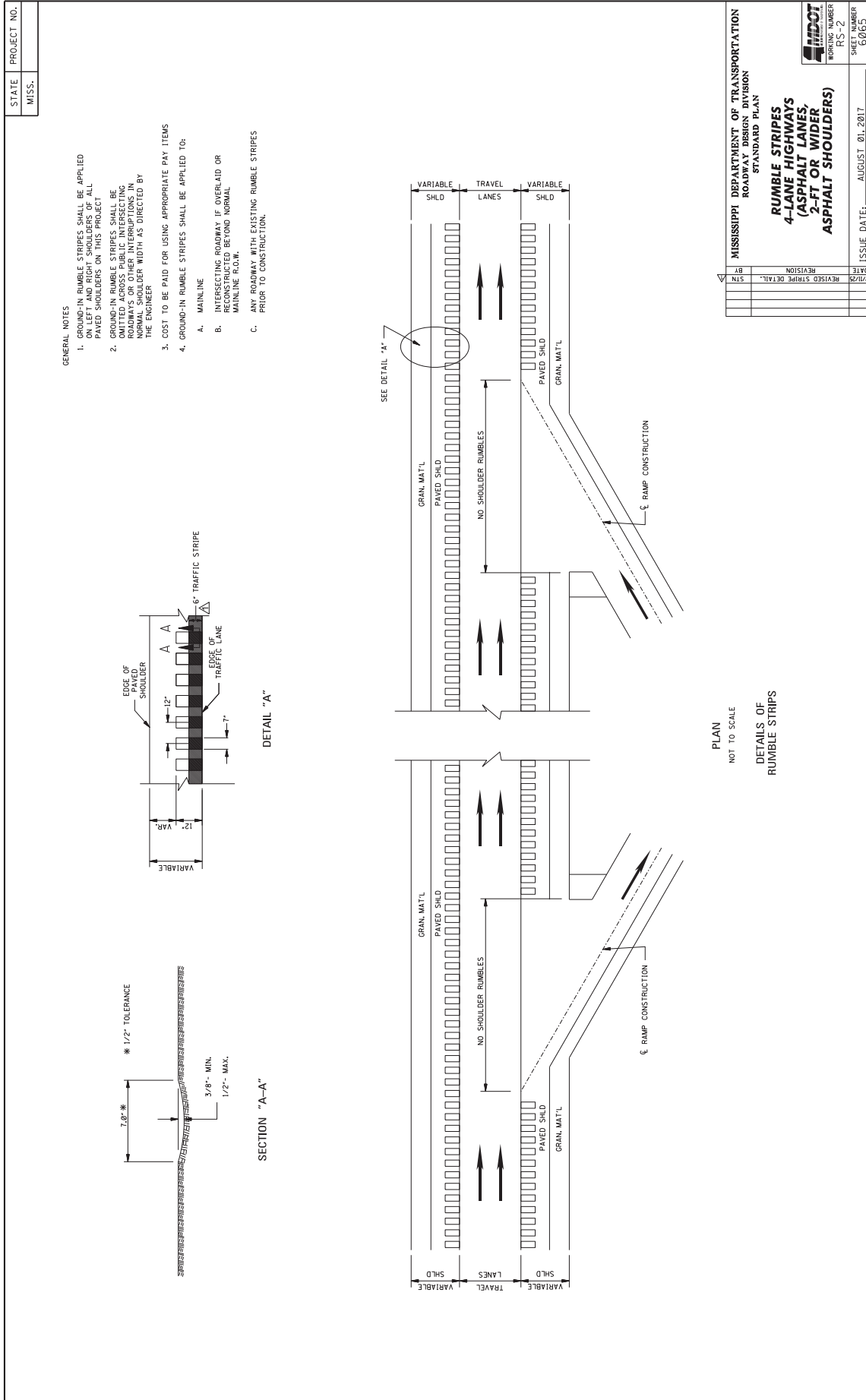


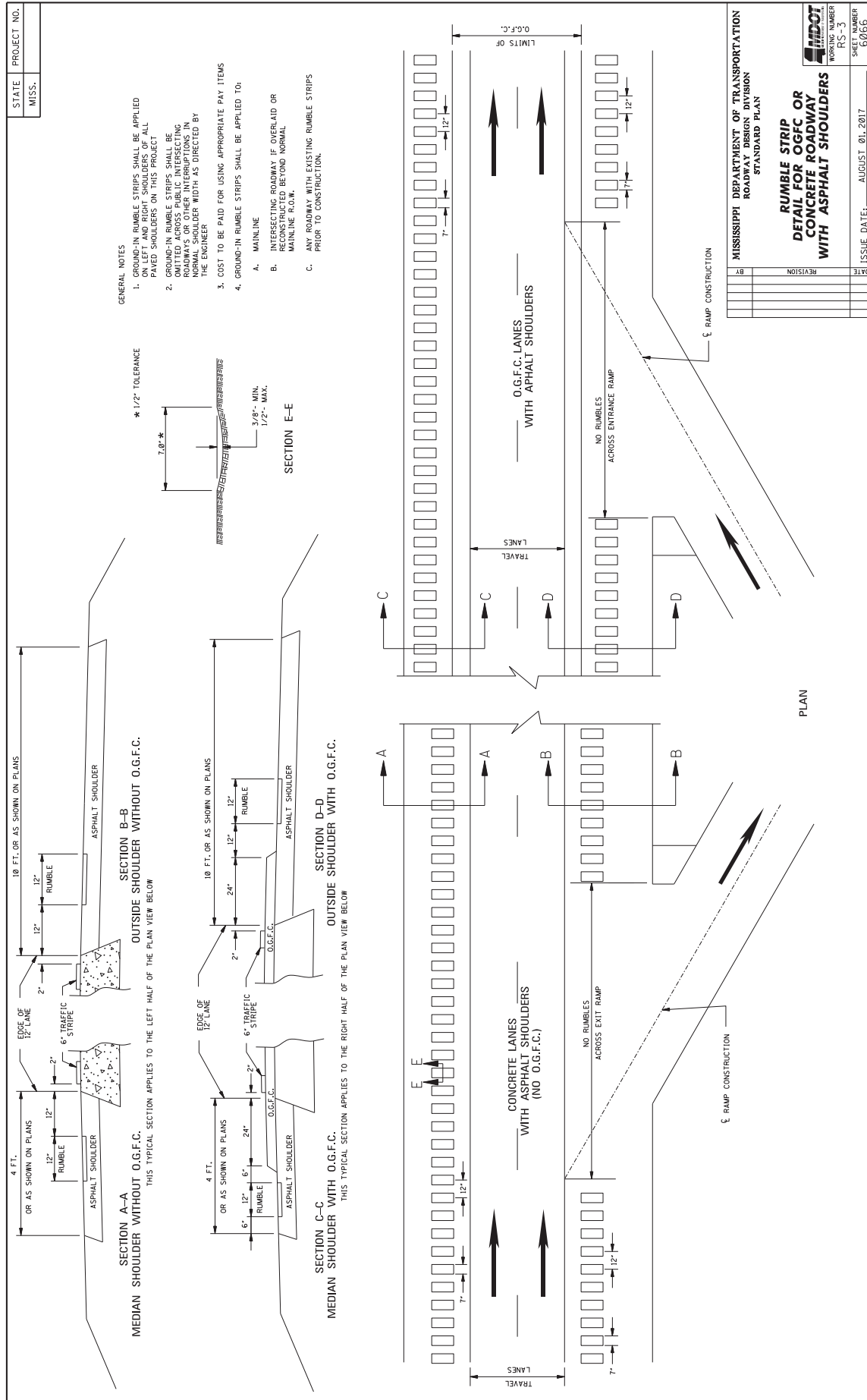


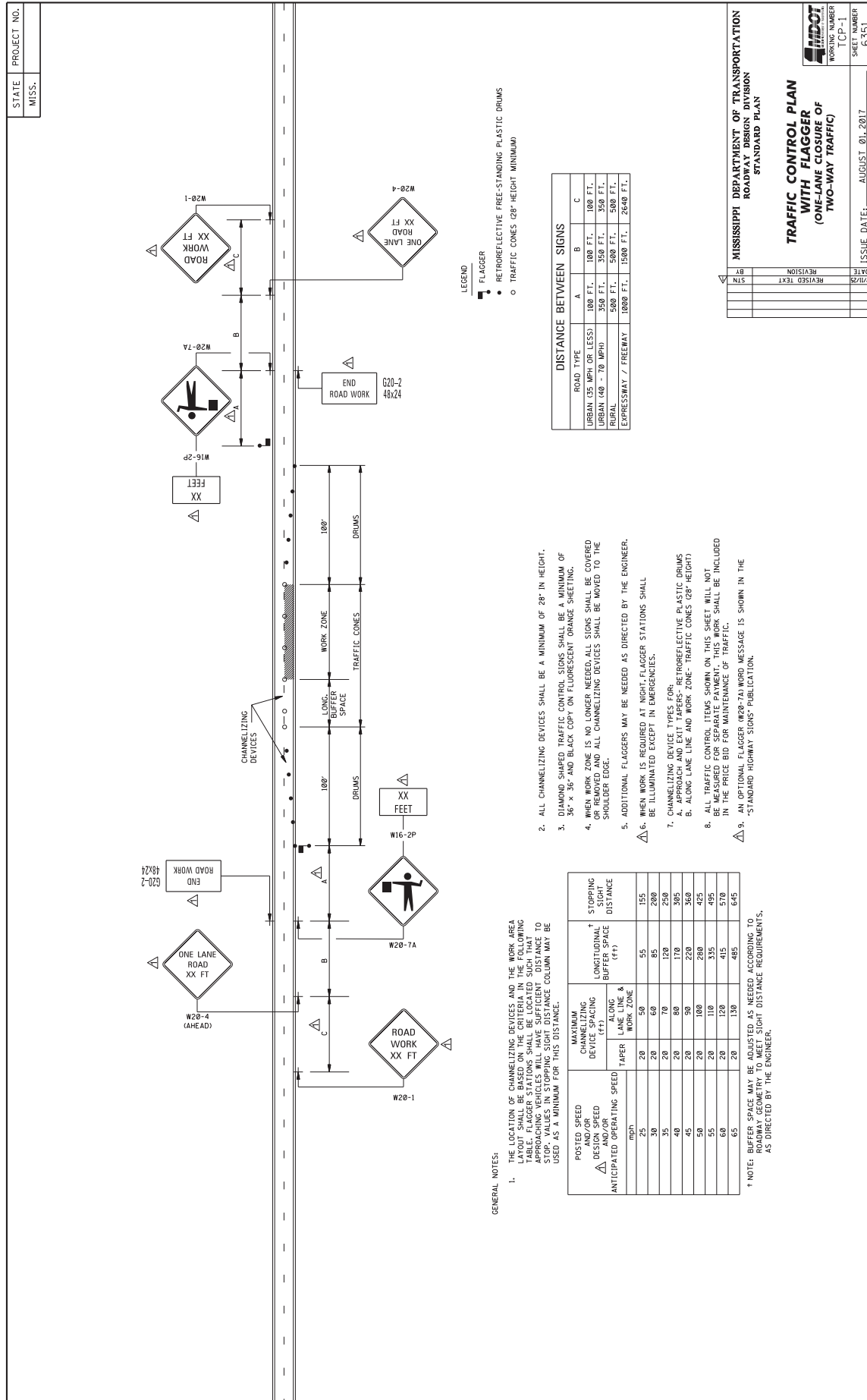


