

## SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO.	<u>1</u>	DATED	<u>1/22/2026</u>	ADDENDUM NO.	_____	DATED	_____
ADDENDUM NO.	_____	DATED	_____	ADDENDUM NO.	_____	DATED	_____
ADDENDUM NO.	_____	DATED	_____	ADDENDUM NO.	_____	DATED	_____

Number	Description
1	Revised Table of Contents; Deleted Notice to Bidder No. 5750; Added Notice to Bidder Nos. 7518 & 7624; Amendment EBSx Download Required.

TOTAL ADDENDA: 1  
(Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE \_\_\_\_\_

\_\_\_\_\_  
Contractor

BY \_\_\_\_\_  
\_\_\_\_\_  
Signature

TITLE \_\_\_\_\_

ADDRESS \_\_\_\_\_

CITY, STATE, ZIP \_\_\_\_\_

PHONE \_\_\_\_\_

FAX \_\_\_\_\_

E-MAIL \_\_\_\_\_

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of \_\_\_\_\_ and the names, titles and business addresses of the executives are as follows:

President	Address
Secretary	Address
Treasurer	Address

The following is my (our) itemized proposal.

SP-0733-00(013)/ 110051301000

Webster County(ies)

Revised 01/26/2016

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**  
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**PROJECT: SP-0733-00(013)/110051301 - Webster**

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Progress Schedule

(REVISIONS TO THE ABOVE WILL BE INDICATED ON THE SECOND SHEET  
OF SECTION 905 AS ADDENDA)

01/21/2026 02:50 PM

## **MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 - NOTICE TO BIDDERS NO. 7518**

**CODE: (SP)**

**DATE: 01/21/2026**

**SUBJECT: Standard Drawing Clarification**

**PROJECT: SP-0733-00(013) / 110051301 – Webster County**

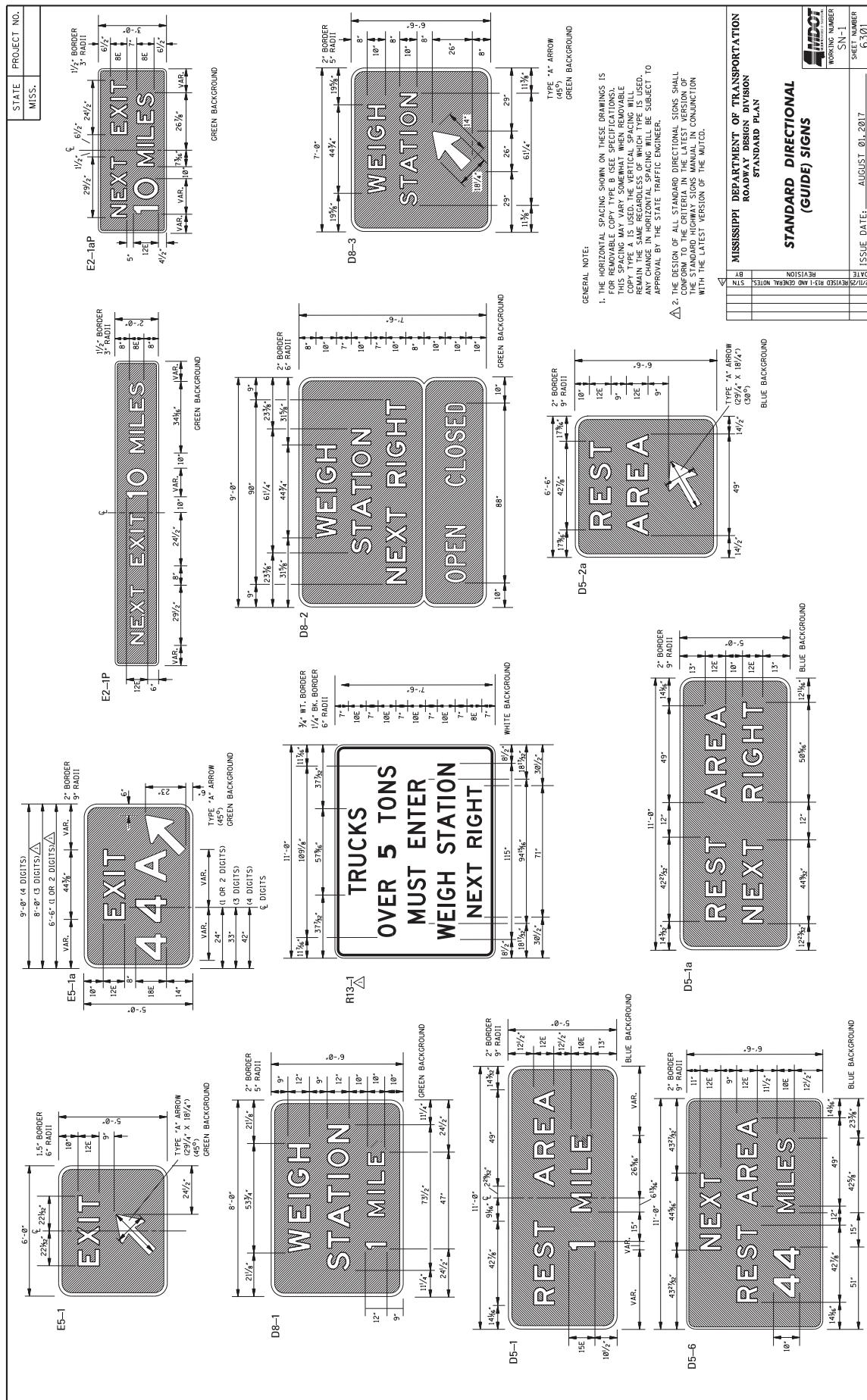
Bidders are hereby advised that in instances where the Contract Plans include previous versions of any drawings included in Notice to Bidders No. 7624, Standard Drawings, and the Supplement to Notice to Bidders No. 7624, the drawings included in Notice to Bidders No. 7624 and the Supplement to Notice to Bidders No. 7624 shall govern.

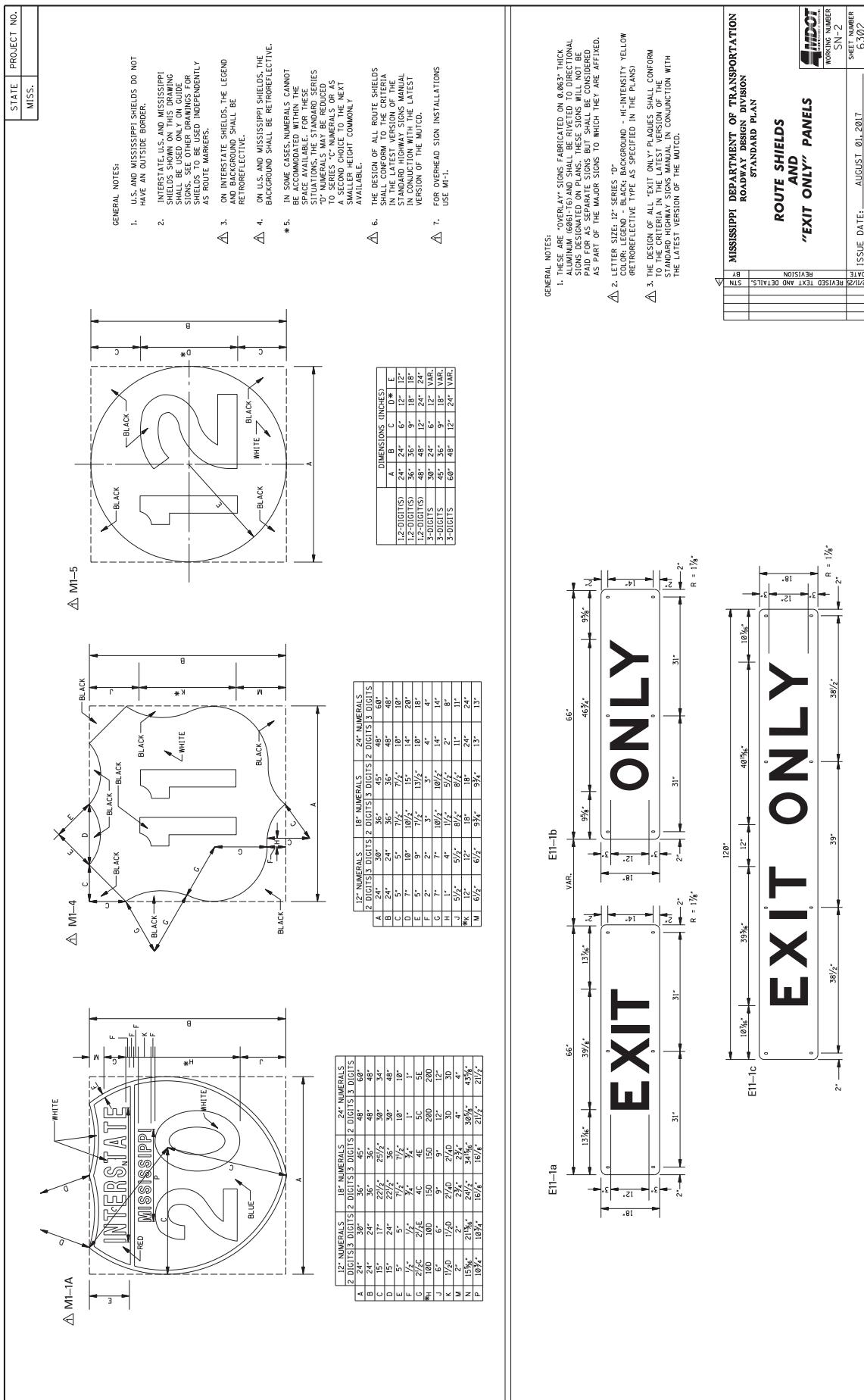
**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SUPPLEMENT TO NOTICE TO BIDDERS NO. 7624**

**DATE:** **01/21/2026**

After the last drawing on page 33, add the following.







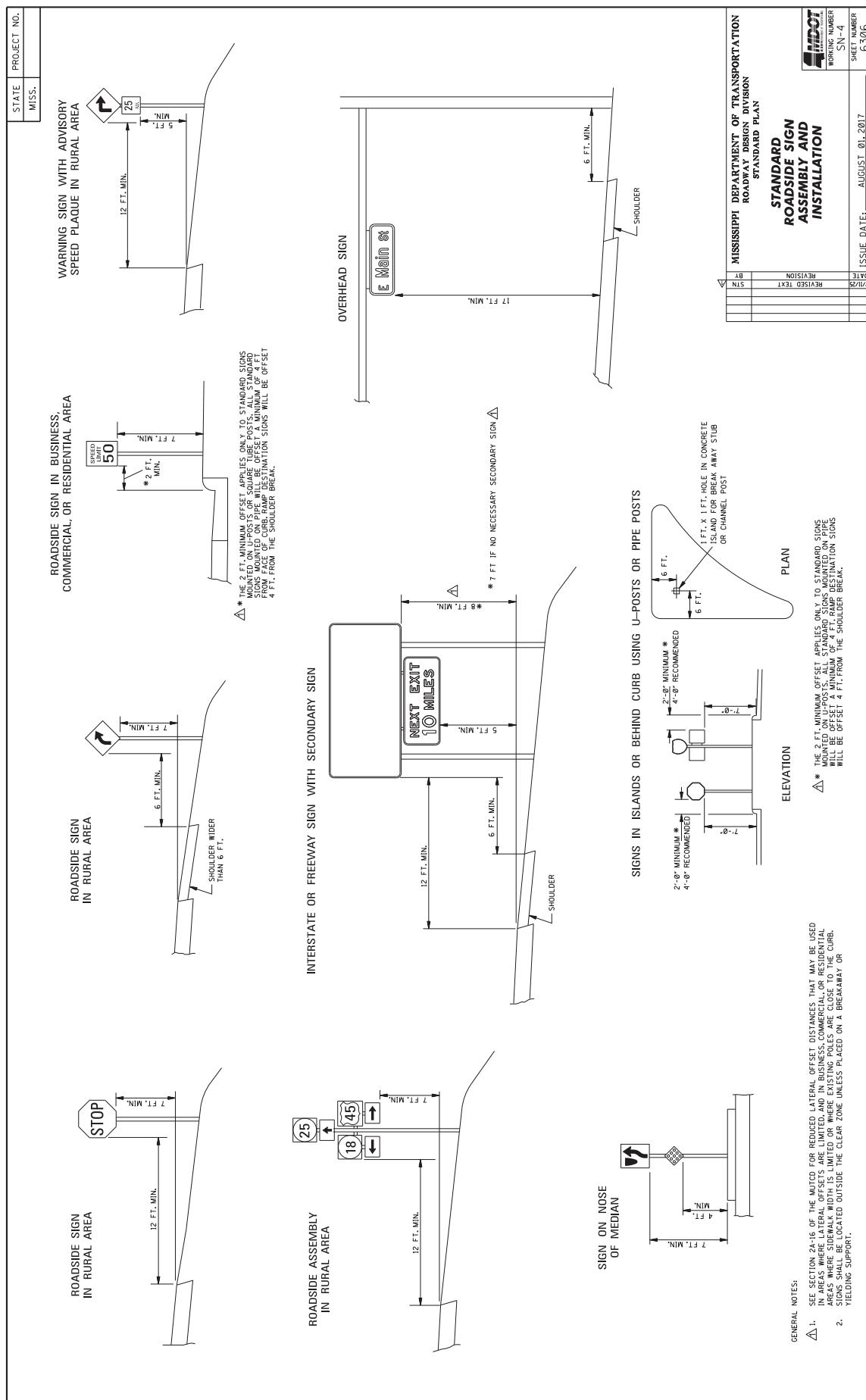


1 THE GIANT

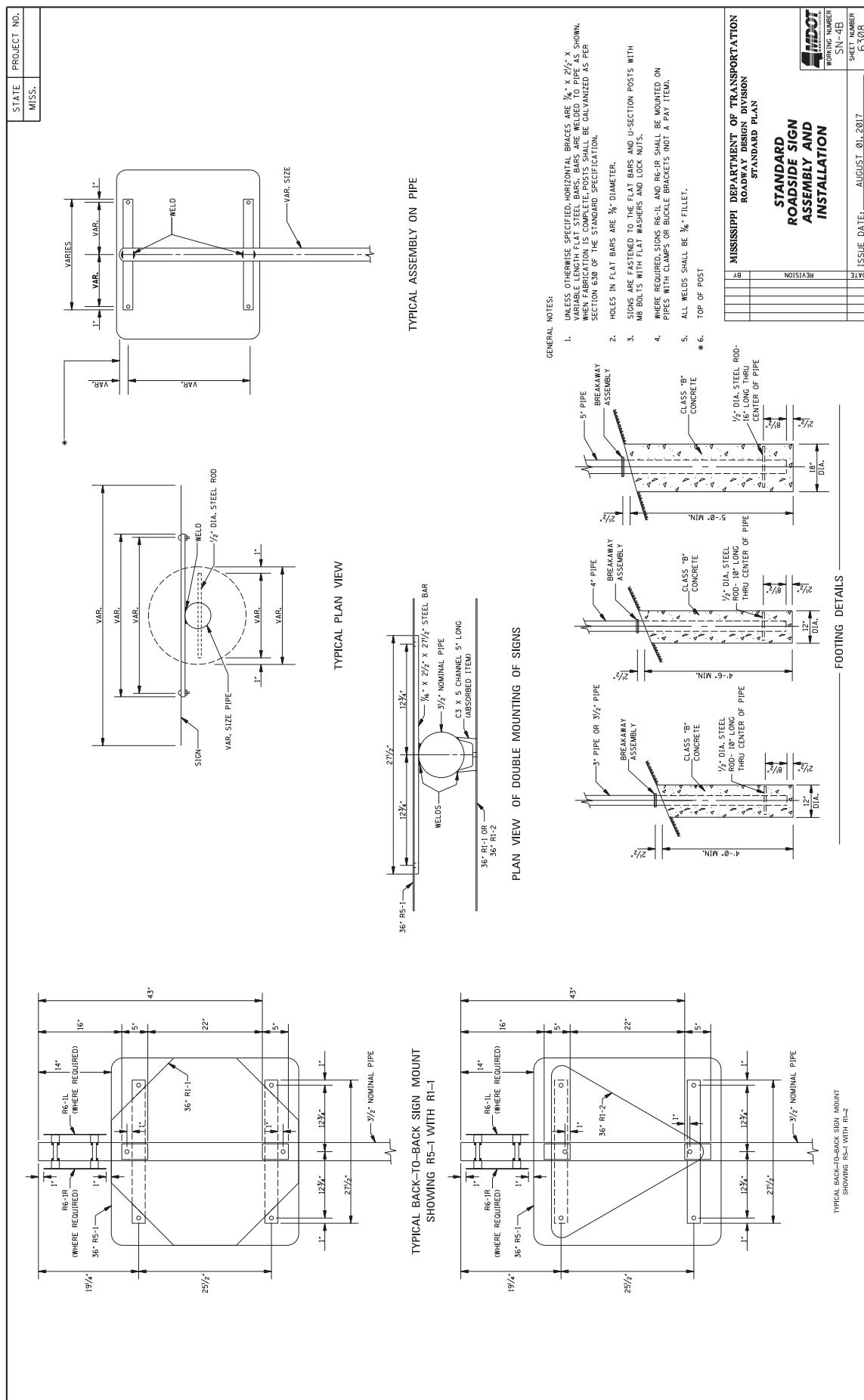
IN THE LATEST VERSION OF THE STANDARD HIGHWAY SIGNS MANUAL IN CONJUNCTION WITH THE LATEST VERSION OF THE MOTOCO SIGN SYSTEM, THE SIGNS SHOWN ON THIS SHEET WILL BE USED AS THE BASIS FOR FINAL PAYMENT, PROVIDED THAT THE SIGNS SHOWN ARE MODIFIED FROM THAT SHOWN.

