

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO. <u> 1 </u>	DATED <u> 1/21/2026 </u>	ADDENDUM NO. _____	DATED _____
ADDENDUM NO. _____	DATED _____	ADDENDUM NO. _____	DATED _____
ADDENDUM NO. _____	DATED _____	ADDENDUM NO. _____	DATED _____

Number	Description
1	Revised Table of Contents; Delete NTB No. 5750; Revised NTB No. 7528; Revised Wage Rates; Revised or Added Plan Sheet Nos. Detail 106110301: 2, 6051, 6061, 6064, 6304-6306, 6314, 6317-6318, 6351, 6358-6359, 6362-6363, 6366, 8001, 8012-8013, 8015-8016; Details 106110302 & 303: 2, 6051, 6064, 6314, 6351, 6358-6359, 6362-6363 & 6366; Amendment EBSx Download Required.

TOTAL ADDENDA: 1
(Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

Contractor

BY _____
Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

_____ President	_____ Address
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_____ Secretary	_____ Address
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_____ Treasurer	_____ Address
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The following is my (our) itemized proposal.

STBG-0400-00(046) / 106110301000, STBG-0400-00(047) / 106110302000 & STBG-0400-00(048) / 106110303000
Warren, Issaquena & Warren County(ies)

Revised 01/26/2016

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION
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**PROJECT: STBG-0400-00(046) /106110301 - Warren
STBG-0400-00(047) /106110302 - Issaquena
STBG-0400-00(048)/106110303 - Warren**

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STBG-0400-00(047) /106110302 - Issaquena
STBG-0400-00(048)/106110303 - Warren**

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PROJECT: STBG-0400-00(046) /106110301 - Warren
STBG-0400-00(047) /106110302 - Issaquena
STBG-0400-00(048)/106110303 - Warren

Pile and Driving Equipment Data Form
Progress Schedule

(REVISIONS TO THE ABOVE WILL BE INDICATED ON THE SECOND SHEET
OF SECTION 905 AS ADDENDA)

01/20/2026 02:56 PM

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 7528

CODE: (SP)

DATE: 1/21/2026

SUBJECT: Oversize Construction Equipment-Material Movement Plan

**PROJECT: STBG-0400-00(046) / 106110301 – Warren County
STBG-0400-00(047) / 106110302 – Issaquena County
STBG-0400-00(048) / 106110303 – Warren County**

Bidders are hereby advised of the following weight restricted roads and bridges along the local routes to the project sites:

- MS 1 – Sharkey and Issaquena Counties
- MS 465 – Warren and Issaquena Counties
- MS 465 Bridge No. 15.7 – Muddy Bayou, Warren County
- MS 465 Bridge No. 8.7 – Steele Bayou Relief, Issaquena County
- MS 465 Bridge No. 6.3 – Steele Bayou, Issaquena County
- MS 465 Bridge No. 2.6 – Unnamed Creek, Warren County

The Contractor shall submit an Oversize Construction Equipment/Material Movement Plan to the Engineer prior to crossing any of the weight restricted roads and bridges. The plan shall include haul routes and load configurations and calculations for each proposed oversize equipment/material move.

"General Decision Number: MS20260124 01/02/2026

Superseded General Decision Number: MS20250124

State: Mississippi

Construction Type: Highway

Counties: Claiborne, Covington, Franklin, Issaquena, Sharkey, Walthall and Wilkinson Counties in Mississippi.

HIGHWAY CONSTRUCTION PROJECTS

Modification Number Publication Date
0 01/02/2026

SUMS2010-047 08/04/2014

	Rates	Fringes
CARPENTER (Form Work Only).....	\$ 12.26	0.12
CARPENTER, Excludes Form Work....	\$ 14.21	0.00
CEMENT MASON/CONCRETE FINISHER...	\$ 13.23	0.00
ELECTRICIAN.....	\$ 22.64	7.73
HIGHWAY/PARKING LOT STRIPING: Truck Driver (Line Striping Truck).....	\$ 12.63	0.00
INSTALLER - GUARDRAIL.....	\$ 11.42	0.00
INSTALLER - SIGN.....	\$ 12.04	0.00
IRONWORKER, REINFORCING.....	\$ 16.43	0.00
LABORER: Asphalt, Includes Raker, Shoveler, Spreader and Distributor.....	\$ 10.26	0.00
LABORER: Common or General, including concrete work.....	\$ 10.24	0.00
LABORER: Flagger.....	\$ 9.83	0.00
LABORER: Grade Checker.....	\$ 10.67	0.00
LABORER: Landscape.....	\$ 9.82	0.00
LABORER: Mason Tender - Cement/Concrete.....	\$ 11.69	0.00
LABORER: Pipelayer.....	\$ 13.13	0.00
LABORER: Laborer-Cones/ Barricades/Barrels - Setter/Mover/Sweeper.....	\$ 10.53	0.00
OPERATOR: Asphalt Spreader.....	\$ 16.13	0.00
OPERATOR:		

Backhoe/Excavator/Trackhoe.....	\$ 13.28	0.00
OPERATOR: Broom/Sweeper.....	\$ 10.17	0.00
OPERATOR: Bulldozer.....	\$ 13.30	0.00
OPERATOR: Concrete Saw.....	\$ 13.60	0.00
OPERATOR: Crane.....	\$ 16.00	0.00
OPERATOR: Distributor.....	\$ 11.70	0.00
OPERATOR: Drill.....	\$ 19.22	0.00
OPERATOR: Grader/Blade.....	\$ 13.84	0.00
OPERATOR: Loader.....	\$ 11.73	0.00
OPERATOR: Mechanic.....	\$ 16.28	0.00
OPERATOR: Milling Machine.....	\$ 15.38	0.00
OPERATOR: Mixer.....	\$ 14.85	0.00
OPERATOR: Oiler.....	\$ 13.08	0.48
OPERATOR: Paver (Asphalt, Aggregate, and Concrete).....	\$ 13.01	0.00
OPERATOR: Piledriver.....	\$ 15.13	0.00
OPERATOR: Roller (All Types)....	\$ 11.05	0.00
OPERATOR: Scraper.....	\$ 12.63	0.00
OPERATOR: Tractor.....	\$ 9.98	0.00
OPERATOR: Trencher.....	\$ 15.00	0.00
TRUCK DRIVER: Flatbed Truck.....	\$ 13.29	0.00
TRUCK DRIVER: Lowboy Truck.....	\$ 12.49	0.00
TRUCK DRIVER: Mechanic.....	\$ 12.35	0.00
TRUCK DRIVER: Off the Road Truck.....	\$ 12.31	0.00
TRUCK DRIVER: Water Truck.....	\$ 13.15	0.00
TRUCK DRIVER: Dump Truck (All Types).....	\$ 11.64	0.00
TRUCK DRIVER: Semi/Trailer Truck.....	\$ 17.34	0.00

WELDERS - Receive rate prescribed for craft performing operation to which welding is incidental.

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Note: Executive Order (EO) 13706, Establishing Paid Sick Leave for Federal Contractors applies to all contracts subject to the Davis-Bacon Act for which the contract is awarded (and any

solicitation was issued) on or after January 1, 2017. If this contract is covered by the EO, the contractor must provide employees with 1 hour of paid sick leave for every 30 hours they work, up to 56 hours of paid sick leave each year. Employees must be permitted to use paid sick leave for their own illness, injury or other health-related needs, including preventive care; to assist a family member (or person who is like family to the employee) who is ill, injured, or has other health-related needs, including preventive care; or for reasons resulting from, or to assist a family member (or person who is like family to the employee) who is a victim of, domestic violence, sexual assault, or stalking. Additional information on contractor requirements and worker protections under the EO is available at <https://www.dol.gov/agencies/whd/government-contracts>.

Note: Executive Order 13658 generally applies to contracts subject to the Davis-Bacon Act that were awarded on or between January 1, 2015 and January 29, 2022, and that have not been renewed or extended on or after January 30, 2022. Executive Order 13658 does not apply to contracts subject only to the Davis-Bacon Related Acts regardless of when they were awarded. If a contract is subject to Executive Order 13658, the contractor must pay all covered workers at least \$13.30 per hour (or the applicable wage rate listed on this wage determination, if it is higher) for all hours spent performing on the contract in 2025. The applicable Executive Order minimum wage rate will be adjusted annually. Additional information on contractor requirements and worker protections under Executive Order 13658 is available at www.dol.gov/whd/govcontracts.

Unlisted classifications needed for work not included within the scope of the classifications listed may be added after award only as provided in the labor standards contract clauses (29CFR 5.5 (a) (1) (iii)).

The body of each wage determination lists the classifications and wage rates that have been found to be prevailing for the type(s) of construction and geographic area covered by the wage determination. The classifications are listed in alphabetical order under rate identifiers indicating whether the particular rate is a union rate (current union negotiated rate), a survey rate, a weighted union average rate, a state adopted rate, or a supplemental classification rate.

Union Rate Identifiers

A four-letter identifier beginning with characters other than ""SU"", ""UAVG"", ?SA?, or ?SC? denotes that a union rate was prevailing for that classification in the survey. Example: PLUM0198-005 07/01/2024. PLUM is an identifier of the union whose collectively bargained rate prevailed in the survey for this classification, which in this example would be Plumbers. 0198 indicates the local union number or district council number where applicable, i.e., Plumbers Local 0198. The next number, 005 in the example, is an internal number used in processing the wage determination. The date, 07/01/2024 in the example, is the effective date of the most current negotiated rate.

Union prevailing wage rates are updated to reflect all changes

over time that are reported to WHD in the rates in the collective bargaining agreement (CBA) governing the classification.

Union Average Rate Identifiers

The UAVG identifier indicates that no single rate prevailed for those classifications, but that 100% of the data reported for the classifications reflected union rates. EXAMPLE:

UAVG-OH-0010 01/01/2024. UAVG indicates that the rate is a weighted union average rate. OH indicates the State of Ohio. The next number, 0010 in the example, is an internal number used in producing the wage determination. The date, 01/01/2024 in the example, indicates the date the wage determination was updated to reflect the most current union average rate.

A UAVG rate will be updated once a year, usually in January, to reflect a weighted average of the current rates in the collective bargaining agreements on which the rate is based.

Survey Rate Identifiers

The "SU" identifier indicates that either a single non-union rate prevailed (as defined in 29 CFR 1.2) for this classification in the survey or that the rate was derived by computing a weighted average rate based on all the rates reported in the survey for that classification. As a weighted average rate includes all rates reported in the survey, it may include both union and non-union rates. Example: SUFL2022-007 6/27/2024. SU indicates the rate is a single non-union prevailing rate or a weighted average of survey data for that classification. FL indicates the State of Florida. 2022 is the year of the survey on which these classifications and rates are based. The next number, 007 in the example, is an internal number used in producing the wage determination. The date, 6/27/2024 in the example, indicates the survey completion date for the classifications and rates under that identifier.

?SU? wage rates typically remain in effect until a new survey is conducted. However, the Wage and Hour Division (WHD) has the discretion to update such rates under 29 CFR 1.6(c)(1).

State Adopted Rate Identifiers

The "SA" identifier indicates that the classifications and prevailing wage rates set by a state (or local) government were adopted under 29 C.F.R 1.3(g)-(h). Example: SAME2023-007 01/03/2024. SA reflects that the rates are state adopted. ME refers to the State of Maine. 2023 is the year during which the state completed the survey on which the listed classifications and rates are based. The next number, 007 in the example, is an internal number used in producing the wage determination. The date, 01/03/2024 in the example, reflects the date on which the classifications and rates under the ?SA? identifier took effect under state law in the state from which the rates were adopted.

WAGE DETERMINATION APPEALS PROCESS

1) Has there been an initial decision in the matter? This can be:

- a) a survey underlying a wage determination

- b) an existing published wage determination
- c) an initial WHD letter setting forth a position on a wage determination matter
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On survey related matters, initial contact, including requests for summaries of surveys, should be directed to the WHD Branch of Wage Surveys. Requests can be submitted via email to davisbaconinfo@dol.gov or by mail to:

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2) If an initial decision has been issued, then any interested party (those affected by the action) that disagrees with the decision can request review and reconsideration from the Wage and Hour Administrator (See 29 CFR Part 1.8 and 29 CFR Part 7). Requests for review and reconsideration can be submitted via email to dba.reconsideration@dol.gov or by mail to:

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The request should be accompanied by a full statement of the interested party's position and any information (wage payment data, project description, area practice material, etc.) that the requestor considers relevant to the issue.

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END OF GENERAL DECISION

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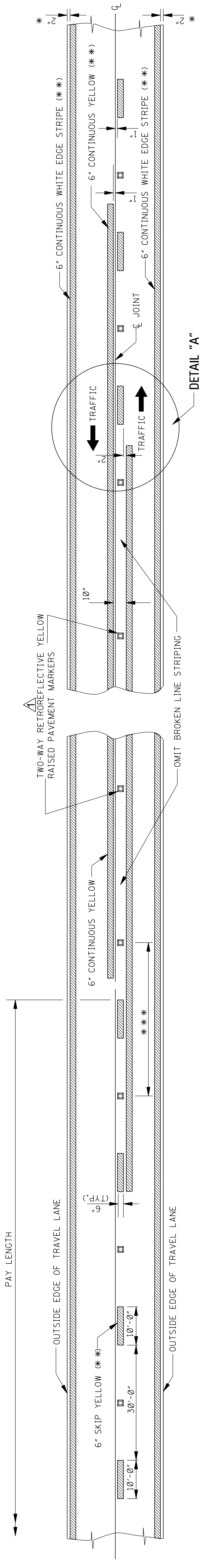
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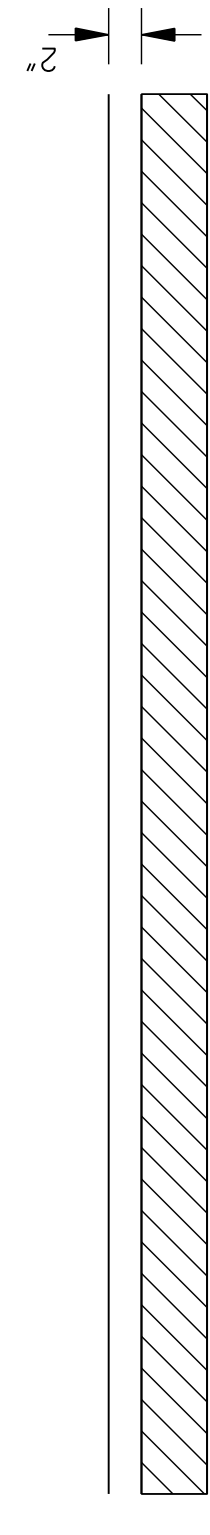
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END OF GENERAL DECISION

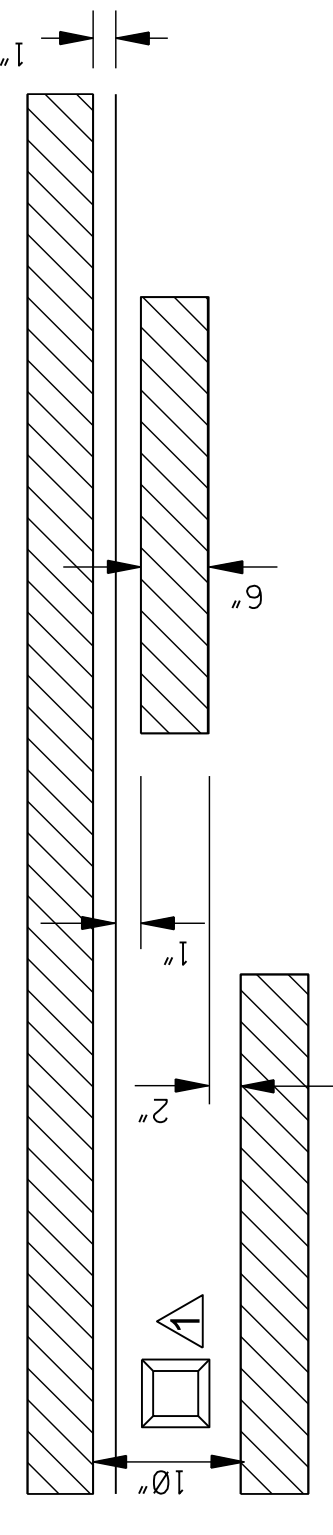
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TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)



TRAFFIC ←



TRAFFIC →

DETAIL "A"

NOTE: THE CRITERIA FOR NO-PASSING ZONES CAN BE FOUND IN THE MDOT ROADWAY DESIGN MANUAL, SECTION II-1.01.

DIRECTION OF TRAFFIC
← →

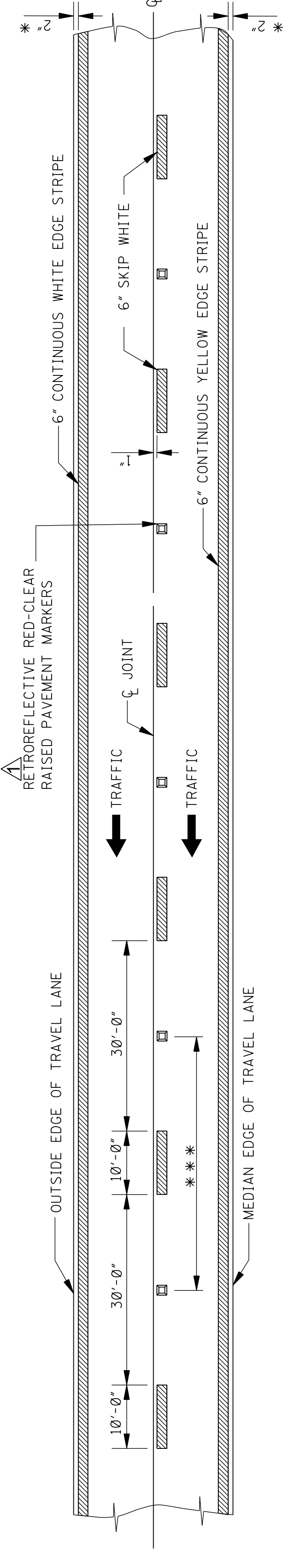
GENERAL NOTES:

- * 1. 2" UNLESS SHOWN ELSEWHERE ON THE PLANS, FOR STRIPING ON RUMBLE STRIP SECTIONS REFER TO WK. SHEETS RS-1, RS-2, AND RS-3.
- ** 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR PLASTIC AS INDICATED IN PAY ITEMS).
- *** 3. SPACING OF RETROREFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

	URBAN AREA (ft-in)	RURAL AREA (ft-in)
TANGENT SECTIONS	40'-0"	80'-0"
HORIZONTAL CURVES	40'-0"	40'-0"
INTERCHANGE LIMITS	40'-0"	+ 40'-0"

+ NOTE: ON THE MAIN FACILITY, RETROREFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINES THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.

- ▲ 4. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RETROREFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MDOT "APPROVED PRODUCTS LIST."