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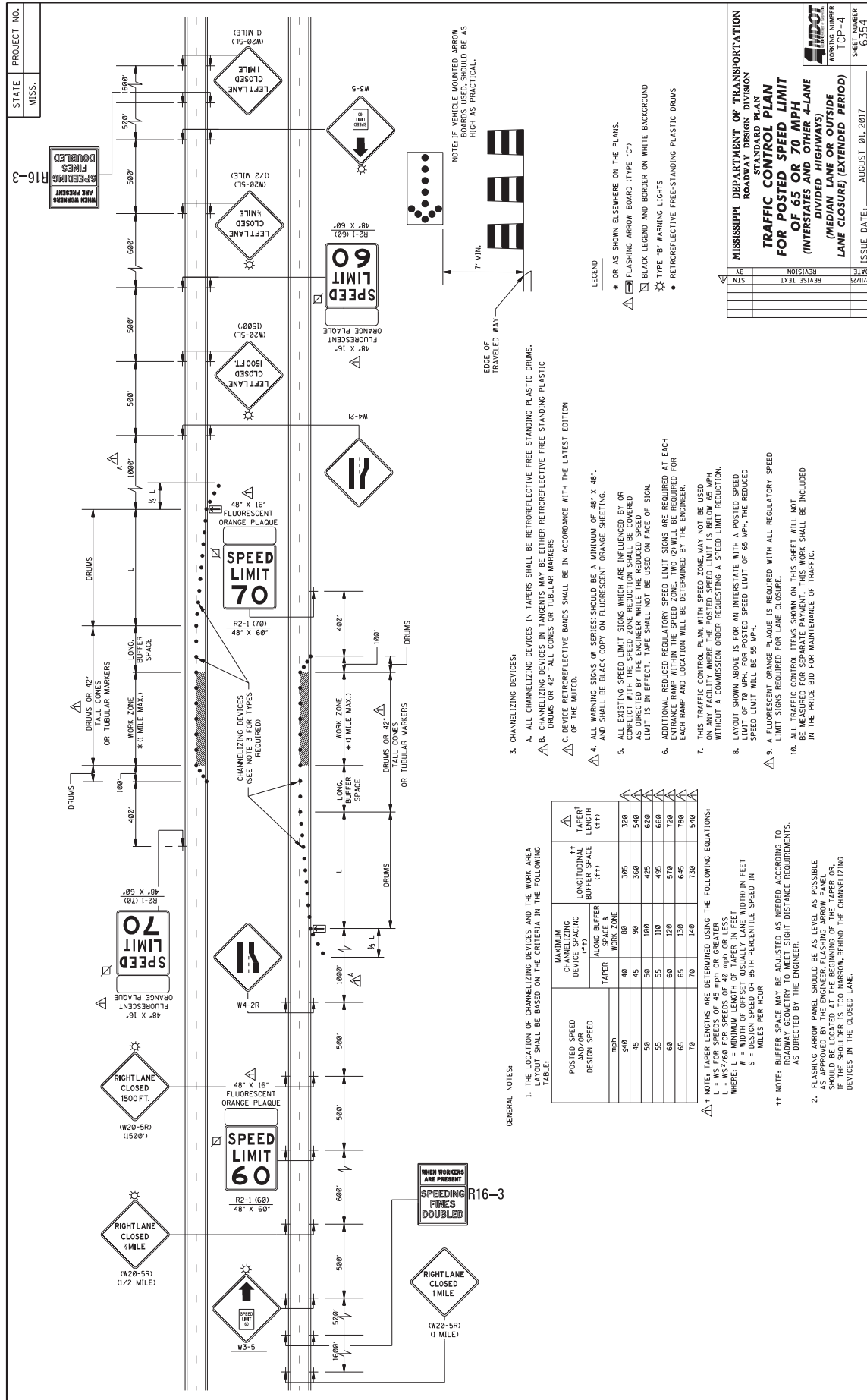












GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA TAPER SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

POSTED SPEED AND/OR DESIGN SPEED mph	MAXIMUM CHANNELIZING DEVICES SPACING		LONGITUDINAL BUFFER SPACE (FT)	TAPER LENGTH (FT)
	ALONG BUFFER SPACE & WORK ZONE	TAPER		
40	40	80	305	320
45	45	90	360	340
50	50	100	420	360
55	55	110	480	380
60	60	120	540	400
65	65	130	600	420
70	70	140	660	440

[†] NOTE: TAPER LENGTHS ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 $L = WS^2/60$ FOR SPEEDS OF 45 MPH OR GREATER
 $L = WS^2/60$ FOR SPEEDS OF 40 MPH OR LESS
 WHERE: L = MINIMUM LENGTH OF TAPER IN FEET
 W = DESIGN SPEED OR 85TH PERCENTILE SPEED IN FEET PER HOUR
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

^{††} NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

2. FLASHING ARROW PANEL SHOULD BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR, IF PRACTICABLE, AT THE BEGINNING OF THE CHANNELIZING DEVICES IN THE CLOSED LANE.

3. CHANNELIZING DEVICES:

- A. ALL CHANNELIZING DEVICES IN TAPERS SHALL BE RETROREFLECTIVE FREE STANDING PLASTIC DRUMS.
- B. CHANNELIZING DEVICES IN TANGENTS MAY BE EITHER RETROREFLECTIVE FREE STANDING PLASTIC DRUMS OR 42" TALL CONES OR TUBULAR MARKERS
- C. DEVICE RETROREFLECTIVE BANDS SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE MUTCD.

4. ALL WARNING SIGNS (W SERIES) SHOULD BE A MINIMUM OF 48" X 48".

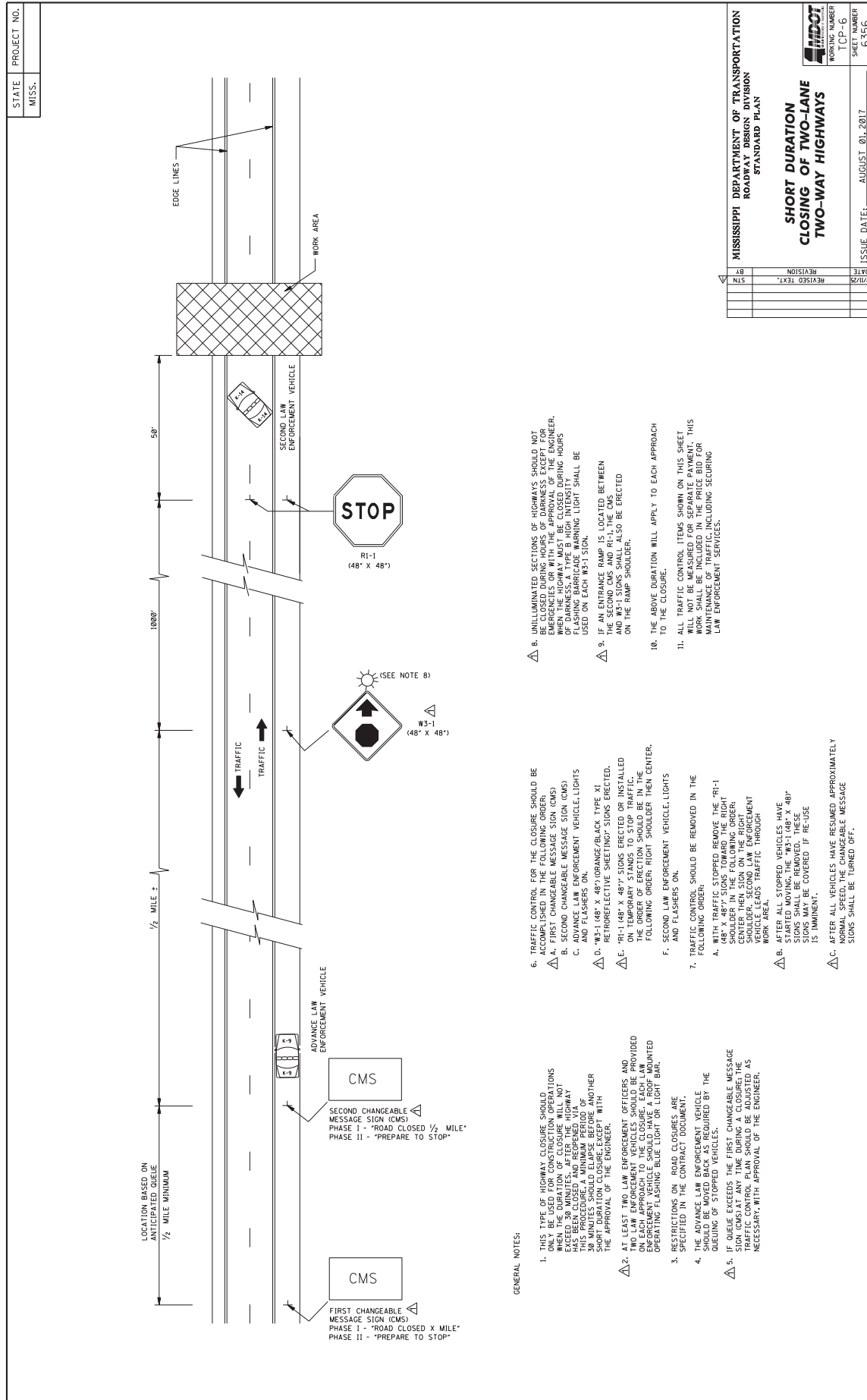
5. ALL EXISTING SPEED LIMIT SIGNS WHICH ARE INFLUENCED BY OR COME IN CONTACT WITH THE SPEED ZONE REDUCTION SHALL BE COVERED AS DIRECTED BY THE ENGINEER WHILE THE REDUCED SPEED LIMIT IS IN EFFECT. TAPE SHALL NOT BE USED ON FACE OF SIGN.
6. ADDITIONAL REDUCED REGULATORY SPEED LIMIT SIGNS ARE REQUIRED AT EACH ENTRANCE RAMP WITHIN THE SPEED ZONE. TWO (2) WILL BE REQUIRED FOR EACH RAMP AND LOCATION WILL BE DETERMINED BY THE ENGINEER.
7. THIS TRAFFIC CONTROL PLAN WITH SPEED ZONE MAY NOT BE USED ON ANY FACILITY WHERE THE POSTED SPEED LIMIT IS BELOW 65 MPH. WITHOUT A COMMISSION ORDER REQUESTING A SPEED LIMIT REDUCTION.
8. LAYOUT SHOWN ABOVE IS FOR AN INTERSTATE WITH A POSTED SPEED LIMIT OF 70 MPH. THE REDUCED SPEED LIMIT WILL BE 65 MPH.
9. A FLUORESCENT ORANGE PLAQUE IS REQUIRED WITH ALL REGULATORY SPEED LIMIT SIGNS REQUIRED FOR LANE CLOSURE.
10. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