2. SIGNS W13-2 AND W13-3 - THE STROKE WIDTH OF THE LETTER AND NUMERALS SHALL BE WIDENED TO 20% OF THE LETTER OR NUMERICAL HEIGHT.

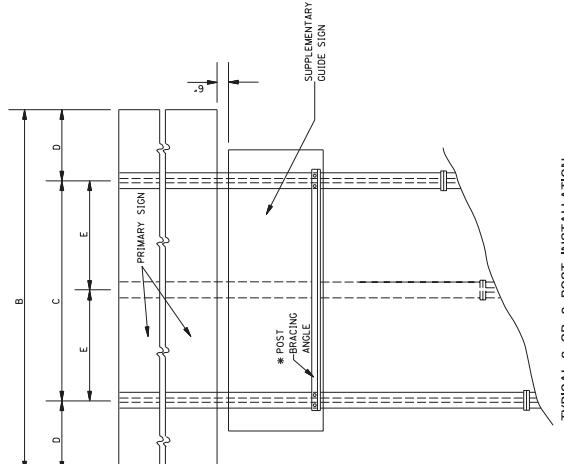
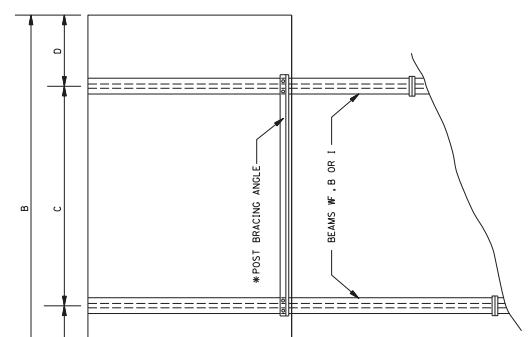
3. THE SPEEDS REQUIRED ON SIGNS M13-1, M13-2 AND W13-3 WILL BE SHOWN IN INDIVIDUAL BOLD SIGN SIGNS.





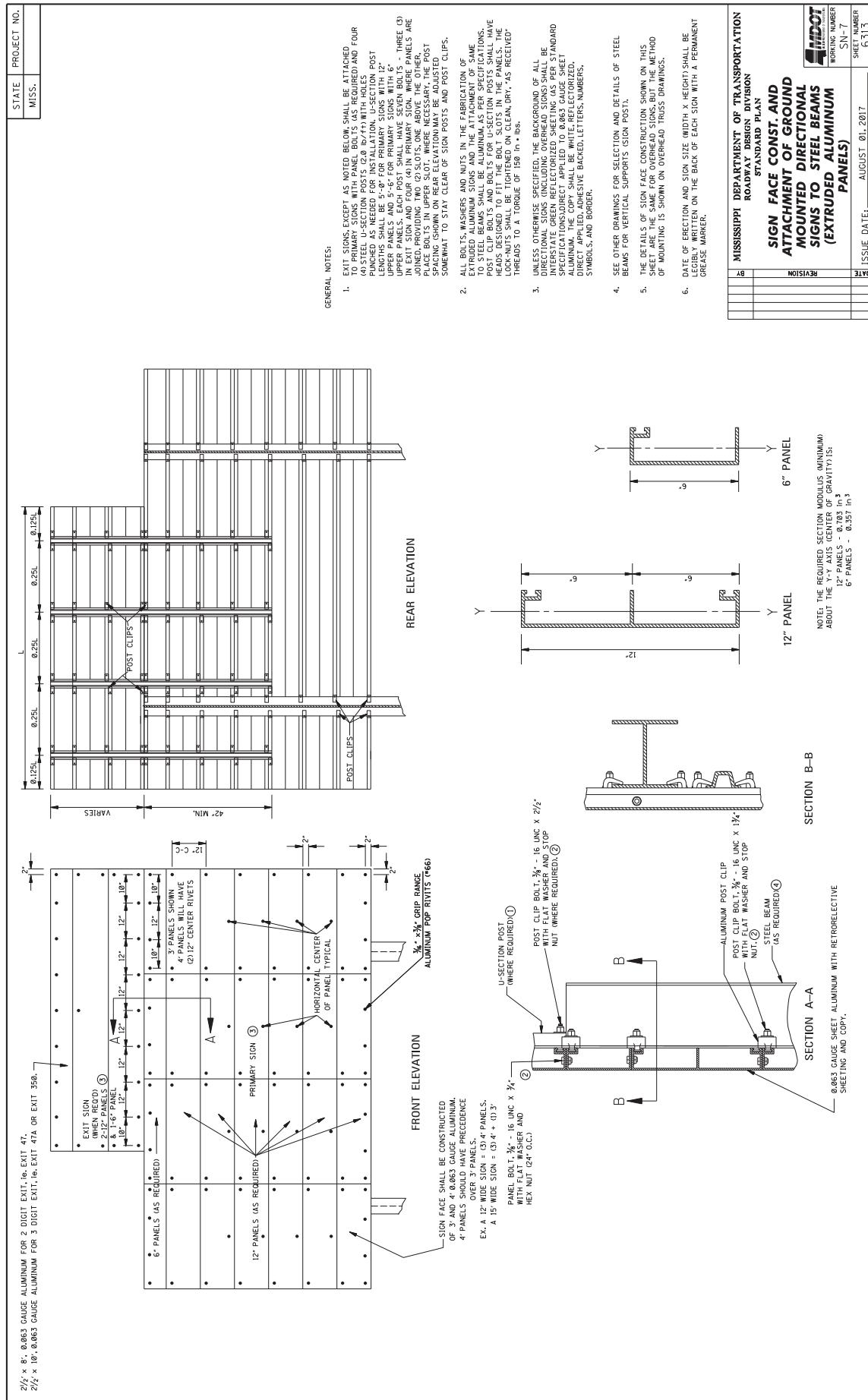




STATE	PROJECT NO.																																																
MISS.																																																	
GENERAL NOTES FOR WORKING SHEETS SN-6, SN-6A AND SN-6B:																																																	
1. ERECTING ALL POSTINGS SHALL BE CLASS "B" CONCRETE. POST STUDS SHALL BE SET IN CONCRETE FOOTING. AT REQUIRED ORGANIC AND ALIGNMENT WITH CARE, SO THAT MINIMUM SHAMING WILL BE REQUIRED.																																																	
2. BASE CONNECTION PROCEDURE ASSEMBLE POST TO TUB WITH BOLTS AND WITH A FLAT WASHER ON EACH BOLT BETWEEN PLATES. SIM AS REQUIRED TO PLUMB POST, TIGHTEN ALL BOLTS THE MAXIMUM POSSIBLE WITH A 12'- TO 15' WRENCH TO BED WASHERS AND SHIMS AND TO CLEAN BOLT THREADS. THEN LOOSEN EACH BOLT IN TURN AND RETIGHTEN IN A SYSTEMATIC ORDER TO THE PREFERRED TORQUE AS SHOWN BY TABLE. BURR THREADS AT JUNCTION WITH NUT USING A CENTER PUNCH. HIGH STRENGTH BOLTS IN BASE CONNECTIONS SHALL BE TIGHTENED TO TORQUE AS SHOWN ON TABLE ON SN-6A. DO NOT OVER TIGHTEN.																																																	
3. POST LENGTH ALL POST LENGTHS SHALL BE VERIFIED AND APPROVED BY THE ENGINEER PRIOR TO FABRICATION, WHERE TIED TO CONDITIONS REQUIRING THE POST LENGTH TO VARY MORE THAN 12'. IT MAY BE NECESSARY TO CHANGE THE SIZE OR NUMBER OF POSTS, SUCH DETERMINATION WILL BE MADE BY THE STATE TRAFFIC ENGINEER. ANY CHANGE OF SIZE OR NUMBER OF POSTS SHALL NOT BE JUSTIFICATION FOR ANY CONTRACT PRICE ADJUSTMENTS.																																																	
4. FABRICATOR NOTE IMPORTANT - ALL FRICTION FUSE BOLTS SHALL BE TIGHTENED IN SHOP BY A METHOD APPROVED BY THE BRIDGE DESIGN ENGINEER. TIGHTENING SHALL BE SUCH A DEGREE AS TO PROVIDE THE MINIMUM TENSION IN EACH BOLT WHEN ALL BOLTS ARE TIGHT AS SHOWN BY TABLE SN-6A.																																																	
5. ALL HOLES IN USE PLATES AND HINGE PLATES SHALL BE DRILLED.																																																	
6. ALL PLATE CUTS SHOULD BE SAW CUTS. FLAME CUTTING WILL BE PERMITTED PROVIDED ALL EDGES ARE GROUND. METAL PROJECTIONS BEYOND THE FLAME OF THE PLATE FACE WILL NOT BE ACCEPTABLE.																																																	
7. WELDING FOR STEEL SIGN SUPPORTS WELDING SHALL BE PERFORMED IN SHOP BY ELECTRIC ARC PROCESS.																																																	
8. MATERIAL SPECIFICATIONS THE MATERIALS USED IN THE CONSTRUCTION OF THE GROUND MOUNTED SIGN SUPPORT STRUCTURES, AS LISTED BELOW, SHALL CONFORM WITH THE REQUIREMENTS OF THE DESIGNATED ASTM SPECIFICATION. ALL OTHER MATERIALS, FABRICATION AND ERECTION SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE SPECIFICATIONS, EXCEPT AS OTHERWISE NOTED ON THE PLANS.																																																	
<table border="1"> <thead> <tr> <th>DESCRIPTION</th> <th>MATERIALS PER ASTM DESIGNATION</th> <th>GALVANIZE ① PER ASTM DESIGNATION</th> </tr> </thead> <tbody> <tr> <td>POSTS OF STEEL PIPE</td> <td>A 36</td> <td>A 53 A 53 (GRADE "B" OR "C")</td> </tr> <tr> <td>BASE CONNECTION PLATES FOR PIPES</td> <td>A 123</td> <td></td> </tr> <tr> <td>POSTS OF STEEL W-B, AND I BEAMS</td> <td>A 588</td> <td></td> </tr> <tr> <td>INCLUDING BASE CONNECTION, FUSE, AND HINGE PLATES</td> <td>OR</td> <td>A 123</td> </tr> <tr> <td>POST BRACING ANGLES AND LAT BARS</td> <td>A 572 GRADE 50</td> <td></td> </tr> <tr> <td>POST IN FABRICATION AND ERECTION OF SIGN SUPPORTS</td> <td>A 36</td> <td>A 123</td> </tr> <tr> <td>STAINLESS STEEL BOLTS, NUTS AND WASHERS</td> <td>A 355</td> <td>A 153</td> </tr> <tr> <td>STAINLESS STEEL BOLTS OTHER THAN HIGH STRENGTH (C)</td> <td>A 153</td> <td>A 153</td> </tr> </tbody> </table>		DESCRIPTION	MATERIALS PER ASTM DESIGNATION	GALVANIZE ① PER ASTM DESIGNATION	POSTS OF STEEL PIPE	A 36	A 53 A 53 (GRADE "B" OR "C")	BASE CONNECTION PLATES FOR PIPES	A 123		POSTS OF STEEL W-B, AND I BEAMS	A 588		INCLUDING BASE CONNECTION, FUSE, AND HINGE PLATES	OR	A 123	POST BRACING ANGLES AND LAT BARS	A 572 GRADE 50		POST IN FABRICATION AND ERECTION OF SIGN SUPPORTS	A 36	A 123	STAINLESS STEEL BOLTS, NUTS AND WASHERS	A 355	A 153	STAINLESS STEEL BOLTS OTHER THAN HIGH STRENGTH (C)	A 153	A 153																					
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<p style="text-align: center;"><b>BREAKAWAY SIGN SUPPORTS</b></p>																																																	
<p>REVISION BY DATE ISSUED: AUGUST 01, 2017</p>																																																	
<p>MDOT WORKING NUMBER SN-6 SHEET NUMBER 6310</p>																																																	



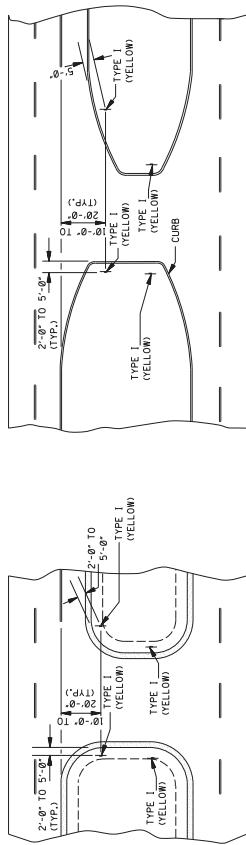




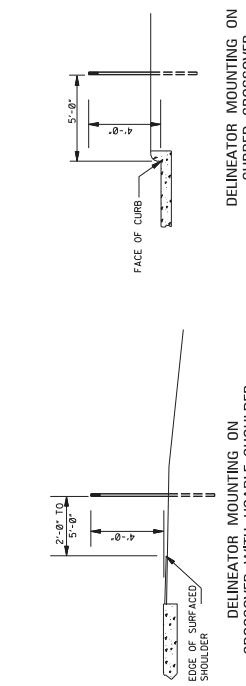




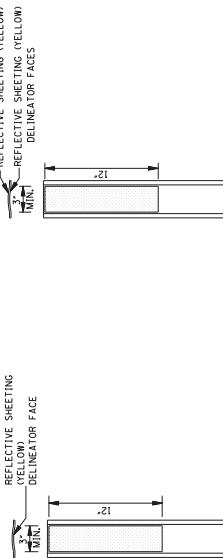
STATE	PROJECT NO.
MISS.	



**TYPICAL DELINEATION AT A CURBED CROSSOVER  
WITH A MEDIAN WIDTH OVER 42'-0"**



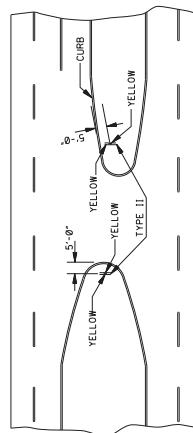
## DELINEATOR MOUNTING ON CROSSOVER WITH USABLE SHOULDER



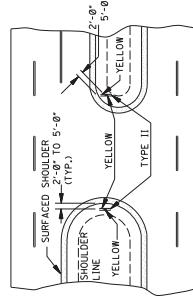
**DETAIL OF TYPE I**  
**FLEXIBLE POST DELINEATOR**

**DETAIL OF TYPE II**  
**FLEXIBLE POST DELINEATOR**

△ NOTE: FLEXIBLE POST DELINEATORS ALLOWED FOR USE MUST BE FROM MOOTS  
APPROVED PRODUCTS LIST.



**TYPICAL DELINEATION AT A CURBED CROSSOVER  
WITH A MEDIAN WIDTH OF 42'-0" OR LESS**



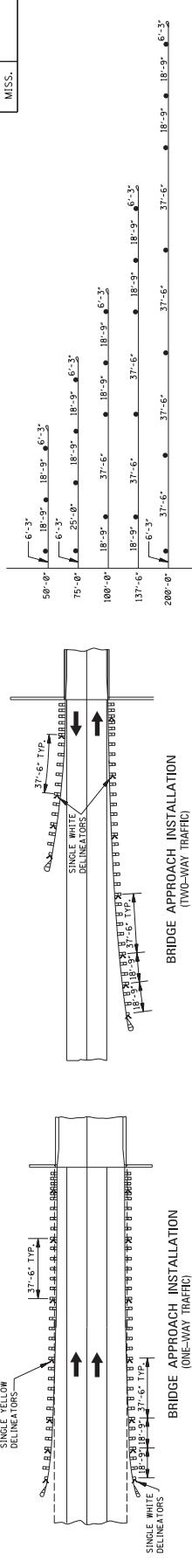
11. TYPICAL DELINEATION AT A CROSSOVER WITH  
12. USABLE SHOULDERS AND A MEDIAN WIDTH  
13. OF 42'-0" OR LESS

NOTE: PLACE DELINEATORS NO MORE THAN 20'-0" FROM EDGE OF TRAVEL LANES EDGES.