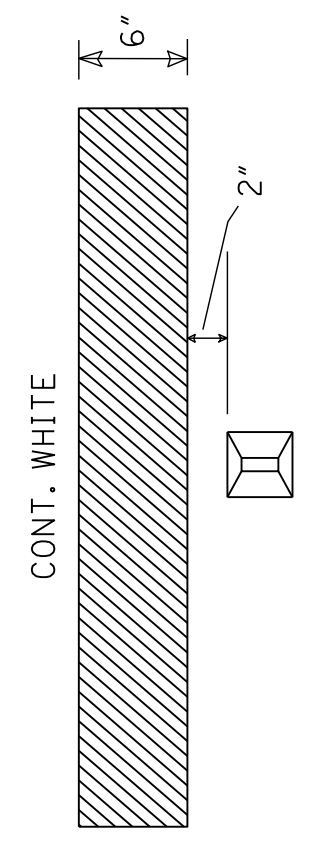
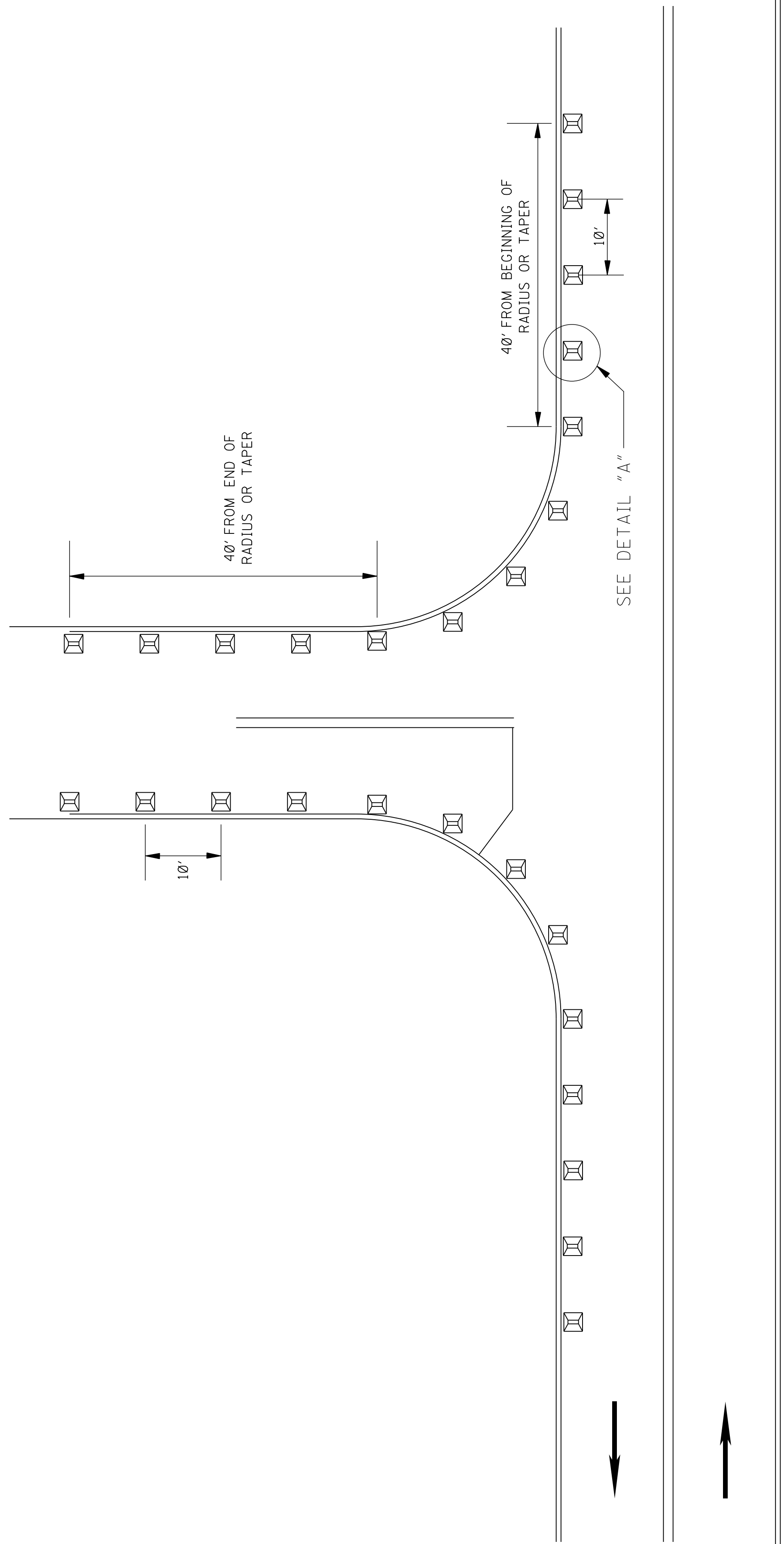


4-LANE WITH ONE-WAY TRAFFIC

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**PAVEMENT MARKING
DETAILS FOR
2-LANE AND 4-LANE
DIVIDED ROADWAYS**

△ TYPICAL PLACEMENT OF RETROREFLECTIVE RAISED PAVEMENT MARKERS ON SIDE ROAD RADIUS 2-LANE, TWO WAY TRAFFIC



DETAIL A

→ DIRECTION OF TRAFFIC

GENERAL NOTES:

- △ 1. RETROREFLECTIVE FACES OF MARKERS SHALL BE VISIBLE TO THE TRAVELING MOTORIST ON STATE DESIGNATED HIGHWAYS.
- △ 2. RETROREFLECTIVE FACES OF MARKERS SHALL BE HIGH PERFORMANCE TWO-WAY CLEAR.
- △ 3. RETROREFLECTIVE FACES OF MARKERS SHALL NOT BE ROTATED WHEN BEING PLACED ALONG RADIUS AND TANGENT SECTION OF LOCAL ROAD.
- △ 4. MARKERS SHALL BE INSTALLED TO THE LIMITS SHOWN ABOVE.

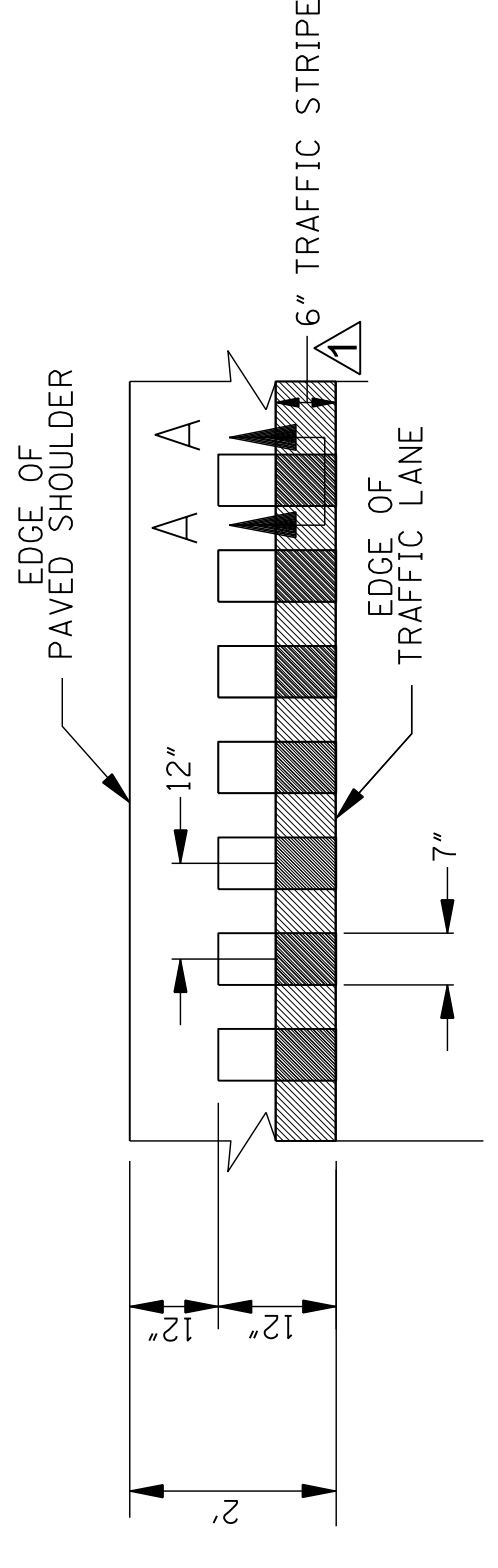
DATE	12/11/25	REVISION	REVISED TEXT.
BY	STN		

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

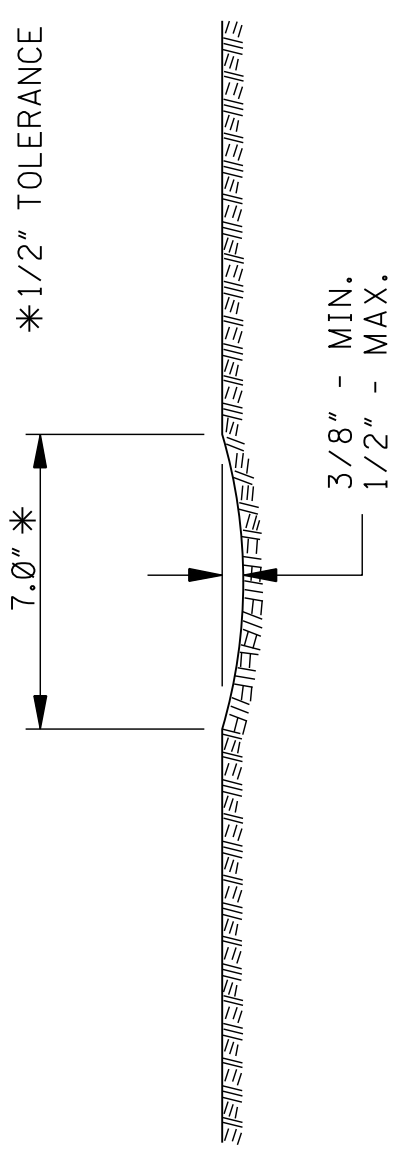
△ **2-WAY RETROREFLECTIVE RAISED PAVEMENT MARKERS AT INTERSECTING ROADS (2-LANE)**

GENERAL NOTES

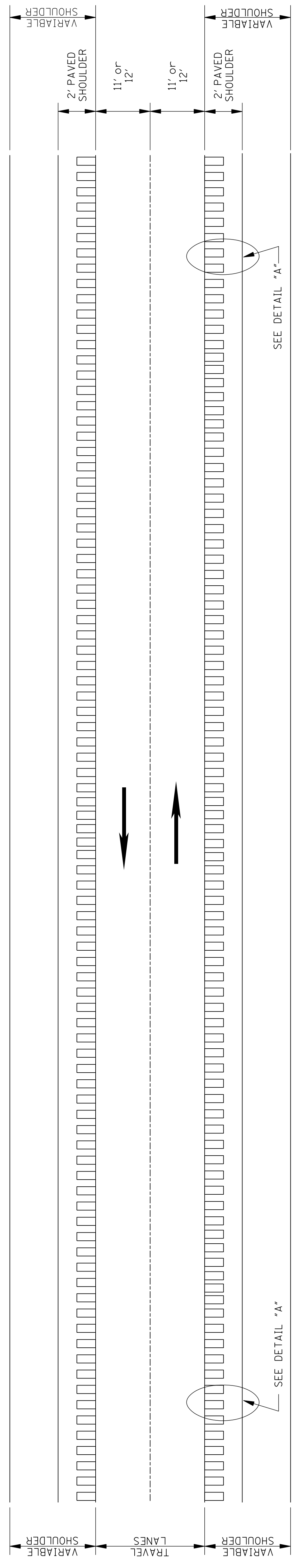
- GROUND-IN RUMBLE STRIPES SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT
- GROUND-IN RUMBLE STRIPES SHALL BE OMITTED ACROSS PUBLIC INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
- COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS
- GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO:
 - MAINLINE
 - INTERSECTING ROADWAY IF OVERLAID OR RECONSTRUCTED BEYOND NORMAL MAINLINE R.O.W.
 - ANY ROADWAY WITH EXISTING RUMBLE STRIPES PRIOR TO CONSTRUCTION.
- OMIT GROUND-IN RUMBLE STRIPS WHERE TRAFFIC LANE IS LESS THAN 11 FEET WIDE.



DETAIL "A"



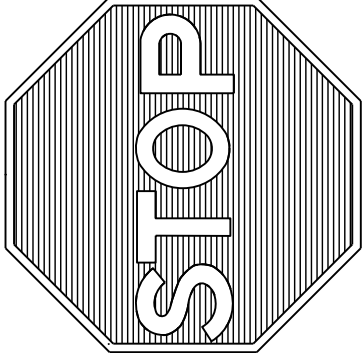
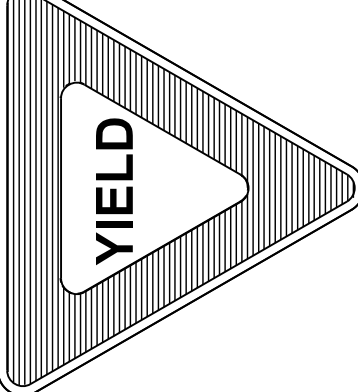
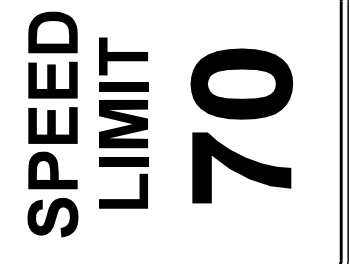

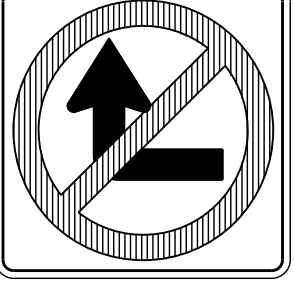
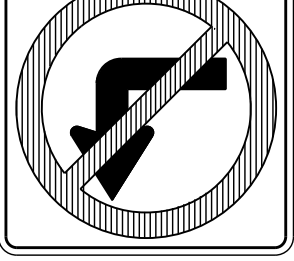
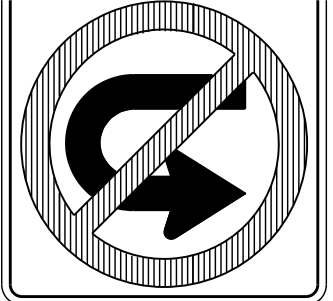
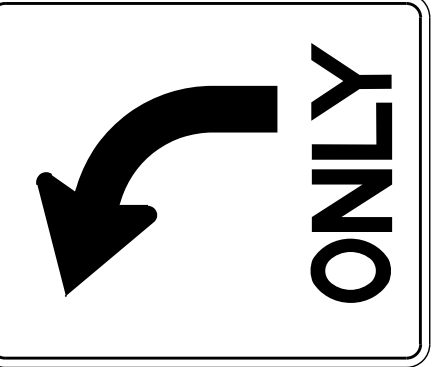
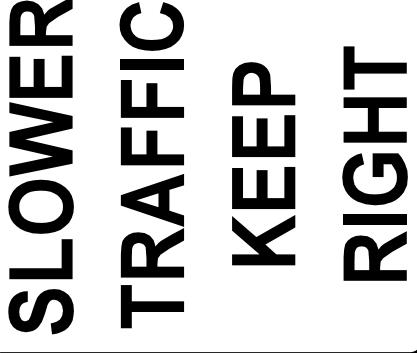
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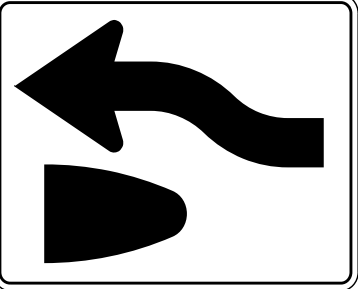
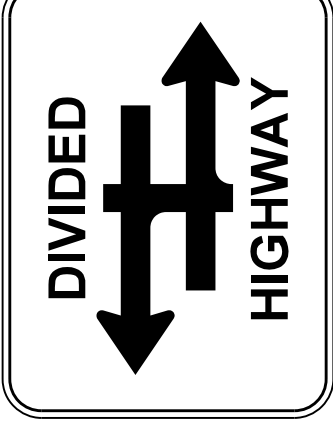
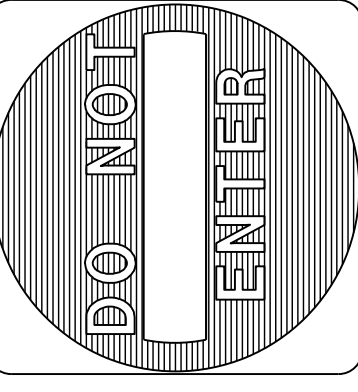
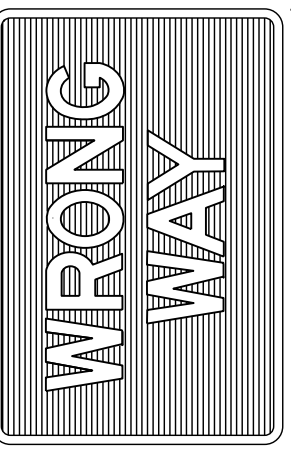


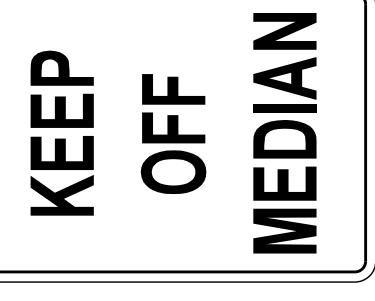


PLAN
NOT TO SCALE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	WORKING NUMBER RS-1	SHEET NUMBER 6064
DATE 12/11/25	REVISION REVISED TEXT AND STRIPE DETAIL.	BY STN
ISSUE DATE: AUGUST 01, 2017		

**RUMBLE STRIPES
2-LANE HIGHWAYS,
(ASPHALT LANES,
2-FT ASPHALT SHOULDERS)**

SIGN NUMBER	R1-1		R1-2		R2-1		R2-40		R3-1		R3-2		R3-4		R3-5		R4-3	
	0.100"	0.125"	0.080"	0.100"	0.080"	0.100"	0.125"	0.100"	0.080"	0.100"	0.080"	0.100"	0.080"	0.100"	0.080"	0.100"	0.125"	0.100"
ALUMINUM (6061-T6) SIGN BLANK THICKNESS																		
LEGEND	        																	
LETTER & NUMERAL SERIES	12" SERIES "C"		3" SERIES "C"		4" SERIES "E"		8" SERIES "E"		4" SERIES "D"		6" SERIES "D"		6" SERIES "D"		6" SERIES "D"		8" SERIES "D"	
WIDTH OF BORDER OUTSIDE	7/8" WHITE		1/4" WHITE		5" RED 3/4" WHITE		6" RED 1" WHITE		8" RED 1 1/2" WHITE		5/8" BLACK 3/8" WHITE		7/8" BLACK 5/8" WHITE		3/4" BLACK 1/2" WHITE		7/8" BLACK 5/8" WHITE	
SIZE (WIDTH X HEIGHT)	36" OCTAGON		48" OCTAGON		36" X 30"		36" X 48"		48" X 96"		24" X 24"		24" X 24"		36" X 36"		30" X 48"	
COLORS	WHITE		WHITE		RED		RED		BLACK		BLACK & RED		BLACK & RED		BLACK & RED		BLACK	
REFLECTORIZATION	ALL		ALL		ALL		ALL		BLACK		BLACK & RED		BLACK & RED		BLACK & RED		BLACK	
NUMBER OF POSTS FOR MOUNTING	1		1		1		1		1		1		1		1		1	
NUMBER OF HOLES TO BE PUNCHED (3/8" DIA.)	2		4		2		4		10		2		2		2		4	
PUNCHING DISTANCE FROM EACH VERTICAL EDGE	18" (VERT. CENTER)		12" (FROM CENTER)		12" (VERT. CENTER)		12" (VERT. CENTER)		12" (VERT. CENTER)		12" (VERT. CENTER)		12" (VERT. CENTER)		15" (VERT. CENTER)		15" (VERT. CENTER)	
PUNCHING DISTANCE FROM TOP EDGE	3"; 33"		3"; 15"		3"; 27"		3"; 27"		4"; 30"; 56"		3"; 21"		3"; 21"		6"; 30"		9"; 39"	

SIGN NUMBER	R4-7		R6-3		R5-1		R5-1a		R6-2L, R6-2R		R8-4		R11-1					
	0.080"	0.125"	0.080"	0.125"	0.100"	0.125"	0.100"	0.080"	0.080"	0.100"	0.080"	0.125"	0.100"	0.125"				
ALUMINUM (6061-T6) SIGN BLANK THICKNESS																		
LEGEND	      																	
LETTER & NUMERAL SERIES	R=1/2"		R=3"		R=1/8"		R=3"		R=1/8"		R=1/2"		R=2/4"		R=3"			
WIDTH OF BORDER OUTSIDE	5/8" BLACK 3/8" WHITE		1/4" BLACK 3/4" WHITE		4" SERIES "D" WHITE OUT-SIDE BORDER		6" SERIES "D" WHITE OUT-SIDE BORDER		8" SERIES "D" 1" WHITE		6" SERIES "D" 5/8" BLACK 3/8" WHITE		4" SERIES "D" 5/8" BLACK 3/8" WHITE		6" SERIES "D" 7/8" BLACK 5/8" WHITE		8" SERIES "C" 10" SERIES "C"	
SIZE (WIDTH X HEIGHT)	24" X 30"		36" X 48"		36" X 36"		48" X 48"		42" X 30"		24" X 30"		30" X 24"		48" X 36"		36" X 48"	
COLORS	BLACK		BLACK		WHITE		WHITE		WHITE		BLACK		BLACK		BLACK		BLACK	
REFLECTORIZATION	BACKGROUND		BACKGROUND		ALL		ALL		ALL		BLACK		BLACK		BLACK		BACKGROUND	
NUMBER OF POSTS FOR MOUNTING	1		1		1		1		1		1		1		1		1	
NUMBER OF HOLES TO BE PUNCHED (3/8" DIA.)	2		6		2		4		4		2		2		2		4	
PUNCHING DISTANCE FROM EACH VERTICAL EDGE	12" (VERT. CENTER)		15" (FROM CENTER)		18" (VERT. CENTER)		12" (VERT. CENTER)		12" (VERT. CENTER)		12" (VERT. CENTER)		12" (VERT. CENTER)		15" (VERT. CENTER)		15" (VERT. CENTER)	
PUNCHING DISTANCE FROM TOP EDGE	3"; 27"		3"; 15"		3"; 27"		3"; 27"		4"; 30"; 56"		3"; 21"		3"; 21"		6"; 30"		9"; 39"	

GENERAL NOTES:

- THE QUANTITIES LISTED ON THE SUMMARY OF QUANTITIES SHEET FOR THE SIGNS SHOWN ON THIS SHEET WILL BE USED AS THE BASIS FOR FINAL PAYMENT, EXCEPT WHERE SIGNS ARE MODIFIED FROM THAT SHOWN.
- THE SPEED LIMITS REQUIRED ON SIGNS R2-1 AND R2-40 WILL BE SHOWN ON INDIVIDUAL PLAN SHEETS.
- THE DESIGN OF ALL STANDARD ROADSIDE SIGNS SHALL CONFORM TO THE CRITERIA IN THE LATEST VERSION OF THE STANDARD HIGHWAY SIGNS MANUAL IN CONJUNCTION WITH THE LATEST VERSION OF THE MUTCD.