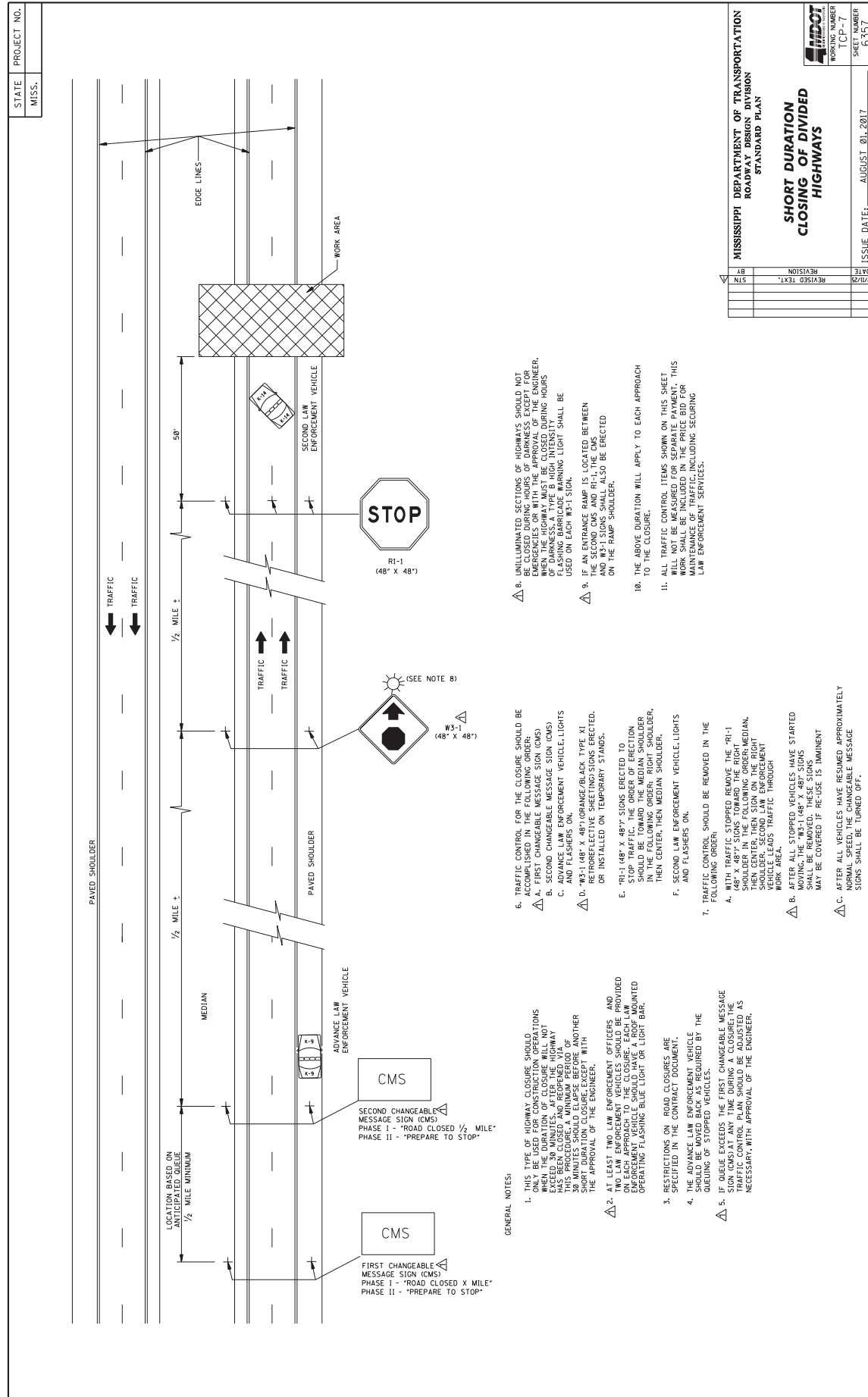
LEGEND

- * OR AS SHOWN ELSEWHERE ON THE PLANS.
- FLASHING ARROW BOARD (TYPE "C")
- BLACK LEGEND AND BORDER ON WHITE BACKGROUND
- TYPE "B" MAPPING LIGHTS
- RETROREFLECTIVE FREE-STANDING PLASTIC DRUMS

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
STANDARD PLAN
TRAFFIC CONTROL PLAN
FOR POSTED SPEED LIMIT
OF 65 OR 70 MPH
(INTERSTATES AND OTHER 4-LANE
DIVIDED HIGHWAYS)
(MEDIAN LANE OR OUTSIDE
LANE CLOSURE) (EXTENDED PERIOD)

DATE: 08/20/17
 REVISION: 01/17/17
 SHEET NUMBER: 6534
 ISSUE DATE: AUGUST 01, 2017





WING BARRICADES

1. WING BARRICADES ARE TYPE III BARRICADES ERRECTED ON THE SHOULDER OF A TWO LANE ROADWAY. THE BARRICADES ARE USED TO CLOSE A NARROWING OR RESTRICTED ROADWAY. WING BARRICADES MAY BE USED AS A MOUNTING FOR THE ADVANCE WARNING SIGNS OR FLASHERS.

2. WING BARRICADES SHOULD BE USED:

- IN ADVANCE OF A CONSTRUCTION PROJECT EVEN WHEN NO CONSTRUCTION IS BEING PERFORMED.
- IN ADVANCE OF ALL BRIDGE OR CULVERT WIDENING OPERATIONS.

BARRICADE CHARACTERISTICS

	I	II	III
WIDTH OF RAIL **	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.
LENGTH OF RAIL **	24" MIN. / 36" MAX.	24" MIN. / 36" MAX.	48" MIN. / 96" MAX.
WIDTH OF STRIPE *	6"	6"	6"
HEIGHT	36" MIN.	36" MIN.	60" MIN.
NUMBER OF RETROREFLECTORIZED RAIL FACES	2 ONE EACH DIRECTION	4 (TWO EACH DIRECTION)	3 IF FACING TRAFFIC IN ONE DIRECTION 6 IF FACING TRAFFIC IN TWO DIRECTIONS

* 1. FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.

** 2. BARRICADES INTENDED FOR USE ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH SPEED ROADWAYS, SHALL HAVE A MINIMUM OF 270 IN. OF REFLECTIVE AREA FACING TRAFFIC.

STANDARD BARRICADES

1. THE RETROREFLECTIVE MARKING FOR BARRICADE RAILS SHALL BE ORANGE AND WHITE SLOPING DOWNWARD AT AN ANGLE OF 30° TO THE HORIZONTAL, STARTING FROM THE TOP PASS.

2. RAIL STRIPE SHOULD BE 6 INCHES EXCEPT THAT 4-INCH WIDE STRIPES MAY BE USED IF RAIL LENGTHS ARE LESS THAN 36 INCHES.

3. DO NOT PLACE SANDBAGS OR OTHER DEVICES ON BARRICADE RAILS TO PROVIDE MASS SANDBAGS; HOWEVER, THEY MAY BE PLACED AS BALLAST TO THE LOWER PARTS OF THE FRAME OR THE STAYS TO BARRICADES.

4. FOR ADDITIONAL INFORMATION OR DETAILS, SEE MUTCD, LATEST EDITION.

5. BARRICADES ARE CLASSIFIED BY FHWA AS CATEGORY II WORK ZONE DEVICES.

6. A LIST OF CRASHWORTHY BARRICADES AND OTHER CRASHWORTHY DEVICES: <http://highways.dot.gov/safety/fhwa/reduce-crash-severity>

7. WHERE ROAD USERS INCLUDE PEDESTRIANS, THE PROVISION OF SUPPLEMENTAL AUDIBLE INFORMATION OR DETECTABLE BARRIERS OR BARRICADES SHOULD BE PROVIDED FOR PEOPLE WITH VISION DISABILITIES.

8. BARRICADE RAIL SUPPORTS SHOULD NOT PROJECT INTO PEDESTRIAN CIRCULATION ROUTES MORE THAN 4 INCHES FOR THE SUPPORTS LOCATED BETWEEN 27 INCHES TO 88 INCHES ABOVE THE EXISTING SURFACE.