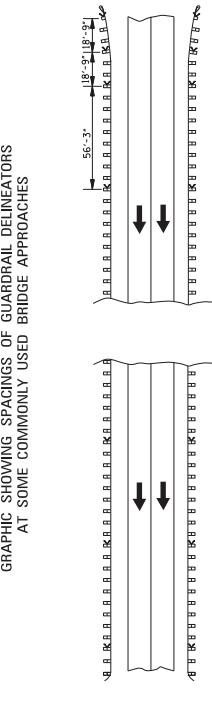
GENERAL NOTES

1. THE UNIT PRICE OF DELINEATOR INCLUDES: COSTS OF DELINEATOR FACE, POST, HARDWARE, AND INSTALLATION. DELINEATOR FACE WILL BE RETROREFLECTIVE SHEETING TYPE AS SPECIFIED IN PLANS.
2. A POST REQUIRING THE INSTALLATION OF A BASE SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.
3. THE COLOR OF DELINEATORS SHALL BE THE COLOR OF THE

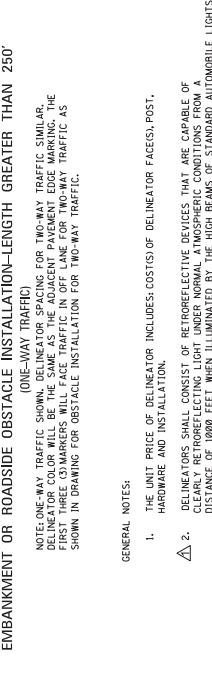
MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
ROADWAY DESIGN DIVISION	
STANDARD PLAN	
<b>TYPICAL CROSSOVER DELINEATION</b>	
REVISED TEXT	STN
11/17/2017	01/2017
REVISED DATE	ISSUE DATE
01/2017	AUGUST 01 2017
WORKING NUMBER	SN 818
SHEET NUMBER	1



GRAPHIC SHOWING SPACINGS OF GUARDRAIL DELINEATORS  
AT SOME COMMONLY USED BRIDGE APPROACHES



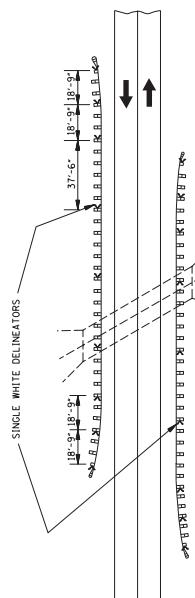
EMBANKMENT OR BOARDSIDE OBSTACLES INSTALLATION LENGTH GREATER THAN 250'



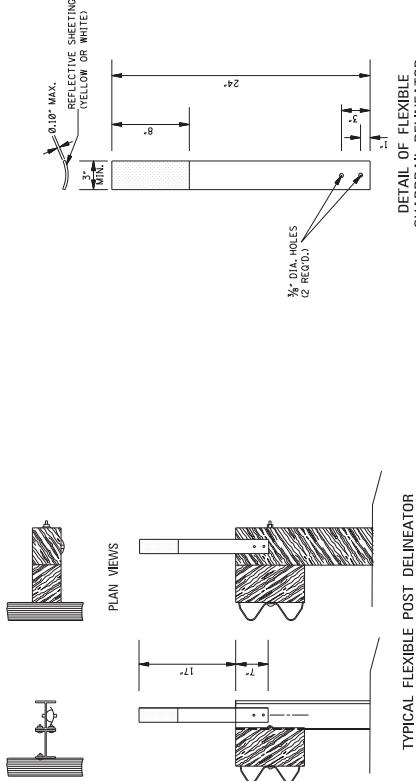
**GENERAL NOTES:**

1. THE UP-PIPE OF DELINEATOR INCLUDES: COSTS OF HELING OR PALE POSTS, HARDWARE AND INSTALLATION.
2. DELINEATORS SHALL CONSIST OF RETROREFLECTIVE DEVICES THAT ARE CAPABLE OF CLEARLY REFLECTING LIGHT UNDER NORMAL, ANCHERIC CONDITIONS FROM A DISTANCE OF 1000 FEET WHEN ILLUMINATED BY THE HIGH BEAMS OF STANDARD AUTOMOBILE LIGHTS.
3. FOR THE SPACING OF DELINEATORS IN HORIZONTAL CURVES ON ROADWAYS OR BRIDGES, SEE TABLE 3-1, MUTCD 11th EDITION, FOR SPACING REQUIREMENTS.
4. DELINEATORS FOR GUARDRAIL SHALL BE MOUNTED ON FLEXIBLE POSTS AS FOLLOWS:
  - △ 2. THE DELINEATOR POSTS WILL BE FROM MOOT'S APPROVED MANUFACTURER'S LIST, AND WILL BE FASTENED TO GUARDRAIL IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATION.
  - △ 3. 1.5 INCHES.

ROADSIDE OBSTACLE INSTALLATION—LENGTH 250' OR LESS  
(TWO-WAY TRAFFIC)



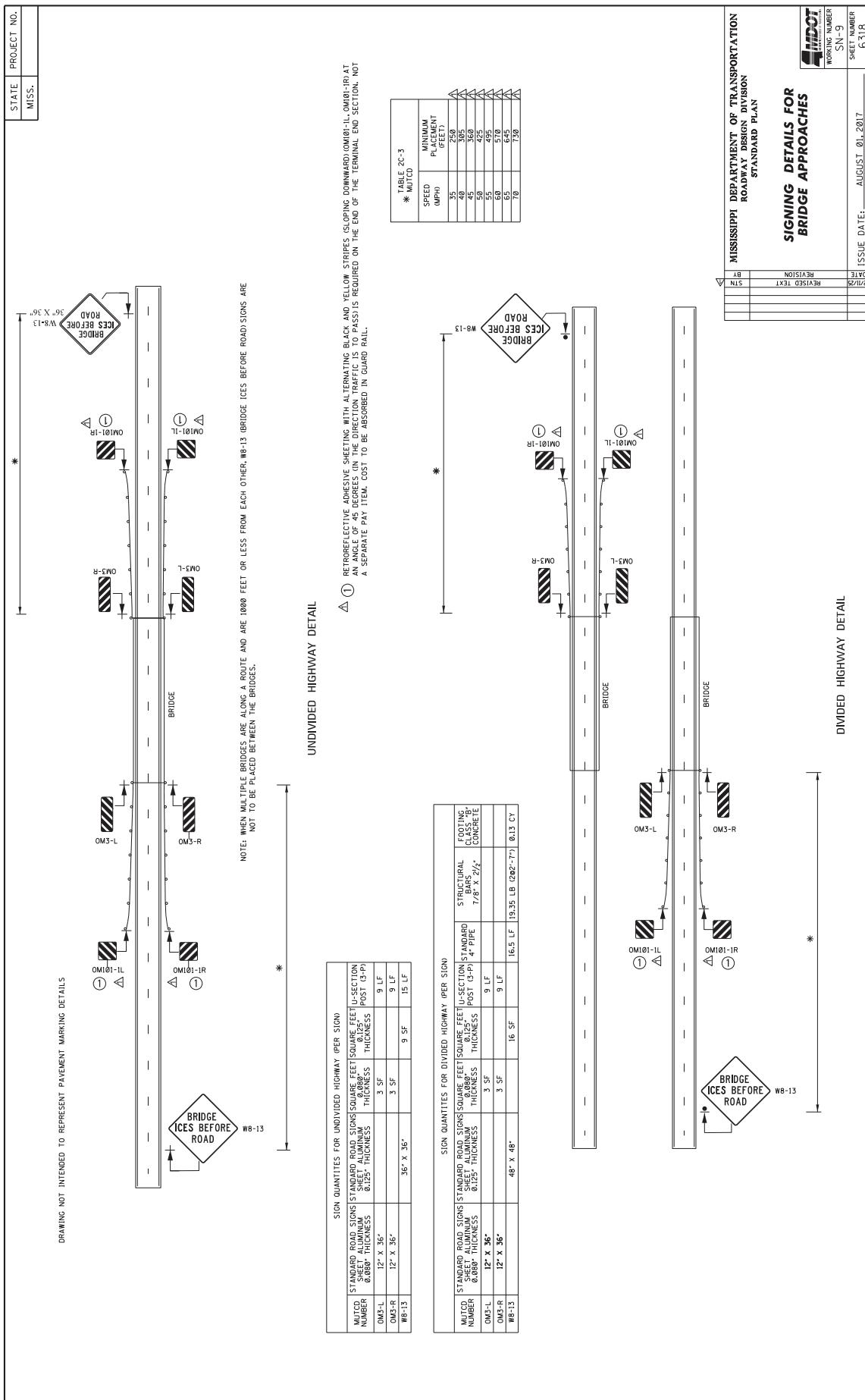
ROADSIDE OBSTACLE INSTALLATION—LENGTH 250' OR LESS  
(ONE WAY TRAFFIC)