DATE	REVISION
12/1/25	REVISED TEXT

ISSUE DATE: AUGUST 01, 2017
SHEET NUMBER 6304

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

STANDARD ROADSIDE SIGNS

MPO
MISSISSIPPI
WORKING NUMBER
SN-3A
SHEET NUMBER
6304

ADDENDUM

STATE PROJECT NO.		MISS.		W3-3		W3-2		W3-1		W1-7		W1-6L W1-6R		W1-4L W1-4R		W1-3L W1-3R		W1-2L W1-2R		W1-1L W1-1R		W4-1L W4-1R		W4-2L W4-2R			
		0.125"		0.125"		0.125"		0.125"		0.100"		0.100"		0.125"		0.125"		0.125"		0.125"		0.125"		0.125"		0.125"	
SIGN NUMBER		W1-1L W1-1R		W1-2L W1-2R		W1-3L W1-3R		W1-4L W1-4R		W1-6L W1-6R		W1-7		W3-1		W3-2		W3-3		W4-1L W4-1R		W4-2L W4-2R		W4-1L W4-1R		W4-2L W4-2R	
ALUMINUM (6061-T6) SIGN BLANK THICKNESS		0.125"		0.125"		0.125"		0.125"		0.100"		0.100"		0.125"		0.125"		0.125"		0.125"		0.125"		0.125"		0.125"	
LEGEND																											
LETTER & NUMERAL SERIES		R=2 1/4" R=3"		R=2 1/4" R=3"		R=2 1/4" R=3"		R=2 1/4" R=3"		R=1 1/8" R=1 1/8"		R=1 1/8" R=1 1/8"		R=2 1/4" R=3"		R=2 1/4" R=3"		R=2 1/4" R=3"		R=2 1/4" R=3"		R=2 1/4" R=3"		R=2 1/4" R=3"		R=2 1/4" R=3"	
WIDTH OF BORDER		1/4" BLACK 3/4" YELLOW		1/4" BLACK 3/4" YELLOW		1/4" BLACK 3/4" YELLOW		1/4" BLACK 3/4" YELLOW		3/4" BLACK 1/2" YELLOW		3/4" BLACK 1/2" YELLOW		1/4" BLACK 3/4" YELLOW		1/4" BLACK 3/4" YELLOW		1/4" BLACK 3/4" YELLOW		1/4" BLACK 3/4" YELLOW		1/4" BLACK 3/4" YELLOW		1/4" BLACK 3/4" YELLOW		1/4" BLACK 3/4" YELLOW	
SIZE (WIDTH X HEIGHT)		36" X 36" 48" X 48"		36" X 36" 48" X 48"		36" X 36" 48" X 48"		36" X 36" 48" X 48"		48" X 24" 48" X 24"		48" X 24" 48" X 24"		36" X 36" 48" X 48"		36" X 36" 48" X 48"		36" X 36" 48" X 48"		36" X 36" 48" X 48"		36" X 36" 48" X 48"		36" X 36" 48" X 48"		36" X 36" 48" X 48"	
COLORS		BLACK YELLOW		BLACK YELLOW		BLACK YELLOW		BLACK YELLOW		BLACK YELLOW		BLACK YELLOW		BLACK YELLOW		BLACK YELLOW		BLACK YELLOW		BLACK YELLOW		BLACK YELLOW		BLACK YELLOW		BLACK YELLOW	
REFLECTORIZATION		BACKGROUND		BACKGROUND		BACKGROUND		BACKGROUND		BACKGROUND		BACKGROUND		BACKGROUND & SYMBOL		BACKGROUND & SYMBOL		BACKGROUND & SYMBOL		BACKGROUND & SYMBOL		BACKGROUND & SYMBOL		BACKGROUND & SYMBOL		BACKGROUND & SYMBOL	
NUMBER OF POSTS FOR MOUNTING		1		1		1		1		2		2		1		1		1		1		1		1		1	
NUMBER OF HOLES TO BE PUNCHED (3/8" DIA.)		2		2		2		2		4		4		2		2		2		2		2		2		2	
PUNCHING DISTANCE FROM EACH VERT. EDGE		15" FROM VERT. CENTER		15" FROM VERT. CENTER		15" FROM VERT. CENTER		15" FROM VERT. CENTER		9"		9"		15" FROM VERT. CENTER		15" FROM VERT. CENTER		15" FROM VERT. CENTER		15" FROM VERT. CENTER		15" FROM VERT. CENTER		15" FROM VERT. CENTER		15" FROM VERT. CENTER	
PUNCHING DISTANCE FROM TOP EDGE		18" FROM HORIZ. CENTER		18" FROM HORIZ. CENTER		18" FROM HORIZ. CENTER		18" FROM HORIZ. CENTER		3"; 21"		3"; 21"		18" FROM HORIZ. CENTER		18" FROM HORIZ. CENTER		18" FROM HORIZ. CENTER		18" FROM HORIZ. CENTER		18" FROM HORIZ. CENTER		18" FROM HORIZ. CENTER		18" FROM HORIZ. CENTER	
SIGN NUMBER		W4-2		W6-1		W6-2		W6-3		W13-1		W13-2		W13-3		W10-1											
ALUMINUM (6061-T6) SIGN BLANK THICKNESS		0.125"		0.125"		0.125"		0.125"		0.080"		0.125"		0.125"		0.100"											
LEGEND																											
LETTER & NUMERAL SERIES		R=2 1/4" R=3"		R=2 1/4" R=3"		R=2 1/4" R=3"		R=2 1/4" R=3"		R=1 1/2" R=1 1/2"		R=3" R=3"		R=3" R=3"		R=3" R=3"		R=3" R=3"		R=3" R=3"		R=3" R=3"		R=3" R=3"		R=3" R=3"	
WIDTH OF BORDER		1/4" BLACK 3/4" YELLOW		1/4" BLACK 3/4" YELLOW		1/4" BLACK 3/4" YELLOW		1/4" BLACK 3/4" YELLOW		3/8" BLACK 3/8" YELLOW		3/8" BLACK 3/8" YELLOW		1/4" BLACK 3/4" YELLOW		1/4" BLACK 3/4" YELLOW		1/4" BLACK 3/4" YELLOW		1/4" BLACK 3/4" YELLOW		1/4" BLACK 3/4" YELLOW		1/4" BLACK 3/4" YELLOW		1/4" BLACK 3/4" YELLOW	
SIZE (WIDTH X HEIGHT)		36" X 36" 48" X 48"		36" X 36" 48" X 48"		36" X 36" 48" X 48"		36" X 36" 48" X 48"		24" X 24" 48" X 24"		48" X 60" 48" X 60"		48" X 60" 48" X 60"		48" X 60" 48" X 60"		48" X 60" 48" X 60"		48" X 60" 48" X 60"		48" X 60" 48" X 60"		48" X 60" 48" X 60"		48" X 60" 48" X 60"	
COLORS		BLACK YELLOW		BLACK YELLOW		BLACK YELLOW		BLACK YELLOW		BLACK YELLOW		BLACK YELLOW		BLACK YELLOW		BLACK YELLOW		BLACK YELLOW		BLACK YELLOW		BLACK YELLOW		BLACK YELLOW		BLACK YELLOW	
REFLECTORIZATION		BACKGROUND		BACKGROUND		BACKGROUND		BACKGROUND		BACKGROUND		BACKGROUND		BACKGROUND		BACKGROUND		BACKGROUND		BACKGROUND		BACKGROUND		BACKGROUND		BACKGROUND	
NUMBER OF POSTS FOR MOUNTING		1		1		1		1		1		1		1		1		1		1		1		1		1	
NUMBER OF HOLES TO BE PUNCHED (3/8" DIA.)		2		2		2		2		2		2		2		2		2		2		2		2		2	
PUNCHING DISTANCE FROM EACH VERT. EDGE		15" FROM VERT. CENTER		15" FROM VERT. CENTER		15" FROM VERT. CENTER		15" FROM VERT. CENTER		12" (VERT. CENTER)		9"		9"		15" FROM VERT. CENTER		15" FROM VERT. CENTER		15" FROM VERT. CENTER		15" FROM VERT. CENTER		15" FROM VERT. CENTER		15" FROM VERT. CENTER	
PUNCHING DISTANCE FROM TOP EDGE		18" FROM HORIZ. CENTER		18" FROM HORIZ. CENTER		18" FROM HORIZ. CENTER		18" FROM HORIZ. CENTER		3"; 21"		4"; 30"; 56"		4"; 30"; 56"		15" FROM HORIZ. CENTER		15" FROM HORIZ. CENTER		15" FROM HORIZ. CENTER		15" FROM HORIZ. CENTER		15" FROM HORIZ. CENTER		15" FROM HORIZ. CENTER	

GENERAL NOTES:

- THE QUANTITIES LISTED ON THE SUMMARY OF QUANTITIES SHEET FOR THE SIGNS SHOWN ON THIS SHEET WILL BE USED AS THE BASIS FOR FINAL PAYMENT, EXCEPT WHERE SIGNS ARE MODIFIED FROM THAT SHOWN.
- SIGNS W13-2 AND W13-3- THE STROKE WIDTH OF THE LETTER AND NUMERALS SHALL BE WIDENED TO 20% OF THE LETTER OR NUMERAL HEIGHT.
- THE SPEEDS REQUIRED ON SIGNS W13-1, W13-2 AND W13-3 WILL BE SHOWN ON INDIVIDUAL PLAN SHEETS.

4. THE DESIGN OF ALL STANDARD ROADWAY SIGNS SHALL CONFORM TO THE CRITERIA IN THE LATEST VERSION OF THE STANDARD HIGHWAY SIGNS MANUAL IN CONJUNCTION WITH THE LATEST VERSION OF THE MUTCD.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**STANDARDS
ROADSIDE SIGNS**

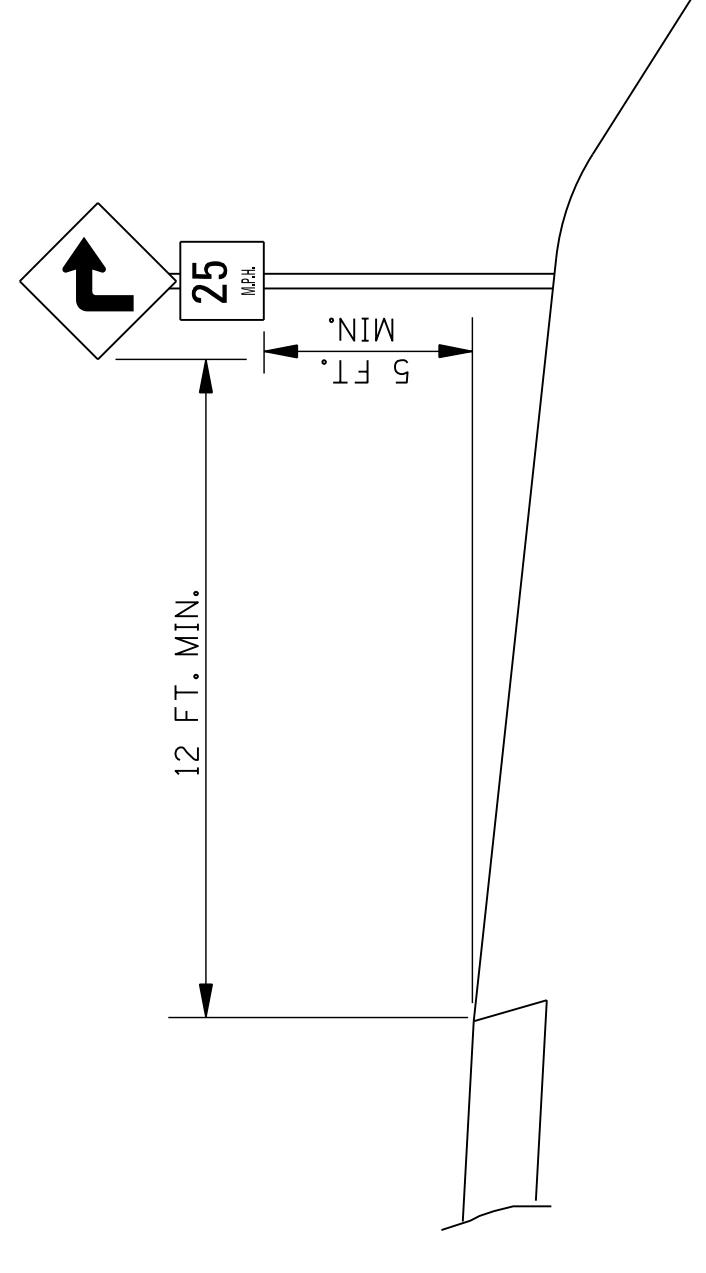
DATE	12/1/23	REVISION	REVISED TEXT
SHEET		NO.	

ISSUE DATE: AUGUST 01, 2017

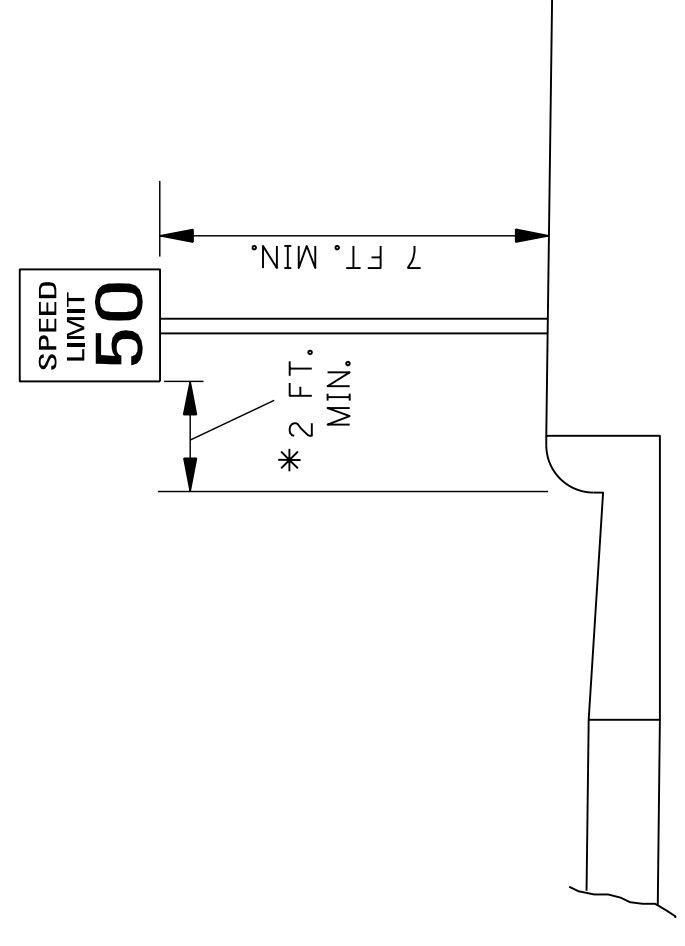
WORKING NUMBER
SN-3B

SHEET NUMBER
6305

WARNING SIGN WITH ADVISORY
SPEED PLAQUE IN RURAL AREA

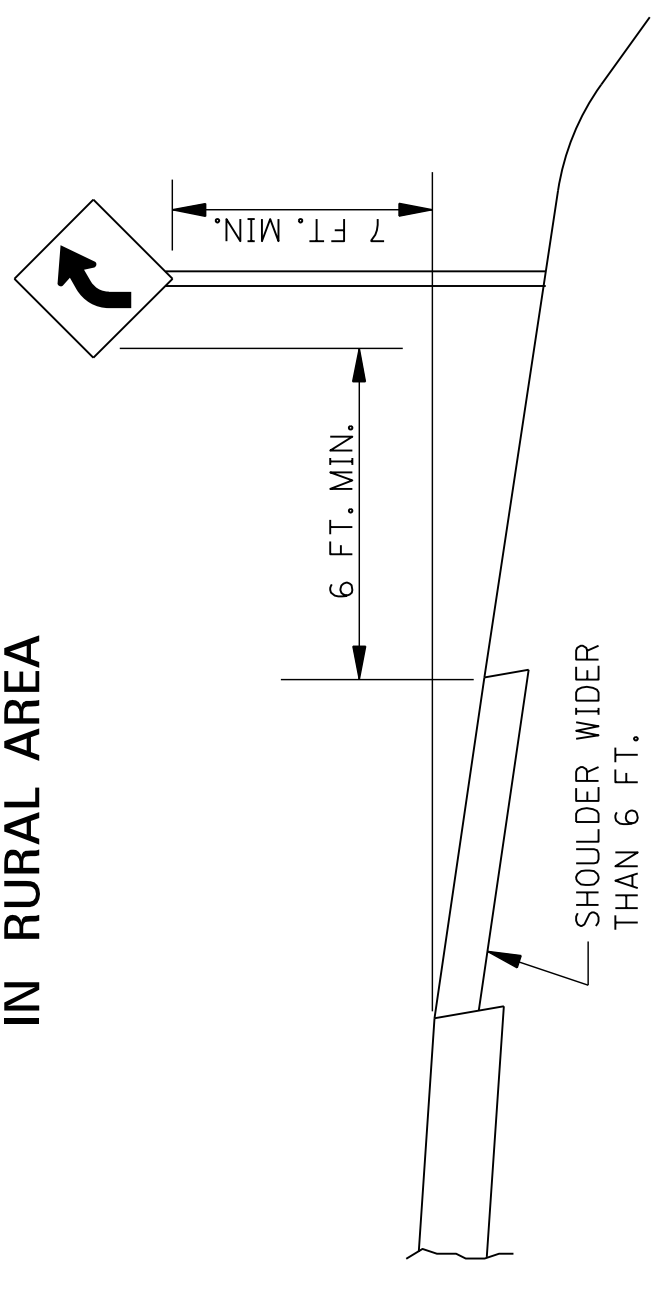


ROADSIDE SIGN IN BUSINESS,
COMMERCIAL, OR RESIDENTIAL AREA

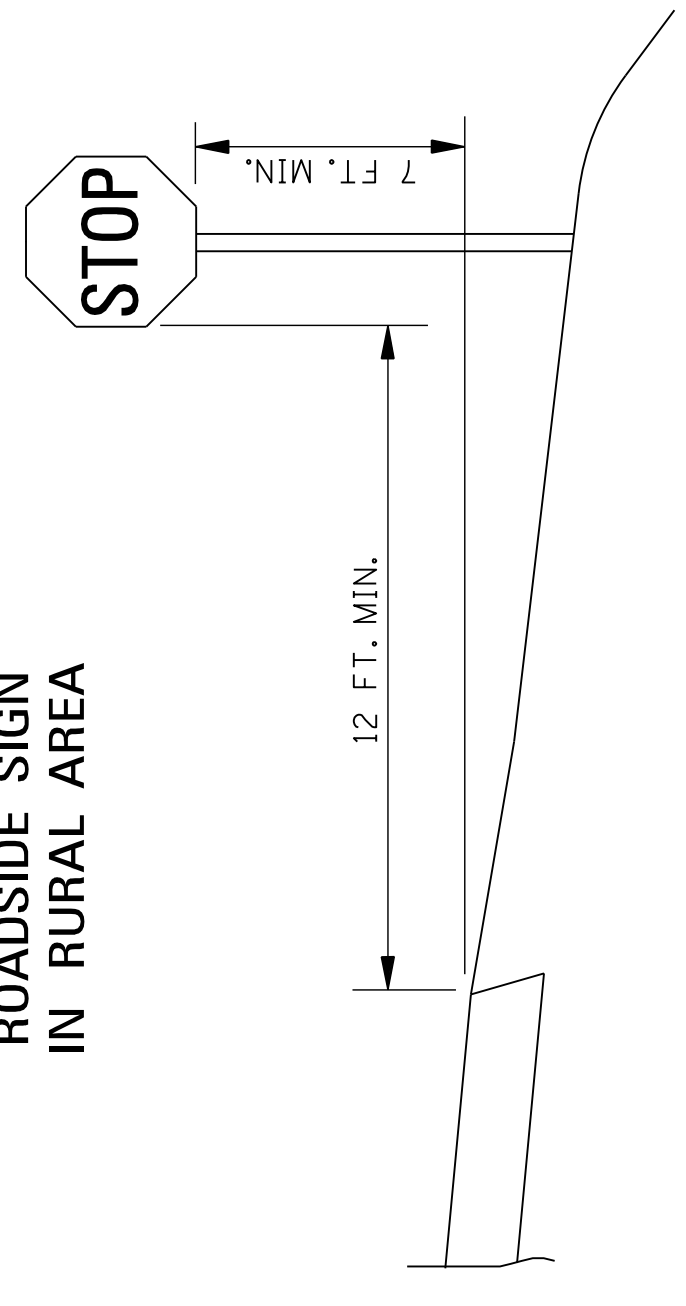


* THE 2 FT. MINIMUM OFFSET APPLIES ONLY TO STANDARD SIGNS MOUNTED ON U-POSTS OR SQUARE TUBE POSTS. ALL STANDARD SIGNS MOUNTED ON PIPE WILL BE OFFSET A MINIMUM OF 4 FT FROM FACE OF CURB. RAMP DESTINATION SIGNS WILL BE OFFSET 4 FT. FROM THE SHOULDER BREAK.