RETRORFLECTIVE CHEVRON ALIGNMENT SIGN DETAIL (WT-8)

1. A CHEVRON SIGN CONSISTS OF A BLACK CHEVRON TYPE MARKING ON AN ORANGE BACKGROUND AND SHALL POINT IN THE DIRECTION OF TRAFFIC FLOW.

2. THE CHEVRON SIGN SHALL BE MOUNTED ON CRASHWORTHY SUPPORT.

3. CHEVRON SIGNS MAY BE USED TO SUPPLEMENT OTHER STANDARD DEVICES WHERE ONE OR MORE LANES ARE CLOSED FOR CONSTRUCTION OR MAINTENANCE. THEY SHOULD BE PLACED APPROXIMATELY 2'-0" BEHIND THE LANE TRANSITION STRIPE.

4. CHEVRON SIGNS SHALL BE INSTALLED AT A MINIMUM HEIGHT OF 4 FEET MEASURED VERTICALLY FROM THE BOTTOM OF THE SIGN TO THE ELEVATION OF THE NEAR SIDE OF THE TRAVELWAY.

BARRICADE CLOSING A ROAD

TYPE 3 OBJECT MARKER (OM3-R)

1. TYPE 3 OBJECT MARKERS SHALL BE USED AT ALL EXPOSED BRIDGE ABUTMENTS AND AT OTHER LOCATIONS AS DEEMED NECESSARY BY THE ENGINEER.

2. THE OM3-R IS SHOWN. THE OM3-L IS SIMILAR EXCEPT THE STRIPES SLOPE DOWNWARD FROM THE UPPER LEFT SIDE TO THE LOWER RIGHT SIDE AND SHALL BE PLACED ON THE LEFT SIDE OF THE OBJECT.

3. USE OF OBJECT MARKERS SHALL BE IN LINE WITH THE INNER YELLOW SLOPING DOWNWARD TOWARD THE SIDE ON WHICH TRAFFIC IS THE PASS.

WING BARRICADES

1. WING BARRICADES ARE TYPE III BARRICADES ERRECTED ON THE SHOULDER OF A TWO LANE ROADWAY. THE BARRICADES ARE USED TO CLOSE A NARROWING OR RESTRICTED ROADWAY. WING BARRICADES MAY BE USED AS A MOUNTING FOR THE ADVANCE WARNING SIGNS OR FLASHERS.

2. WING BARRICADES SHOULD BE USED:

- IN ADVANCE OF A CONSTRUCTION PROJECT EVEN WHEN NO CONSTRUCTION IS BEING PERFORMED.
- IN ADVANCE OF ALL BRIDGE OR CULVERT WIDENING OPERATIONS.

BARRICADE CLOSING A ROAD

WING BARRICADES

1. WING BARRICADES ARE TYPE III BARRICADES ERRECTED ON THE SHOULDER OF A TWO LANE ROADWAY. THE BARRICADES ARE USED TO CLOSE A NARROWING OR RESTRICTED ROADWAY. WING BARRICADES MAY BE USED AS A MOUNTING FOR THE ADVANCE WARNING SIGNS OR FLASHERS.

2. WING BARRICADES SHOULD BE USED:

- IN ADVANCE OF A CONSTRUCTION PROJECT EVEN WHEN NO CONSTRUCTION IS BEING PERFORMED.
- IN ADVANCE OF ALL BRIDGE OR CULVERT WIDENING OPERATIONS.

BARRICADE CLOSING A ROAD

STATE PROJECT NO. MISS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

HIGHWAY SIGN AND BARRICADE DETAILS
FOR CONSTRUCTION PROJECTS

ISSUED: AUGUST 01, 2017

STATE | PROJECT NO.
MISS. |

MOBILE OPERATIONS ON MULTILANE ROAD

TRUCK-MOUNTED FLASHING ARROW BOARD

WORK VEHICLE

SHADOW VEHICLE 1

SHADOW VEHICLE 2

TRUCK-MOUNTED ATTENUATOR

NOTE: USE OF CHANGEABLE MESSAGE SIGN IS OPTIONAL

LEFT LANE CLOSED AHEAD

MOBILE OPERATIONS ON TWO-LANE ROAD

WORK VEHICLE

SHADOW VEHICLE

TRUCK-MOUNTED FLASHING ARROW PANEL

TRUCK-MOUNTED ATTENUATOR

USE SIGN SHADE AND LEGEND APPROPRIATE TO TYPE OF WORK

OPTIONAL SIGN FOR SHORT DURATION OPERATION

MOBILE OPERATIONS ON TWO-LANE ROAD

WORK VEHICLE

SHADOW VEHICLE

TRUCK-MOUNTED FLASHING ARROW PANEL

TRUCK-MOUNTED ATTENUATOR

USE SIGN SHADE AND LEGEND APPROPRIATE TO TYPE OF WORK

OPTIONAL SIGN FOR SHORT DURATION OPERATION

NOTES FOR MULTILANE LANE OPERATION:

- SHADOW AND WORK VEHICLES SHALL DISPLAY HIGH-INTENSITY RED FLASHING LIGHTS. A TRUCK-MOUNTED FLASHING ARROW BOARD SHALL BE USED WHEN A FREEWAY LANE IS CLOSED. WHEN MORE THAN ONE FREEWAY LANE IS CLOSED, A SEPARATE ARROW BOARD SHALL BE USED FOR EACH CLOSED LANE.
- SHADOW VEHICLE 2 SHOULD BE EQUIPPED WITH AN ARROW BOARD. WHEN THE WORK VEHICLE OCCUPIES AN INTERIOR LANE (A LANE OTHER THAN THE FAR RIGHT OR FAR LEFT) OF A FREEWAY, SHADOW VEHICLE 2 SHOULD BE EQUIPPED WITH AN ARROW BOARD. WHEN THE WORK VEHICLE OCCUPIES THE FAR RIGHT OR FAR LEFT OF A FREEWAY, SHADOW VEHICLE 2 SHOULD BE EQUIPPED WITH AN ARROW BOARD. SHADOW VEHICLE 2 SHOULD BE PLACED ON THE RIGHT-HAND SHOULDER WITH A SIGN INDICATING THAT WORK IS TAKING PLACE IN THE INTERIOR LANE.
- SHADOW VEHICLE 1 SHOULD BE EQUIPPED WITH AN ARROW BOARD AND TRUCK-MOUNTED ATTENUATOR (TMA).
- SHADOW VEHICLE 2 SHOULD TRAVEL AT A VARYING DISTANCE FROM THE WORK OPERATION SO AS TO PROVIDE ADEQUATE SIGHT DISTANCE FOR TRAFFIC APPROACHING FROM THE REAR.
- THE SPACING BETWEEN THE WORK VEHICLES AND THE SHADOW VEHICLES, AND BETWEEN EACH SHADOW VEHICLE, SHOULD BE MINIMIZED TO DETERMINE ROAD USERS FROM DRIVING IN BETWEEN.
- ON HIGH-SPEED ROADWAYS, A THIRD SHADOW VEHICLE (NOT SHOWN) MAY BE USED WITH SHADOW VEHICLE 1 IN THE CLOSED LANE, SHADOW VEHICLE 2 STRADDLING THE EDGE LINE, AND SHADOW VEHICLE 3 ON THE SHOULDER.
- WHERE ADEQUATE SHOULDER WIDTH IS NOT AVAILABLE, SHADOW VEHICLE 3 MAY ALSO STRADDLE THE EDGE LINE.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

NOTES FOR TWO-LANE OPERATION:

- WHERE PRACTICAL AND WHEN NEEDED, THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS. IF THIS CAN NOT BE DONE FREQUENTLY AS AN ALTERNATIVE, A "DO NOT PASS" SIGN MAY BE PLACED ON THE REAR OF THE VEHICLE BLOCKING THE LANE.
- THE DISTANCE BETWEEN THE WORK AND SHADOW VEHICLES MAY VARY ACCORDING TO TERRAIN, PAINT DRYING TIME, AND OTHER FACTORS. SHADOW VEHICLES ARE USED TO WARN TRAFFIC OF THE OPERATION AHEAD. WHENEVER ADEQUATE SIGHT DISTANCE CANNOT BE MAINTAINED, SHADOW VEHICLES SHOULD BE USED TO MAINTAIN THE MINIMUM DISTANCE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. THE SHADOW VEHICLE SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- ADDITIONAL SHADOW VEHICLES TO WARN AND REDUCE THE SPEED OF ONCOMING OR OPPOSING TRAFFIC MAY BE USED. POLICE PATROL CARS MAY BE USED FOR THIS PURPOSE.
- A TRUCK-MOUNTED ATTENUATOR (TMA) SHOULD BE USED ON THE SHADOW VEHICLE AND MAY BE USED ON THE WORK VEHICLE.
- THE WORK VEHICLE SHALL BE EQUIPPED WITH BEACONS, AND THE SHADOW VEHICLES SHALL BE EQUIPPED WITH BEACONS. BEACONS SHOULD BE MOUNTED ON THE REAR, ADJACENT TO THE SIGN. SHADOW AND WORK VEHICLES SHALL DISPLAY FLASHING OR ROTATING BEACONS BOTH FORWARD AND TO THE REAR.
- VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBTURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
- ARROW BOARD TO BE USED IN CAUTION MODE.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

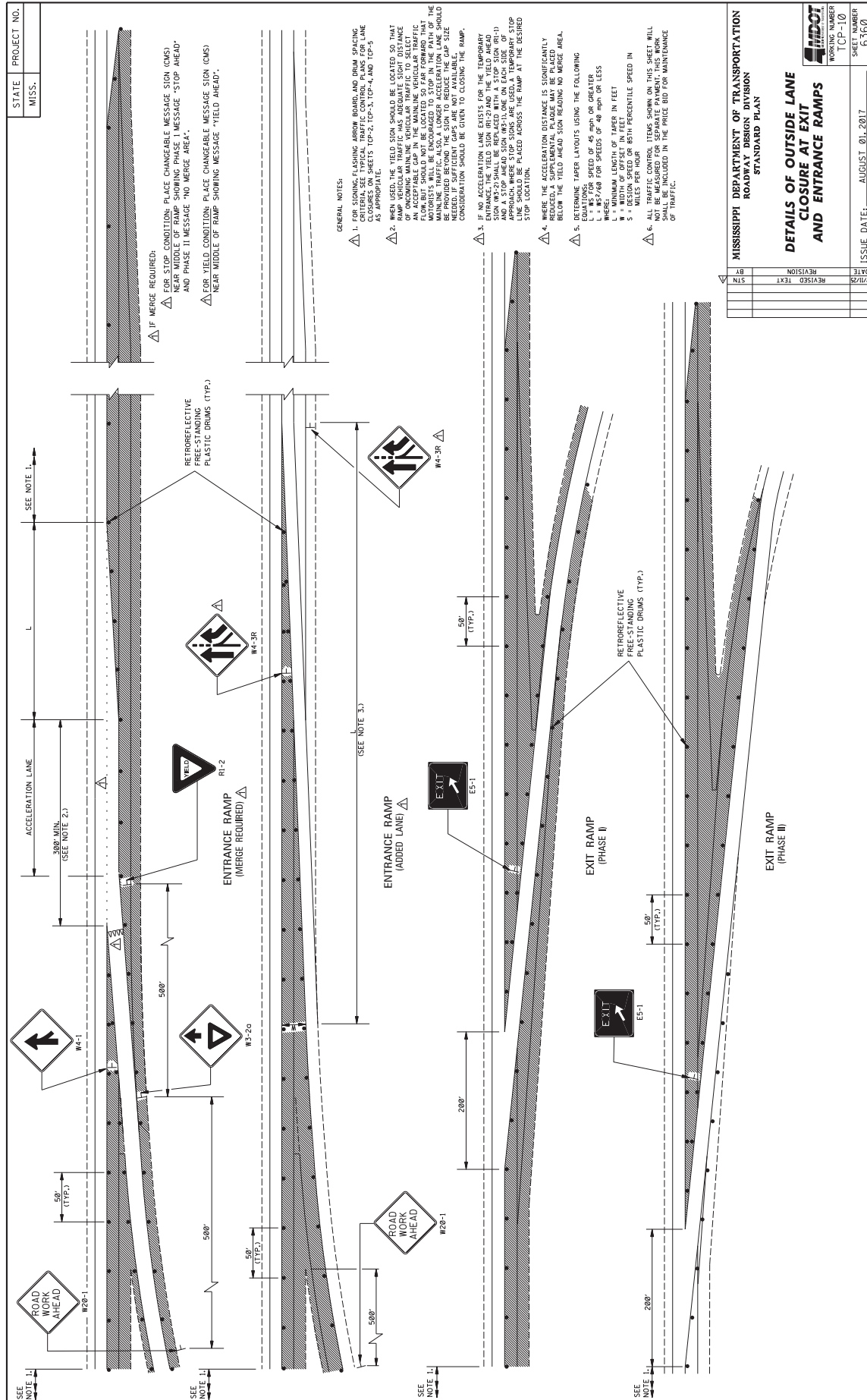
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