**DETAIL OF FLEXIBLE  
GUARDRAIL DELINEATOR**

## **TYPICAL GUARDRAIL DELINEATION**





## MISSISSIPPI DEPARTMENT OF TRANSPORTATION

**SECTION 904 – NOTICE TO BIDDERS NO. 7624**

**CODE: (SP)**

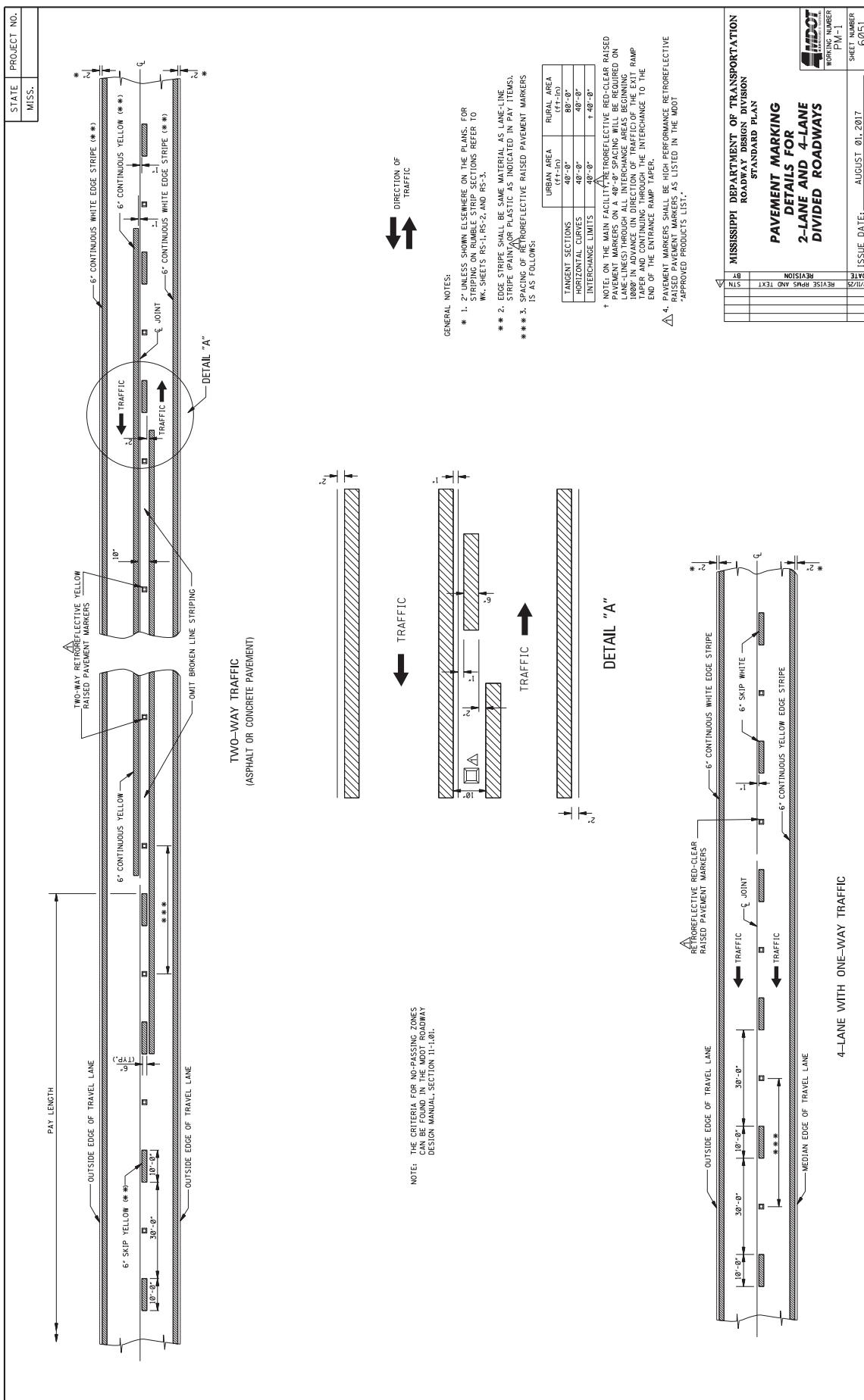
**DATE: 01/20/2026**

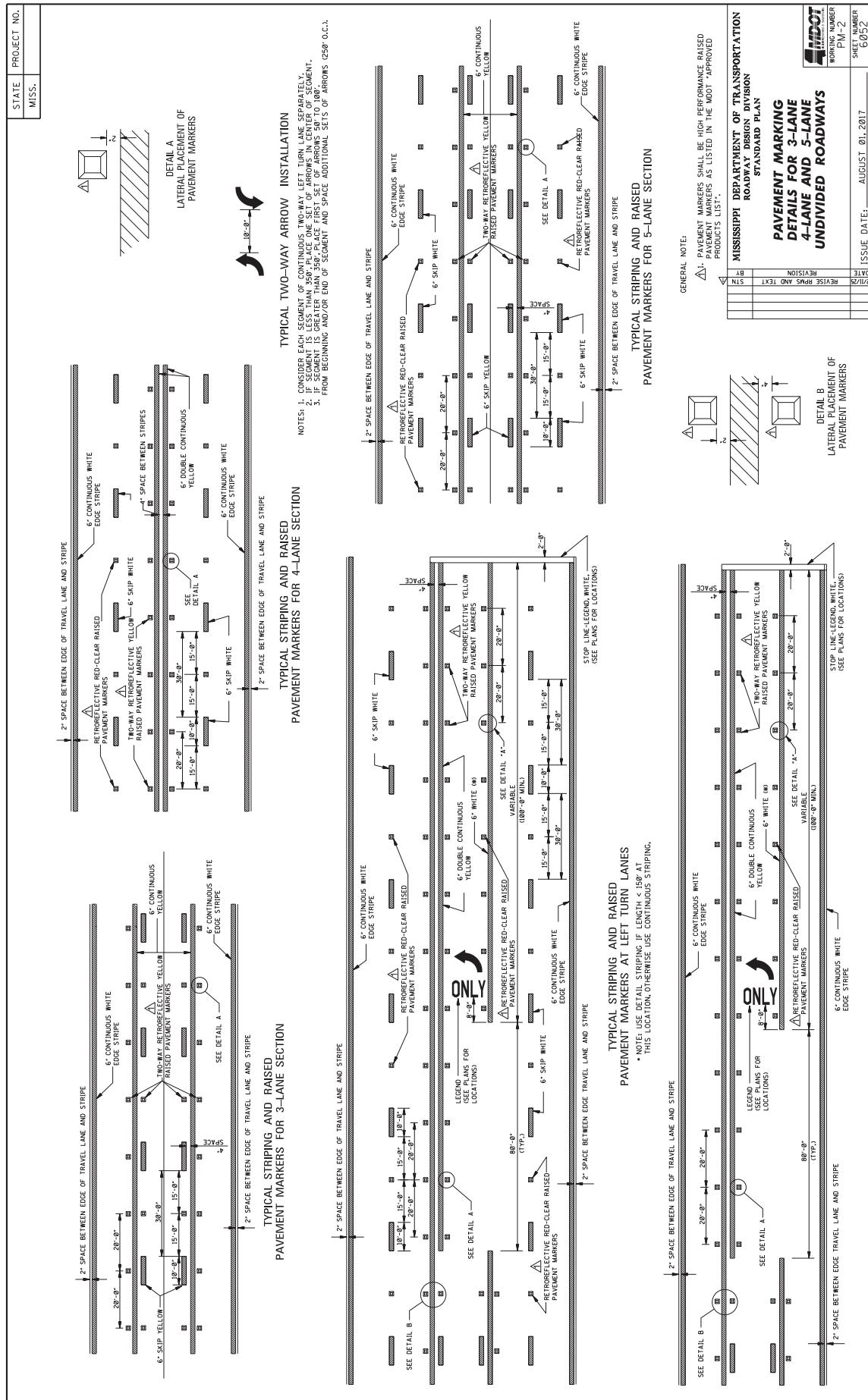
**SUBJECT: Standard Drawings**

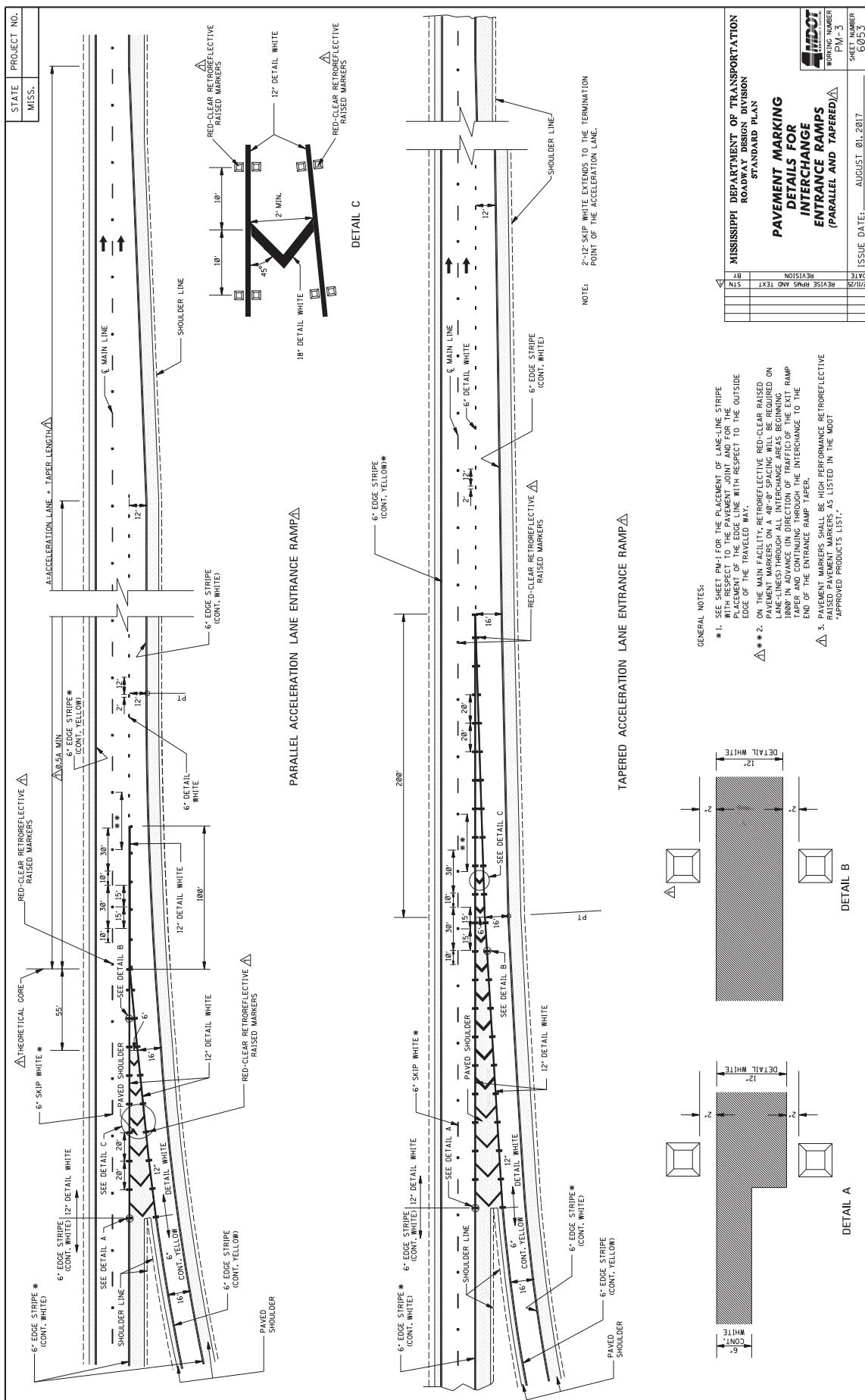
Standard Drawings attached hereto shall govern appropriate items of required work.

Larger copies of Standard Drawings may be purchased from:

MDOT Plans Print Shop  
MDOT Shop Complex, Building C, Room 114  
2567 North West Street  
P.O. Box 1850  
Jackson, MS 39215-1850  
Telephone: (601) 359-7460  
or FAX: (601) 359-7461  
or e-mail: [plans@mdot.state.ms.us](mailto:plans@mdot.state.ms.us)

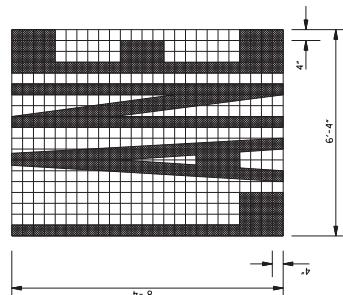








<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td style="width: 50%;">STATE</td> <td style="width: 50%;">PROJECT NO.</td> </tr> <tr> <td colspan="2" style="text-align: center;">MISS.</td> </tr> </table>	STATE	PROJECT NO.	MISS.		<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td style="width: 10%; text-align: center; padding: 5px;">  </td> <td style="width: 90%; text-align: right; padding: 5px;"> <b>MDOT</b>          WORKING NUMBER          PM-5          SHEET NUMBER          6055       </td> </tr> </table>		<b>MDOT</b> WORKING NUMBER PM-5 SHEET NUMBER 6055
STATE	PROJECT NO.						
MISS.							
	<b>MDOT</b> WORKING NUMBER PM-5 SHEET NUMBER 6055						
<b>PAVEMENT MARKING LEGEND DETAILS</b>							
<b>MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN</b>							
<b>PAVEMENT MARKING DETAILS</b>							
<b>ISSUE DATE: AUGUST 01, 2017</b>							
<b>DATE: 01/01/2017</b>							
<b>REVISION: BY: STN</b>							
<b>REVISED GENERAL NOTES</b>							



GENERAL NOTES:

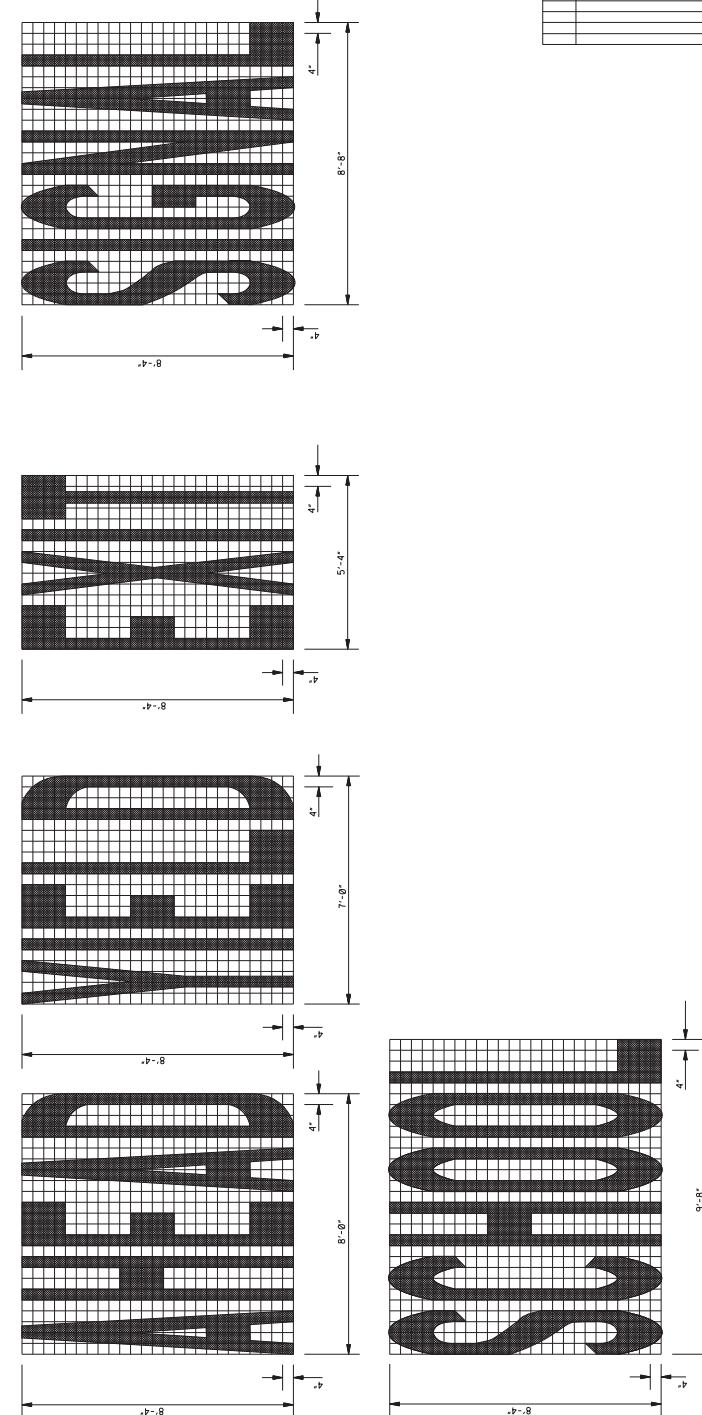
1. TWO HORIZONTAL GAPS, CAUSED BY  
TEMPERATURE CONNECTORS OF  $1\frac{1}{2}$ '', BY  
OR LESS, AND EXTENDING THE FULL WIDTH  
ARE PERMITTED IN EACH LETTER.

2. PAVEMENT MARKING LETTERS,  
NUMERALS, SYMBOLS, AND ARROWS  
SHALL BE INSTALLED IN ACCORDANCE  
WITH THE DESIGN CALLS IN THE  
"STANDARD HIGHWAY SIGNS"  
SECTION 1A.05 OF THE LATEST  
EDITION OF THE MUTCD.

3. WORD, SYMBOL, AND ARROW MARKINGS  
SHALL BE INSTALLED IN ACCORDANCE  
WITH THE DESIGN CALLS IN THE  
"STANDARD HIGHWAY SIGNS"  
SECTION 1A.05 OF THE LATEST  
EDITION OF THE MUTCD.

4. PAVEMENT QUANTITIES FOR PAVEMENT MARKING  
LEGENDS ARE AS FOLLOWS:

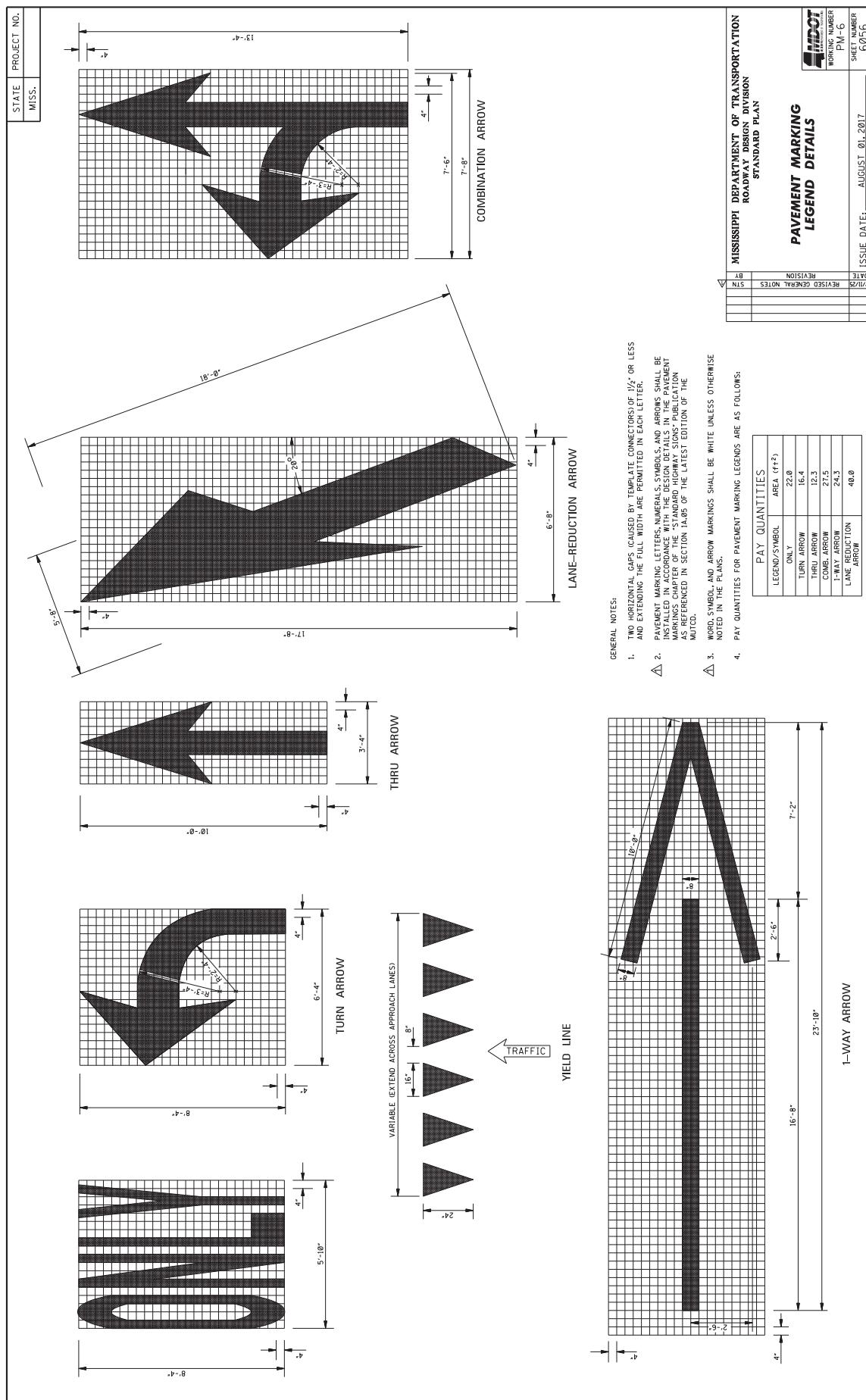
LEGEND	AREA (ft <sup>2</sup> )
STOP	24.6
RIGHT	28.6
LEFT	19.5
TURN	21.3
LANE	22.7
AHEAD	32.3
YIELD	26.8
SIGNAL	32.5
SCHOOL	35.5



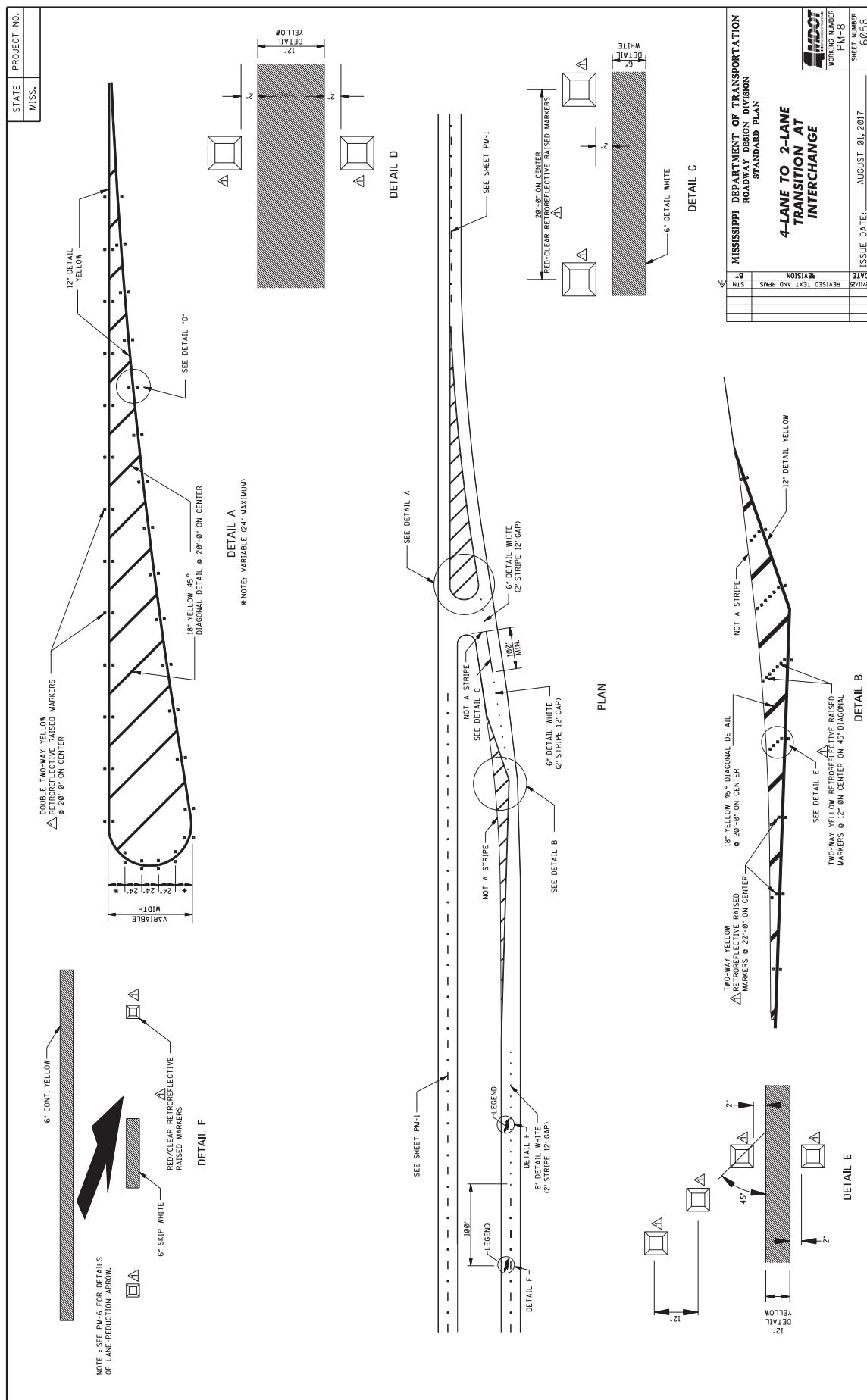
The diagram illustrates various pavement marking symbols and their dimensions. It includes:
 

- STOP: 8'-0" wide, 4'-0" high, with a 1'-0" gap at the top.
- RIGHT: 8'-0" wide, 4'-0" high, with a 1'-0" gap at the top.
- LEFT: 8'-0" wide, 4'-0" high, with a 1'-0" gap at the top.
- TURN: 8'-0" wide, 4'-0" high, with a 1'-0" gap at the top.
- LANE: 8'-0" wide, 4'-0" high, with a 1'-0" gap at the top.
- AHEAD: 8'-0" wide, 4'-0" high, with a 1'-0" gap at the top.
- YIELD: 8'-0" wide, 4'-0" high, with a 1'-0" gap at the top.
- SIGNAL: 8'-0" wide, 4'-0" high, with a 1'-0" gap at the top.
- SCHOOL: 8'-0" wide, 4'-0" high, with a 1'-0" gap at the top.