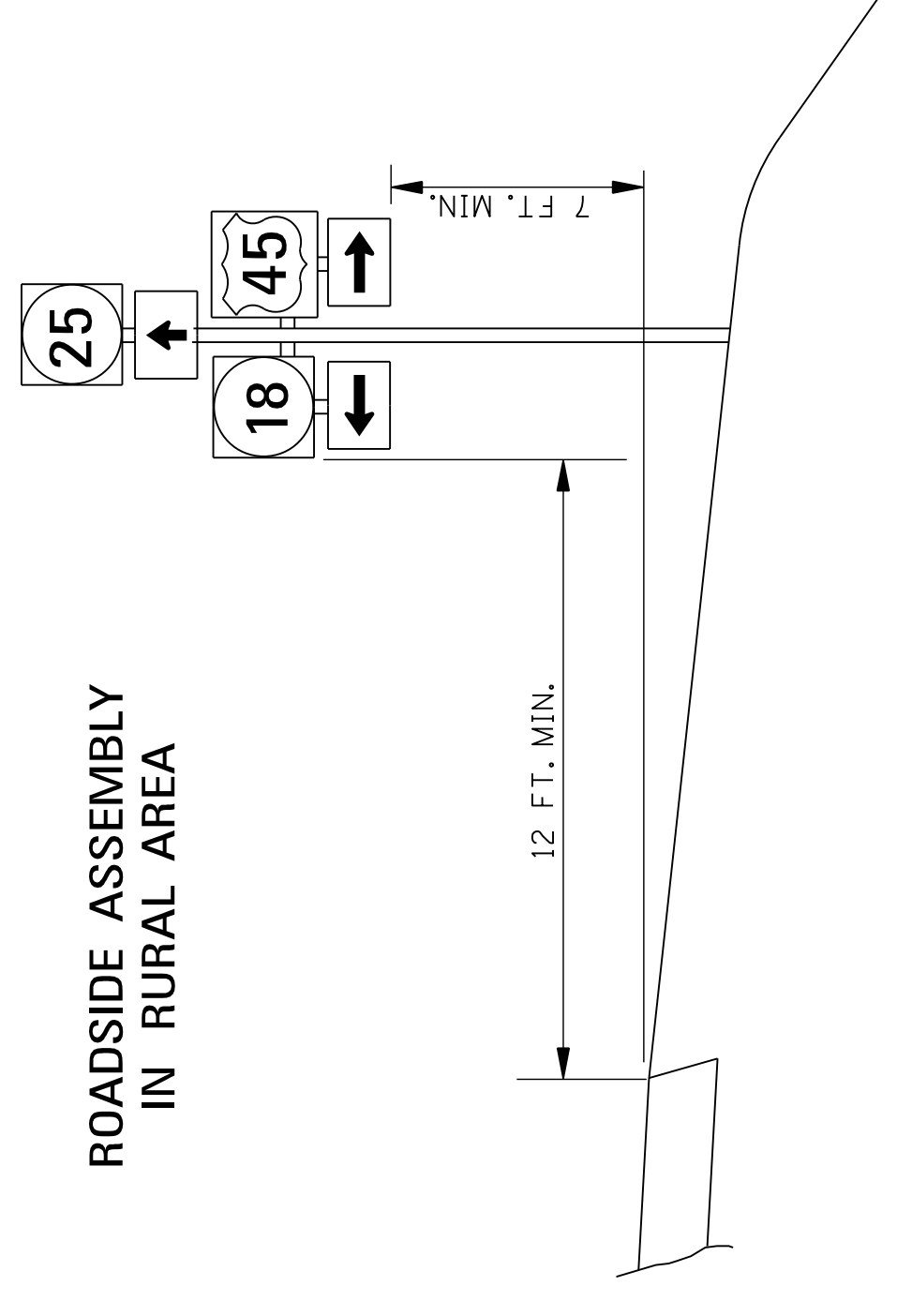
ROADSIDE SIGN
IN RURAL AREA



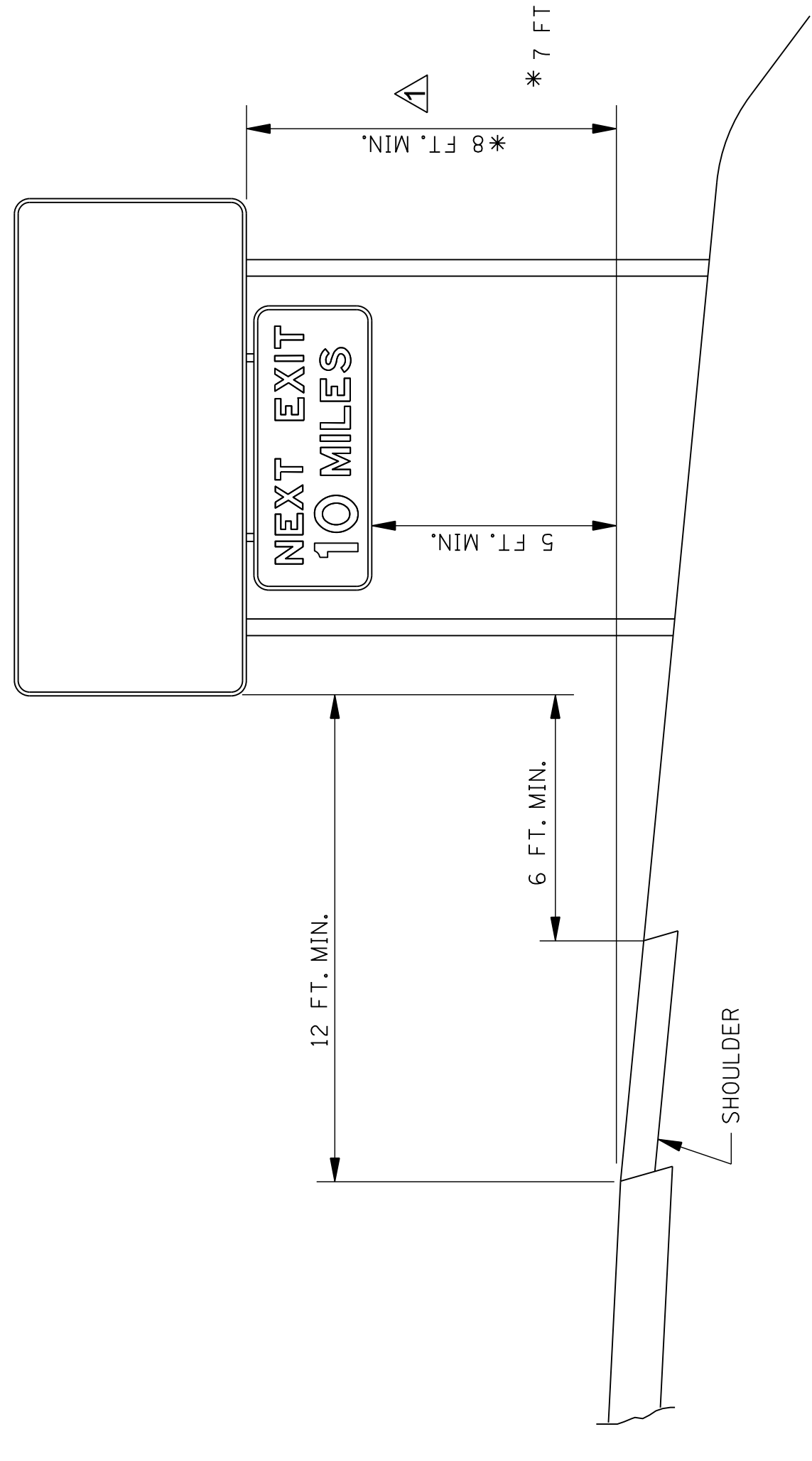
ROADSIDE SIGN
IN RURAL AREA



ROADSIDE ASSEMBLY
IN RURAL AREA

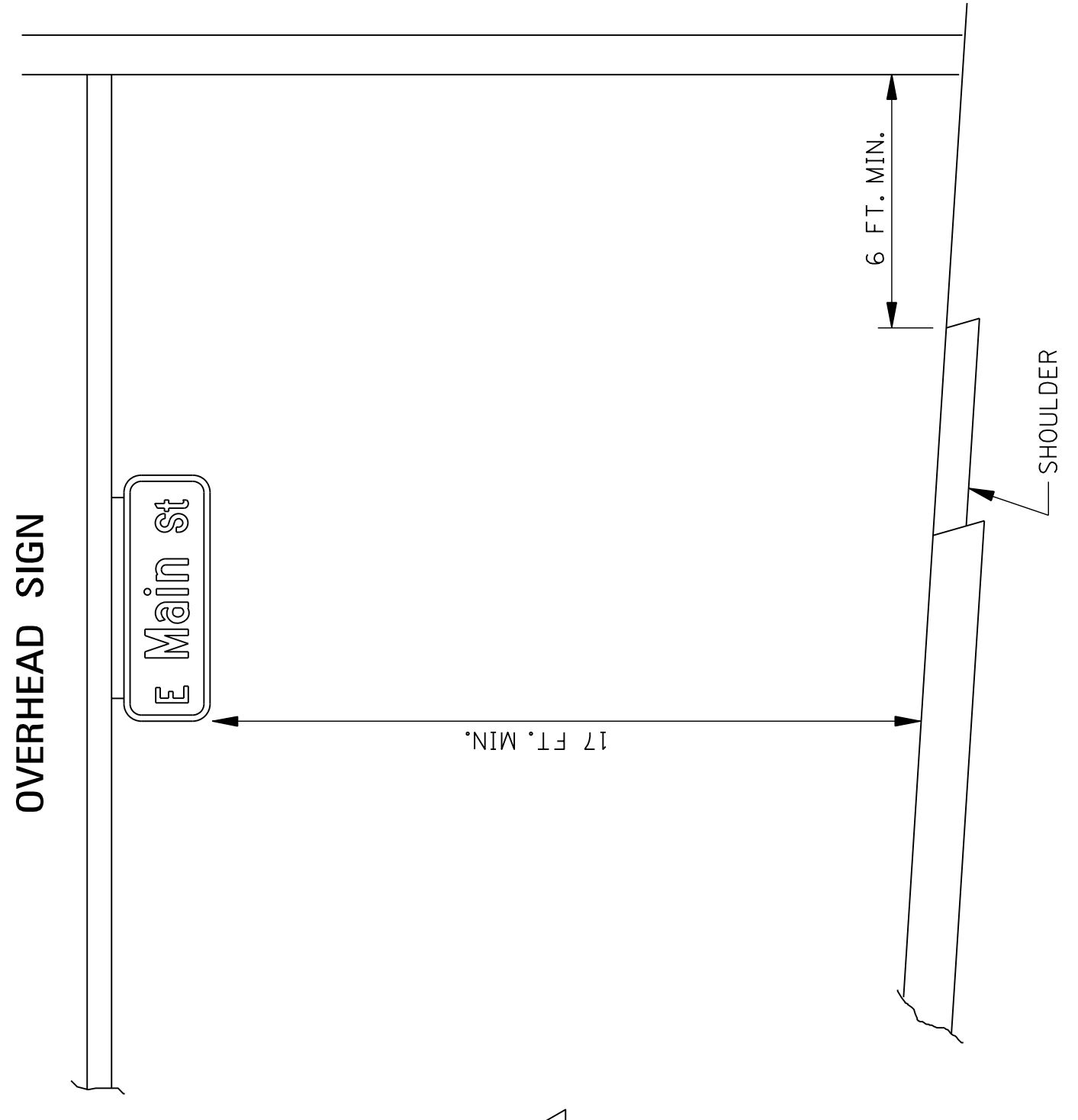


INTERSTATE OR FREEWAY SIGN WITH SECONDARY SIGN

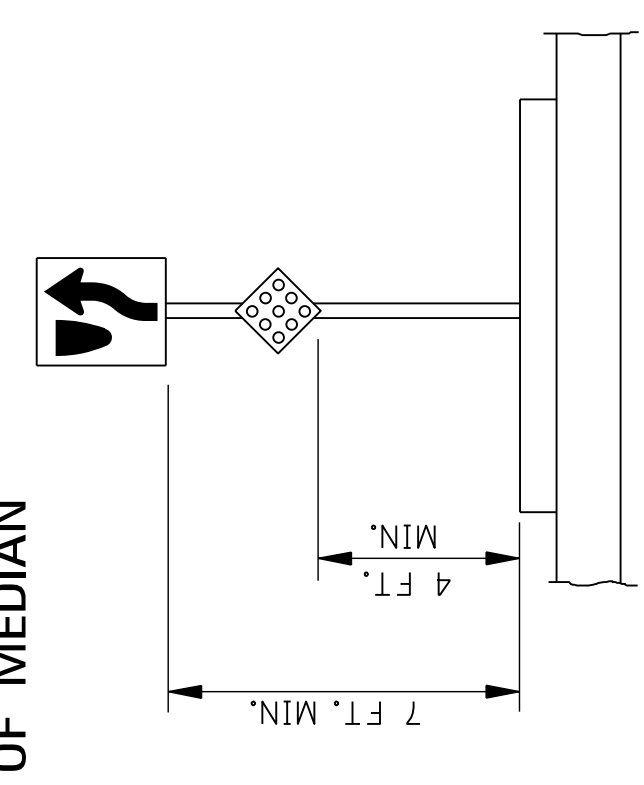


* 7 FT IF NO NECESSARY SECONDARY SIGN

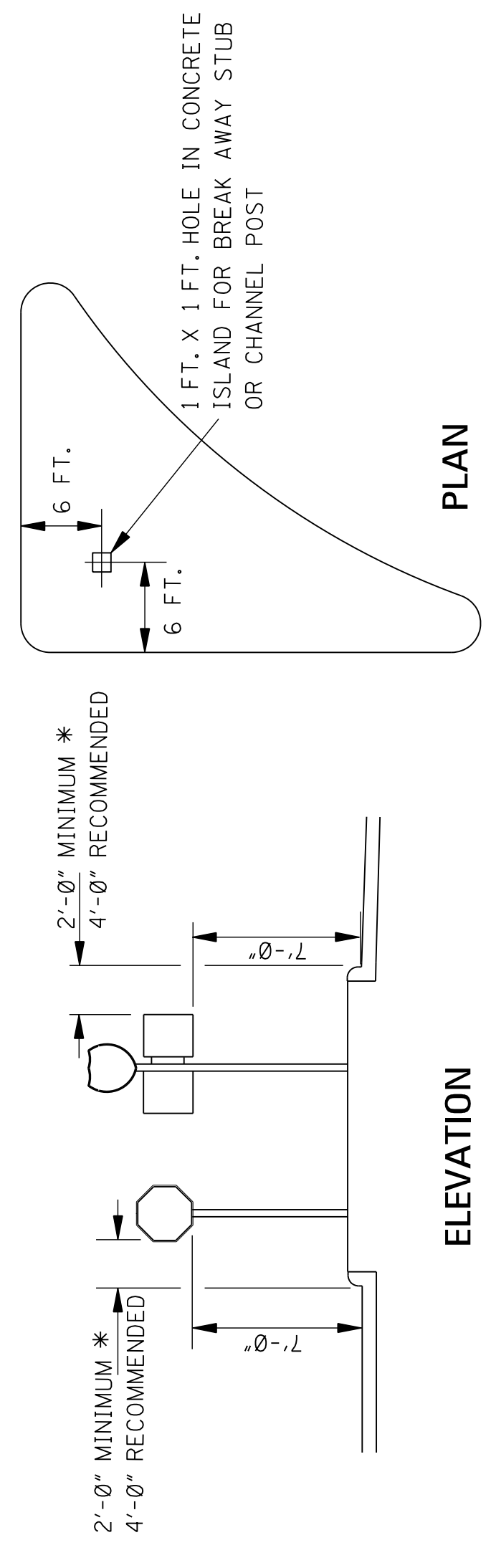
OVERHEAD SIGN



SIGN ON NOSE
OF MEDIAN



SIGNS IN ISLANDS OR BEHIND CURB USING U-POSTS OR PIPE POSTS



ELEVATION

PLAN

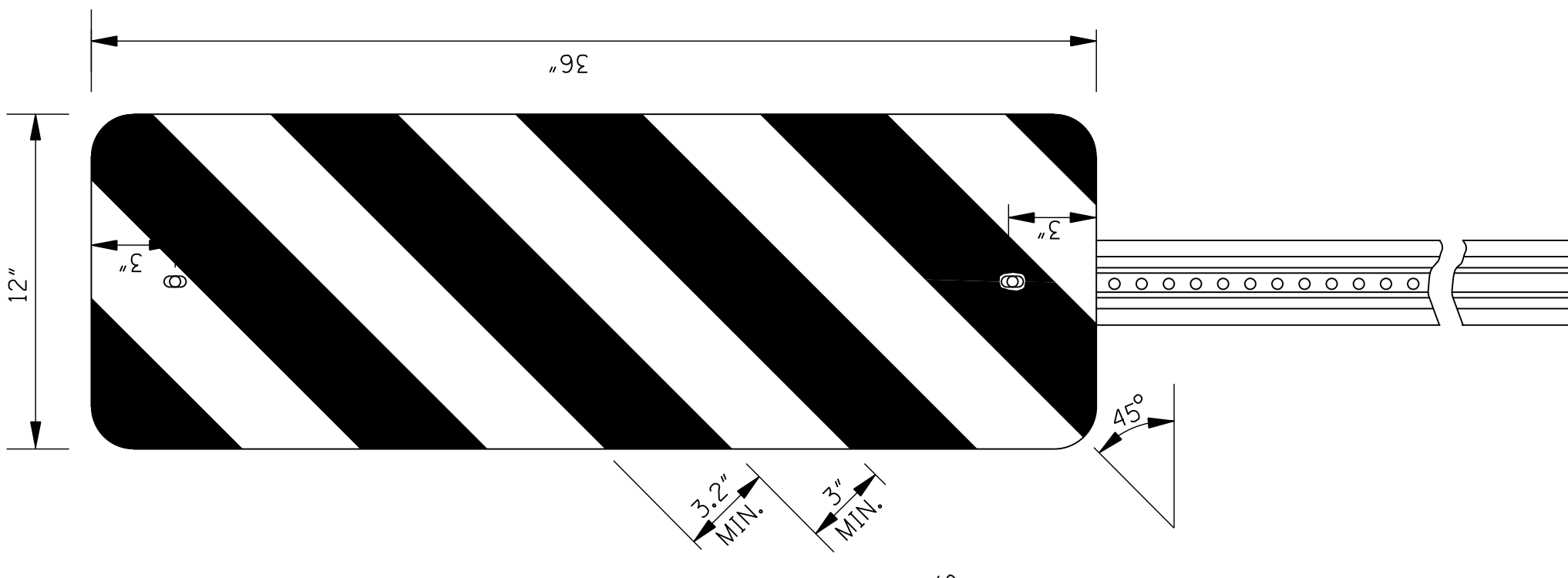
GENERAL NOTES:

- SEE SECTION 2A-16 OF THE MUTCD FOR REDUCED LATERAL OFFSET DISTANCES THAT MAY BE USED IN AREAS WHERE LATERAL OFFSETS ARE LIMITED, AND IN BUSINESS, COMMERCIAL, OR RESIDENTIAL AREAS WHERE SIDEWALK WIDTH IS LIMITED OR WHERE EXISTING POLES ARE CLOSE TO THE CURB. SIGNS SHALL BE LOCATED OUTSIDE THE CLEAR ZONE UNLESS PLACED ON A BREAKAWAY OR YIELDING SUPPORT.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
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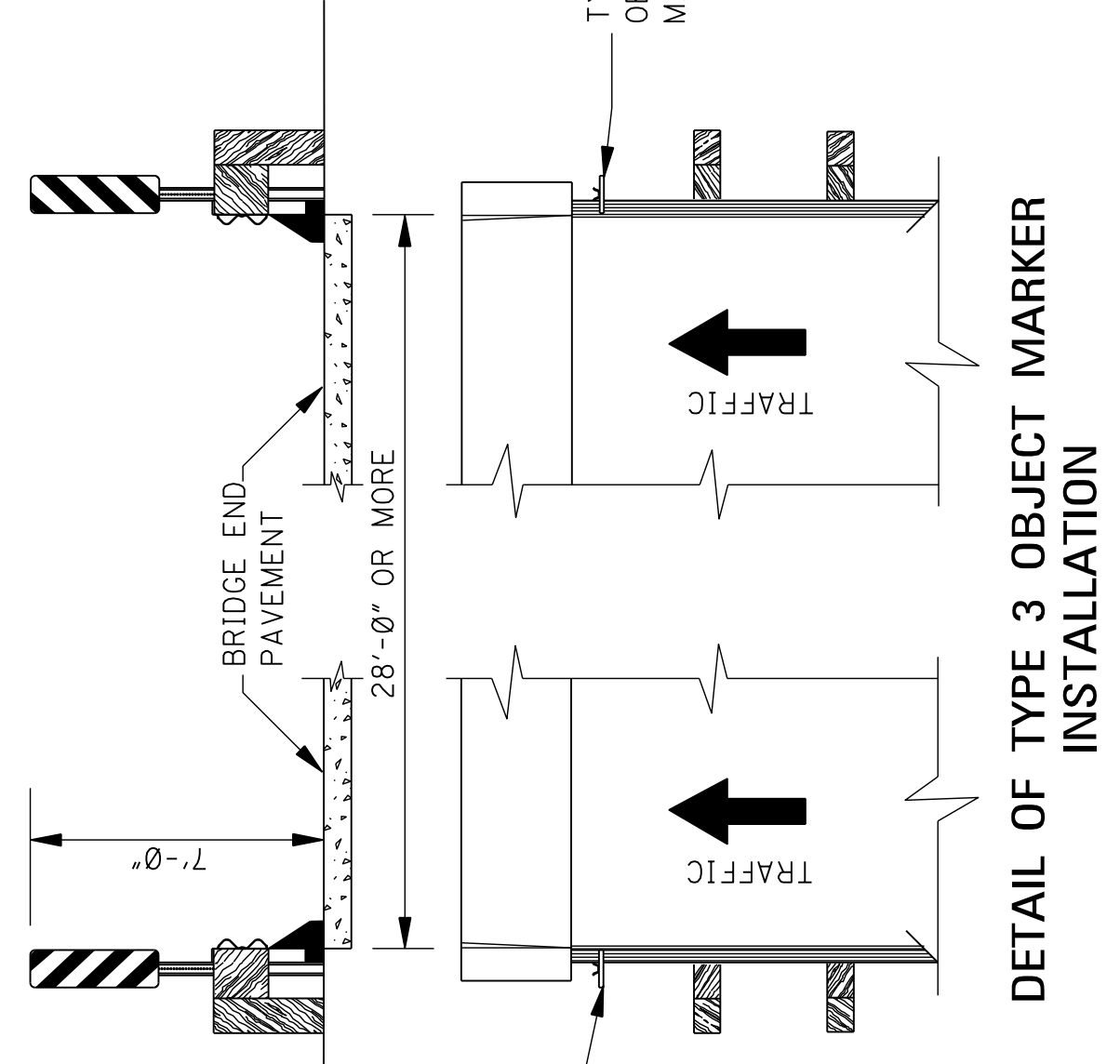
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STANDARD SIGN
ROADSIDE SIGN
ASSEMBLY AND
INSTALLATION