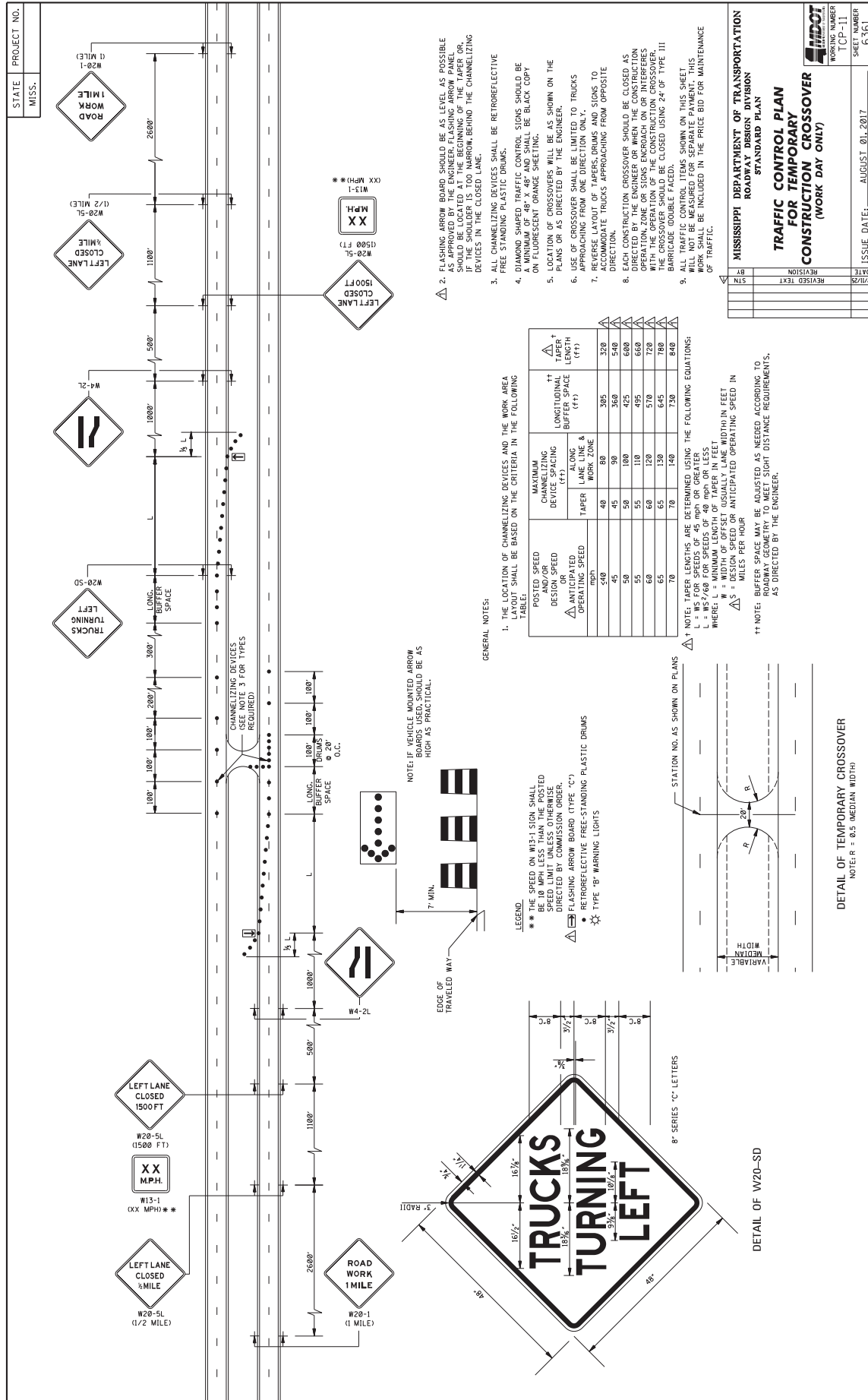
TRAFFIC CONTROL PLAN
MOBILE OPERATIONS
MULTILANE ROADS
TWO-LANE ROADS

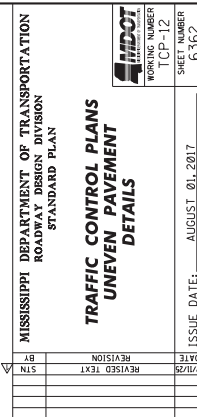
WORKING NUMBER
TCP-9

SHEET NUMBER
6359

ISSUE DATE: AUGUST 01, 2017







TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)

TWO-WAY TRAFFIC
(GRAVEL OR DIRT PAVEMENT)

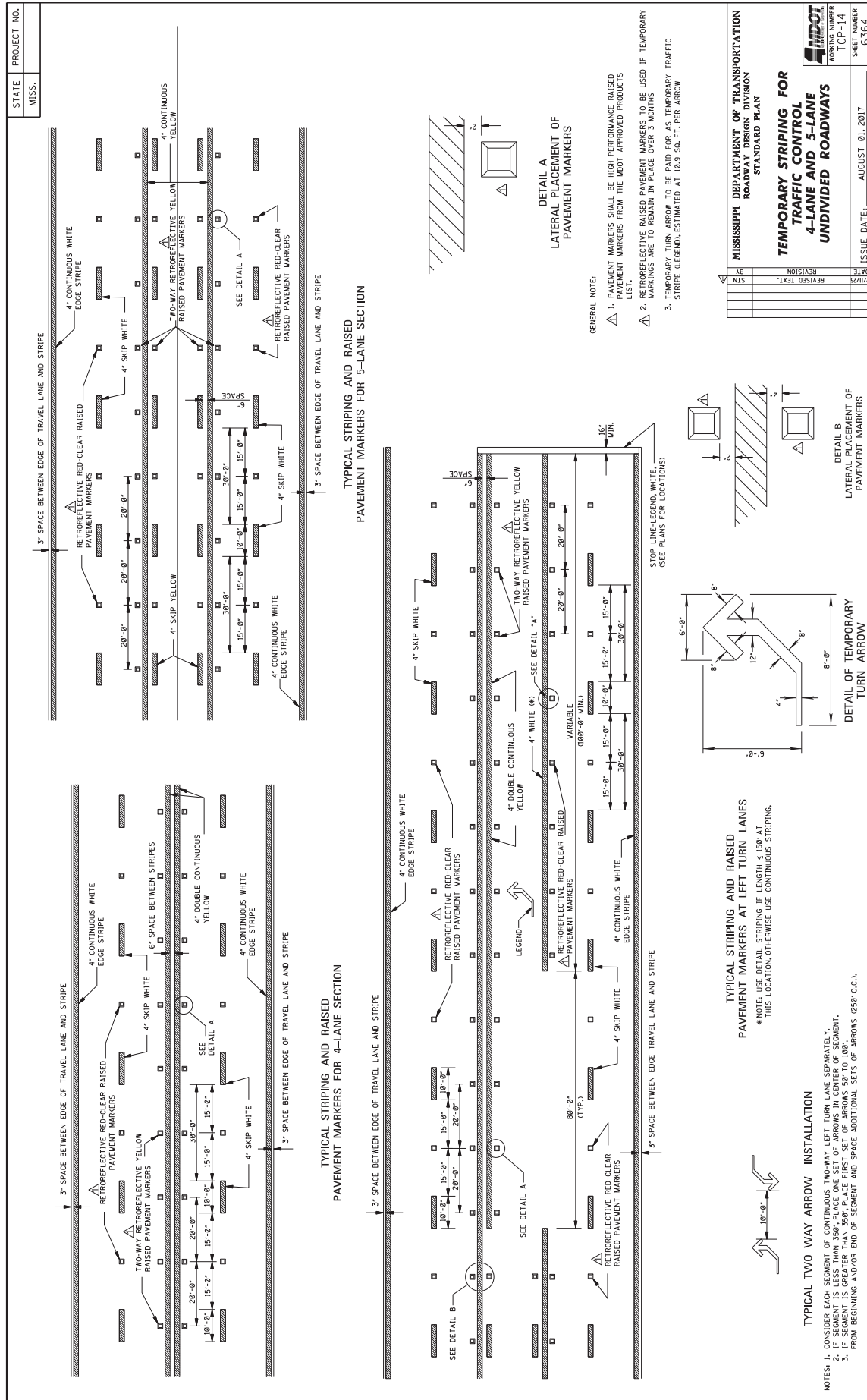
TANGENT SECTIONS	URBAN AREA	RURAL AREA
HORIZONTAL CURVES	40'-0"	80'-0"
INTERCHANGE LIMITS	40'-0"	140'-0"