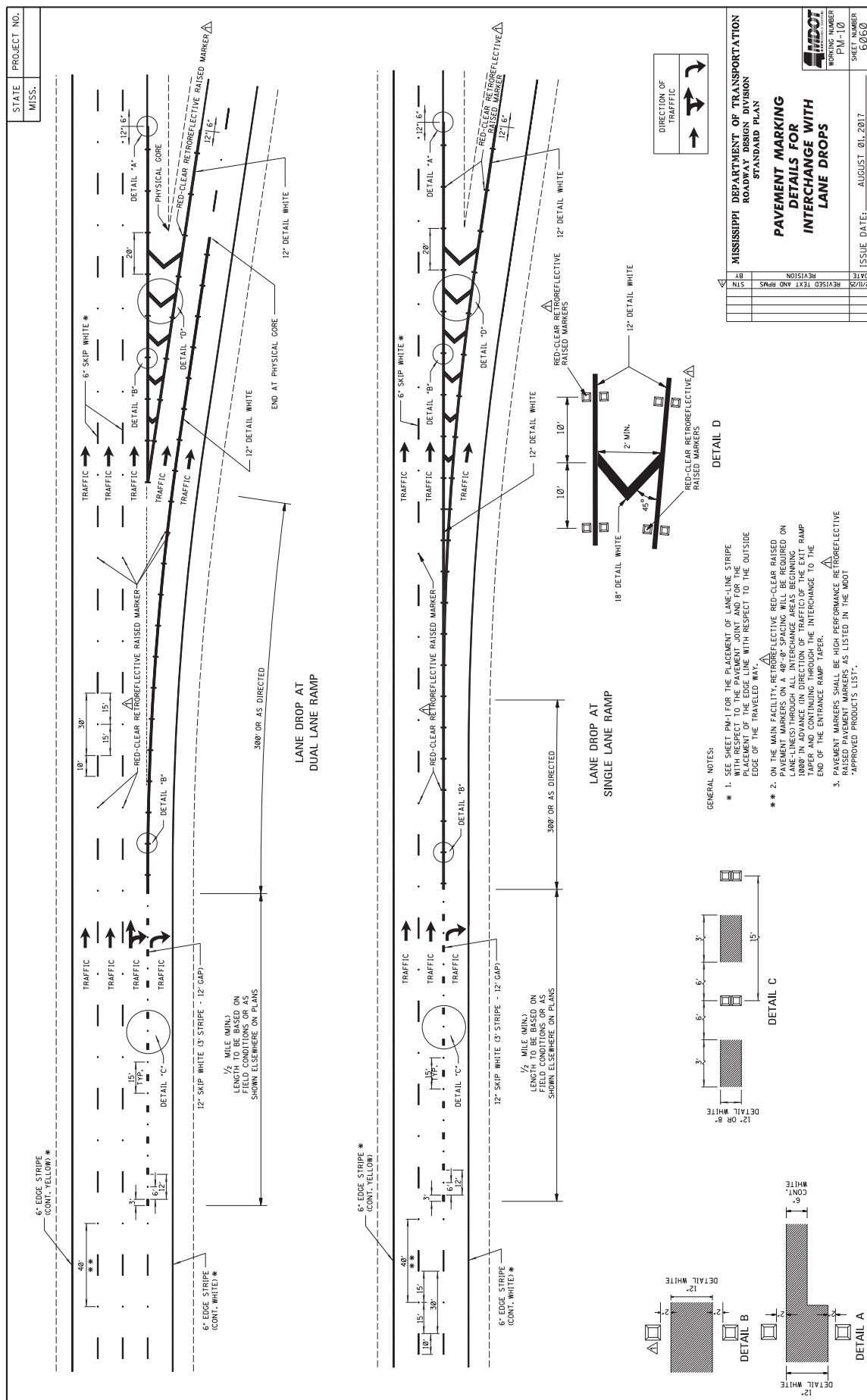
 The symbols are composed of thick black lines on a white background, with a 1'-0" gap at the top and a 1'-0" gap at the bottom. The width of the symbols is 8'-0", and the height is 4'-0". The gap at the top and bottom is 1'-0".