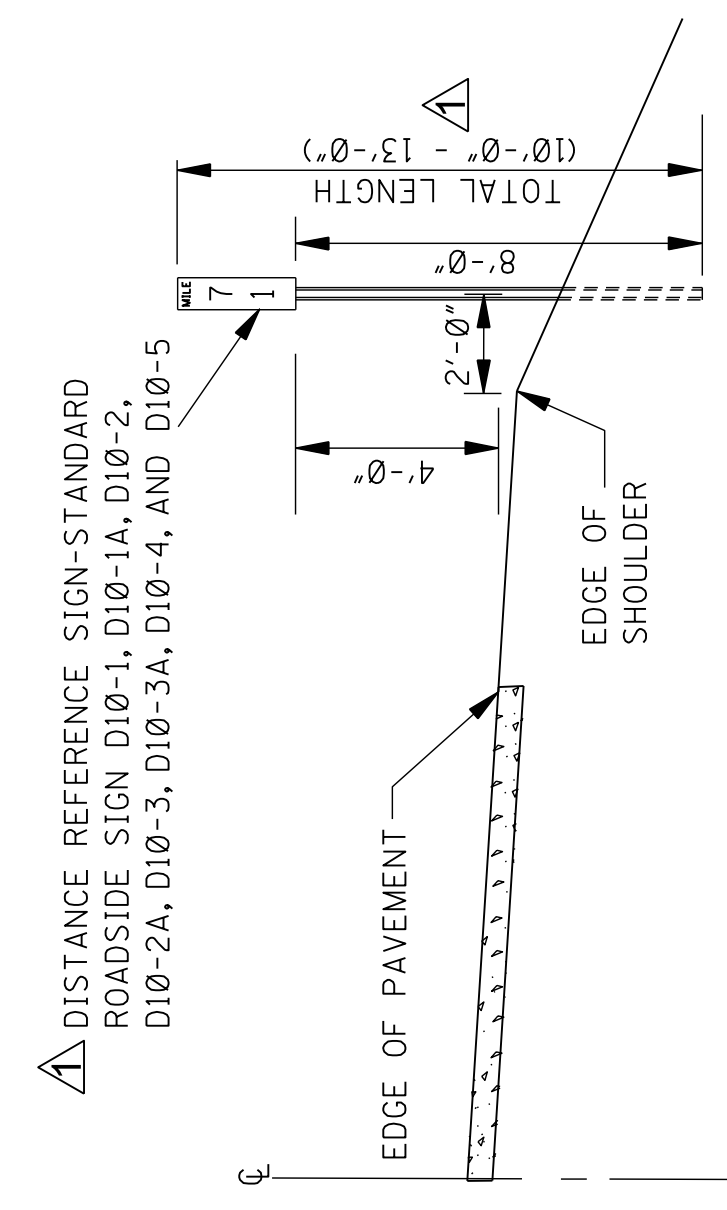


REAR VIEW OF TYPE 3 OBJECT MARKER OR DISTANCE REFERENCE SIGN ASSEMBLY

NOTE: TYPE 3 OBJECT MARKER AND DISTANCE REFERENCE SIGNS SHALL BE FASTENED TO U-SECTION POSTS WITH 3/8" DIA. BLIND FASTENERS OF THE COLLAR TYPE.

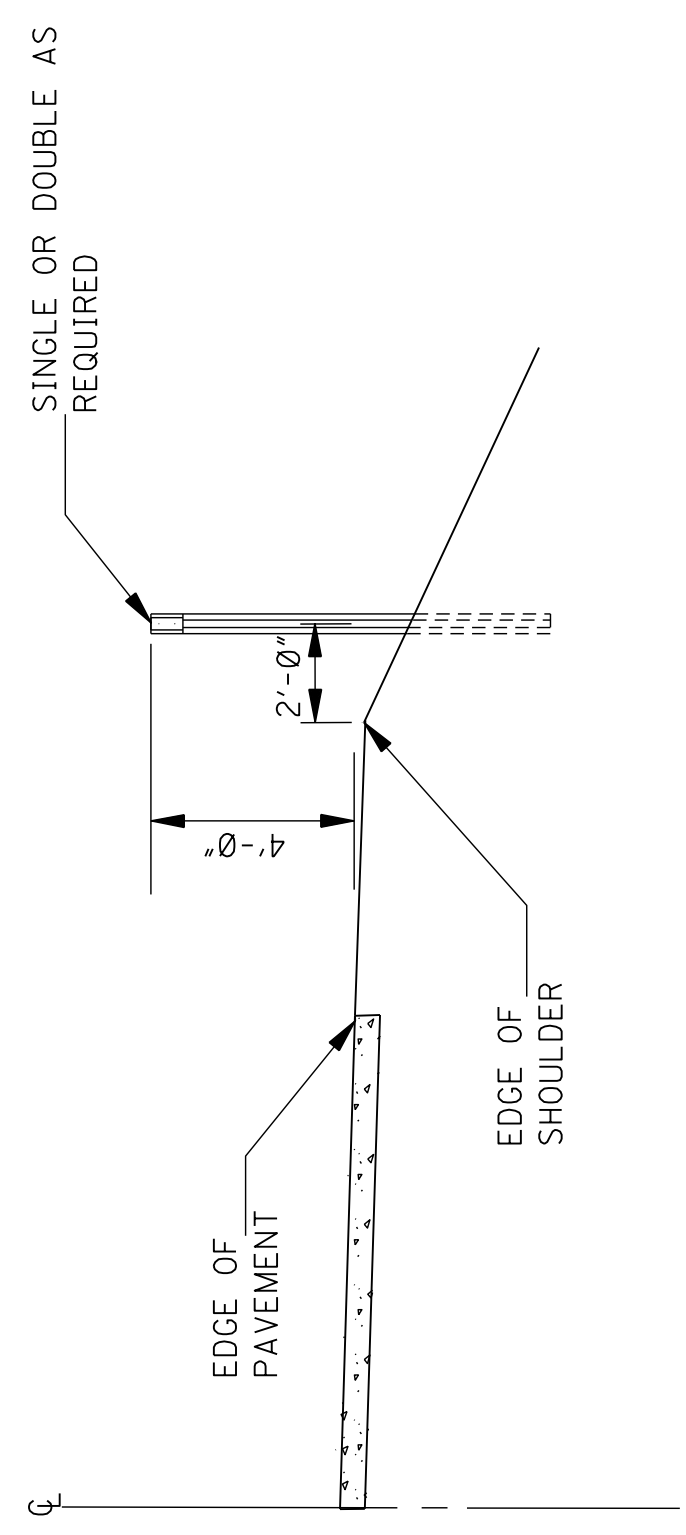


DETAIL OF TYPE 3 OBJECT MARKER INSTALLATION

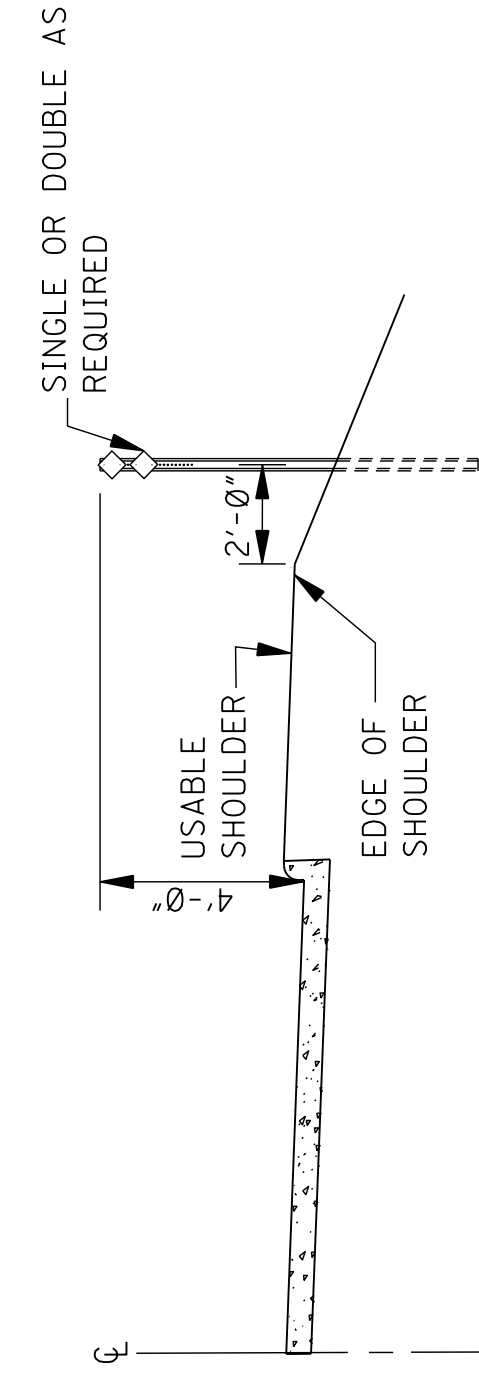


DISTANCE REFERENCE SIGN MOUNTING ON OUTSIDE SHOULDER ALONG MAIN FACILITY

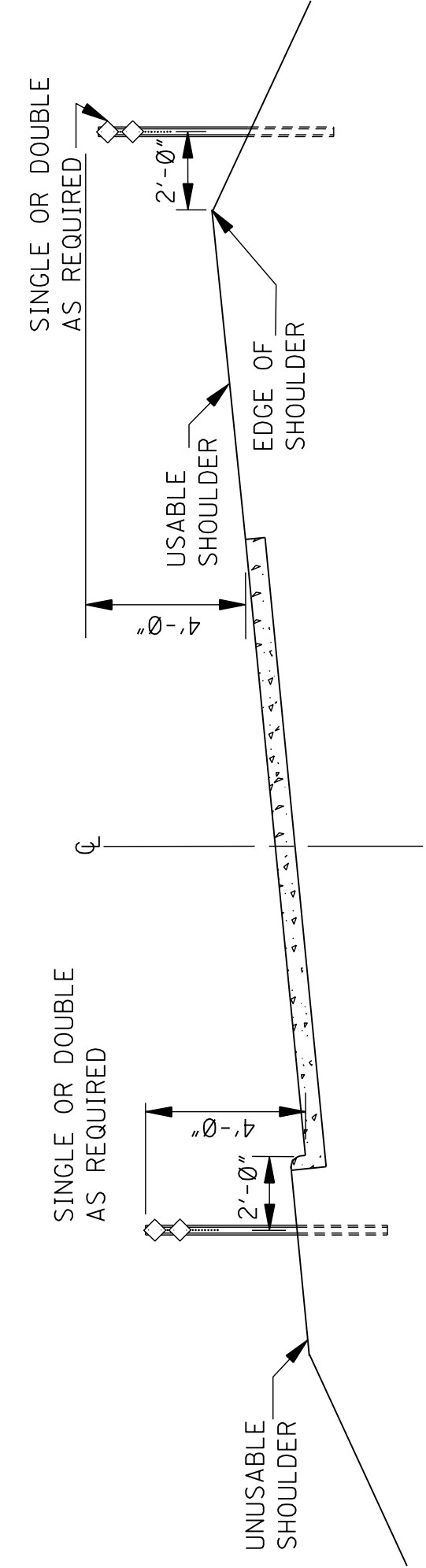
NOTE: SIGN MOUNTING ON LEFT LANE SHOULDER SHALL BE 90° OPPOSITE THE RIGHT LANE STATION. IF CONDITIONS ARE SUCH THAT MILE SIGN CANNOT BE LOCATED WITHIN 50 FEET OF ITS TRUE LOCATION, IT SHALL BE OMITTED ENTIRELY.



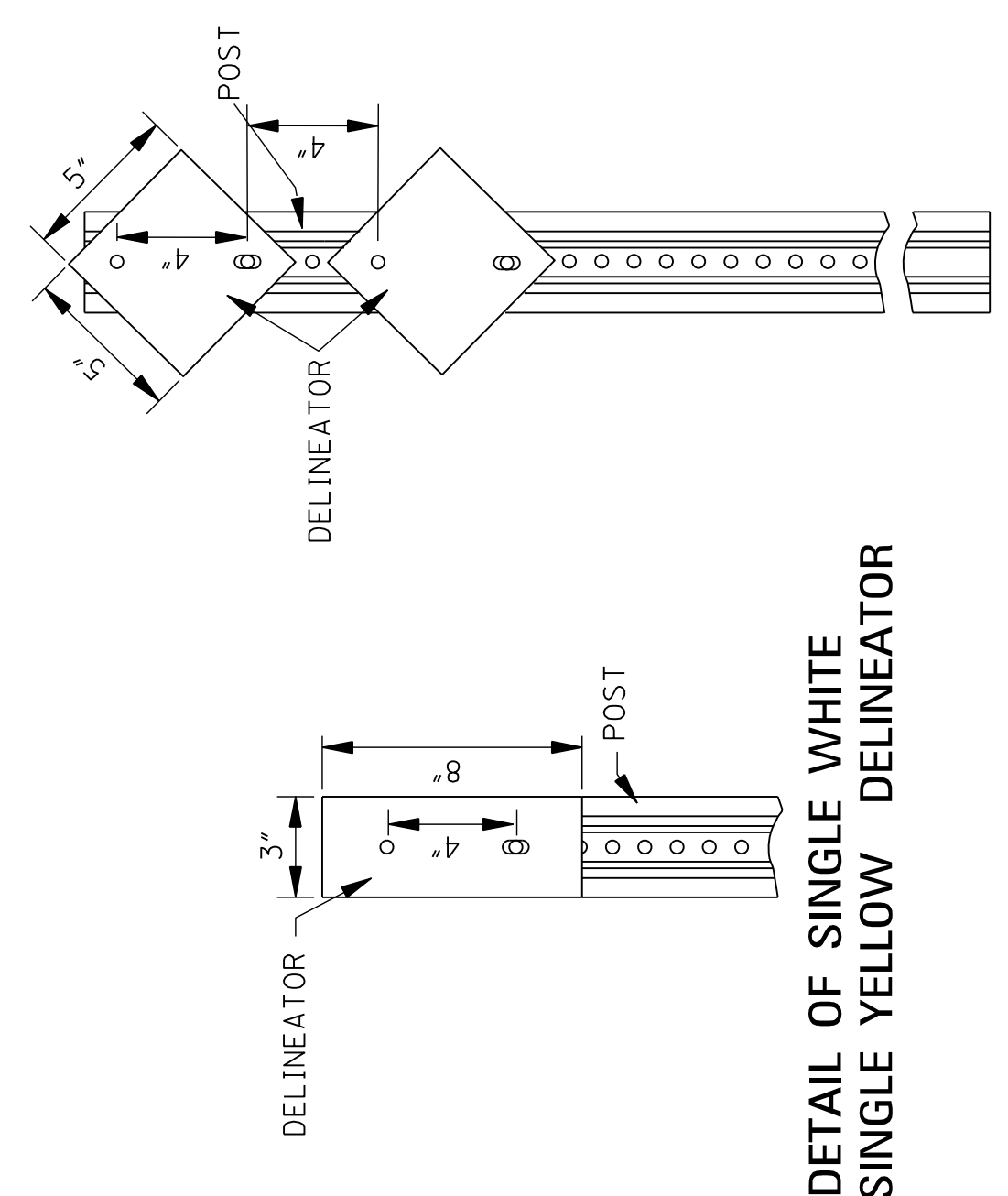
DELINicator MOUNTING ON OUTSIDE SHOULDER ALONG MAIN FACILITY OR RAMP



DELINicator MOUNTING ON OUTSIDE SHOULDER WITH MOUNTABLE CURB ALONG MAIN FACILITY OR RAMP

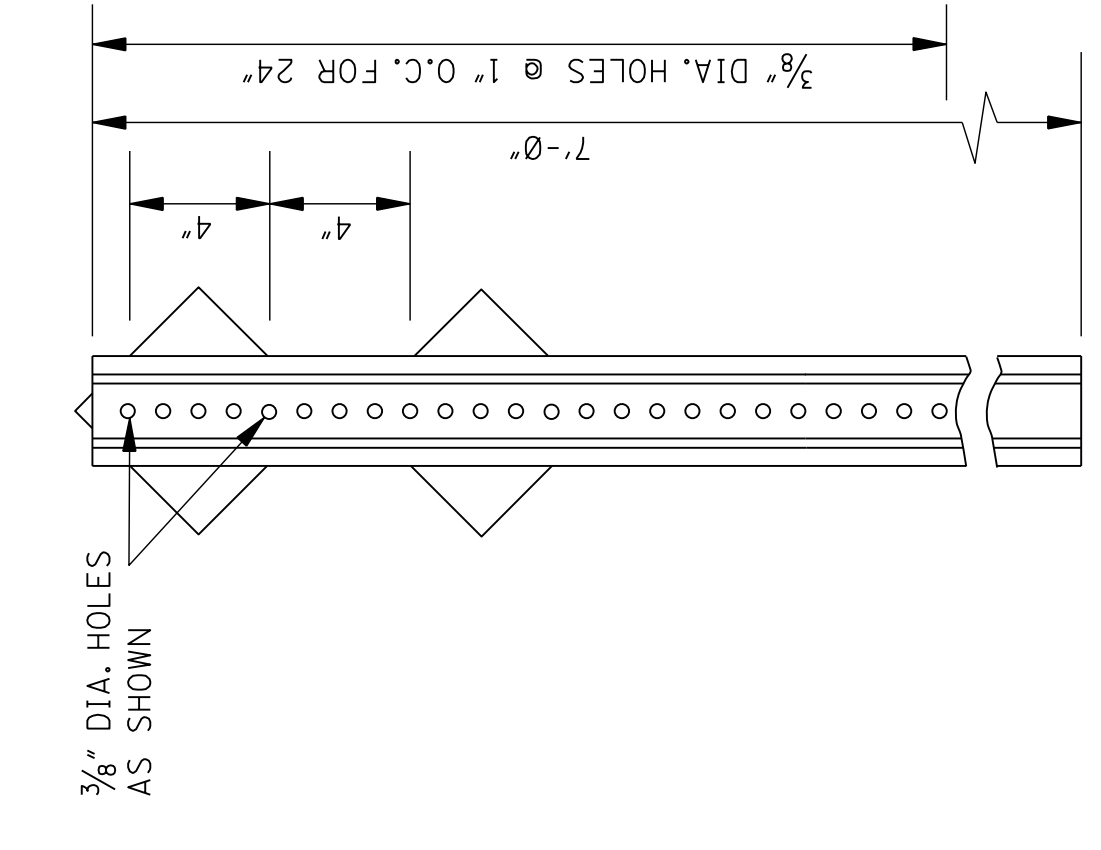


DELINicator MOUNTING ON INTERCHANGE LOOPS WITH UNMOUNTABLE CURB ON INSIDE

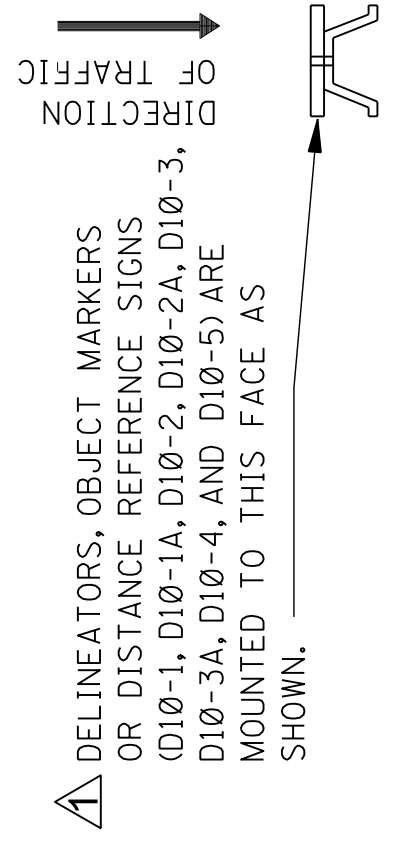


DETAIL OF SINGLE WHITE OR SINGLE YELLOW DELINicator

DETAIL OF DOUBLE WHITE OR DOUBLE YELLOW DELINicator



REAR VIEW OF DELINicator ASSEMBLY



MOUNTING DETAIL

NOTE: DELINicators SHALL BE FASTENED TO U-SECTION POSTS WITH 1/4" DIA. BLIND FASTENERS OF THE COLLAR TYPE.