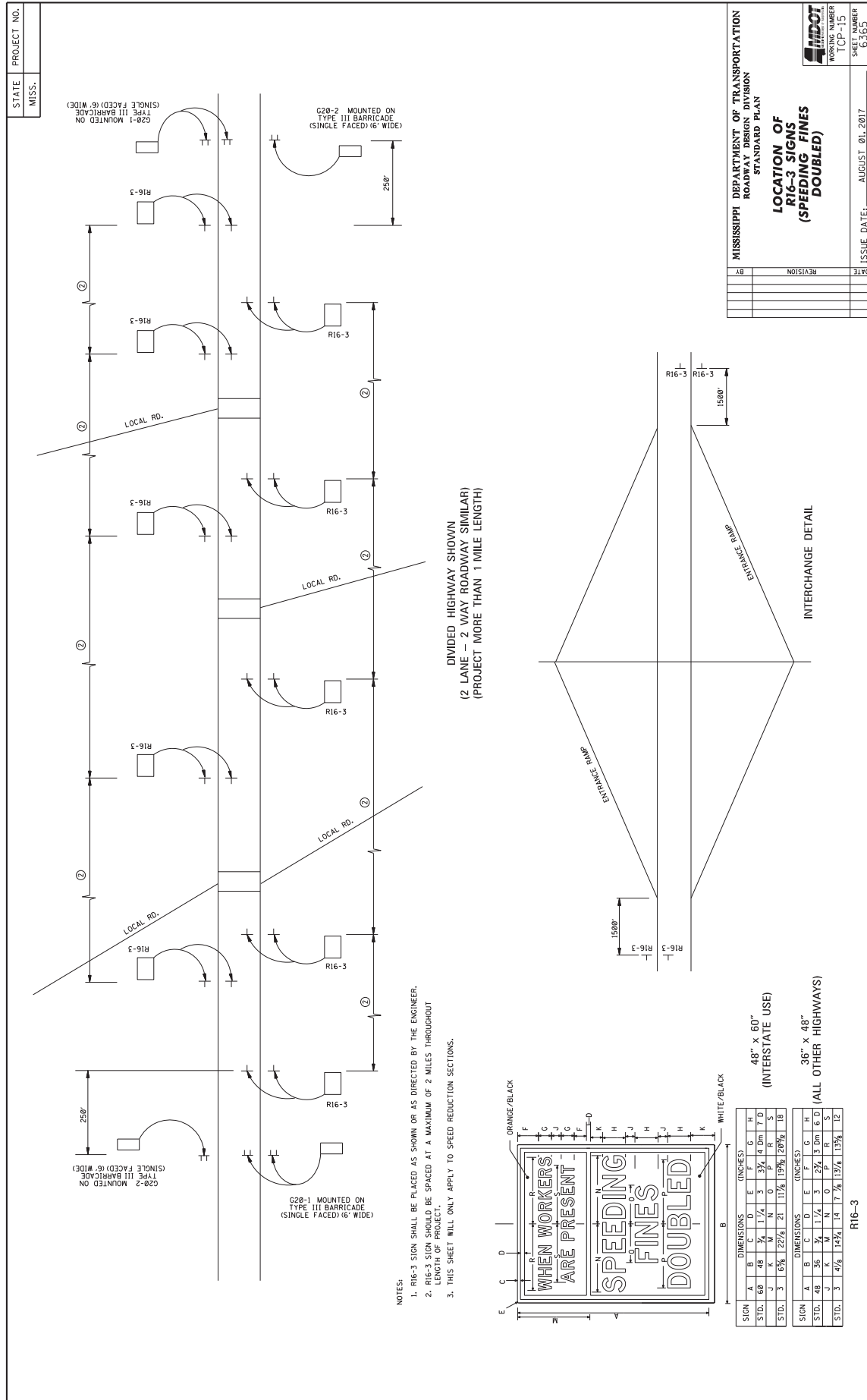
GENERAL NOTES:

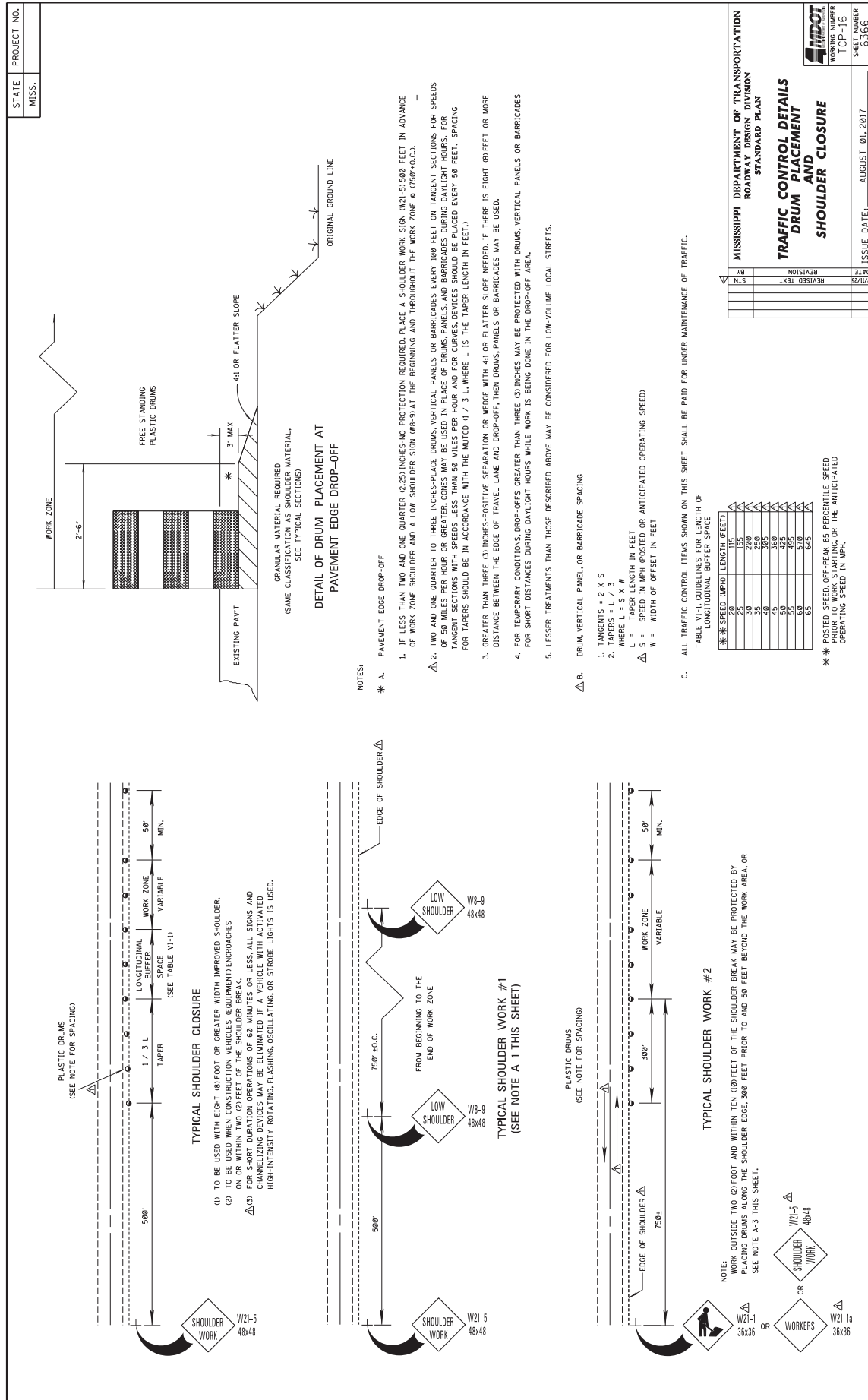
1. 3" UNLESS SHOWN ELSEWHERE ON THE PLANS.
2. EDGE STRIPES SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR TAPE AS INDICATED IN PAY ITEMS).
3. RETROREFLECTIVE RAISED PAVEMENT MARKERS TO BE USED IF TEMPORARY MARKINGS ARE TO REMAIN IN PLACE OVER 3 MONTHS.
4. SPACING OF RETROREFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

NOTE: ON THE MAIN FACILITY, RETROREFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON 4'-0" SPACING WILL BE REQUIRED ON LANE-LINES THROUGH ALL INTERCHANGE AREAS BEGINNING 100' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP AND 100' BEYOND THE INTERCHANGE. INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.

NOTE: PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RETROREFLECTIVE RAISED PAVEMENT MARKERS FROM THE MOST "APPROVED PRODUCTS LIST".







MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	TRAFFIC CONTROL DETAILS DRUM PLACEMENT SHOULDER CLOSURE
WORKING NUMBER TCP-16	SHEET NUMBER 0366
DATE 08/20/17	ISSUE DATE AUGUST 01, 2017