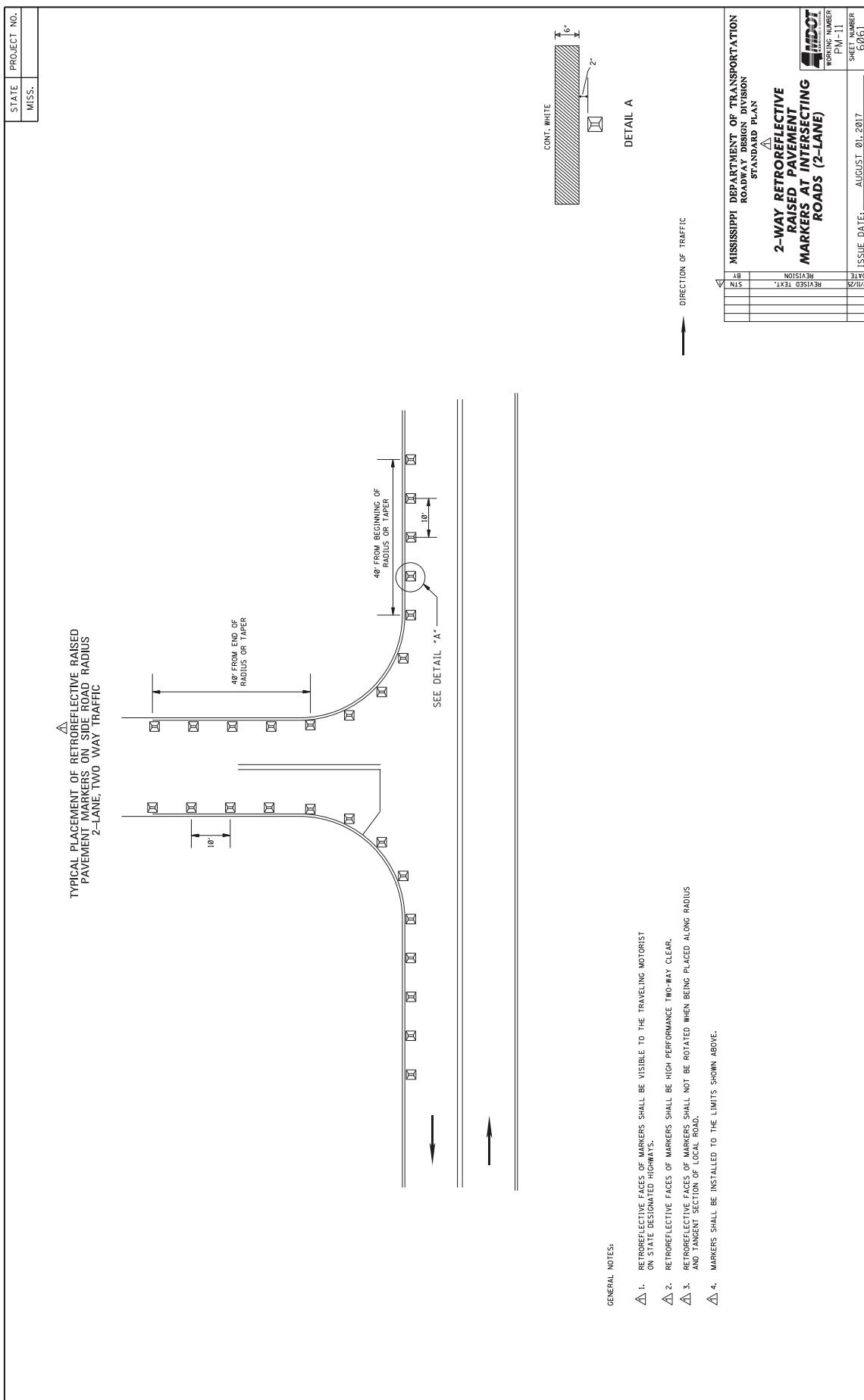


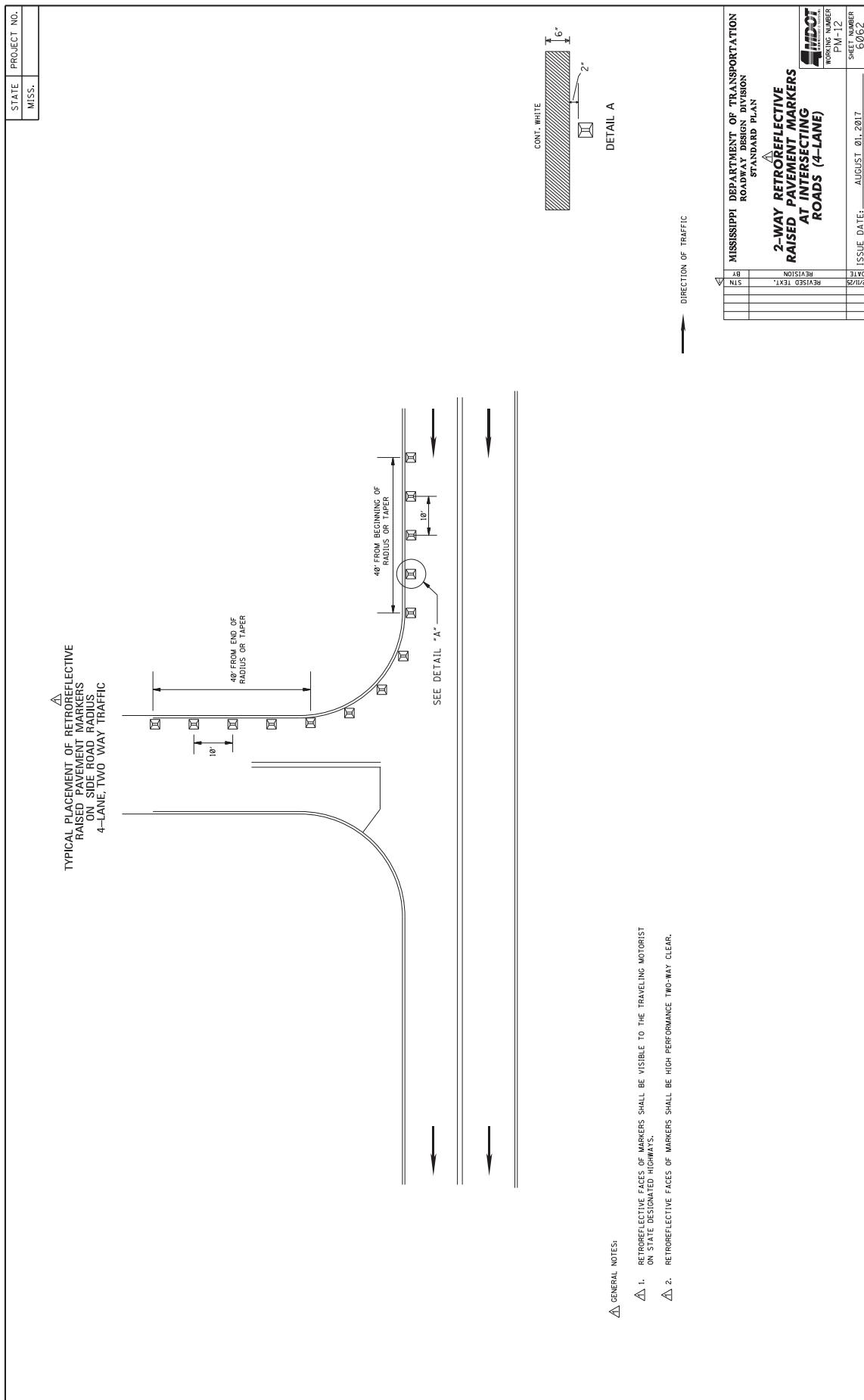


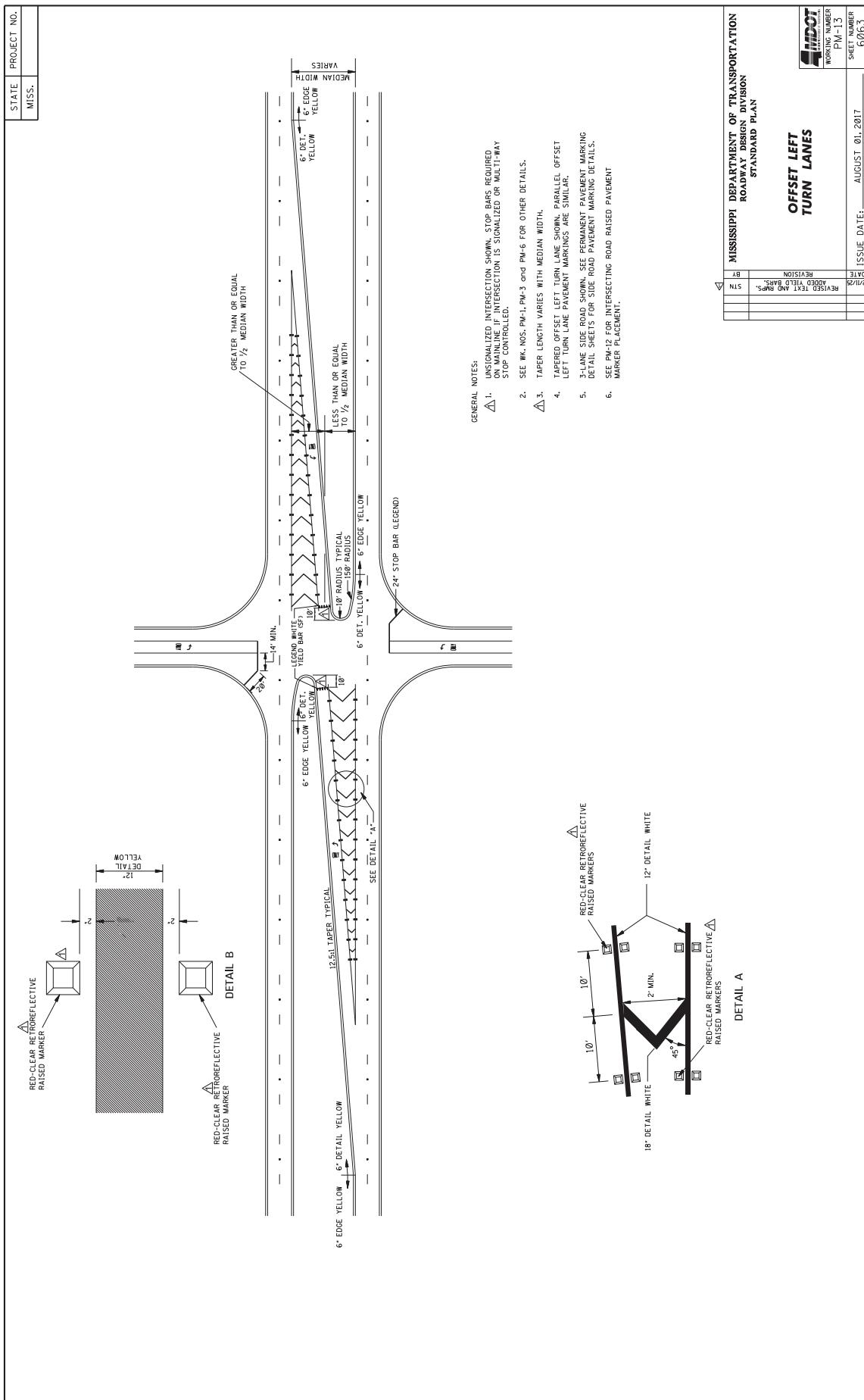


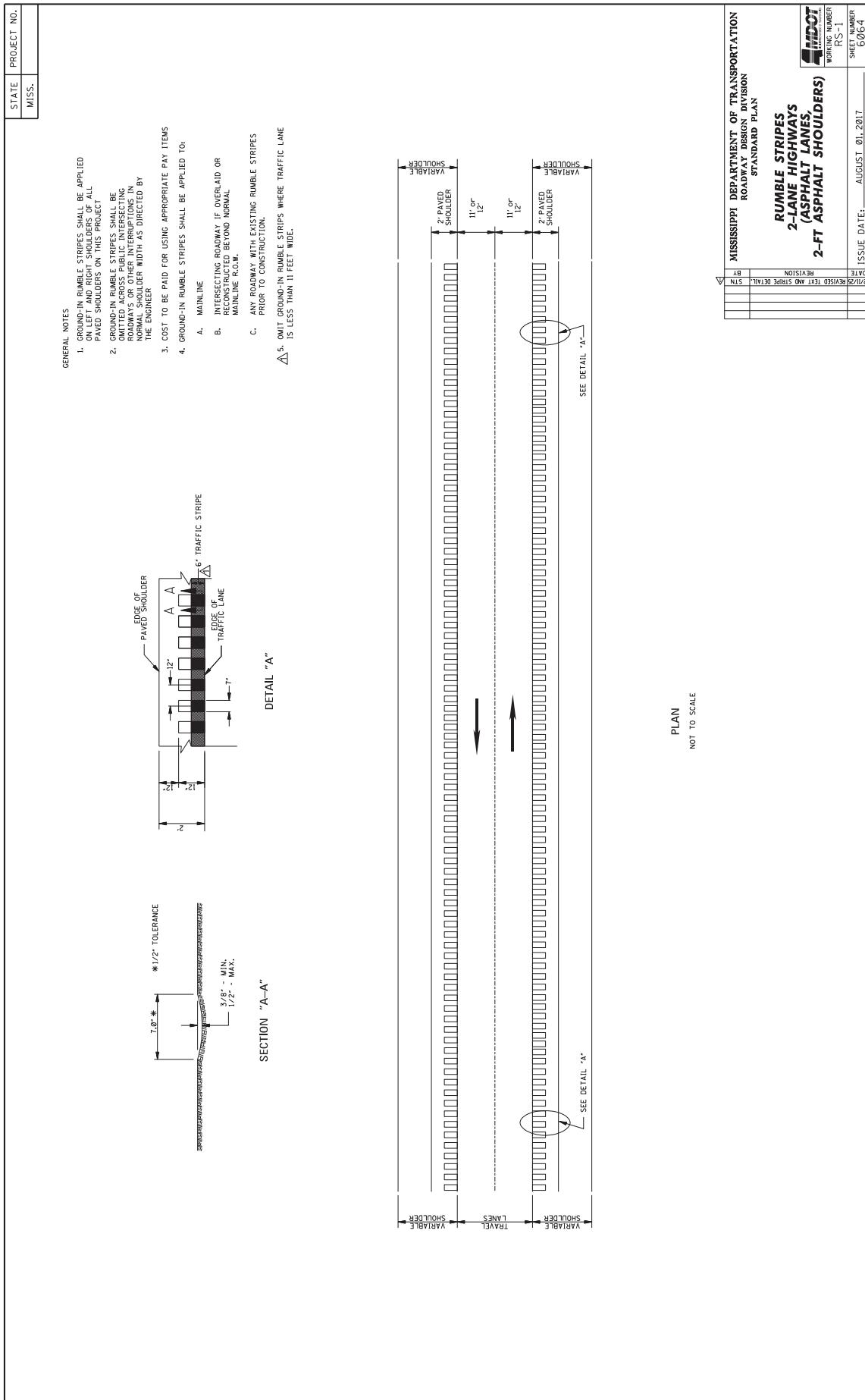


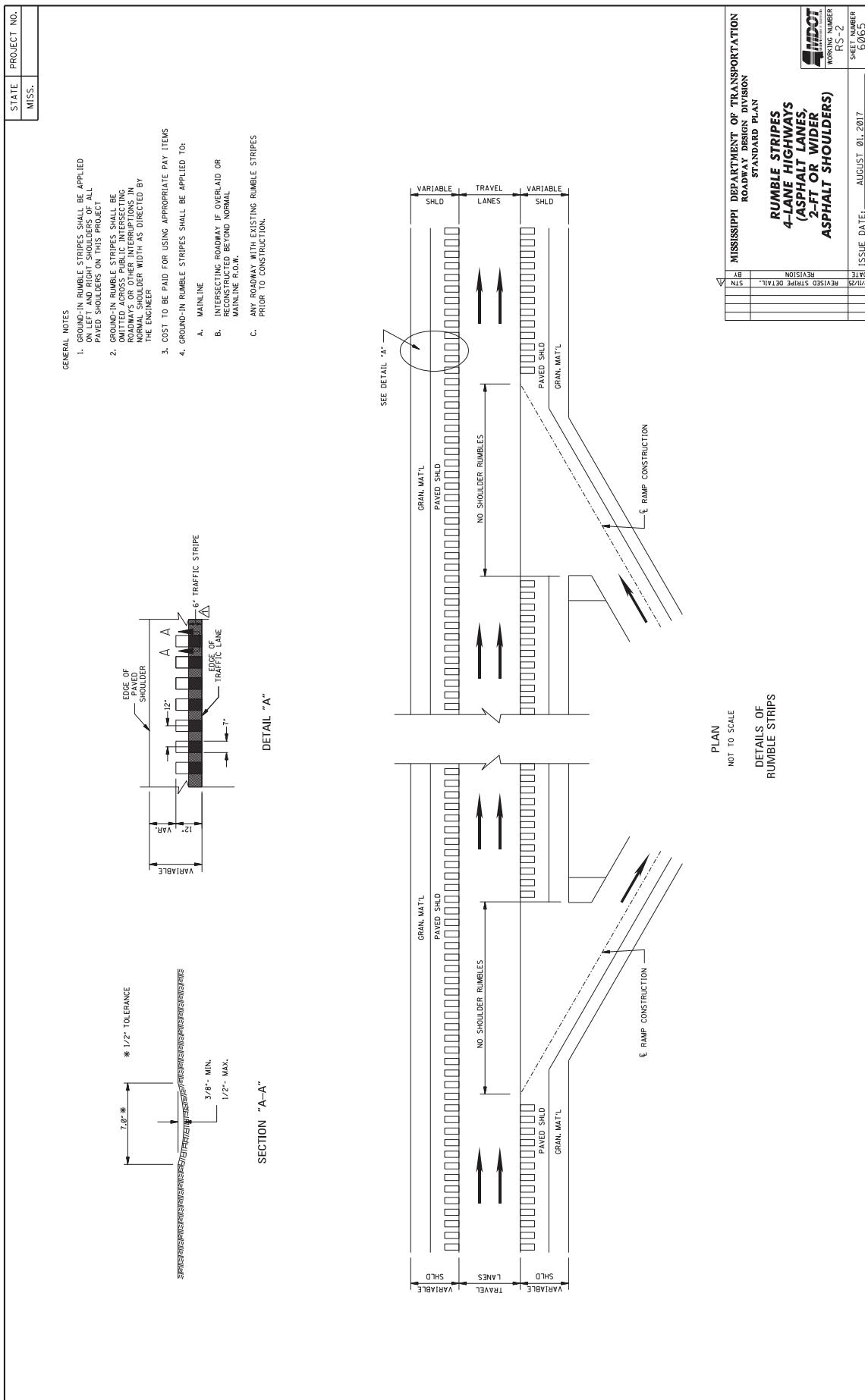


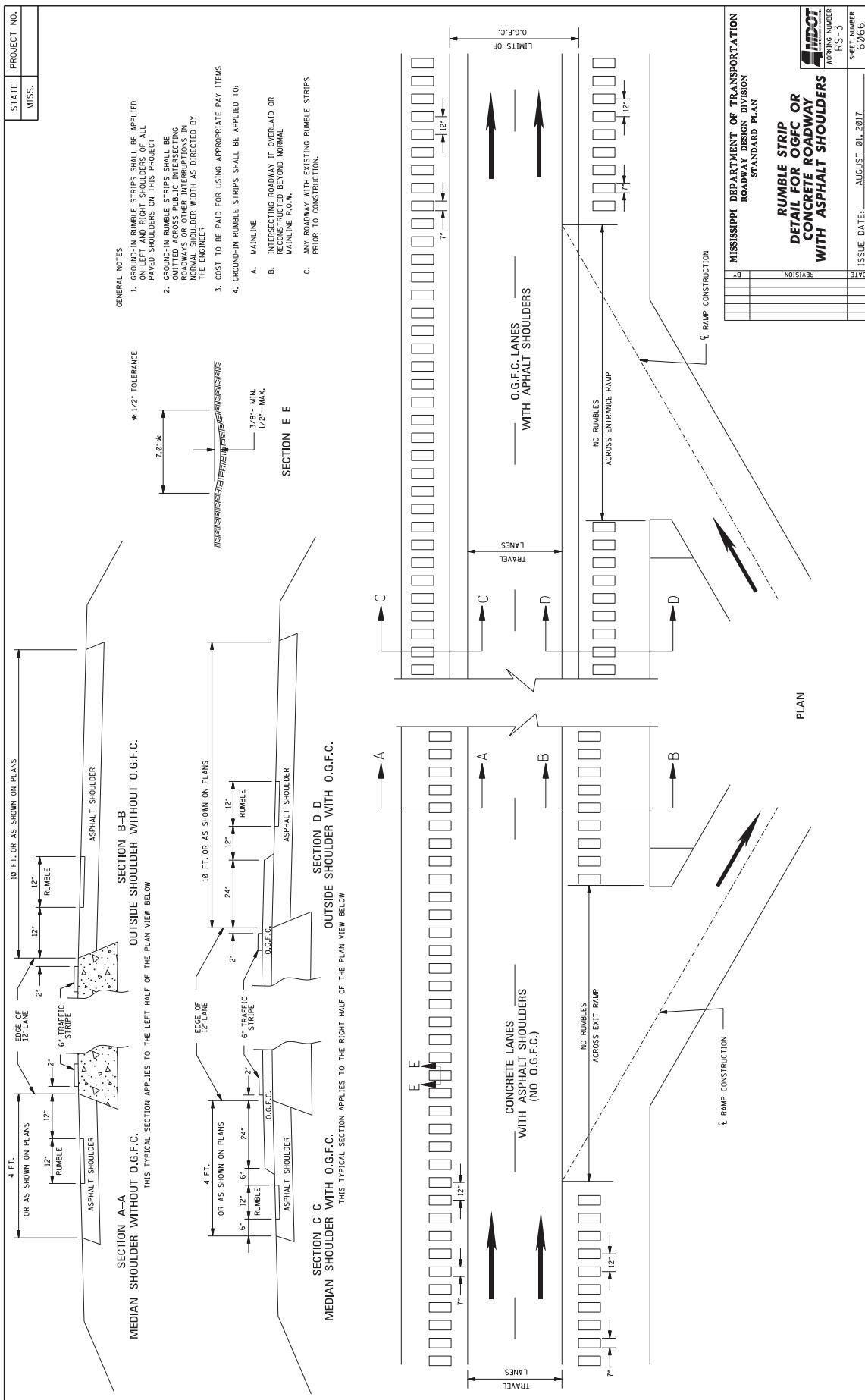


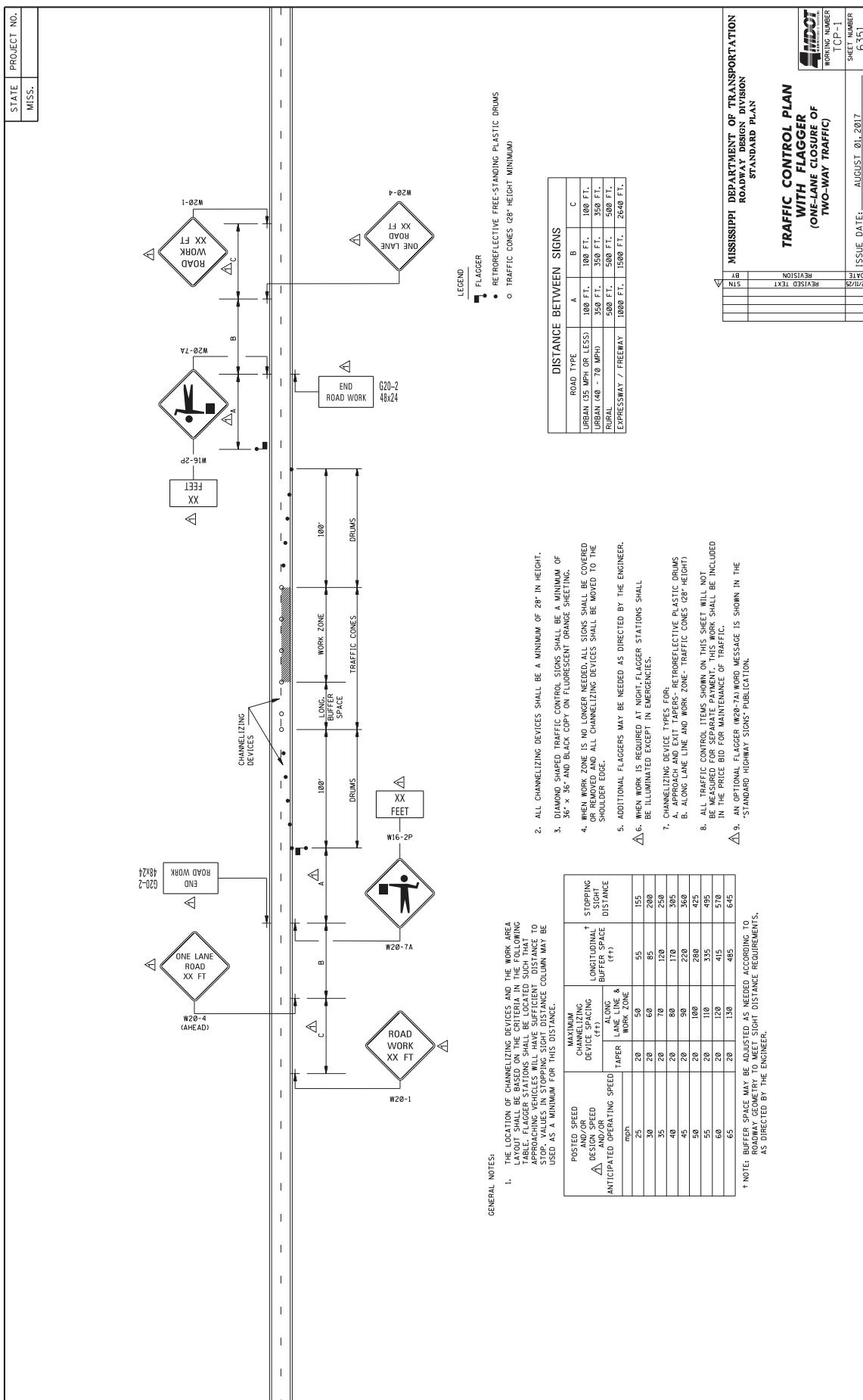


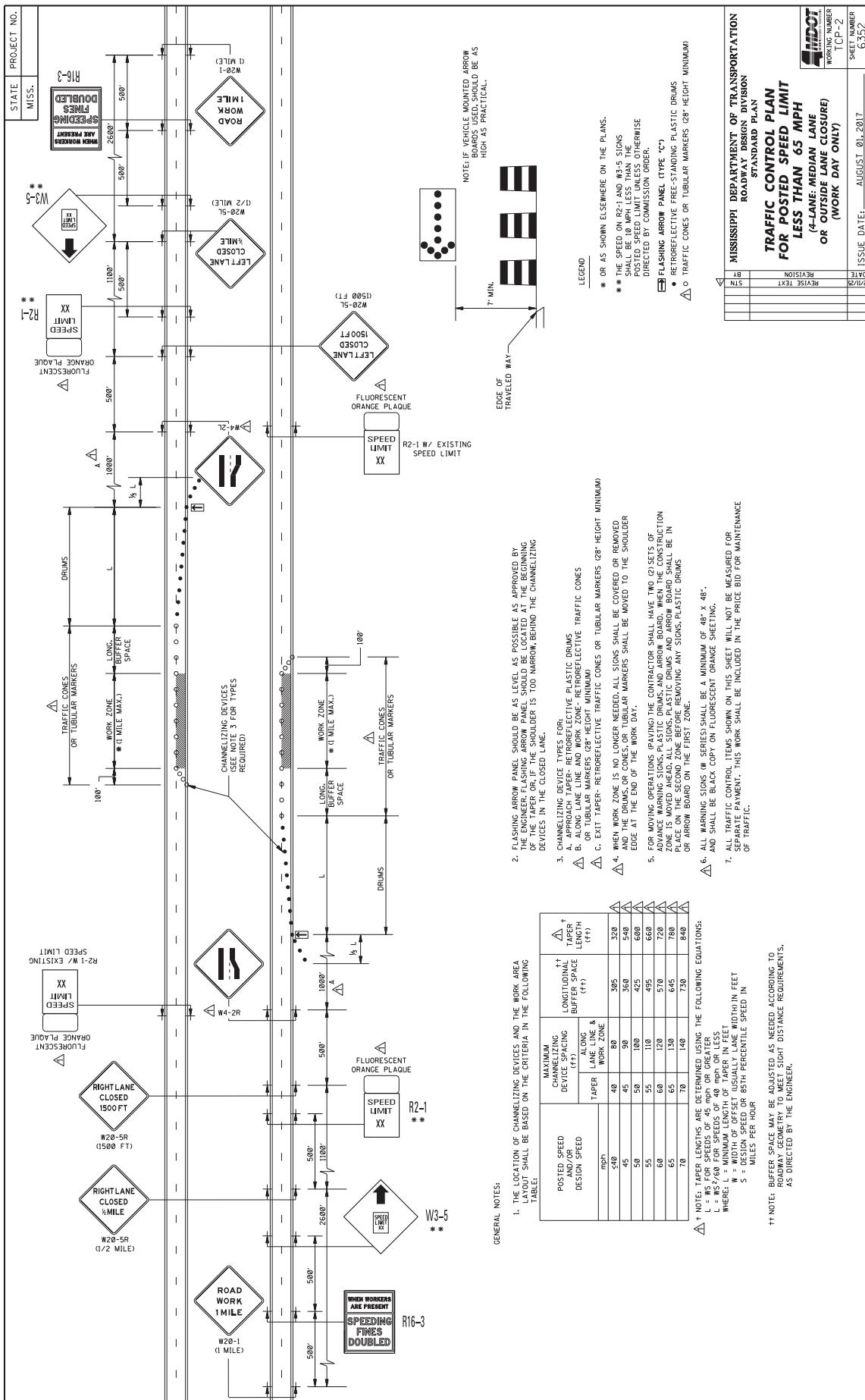


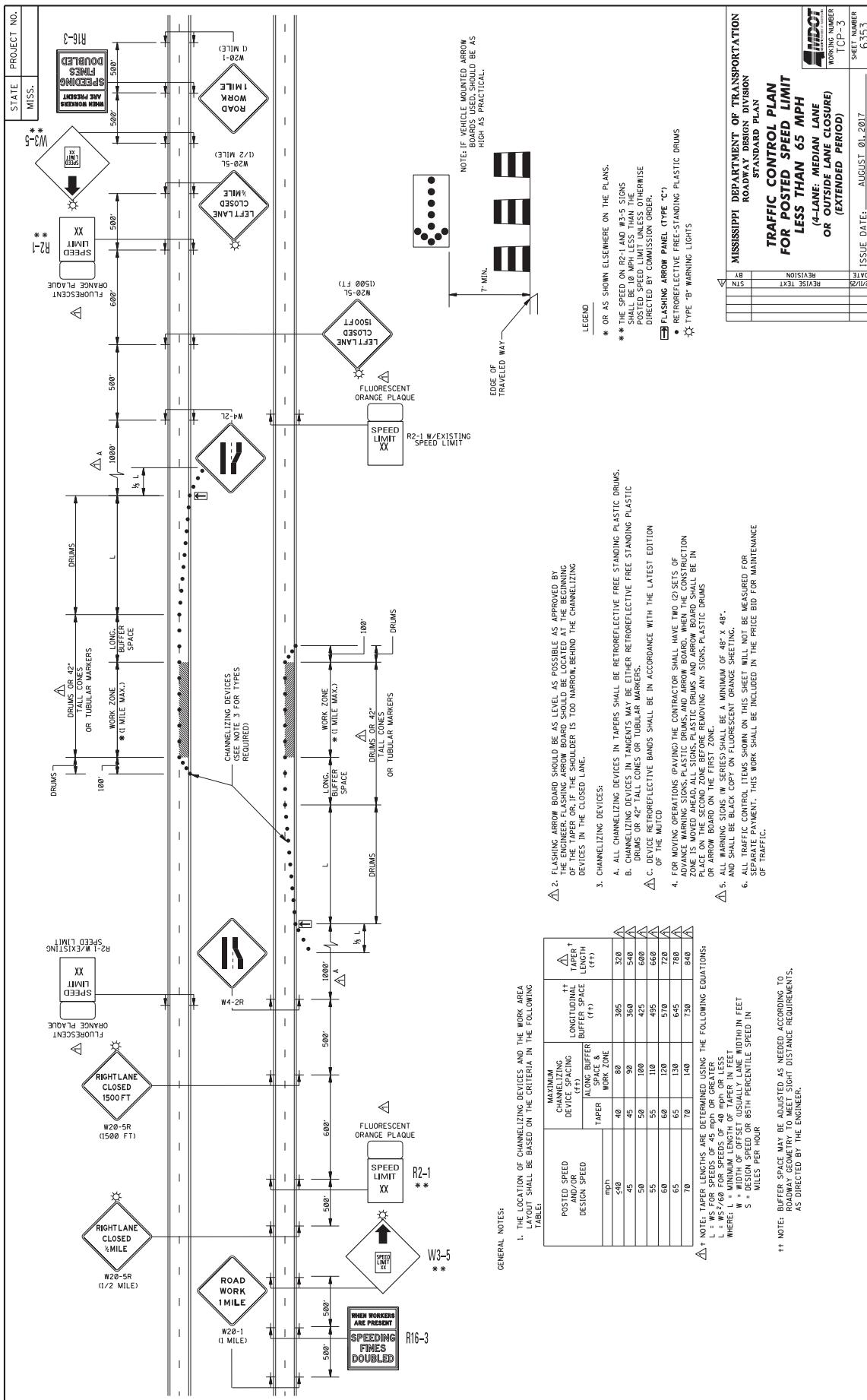


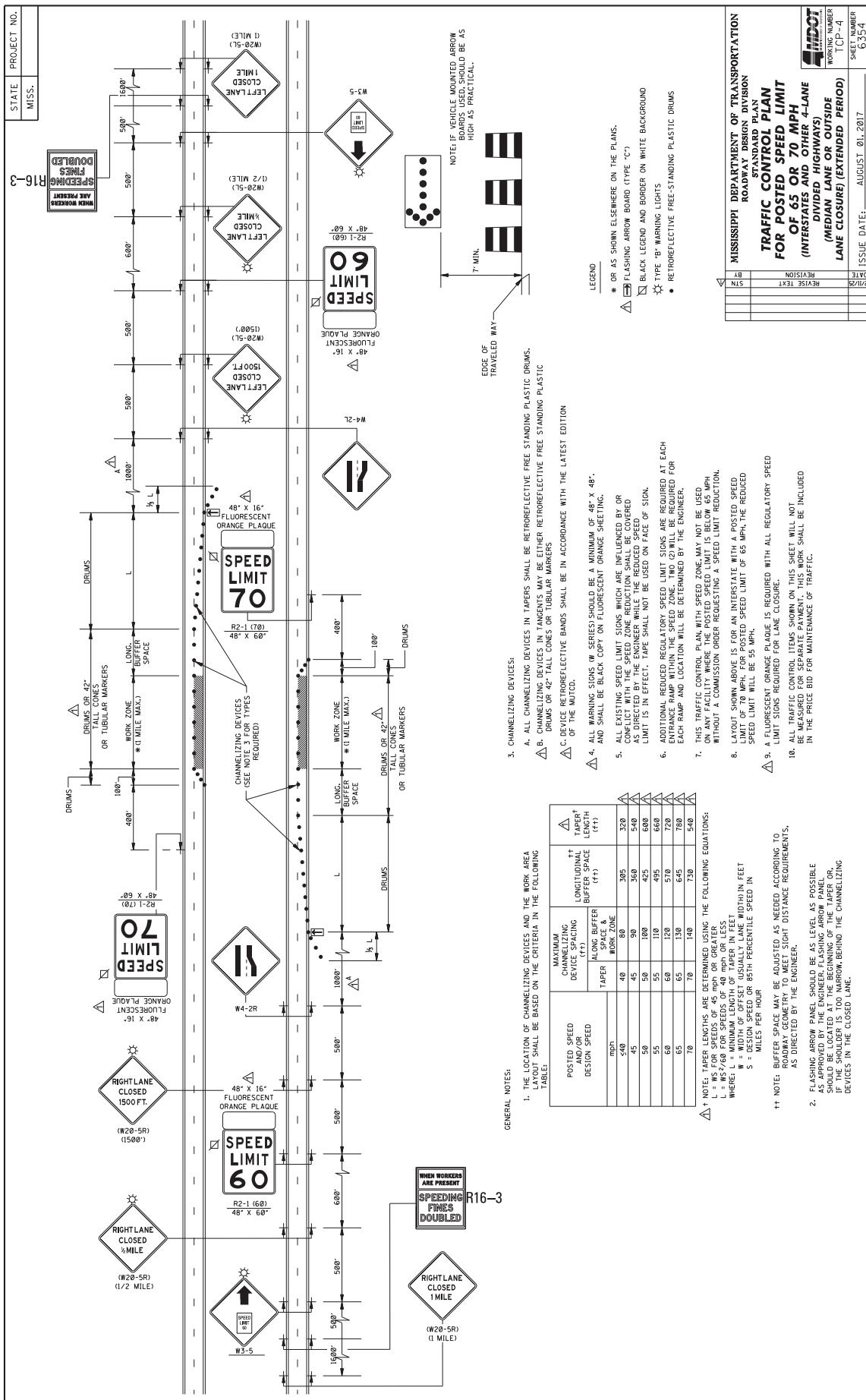


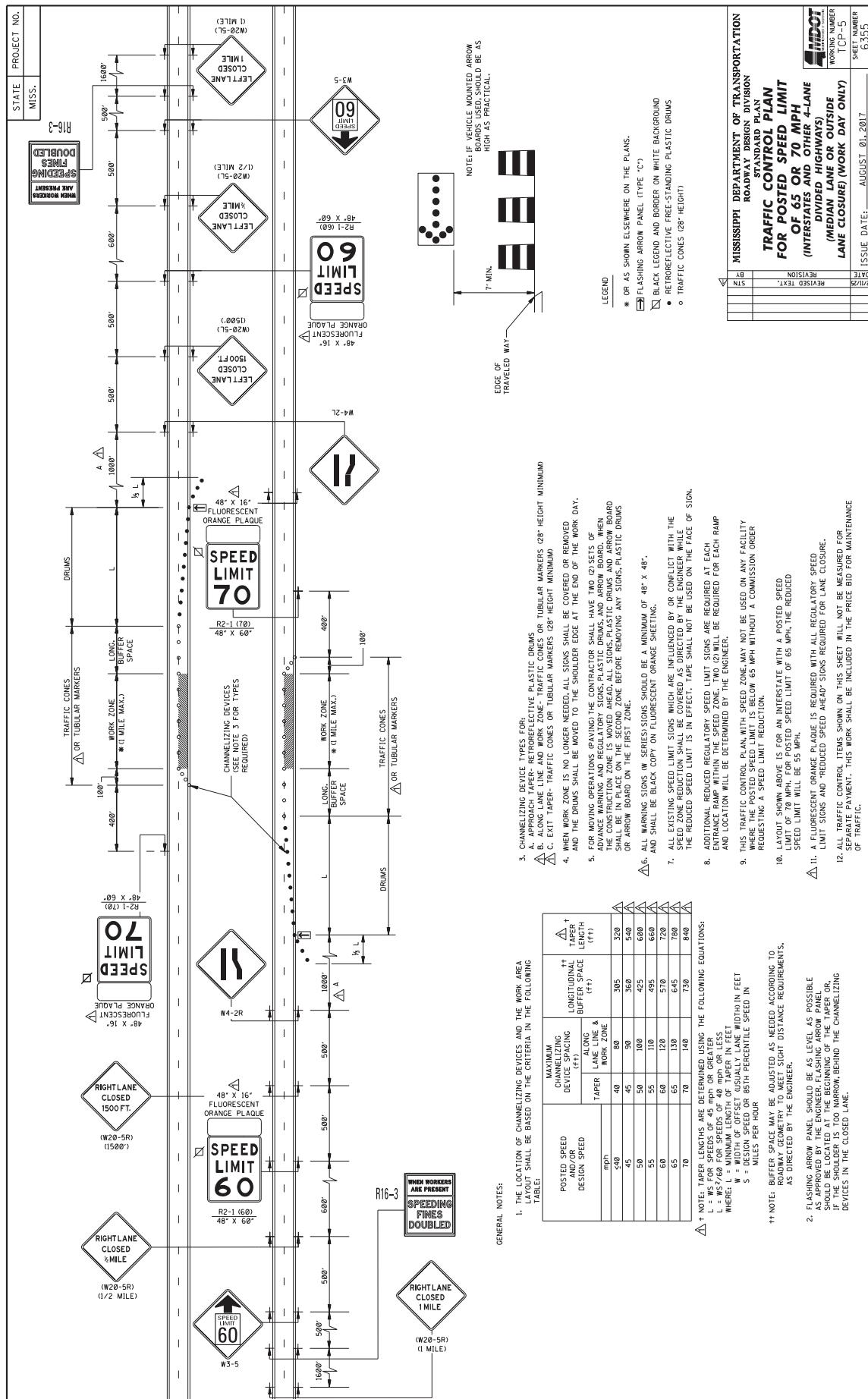












STATE: MISS.

PROJECT NO.:

LOCATION BASED ON ANTICIPATED QUEUE  $\frac{1}{2}$  MILE MINIMUM

EDGE LINES

WORK AREA

SECOND LAW ENFORCEMENT VEHICLE

ADVANCE LAW ENFORCEMENT VEHICLE

STOP R1-1 (48" x 48")

W3-1 (48" x 48") (SEE NOTE B)

TRAFFIC

CMS

FIRST CHANGEABLE MESSAGE SIGN (CMS) PHASE I - "ROAD CLOSED X MILE" PHASE II - "PREPARE TO STOP"

CMS

SECOND CHANGEABLE MESSAGE SIGN (CMS) PHASE I - "ROAD CLOSED 1/2 MILE" PHASE II - "PREPARE TO STOP"

GENERAL NOTES:

1. THIS TYPE OF HIGHWAY CLOSURE SHOULD NOT OCCUR ON BIG GROWING HIGHWAYS. WHEN THE DURATION OF CLOSURE WILL NOT EXCEED 30 MINUTES, AFTER THE HIGHWAY HAS BEEN CLOSED AND REOPENED, VEHICLE QUEUES SHOULD NOT EXCEED 10 MINUTES. SHOULD THE CLOSURE EXCEED 30 MINUTES, THE APPROVAL OF THE ENGINEER.
2. AT LEAST TWO LAW ENFORCEMENT OFFICERS AND TWO LAW ENFORCEMENT VEHICLES SHOULD BE PROVIDED ON EACH APPROACH TO THE CLOSURE. EACH LAW ENFORCEMENT VEHICLE SHOULD HAVE A ROOF-MOUNTED OPERATING FLASHING BLUE LIGHT OR LIGHT BAR.
3. RESTRICTIONS ON ROAD CLOSURES ARE SPECIFIED IN THE CONTRACT DOCUMENT.
4. THE ADVANCE LAW ENFORCEMENT VEHICLE SHOULD BE MOVED BACK AS REQUIRED BY THE QUEUING OF STOPPED VEHICLES.