GENERAL NOTES:

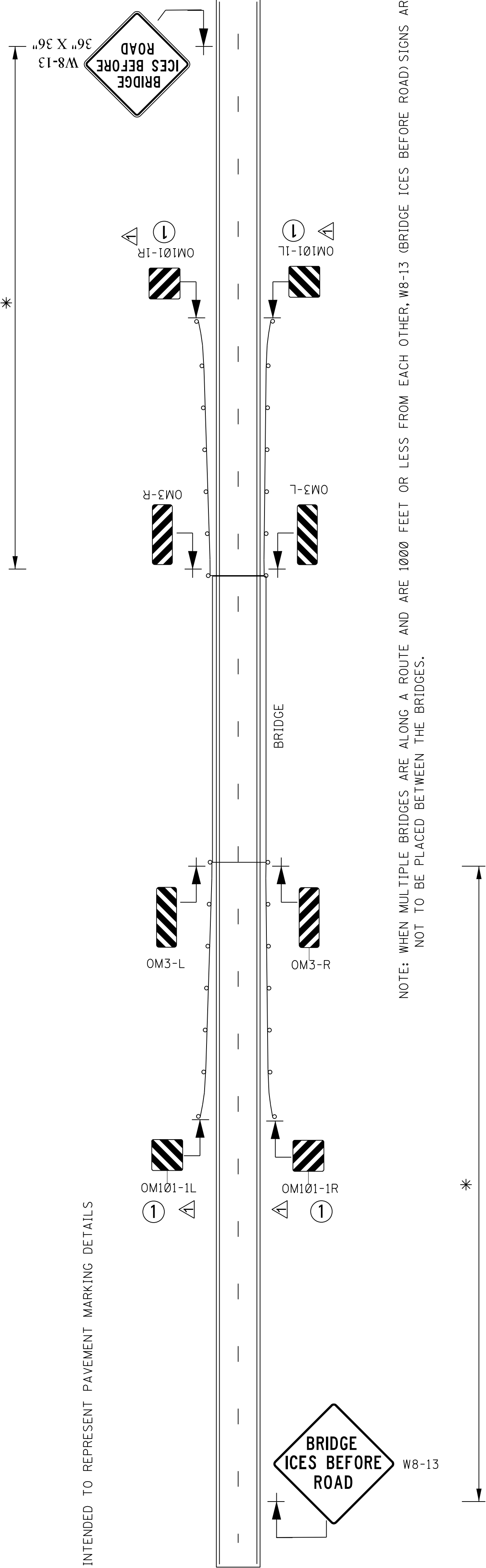
- DELINicators AND TYPE 3 OBJECT MARKER SHALL BE RETROREFLECTIVE SHEETING ON 0.080" THICK ALUMINUM SHEET OR 14 GAGE GALVANIZED SHEET STEEL.
- DELINicator, TYPE 3 OBJECT MARKER AND DISTANCE REFERENCE SIGN POSTS SHALL BE GALVANIZED STEEL. THE POSTS SHALL BE FABRICATED BEFORE THE METAL IS GALVANIZED.
- WEIGHT WITHOUT GROUND PLATES:
 - A. DELINicator POST 7'-0" - 2.0 lb/ft TO 2.5 lb/ft
 - B. TYPE 3 OBJECT MARKER POST 9'-0" - 2.5 lb/ft TO 3.0 lb/ft
 - C. DISTANCE REFERENCE SIGN POST 10'-0", 11'-0", & 12'-0" - 3.0 lb/ft TO 3.5 lb/ft
- UNIT PRICE OF DELINicators AND TYPE 3 OBJECT MARKERS SHALL INCLUDE COST OF POST. DISTANCE REFERENCE SIGN POST WILL BE PAID PER FOOT.
- RADIUS IN BENDS OF POST CROSS SECTION NOT TO EXCEED 1/8" FOR HOT ROLLED SECTION.
- GROUND PLATE NOT REQUIRED ON U-SECTION POST.

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MISSISSIPPI DEPARTMENT OF TRANSPORTATION
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STANDARD PLAN

TYPICAL INSTALLATION AND DETAILS OF DELINicators AND DISTANCE REFERENCE SIGNS

DRAWING NOT INTENDED TO REPRESENT PAVEMENT MARKING DETAILS



NOTE: WHEN MULTIPLE BRIDGES ARE ALONG A ROUTE AND ARE 1000 FEET OR LESS FROM EACH OTHER, W8-13 (BRIDGE ICES BEFORE ROAD) SIGNS ARE NOT TO BE PLACED BETWEEN THE BRIDGES.

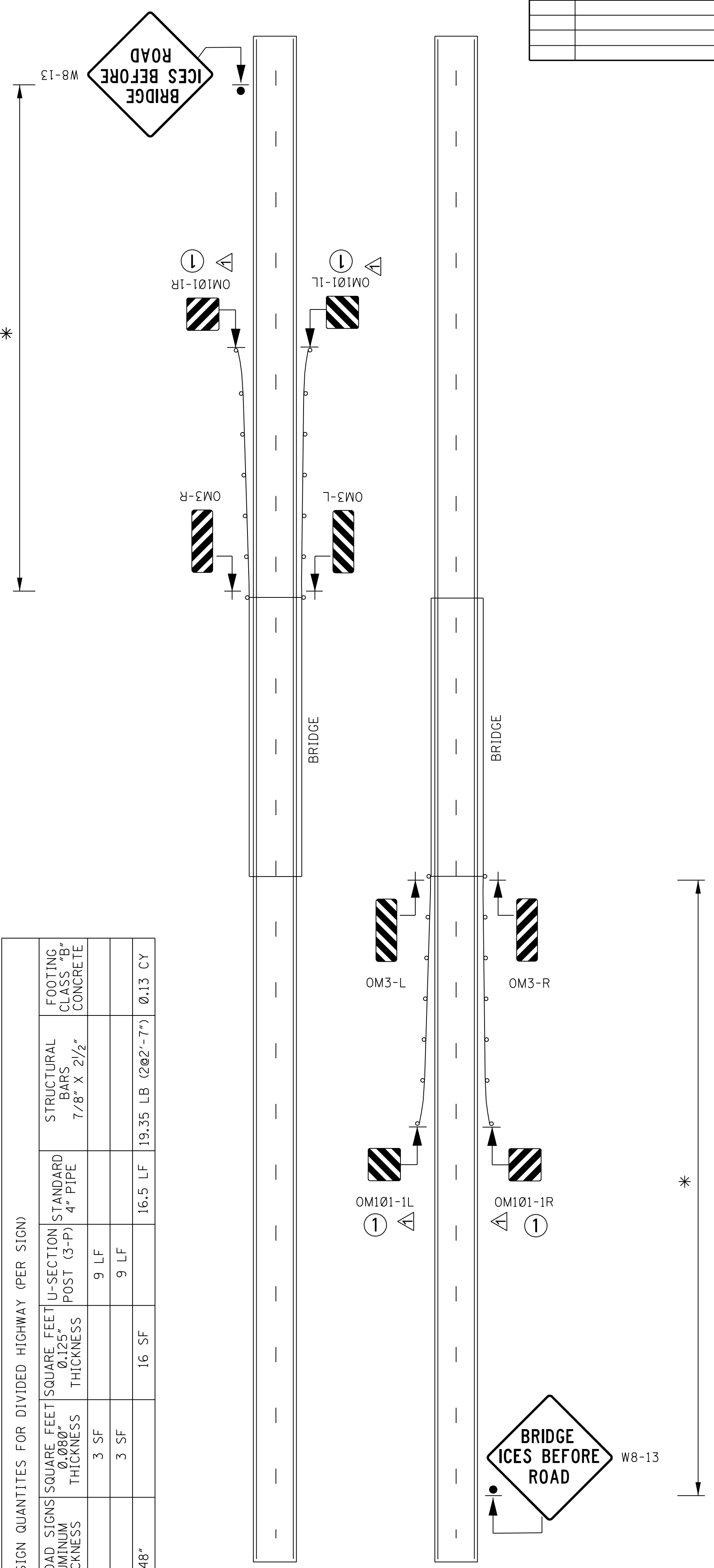
UNDIVIDED HIGHWAY DETAIL

SIGN QUANTITIES FOR UNDIVIDED HIGHWAY (PER SIGN)				
MUTCD NUMBER	STANDARD ROAD SIGN SHEET ALUMINUM THICKNESS	STANDARD ROAD SIGN SQUARE FEET THICKNESS	U-SECTION POST (3-P)	U-SECTION POST (3-P)
OM3-L	12" X 36"	3 SF	9 LF	9 LF
OM3-R	12" X 36"	3 SF	9 LF	9 LF
W8-13	36" X 36"	9 SF	15 LF	15 LF

SIGN QUANTITIES FOR DIVIDED HIGHWAY (PER SIGN)				
MUTCD NUMBER	STANDARD ROAD SIGN SHEET ALUMINUM THICKNESS	STANDARD ROAD SIGN SQUARE FEET THICKNESS	U-SECTION POST (3-P)	U-SECTION POST (3-P)
OM3-L	12" X 36"	3 SF	9 LF	9 LF
OM3-R	12" X 36"	3 SF	9 LF	9 LF
W8-13	48" X 48"	16 SF	16.5 LF	16.5 LF (2@2'-7")

TABLE 2C-3 * MUTCD	
SPEED (MPH)	MINIMUM PLACEMENT (FEET)
35	250
40	305
45	360
50	425
55	495
60	570
65	645
70	730

① RETROREFLECTIVE ADHESIVE SHEETING WITH ALTERNATING BLACK AND YELLOW STRIPES (SLOPING DOWNWARD) (OM101-1L, OM101-1R) AT AN ANGLE OF 45 DEGREES (IN THE DIRECTION TRAFFIC IS TO PASS) IS REQUIRED ON THE END OF THE TERMINAL END SECTION. NOT A SEPARATE PAY ITEM. COST TO BE ABSORBED IN GUARD RAIL.

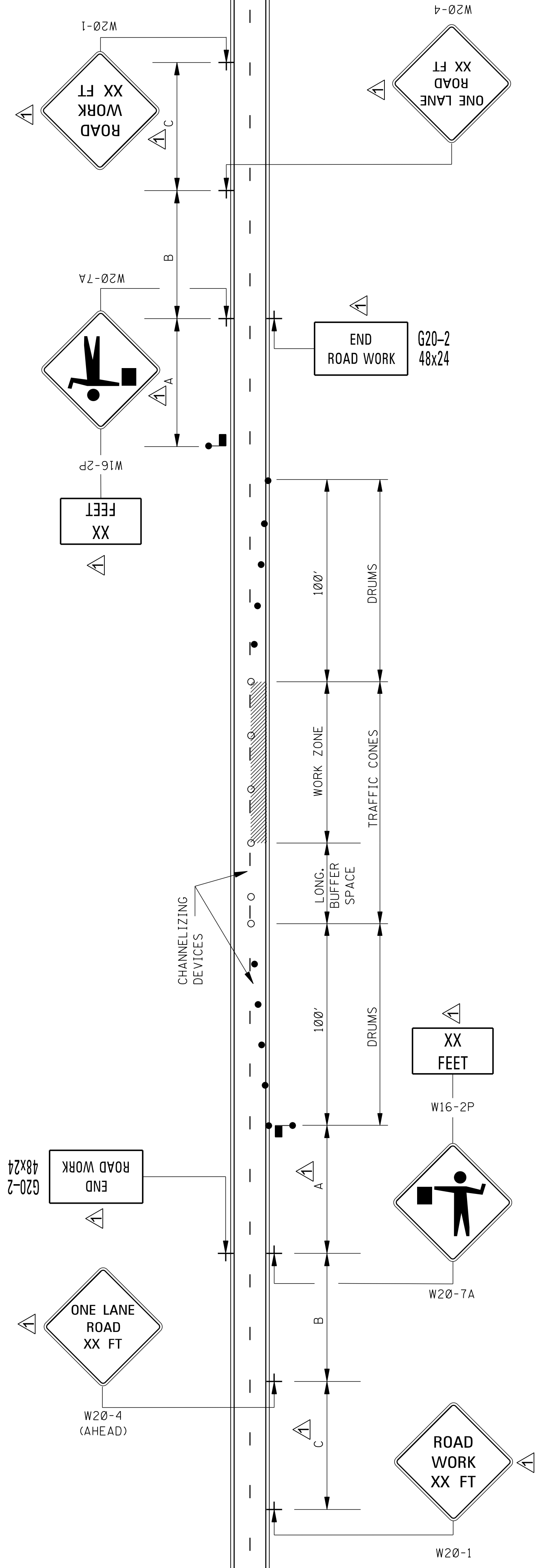


DIVIDED HIGHWAY DETAIL

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MISSISSIPPI DEPARTMENT OF TRANSPORTATION
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STANDARD PLAN

**SIGNING DETAILS FOR
BRIDGE APPROACHES**



- LEGEND
- ▲ FLAGGER
 - RETROREFLECTIVE FREE-STANDING PLASTIC DRUMS
 - TRAFFIC CONES (28" HEIGHT MINIMUM)

DISTANCE BETWEEN SIGNS

ROAD TYPE	A	B	C
URBAN (35 MPH OR LESS)	100 FT.	100 FT.	100 FT.
URBAN (40 - 70 MPH)	350 FT.	350 FT.	350 FT.
RURAL	500 FT.	500 FT.	500 FT.
EXPRESSWAY / FREEWAY	1000 FT.	1500 FT.	2640 FT.

GENERAL NOTES:

- THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE. FLAGGER STATIONS SHALL BE LOCATED SUCH THAT APPROACHING VEHICLES WILL HAVE SUFFICIENT DISTANCE TO STOP. VALUES IN STOPPING SIGHT DISTANCE COLUMN MAY BE USED AS A MINIMUM FOR THIS DISTANCE.
- ALL CHANNELIZING DEVICES SHALL BE A MINIMUM OF 28" IN HEIGHT.
- DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 36" x 36" AND BLACK COPY ON FLUORESCENT ORANGE SHEETING.
- WHEN WORK ZONE IS NO LONGER NEEDED, ALL SIGNS SHALL BE COVERED OR REMOVED AND ALL CHANNELIZING DEVICES SHALL BE MOVED TO THE SHOULDER EDGE.
- ADDITIONAL FLAGGERS MAY BE NEEDED AS DIRECTED BY THE ENGINEER.
- WHEN WORK IS REQUIRED AT NIGHT, FLAGGER STATIONS SHALL BE ILLUMINATED EXCEPT IN EMERGENCIES.
- CHANNELIZING DEVICE TYPES FOR:
 - APPROACH AND EXIT TAPERS- RETROREFLECTIVE PLASTIC DRUMS
 - ALONG LANE LINE AND WORK ZONE- TRAFFIC CONES (28" HEIGHT)
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
- AN OPTIONAL FLAGGER (W20-7A) WORD MESSAGE IS SHOWN IN THE "STANDARD HIGHWAY SIGNS" PUBLICATION.

POSTED SPEED AND/OR DESIGN SPEED AND/OR ANTICIPATED OPERATING SPEED ▲ mph	MAXIMUM CHANNELIZING DEVICE SPACING (ft)		LONGITUDINAL BUFFER SPACE (ft) †	STOPPING SIGHT DISTANCE
	TAPER	ALONG LANE LINE & WORK ZONE		
25	20	50	55	155
30	20	60	85	200
35	20	70	120	250
40	20	80	170	305
45	20	90	220	360
50	20	100	280	425
55	20	110	335	495
60	20	120	415	570
65	20	130	485	645

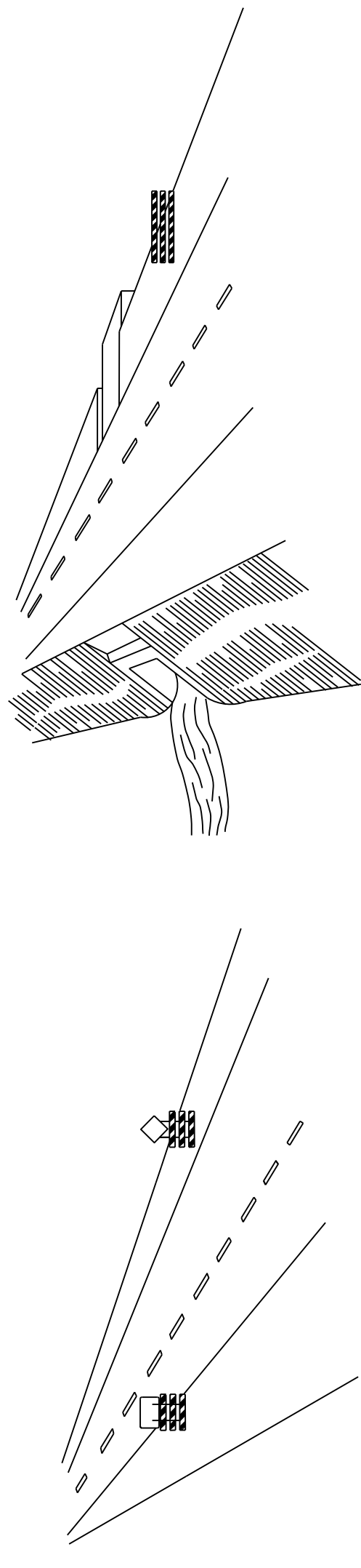
† NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
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**TRAFFIC CONTROL PLAN
WITH FLAGGER
(ONE-LANE CLOSURE OF
TWO-WAY TRAFFIC)**

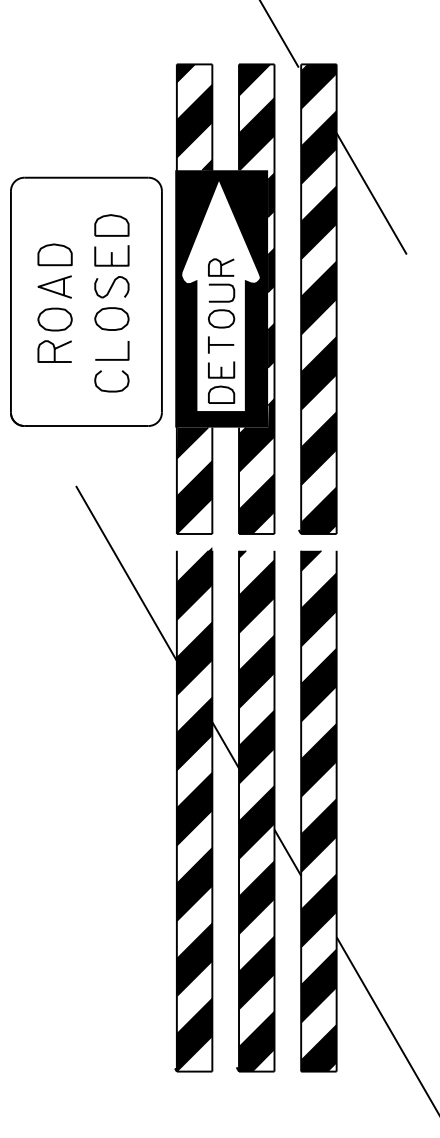
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WING BARRICADES

1. WING BARRICADES ARE TYPE III BARRICADES ERECTED ON THE SHOULDER ON ONE OR BOTH SIDES OF THE PAVEMENT TO GIVE THE SENSATION OF A NARROWING OR RESTRICTED ROADWAY. WING BARRICADES MAY BE USED AS A MOUNTING FOR THE ADVANCE WARNING SIGNS OR FLASHERS.
2. WING BARRICADES SHOULD BE USED:
 - A. IN ADVANCE OF A CONSTRUCTION PROJECT EVEN WHEN NO PART OF THE ROADWAY IS ACTUALLY CLOSED.
 - B. IN ADVANCE OF ALL BRIDGE OR CULVERT WIDENING OPERATIONS.



BARRICADE CLOSING A ROAD

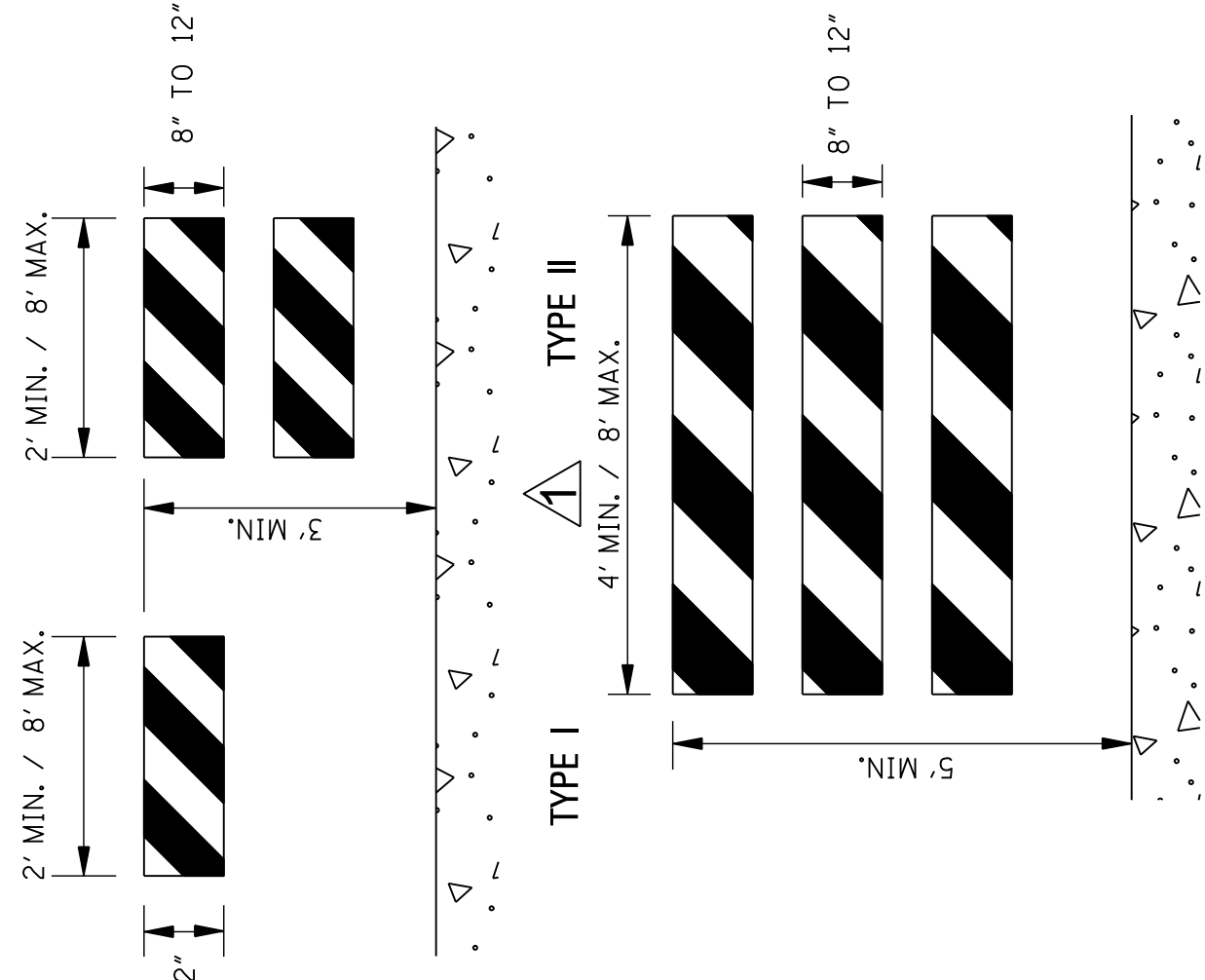
BARRICADE CHARACTERISTICS

	I	II	III
WIDTH OF RAIL **	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.
LENGTH OF RAIL **	24" MIN. / 96" MAX.	24" MIN. / 96" MAX.	48" MIN. / 96" MAX.
WIDTH OF STRIPE *	6"	6"	6"
HEIGHT	36" MIN.	36" MIN.	60" MIN.
NUMBER OF RETROREFLECTORIZED RAIL FACES	2 (ONE EACH DIRECTION)	4 (TWO EACH DIRECTION)	3 IF FACING TRAFFIC IN ONE DIRECTION 6 IF FACING TRAFFIC IN TWO DIRECTIONS

- * 1. FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.
- ** 2. BARRICADES INTENDED FOR USE ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH SPEED ROADWAYS, SHALL HAVE A MINIMUM OF 270 in² OF REFLECTIVE AREA FACING TRAFFIC.

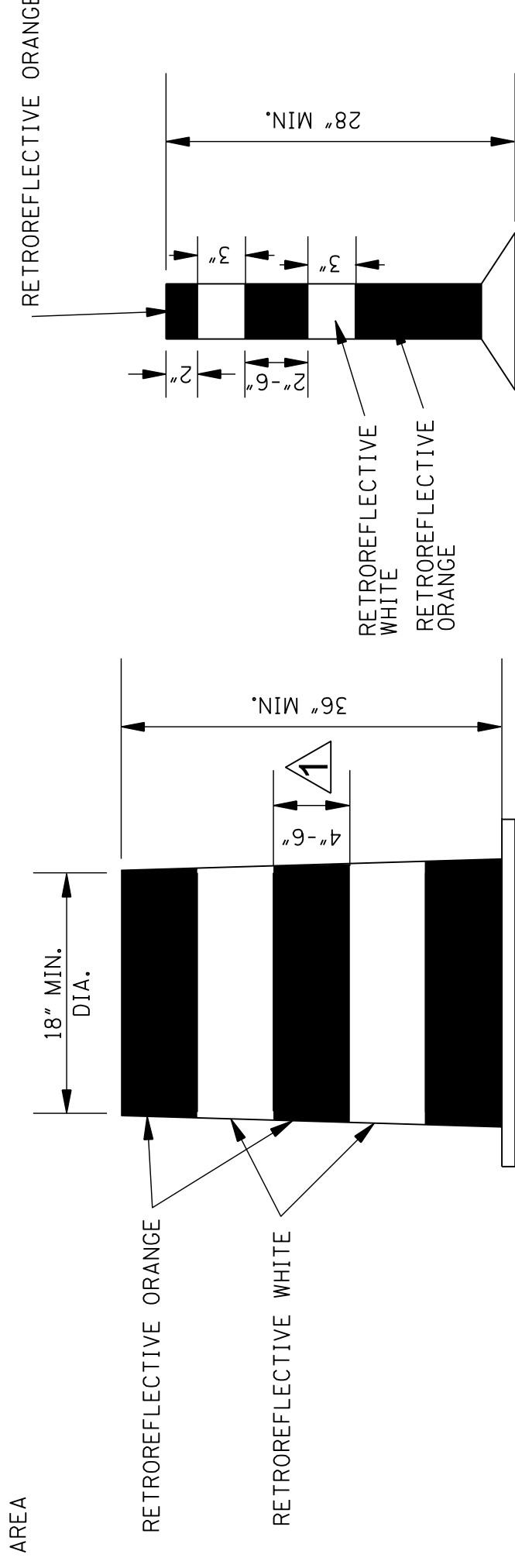
1. THE RETROREFLECTIVE MARKING FOR BARRICADE RAILS SHALL BE ORANGE AND WHITE (SLOPING DOWNWARD AT AN ANGLE OF 45° IN THE DIRECTION THAT TRAFFIC IS TO PASS).
2. RAIL STRIPE SHOULD BE 6 INCHES, EXCEPT THAT 4-INCH WIDE STRIPES MAY BE USED IF RAIL LENGTHS ARE LESS THAN 36 INCHES.
3. DO NOT PLACE SANDBAGS OR OTHER DEVICES ON BARRICADE RAILS TO PROVIDE MASS SANDBAGS, HOWEVER, MAY BE PLACED AS BALLAST TO THE LOWER PARTS OF THE FRAME OR THE STAYS TO BARRICADES.
4. FOR ADDITIONAL INFORMATION OR DETAILS, SEE MUTCD, LATEST EDITION.
5. BARRICADES ARE CLASSIFIED BY FHWA AS CATEGORY II WORK ZONE DEVICES. A LIST OF CRASHWORTHY BARRICADES AND OTHER CATEGORY II DEVICES CAN BE FOUND ON FHWA'S WEBSITE: <http://highways.dot.gov/safety/Rwd/Reduce-crash-severity>
6. WHERE ROAD USERS INCLUDE PEDESTRIANS, THE PROVISION OF SUPPLEMENTAL AUDIBLE INFORMATION OR DETECTABLE BARRIERS OR BARRICADES SHOULD BE PROVIDED FOR PEOPLE WITH VISION DISABILITIES.
7. BARRICADE RAIL SUPPORTS SHOULD NOT PROJECT INTO PEDESTRIAN CIRCULATION ROUTES MORE THAN 4 INCHES FOR THE SUPPORTS LOCATED BETWEEN 27 INCHES TO 80 INCHES ABOVE THE EXISTING SURFACE.

STANDARD BARRICADES

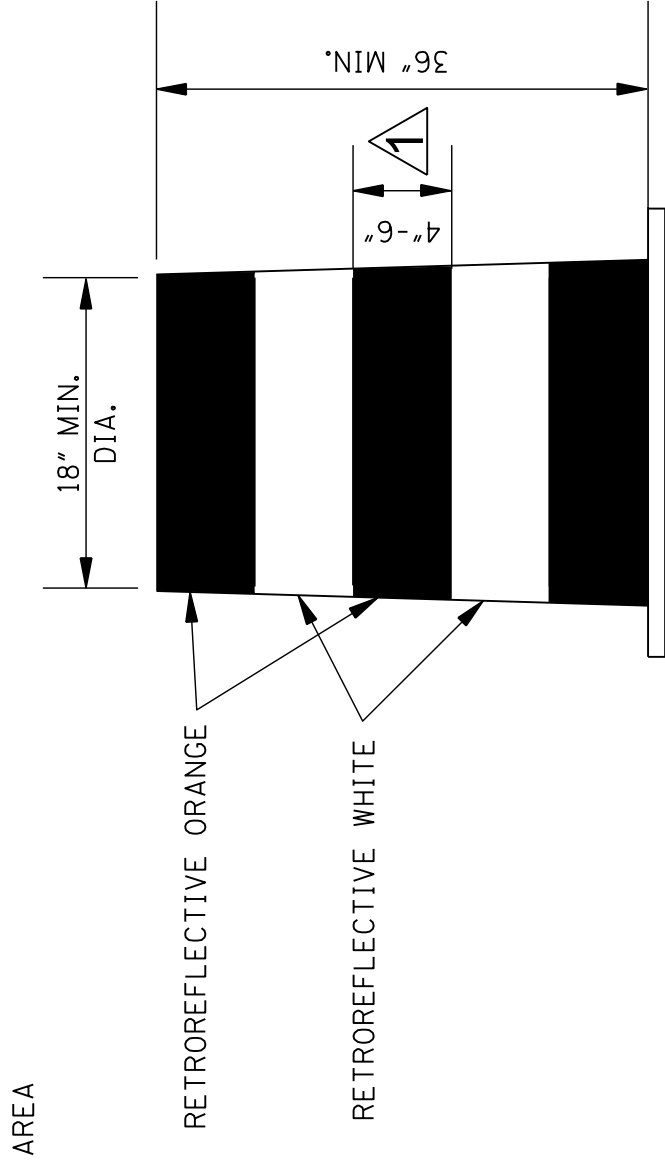


ROAD CLOSED

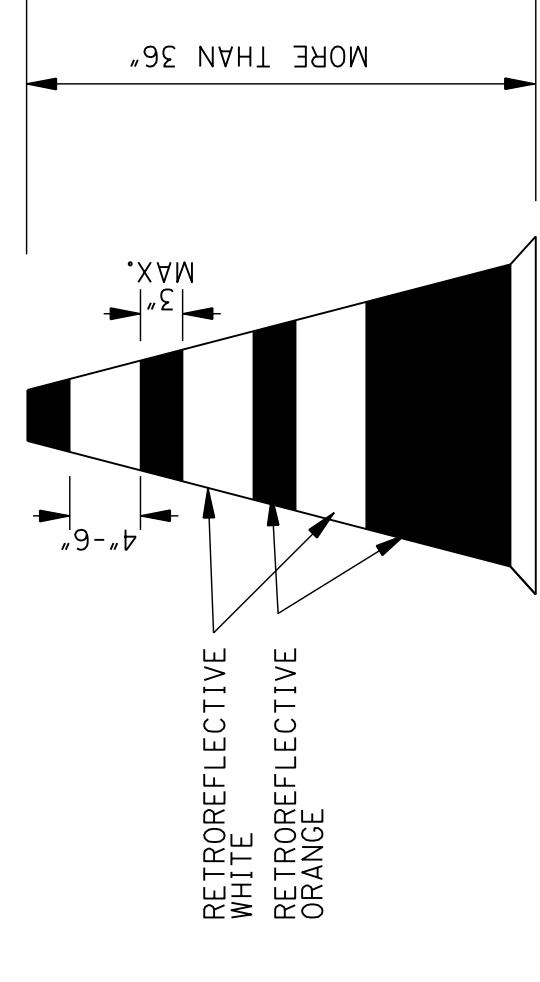
DETOUR



TUBULAR MARKER DETAIL

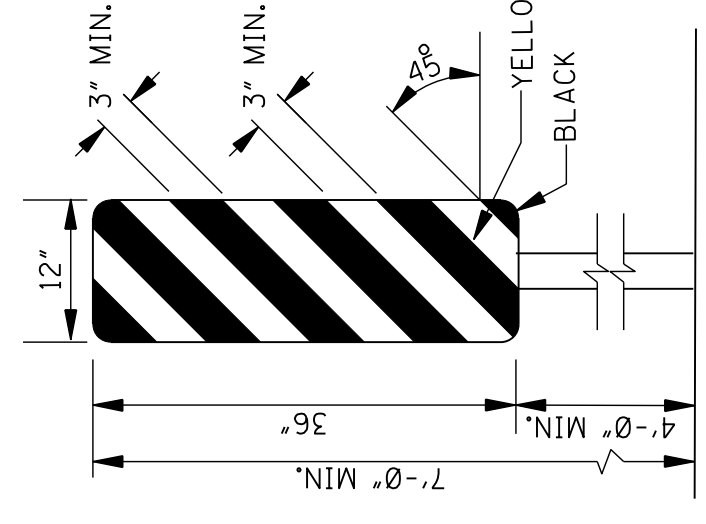


PLASTIC DRUM DETAIL



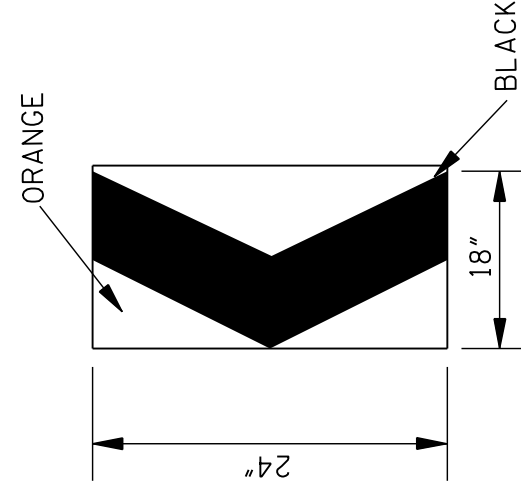
PLASTIC CONE DETAIL (36" OR GREATER)

TYPE 3 OBJECT MARKER (OM3-R)



1. TYPE 3 OBJECT MARKERS SHALL BE USED AT ALL EXPOSED BRIDGE ABUTMENTS AND AT OTHER LOCATIONS AS DEEMED NECESSARY BY THE ENGINEER.
2. THE OM3-R IS SHOWN. THE OM3-L IS SIMILAR EXCEPT THE STRIPES SLOPE DOWNWARD FROM THE UPPER LEFT SIDE TO THE LOWER RIGHT SIDE AND SHALL BE PLACED ON THE LEFT SIDE OF THE OBJECT.
3. THE INSIDE EDGE OF THE MARKER SHALL BE IN LINE WITH THE INNER EDGE OF THE OBSTRUCTION.
4. THE STRIPING SHALL CONSIST OF ALTERNATING BLACK AND RETROREFLECTIVE YELLOW SLOPING DOWNWARD TOWARD THE SIDE ON WHICH TRAFFIC IS THE PASS.

RETROREFLECTIVE CHEVRON ALIGNMENT SIGN DETAIL (W1-8)



1. A CHEVRON SIGN CONSISTS OF A BLACK CHEVRON TYPE MARKING ON AN ORANGE BACKGROUND AND SHALL POINT IN THE DIRECTION OF TRAFFIC FLOW.
2. THE CHEVRON SIGN SHALL BE MOUNTED ON CRASHWORTHY SUPPORT.
3. CHEVRON SIGNS MAY BE USED TO SUPPLEMENT OTHER STANDARD DEVICES WHERE ONE OR MORE LANES ARE CLOSED FOR CONSTRUCTION OR MAINTENANCE. THEY SHOULD BE PLACED APPROXIMATELY 2'-0" BEHIND THE LANE TRANSITION STRIPE.
4. CHEVRON SIGNS SHALL BE INSTALLED AT A MINIMUM HEIGHT OF 4 FEET MEASURED VERTICALLY FROM THE BOTTOM OF THE SIGN TO THE ELEVATION OF THE NEAR EDGE OF THE TRAVELWAY.

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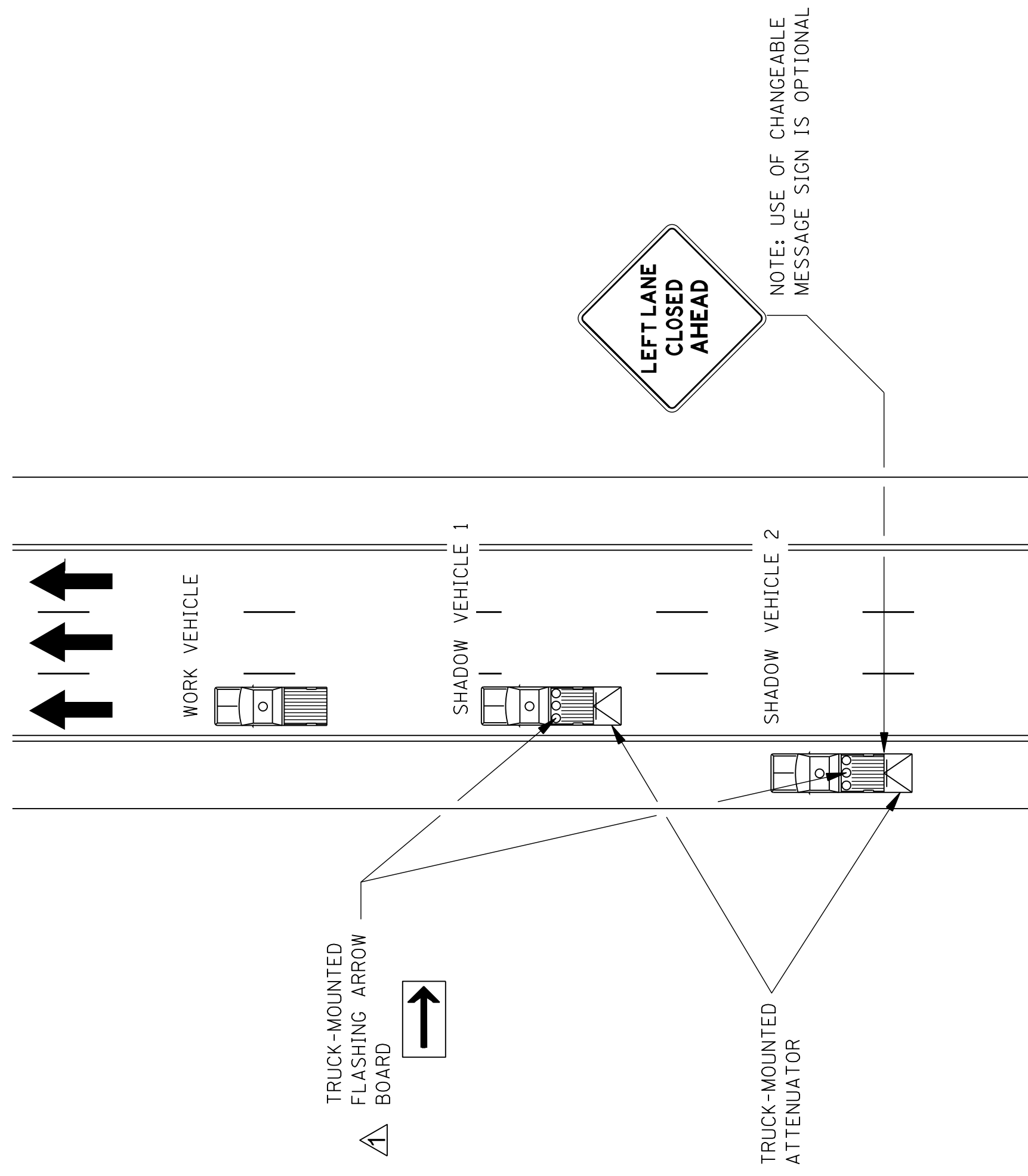
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WORKING NUMBER: TCP-8