5. IF QUEUE EXCEEDS THE FIRST CHANGEABLE MESSAGE SIGN CONS AT ANY TIME DURING A CLOSURE, THE TRAFFIC CONTROL PLAN SHOULD BE ADJUSTED AS NECESSARY, WITH APPROVAL OF THE ENGINEER.

△ 6. TRAFFIC CONTROL FOR THE CLOSURE SHOULD BE ACCOMPLISHED IN THE FOLLOWING ORDER.

△ A. FIRST CHANGEABLE MESSAGE SIGN (CMS)

△ B. SECOND CHANGEABLE MESSAGE SIGN (CMS)

△ C. ADVANCE LAW ENFORCEMENT VEHICLE LIGHTS AND FLASHERS ON.

△ D. "W3-1 48" x 48" ORANGE/BLACK TYPE XI RETROREFLECTIVE SHEETING SIGNS ERECTED.

△ E. "R1-1 48" x 48" SIGNS ERECTED OR INSTALLED ON TEMPORARY STANDS TO STOP TRAFFIC. THE ORDER OF ERECTION SHOULD BE IN THE FOLLOWING ORDER: RIGHT SHOULDER THEN CENTER. F. SECOND LAW ENFORCEMENT VEHICLE LIGHTS AND FLASHERS ON.

△ 7. TRAFFIC CONTROL SHOULD BE REMOVED IN THE FOLLOWING ORDER:

A. WITH TRAFFIC STOPPED REMOVE THE "R1-1 48" x 48" SIGNS TOWARD THE RIGHT SHOULDER IN THE FOLLOWING ORDER: CENTER, THEN RIGHT, THEN LEFT. NO SECOND LAW ENFORCEMENT VEHICLE LEADS TRAFFIC THROUGH WORK AREA.

△ B. AFTER ALL STOPPED VEHICLES HAVE STARTED MOVING, THE "W3-1 48" x 48" SIGNS SHALL BE REMOVED. THESE SIGNS MAY BE COVERED IF RE-USE IS IMMEDIATE.

△ C. AFTER ALL VEHICLES HAVE RESUMED APPROXIMATELY NORMAL SPEED, THE CHANGEABLE MESSAGE SIGNS SHALL BE TURNED OFF.

△ 8. UNILLUMINATED SECTIONS OF HIGHWAYS SHOULD NOT BE CLOSED DURING HOURS OF DARKNESS EXCEPT FOR WHEN THE HIGHWAY MUST BE CLOSED DURING THESE HOURS. WHEN THE HIGHWAY IS CLOSED DURING DARKNESS, A TYPE B HIGH INTENSITY FLASHING BARRICADE WARNING LIGHT SHALL BE USED ON EACH W3-1 SIGN.

△ 9. IF AN ENTRANCE RAMP IS LOCATED BETWEEN THE SECOND CMS AND R1-1, THE CMS AND W3-1 SIGNS SHALL ALSO BE ERECTED ON THE RAMP SHOULDER.

10. THE ABOVE DURATION WILL APPLY TO EACH APPROACH TO THE CLOSURE.

11. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE INCLUDED FOR SEPARATE PAYMENT. THIS WORK SHOULD BE INCLUDED IN THE PRICE BID. MANAGEMENT OF THE CLOSURE INCLUDING SECURING LAW ENFORCEMENT SERVICES.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
ROADWAY DESIGN DIVISION  
STANDARD PLAN

SHORT DURATION  
CLOSING OF TWO-LANE  
TWO-WAY HIGHWAYS

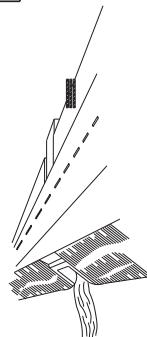
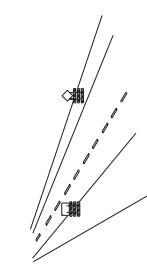
ISSUE DATE: AUGUST 01, 2017

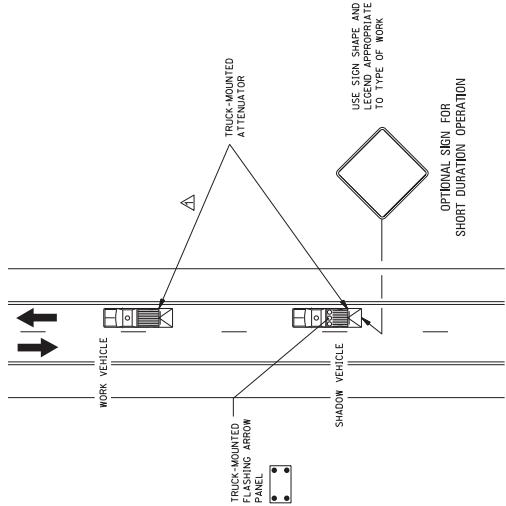
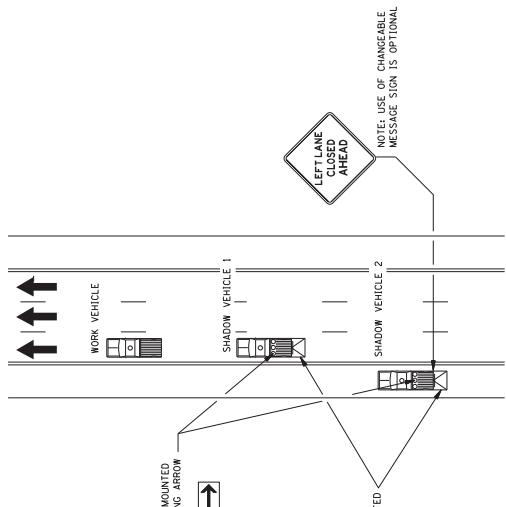
REVISED TEXT: 5/15/2017  
DATE: 5/15/2017  
MEETING: 5/15/2017  
ATTACH: 5/15/2017

WORKING NUMBER: TCP-6  
SHEET NUMBER: 6/25C

DOT WORKING NUMBER: TCP-6  
SHEET NUMBER: 6/25C



STATE MISS.	PROJECT NO.																								
 <p><b>WING BARRICADES</b></p> <p>WING BARRICADES ARE TYPE III BARRICADES ERECTED ON THE SHOULDER OF A ROAD OR RESTRICTED ROADWAY. BARRICADES MAY BE USED AS A MOUNTING OR A DISTANCE MARKING DEVICE OR FLASHERS.</p> <p>1. IN ADVANCE OF A CONSTRUCTION PROJECT, PROPOSED:</p> <ol style="list-style-type: none"> <li>IN ADVANCE OF ALL BRIDGE OR CULVER CROSSING OPERATIONS.</li> </ol>																									
 <p><b>BARRICADE CLOSING A ROAD</b></p> <p>1. BARRICADES ARE INTENDED FOR USE ON EXPRESSWAYS, FREEWAYS, AND OTHER HIGH SPEED FACING TRAFFIC. HAVE A MINIMUM OF 24' OF REFLECTIVE AREA.</p> <p>2. BARRICADES SHOULD BE USED:</p> <ol style="list-style-type: none"> <li>IN ADVANCE OF A CONSTRUCTION PROJECT, PROPOSED:</li> <li>IN ADVANCE OF ALL BRIDGE OR CULVER CROSSING OPERATIONS.</li> </ol>																									
<p><b>BARRICADE CHARACTERISTICS</b></p> <table border="1"> <thead> <tr> <th>TYPE</th> <th>WIDTH OF RAIL *</th> <th>LENGTH OF RAIL *</th> <th>WIDTH OF STRIPE *</th> <th>HEIGHT</th> <th>NUMBER OF BARRICADES/LOCATED RAIL FACES</th> </tr> </thead> <tbody> <tr> <td>I</td> <td>8' MIN. - 12' MAX.</td> <td>24' MIN. - 36' MAX.</td> <td>6"</td> <td>36' MIN.</td> <td>4 (TWO EACH DIRECTION)</td> </tr> <tr> <td>II</td> <td>8' MIN. - 12' MAX.</td> <td>24' MIN. - 36' MAX.</td> <td>6"</td> <td>60' MIN.</td> <td>3 IF FACING TRAFFIC IN ONE DIRECTION 6 IF FACING TRAFFIC IN TWO DIRECTIONS</td> </tr> <tr> <td>III</td> <td>8' MIN. - 12' MAX.</td> <td>48' MIN. / 56' MAX.</td> <td>6"</td> <td>60' MIN.</td> <td>3 IF FACING TRAFFIC IN ONE DIRECTION 6 IF FACING TRAFFIC IN TWO DIRECTIONS</td> </tr> </tbody> </table>		TYPE	WIDTH OF RAIL *	LENGTH OF RAIL *	WIDTH OF STRIPE *	HEIGHT	NUMBER OF BARRICADES/LOCATED RAIL FACES	I	8' MIN. - 12' MAX.	24' MIN. - 36' MAX.	6"	36' MIN.	4 (TWO EACH DIRECTION)	II	8' MIN. - 12' MAX.	24' MIN. - 36' MAX.	6"	60' MIN.	3 IF FACING TRAFFIC IN ONE DIRECTION 6 IF FACING TRAFFIC IN TWO DIRECTIONS	III	8' MIN. - 12' MAX.	48' MIN. / 56' MAX.	6"	60' MIN.	3 IF FACING TRAFFIC IN ONE DIRECTION 6 IF FACING TRAFFIC IN TWO DIRECTIONS
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<p><b>STANDARD BARRICADES</b></p> <p>1. THE RETROREFLECTIVE MARKING FOR BARRICADE RAILS SHALL BE ORANGE AND WHITE ISOLATING DOWNWARD AT AN ANGLE OF 45° IN THE DIRECTION THAT TRAFFIC IS TO PASS.</p> <p>2. RAIL STRIPE SHOULD BE 6 INCHES, EXCEPT THAT 1-INCH WIDE STRIPES MAY BE USED IF RAIL LENGTHS ARE LESS THAN 36 INCHES.</p> <p>3. DO NOT PLACE SANDbags OR OTHER DEVICES ON BARRICADE RAILS TO PROVIDE MASS. SANDbags, however, may be placed as ballast to the lower parts of the frame or the stays to barricades.</p> <p>4. FOR ADDITIONAL INFORMATION OR DETAILS SEE MUTCD, LATEST EDITION.</p> <p>5. BARRICADES ARE CLASSIFIED BY FMVSS 15, CATEGORY II, WORK ZONE DEVICES. A LIST OF TRANSMITTING BARRICADES, AND ONE, CATEGORY II DEVICES CAN BE FOUND ON FMVSS'S WEBSITE: <a href="http://highways.dot.gov/safety/road-reduce-risk/safety">http://highways.dot.gov/safety/road-reduce-risk/safety</a>.</p> <p>6. WHERE ROAD USERS INCLUDE PEDESTRIANS, THE PROVISION OF SUPPLEMENTAL AUDIBLE INFORMATION, OR DETECTABLE BARRIERS, OR BARRICADES SHOULD BE PROVIDED FOR PEOPLE WITH VISION DISABILITIES.</p> <p>7. BARRICADE RAIL SUPPORTS SHOULD NOT PROJECT INTO PROPOSED CIRCULATION AREAS ABOVE THAN 4 INCHES FOR THE SUPPORTS LOCATED BETWEEN 27 INCHES TO 66 INCHES ABOVE THE EXISTING SURFACE.</p>																									
<p><b>RETROREFLECTIVE MARKINGS</b></p> <p>1. THE RETROREFLECTIVE MARKING FOR BARRICADE RAILS SHALL BE ORANGE AND WHITE ISOLATING DOWNWARD AT AN ANGLE OF 45° IN THE DIRECTION THAT TRAFFIC IS TO PASS.</p> <p>2. RAIL STRIPE SHOULD BE 6 INCHES, EXCEPT THAT 1-INCH WIDE STRIPES MAY BE USED IF RAIL LENGTHS ARE LESS THAN 36 INCHES.</p> <p>3. DO NOT PLACE SANDbags OR OTHER DEVICES ON BARRICADE RAILS TO PROVIDE MASS. SANDbags, however, may be placed as ballast to the lower parts of the frame or the stays to barricades.</p> <p>4. FOR ADDITIONAL INFORMATION OR DETAILS SEE MUTCD, LATEST EDITION.</p> <p>5. BARRICADES ARE CLASSIFIED BY FMVSS 15, CATEGORY II, WORK ZONE DEVICES. A LIST OF TRANSMITTING BARRICADES, AND ONE, CATEGORY II DEVICES CAN BE FOUND ON FMVSS'S WEBSITE: <a href="http://highways.dot.gov/safety/road-reduce-risk/safety">http://highways.dot.gov/safety/road-reduce-risk/safety</a>.</p> <p>6. WHERE ROAD USERS INCLUDE PEDESTRIANS, THE PROVISION OF SUPPLEMENTAL AUDIBLE INFORMATION, OR DETECTABLE BARRIERS, OR BARRICADES SHOULD BE PROVIDED FOR PEOPLE WITH VISION DISABILITIES.</p> <p>7. BARRICADE RAIL SUPPORTS SHOULD NOT PROJECT INTO PROPOSED CIRCULATION AREAS ABOVE THAN 4 INCHES FOR THE SUPPORTS LOCATED BETWEEN 27 INCHES TO 66 INCHES ABOVE THE EXISTING SURFACE.</p>																									
<p><b>TYPE 3 OBJECT MARKER (OM-3-R)</b></p> <p>1. TYPE 3 OBJECT MARKERS SHALL BE USED AT ALL EXPOSED BRIDGE ABUTMENTS AND AT OTHER LOCATIONS AS DEEMED NECESSARY BY THE ENGINEER.</p> <p>2. THE OM-3-R IS SIMILAR EXCEPT THE STRIPES SLOPE DOWNWARD FROM THE UPPER LEFT SIDE TO THE LOWER RIGHT SIDE AND SHALL BE PLACED ON THE LEFT SIDE OF THE OBJECT.</p> <p>3. THE INSIDE EDGE OF THE MARKER SHALL BE IN LINE WITH THE INNER EDGE OF THE RESTRICTION.</p> <p>4. THE STRIPING SHALL CONSIST OF ALTERNATING BLACK AND RETROREFLECTIVE YELLOW SLOPING DOWNWARD TOWARD THE SIDE ON WHICH TRAFFIC IS THE PASS.</p>																									
<p><b>RETROREFLECTIVE CHEVRON ALIGNMENT SIGN DETAIL (W1-8)</b></p> <p>1. A CHEVRON SIGN CONSISTS OF A BLACK CHEVRON TYPE MARKING ON AN ORANGE BACKGROUND AND SHALL POINT IN THE DIRECTION OF TRAFFIC FLOW.</p> <p>2. THE CHEVRON SIGN SHALL BE MOUNTED ON CROSMORTY SUPPORT.</p> <p>3. CHEVRON SIGNS MAY BE USED TO SUPPLEMENT OTHER STANDARD DEVICES WHERE ONE OR MORE LANES ARE CLOSED FOR CONSTRUCTION OR MAINTENANCE. THEY SHOULD BE PLACED APPROXIMATELY 2' BEHIND THE LINE TRANSITION STRIPE.</p> <p>4. CHEVRON SIGNS SHALL BE INSTALLED AT A MINIMUM HEIGHT OF 4 FEET MEASURED VERTICALLY FROM THE BOTTOM OF THE SIGN TO THE ELEVATION OF THE NEAR EDGE OF THE TRAVELWAY.</p>																									
<p><b>MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN</b></p> <p><b>HIGHWAY SIGN AND BARRICADE DETAILS FOR CONSTRUCTION PROJECTS</b></p> <p>DATE: 01/01/2017 ISSUE DATE: AUGUST 01, 2017</p> <p>REVISION: BY: STN: 01/01/2017</p> <p>MDOT WORKING NUMBER: TCP-8 SHEET NUMBER: 6358</p>																									

STATE MISS.	PROJECT NO. TCI-9
MOBILE OPERATIONS ON TWO-LANE ROAD	
	
MOBILE OPERATIONS ON TWO-LANE ROAD	
<p>NOTES FOR TWO-LANE OPERATION:</p> <ol style="list-style-type: none"> <li>1. WHERE PRACTICAL AND WHEN NEEDED, THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS. THIS CAN BE DONE ON THE REAR OF THE VEHICLE BLOCKING THE LANE. DO NOT PASS SIGN MAY BE PLACED ON THE REAR OF THE VEHICLE BLOCKING THE LANE.</li> <li>2. THE DISTANCE BETWEEN THE WORK AND SHADOW VEHICLES MAY VARY ACCORDING TO TERRAIN, PAINT DRYING TIME, AND OTHER FACTORS. SHADOW VEHICLES ARE USED TO WARN TRAFFIC OF THE OPERATION AREA, WHENEVER ADEQUATE SHADING IS PROVIDED. SHADOW VEHICLES SHOULD NOT OBSTRUCT THE WORK VEHICLE. THE SHADOW VEHICLE SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.</li> <li>3. ADDITIONAL SHADOW VEHICLES TO WARN AND REDUCE THE SPEED OF ONCOMING TRAFFIC MAY BE USED. POLICE PATROL CARS MAY BE USED FOR THIS PURPOSE.</li> <li>4. A TRUCK-MOUNTED ATTENUATOR (TA) SHOULD BE USED ON THE SHADOW VEHICLE AND MAY BE USED ON THE WORK VEHICLE.</li> </ol>	
<p>MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN</p> <p><b>TRAFFIC CONTROL PLAN MOBILE OPERATIONS MULTILANE ROADS AND TWO-LANE ROADS</b></p>	
<p>DATE: 01/01/2017 REVISED TEXT AND DETAILS: 01/01/2017 SHEET NUMBER: 6359 ISSUE DATE: AUGUST 01, 2017</p>	
<p>MOBILE OPERATIONS ON MULTILANE ROAD</p>	
	
<p>NOTES FOR MULTILANE LANE OPERATION:</p> <ol style="list-style-type: none"> <li>1. SHADOW AND WORK VEHICLES SHALL DISPLAY HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING OR STROBE LIGHTS. AN ARROW BOARD SHALL BE USED WHEN A FREEWAY, HIGHWAY, OR SEPARATE SHOULDER IS USED. WHEN A FREEWAY, HIGHWAY, OR SEPARATE SHOULDER IS USED, A SEPARATE ARROW BOARD SHALL BE USED FOR EACH CLOSED LANE.</li> <li>2. SHADOW VEHICLE 2 SHOULD BE EQUIPPED WITH AN ARROW BOARD AND TWO TRUCK-MOUNTED ATTENUATORS. A WORK VEHICLE, SHADOW VEHICLE 1, AND SHADOW VEHICLE 2, MAY BE USED. SHADOW VEHICLE 2, ON SHADOW VEHICLE 1, AS NOT TO OBSCURE THE ARROW BOARD.</li> <li>3. SHADOW VEHICLE 1 SHOULD BE EQUIPPED WITH AN ARROW BOARD AND TRUCK-MOUNTED ATTENUATOR (TA).</li> <li>4. SHADOW VEHICLE 2 SHOULD TRAVEL AT A VARYING DISTANCE FROM THE WORK OPERATION SO AS TO PROVIDE ADEQUATE SIGHT DISTANCE FOR TRAFFIC APPROACHING FROM THE REAR.</li> <li>5. THE SPACING BETWEEN THE WORK VEHICLES AND THE SHADOW VEHICLES, AND BETWEEN EACH SHADOW VEHICLE, SHOULD BE MINIMIZED TO DEFER ROAD USERS FROM DRIVING IN BETWEEN.</li> <li>6. ON HIGH-SPEED ROADWAYS, A THIRD SHADOW VEHICLE NOT IN THE SAME LANE AS THE WORK VEHICLE, SHOULD BE USED. SHADOW VEHICLE 2, STANDING IN THE EDGE LINE, AND SHADOW VEHICLE 3, ON THE SHOULDER.</li> <li>7. ARROW BOARD SHALL BE AS A MINIMUM TYPE B, 60° X 30° IN ACCORDANCE WITH THE CRITERIA PRESENTED IN THE MUTCD.</li> <li>8. WORK SHOULD NORMALLY BE DONE DURING OFF-PEAK HOURS.</li> </ol>	