SHEET NUMBER: 6358

HIGHWAY SIGN AND BARRICADE DETAILS FOR CONSTRUCTION PROJECTS

MOBILE OPERATIONS ON MULTILANE ROAD

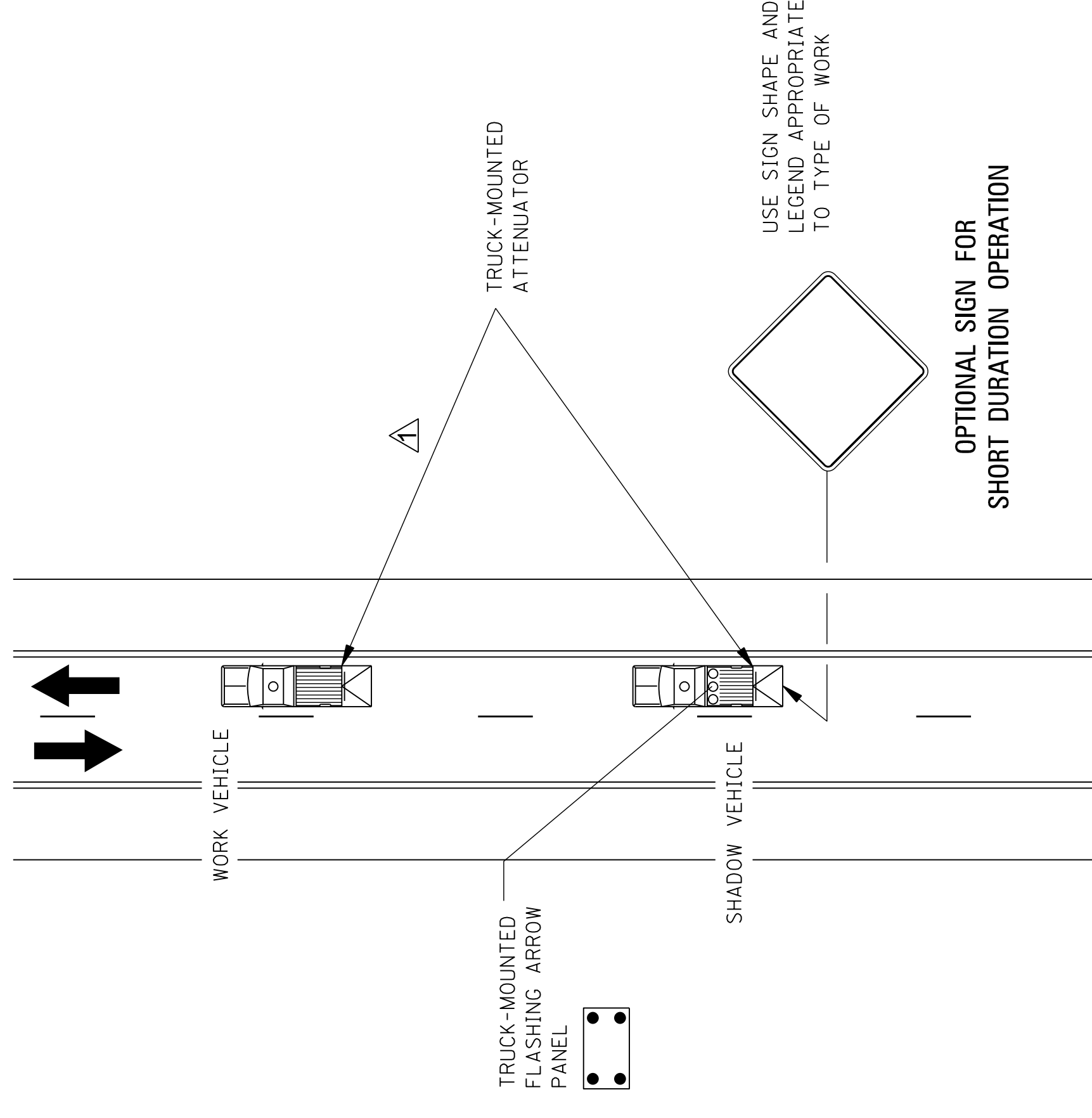


MOBILE OPERATIONS ON MULTILANE ROAD

NOTES FOR MULTILANE LANE OPERATION:

1. SHADOW AND WORK VEHICLES SHALL DISPLAY HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS. AN ARROW BOARD SHALL BE USED WHEN A FREEWAY LANE IS CLOSED. WHEN MORE THAN ONE FREEWAY LANE IS CLOSED, A SEPARATE ARROW BOARD SHALL BE USED FOR EACH CLOSED LANE.
2. SHADOW VEHICLE 2 SHOULD BE EQUIPPED WITH AN ARROW BOARD AND TRUCK MOUNTED ATTENUATOR (TMA). AN APPROPRIATE LANE CLOSURE SIGN SHOULD BE PLACED ON SHADOW VEHICLE 2 SO AS NOT TO OBSCURE THE ARROW BOARD.
3. SHADOW VEHICLE 1 SHOULD BE EQUIPPED WITH AN ARROW BOARD AND TRUCK-MOUNTED ATTENUATOR (TMA).
4. SHADOW VEHICLE 2 SHOULD TRAVEL AT A VARYING DISTANCE FROM THE WORK OPERATION SO AS TO PROVIDE ADEQUATE SIGHT DISTANCE FOR TRAFFIC APPROACHING FROM THE REAR.
5. THE SPACING BETWEEN THE WORK VEHICLES AND THE SHADOW VEHICLES, AND BETWEEN EACH SHADOW VEHICLE, SHOULD BE MINIMIZED TO DETERMINE ROAD USERS FROM DRIVING IN BETWEEN.
6. ON HIGH-SPEED ROADWAYS, A THIRD SHADOW VEHICLE (NOT SHOWN) MAY BE USED WITH SHADOW VEHICLE 1 IN THE CLOSED LANE, SHADOW VEHICLE 2 STRADDLING THE EDGE LINE, AND SHADOW VEHICLE 3 ON THE SHOULDER.
7. ARROW BOARD SHALL BE AS A MINIMUM TYPE B, 60" X 30" IN ACCORDANCE WITH THE CRITERIA PRESENTED IN THE MUTCD.
8. WORK SHOULD NORMALLY BE DONE DURING OFF-PEAK HOURS.
9. VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBSCURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
10. WHEN THE WORK VEHICLE OCCUPIES AN INTERIOR LANE (A LANE OTHER THAN THE FAR RIGHT OR FAR LEFT) OF A DIRECTIONAL ROADWAY HAVING A RIGHT-HAND SHOULDER, 10 FEET OR MORE IN WIDTH, SHADOW VEHICLE 2 SHOULD DRIVE ON THE RIGHT-HAND SHOULDER WITH A SIGN INDICATING THAT WORK IS TAKING PLACE IN THE INTERIOR LANE.
11. ON HIGH-SPEED ROADWAYS, A THIRD SHADOW VEHICLE (NOT SHOWN) MAY BE USED WITH SHADOW VEHICLE 1 IN THE CLOSED LANE, SHADOW VEHICLE 2 STRADDLING THE EDGE LINE, AND SHADOW VEHICLE 3 ON THE SHOULDER.
12. WHERE ADEQUATE SHOULDER WIDTH IS NOT AVAILABLE, SHADOW VEHICLE 3 MAY ALSO STRADDLE THE EDGE LINE.
13. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

MOBILE OPERATIONS ON TWO-LANE ROAD



MOBILE OPERATIONS ON TWO-LANE ROAD

NOTES FOR TWO-LANE OPERATION:

1. WHERE PRACTICAL AND WHEN NEEDED, THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS. IF THIS CAN NOT BE DONE FREQUENTLY, AS AN ALTERNATIVE, A "DO NOT PASS" SIGN MAY BE PLACED ON THE REAR OF THE VEHICLE BLOCKING THE LANE.
2. THE DISTANCE BETWEEN THE WORK AND SHADOW VEHICLES MAY VARY ACCORDING TO TERRAIN, PAINT DRYING TIME, AND OTHER FACTORS. SHADOW VEHICLES ARE USED TO WARN TRAFFIC OF THE OPERATION AHEAD. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, THE SHADOW VEHICLE SHOULD MAINTAIN THE MINIMUM DISTANCE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. THE SHADOW VEHICLE SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
3. ADDITIONAL SHADOW VEHICLES TO WARN AND REDUCE THE SPEED OF ONCOMING OR OPPOSING TRAFFIC MAY BE USED. POLICE PATROL CARS MAY BE USED FOR THIS PURPOSE.
4. A TRUCK-MOUNTED ATTENUATOR (TMA) SHOULD BE USED ON THE SHADOW VEHICLE AND MAY BE USED ON THE WORK VEHICLE.
5. THE WORK VEHICLE SHALL BE EQUIPPED WITH BEACONS, AND THE SHADOW VEHICLES SHALL BE EQUIPPED WITH TWO HIGH-INTENSITY FLASHING LIGHTS MOUNTED ON THE REAR, ADJACENT TO THE SIGN. SHADOW AND WORK VEHICLES SHALL DISPLAY FLASHING OR ROTATING BEACONS BOTH FORWARD AND TO THE REAR.
6. VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBSCURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
7. ARROW BOARD TO BE USED IN CAUTION MODE.
8. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

DATE	BY	REVISION
12/1/25	STN	REVISED TEXT AND DETAILS.

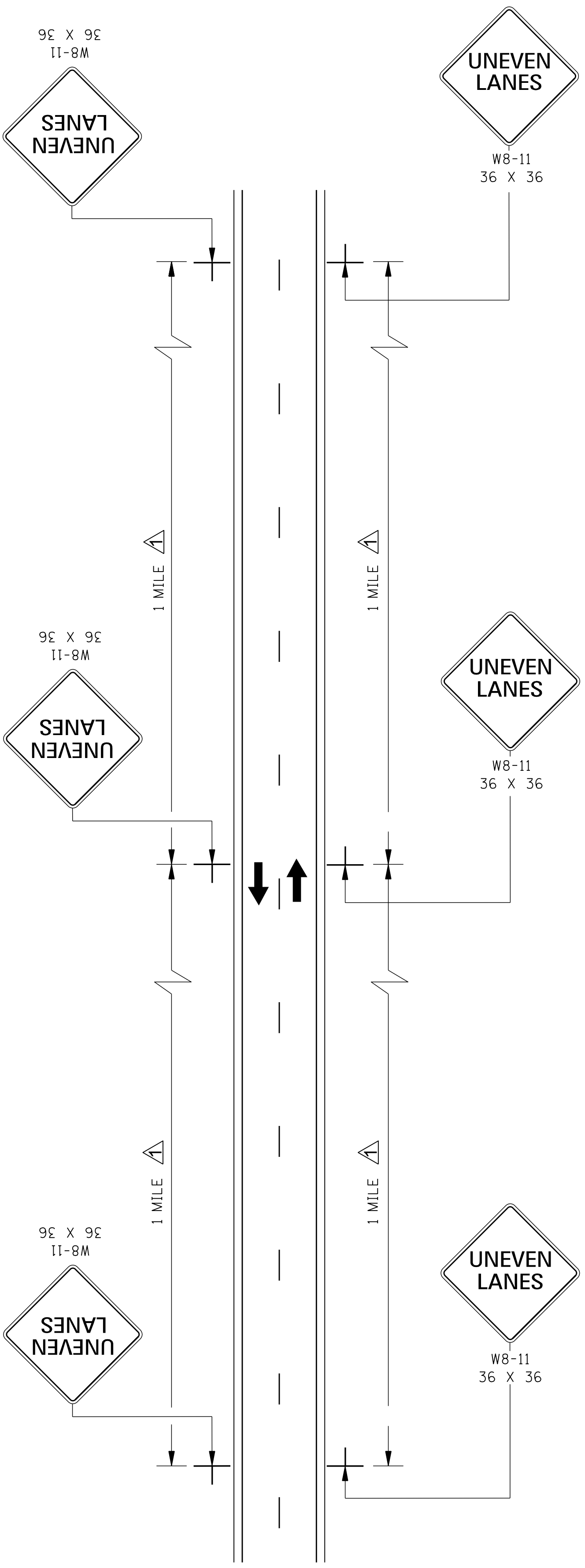
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**TRAFFIC CONTROL PLAN
MOBILE OPERATIONS
MULTILANE ROADS
AND
TWO-LANE ROADS**

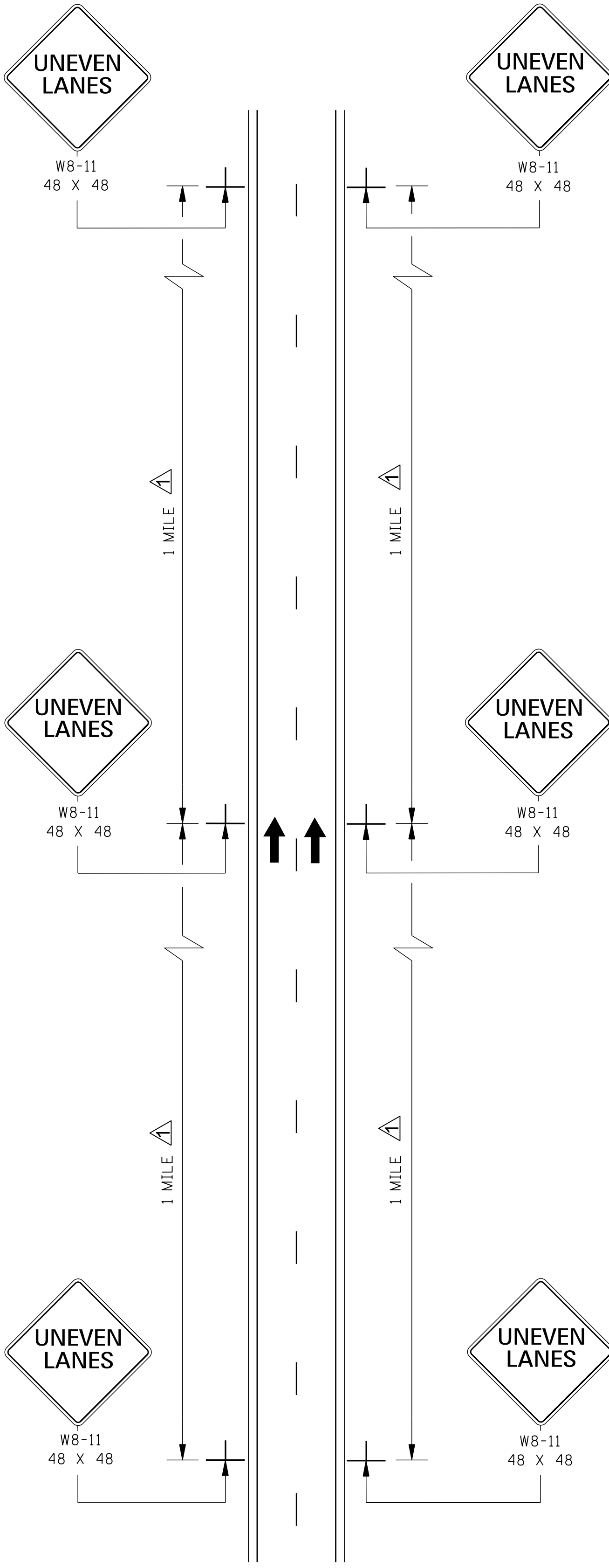
ISSUE DATE: AUGUST 01, 2017

WORKING NUMBER
TCP-9

SHEET NUMBER
6359



TWO-WAY TRAFFIC



ONE-WAY TRAFFIC

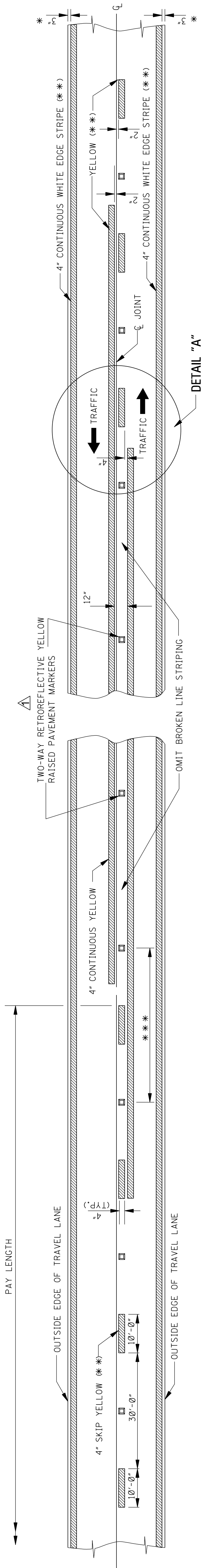
GENERAL NOTES:

1. UNEVEN LANE LINE:
 - A. IF LESS THAN OR EQUAL TO 1/2", NO SIGNS REQUIRED.
 - B. IF GREATER THAN 1/2" AND LESS THAN OR EQUAL TO 2/4", PLACE SIGNS AS SHOWN ON THIS SHEET.
 - C. IF GREATER THAN 2/4", TRAFFIC SHOULD NOT BE ALLOWED TO CROSS UNEVEN LANE LINE.
2. THE W8-11 SIGNS SHOULD BE SPACED AT 1 MILE INTERVALS THROUGHOUT UNEVEN LANE LINE LIMITS.
3. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.
4. SIGNS SHALL BE RETROREFLECTIVE WITH ORANGE BACKGROUND/BLACK COPY.

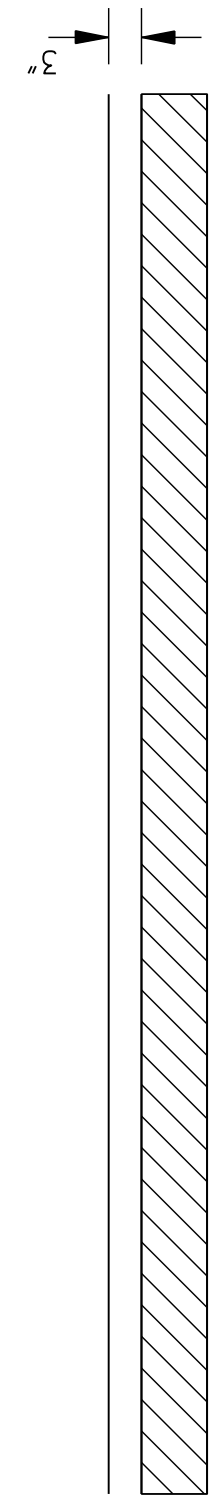
DATE	BY	REVISION
12/11/25	STN	REVISED TEXT

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

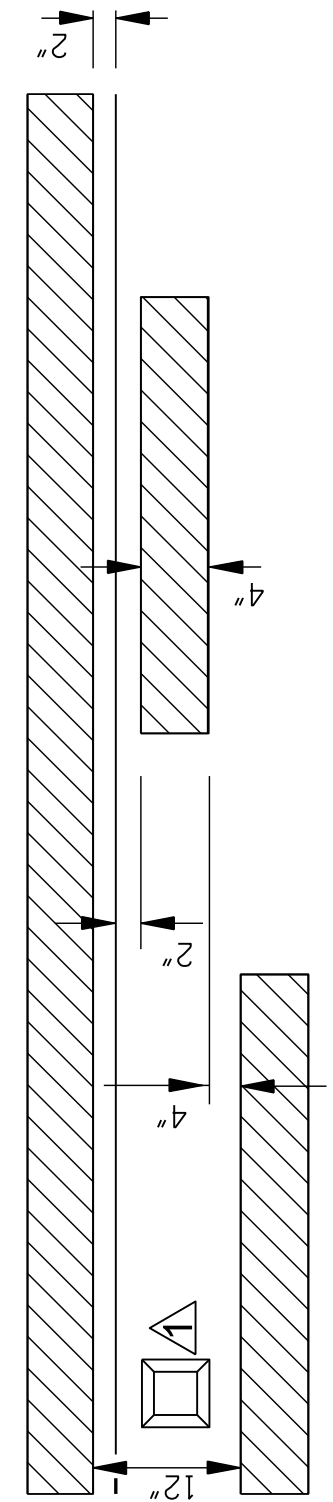
**TRAFFIC CONTROL PLANS
UNEVEN PAVEMENT
DETAILS**



TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)

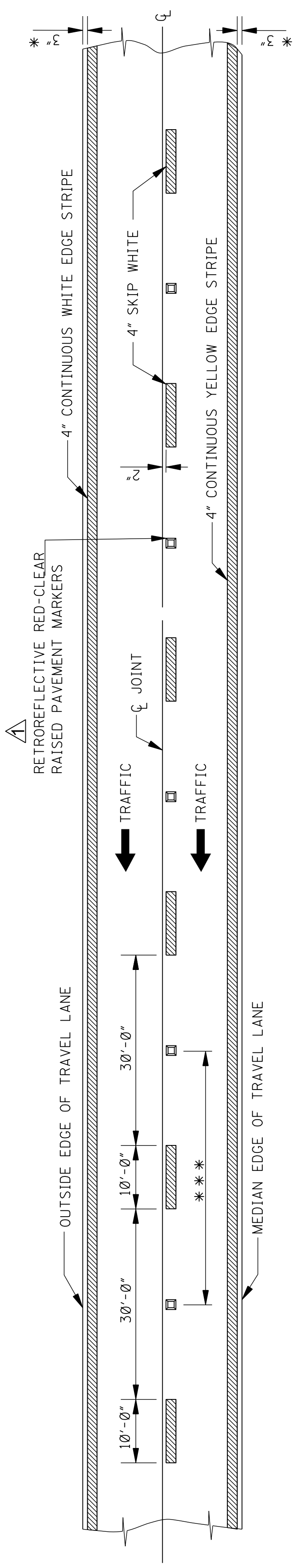


TRAFFIC



TRAFFIC

DETAIL "A"



4-LANE WITH ONE-WAY TRAFFIC



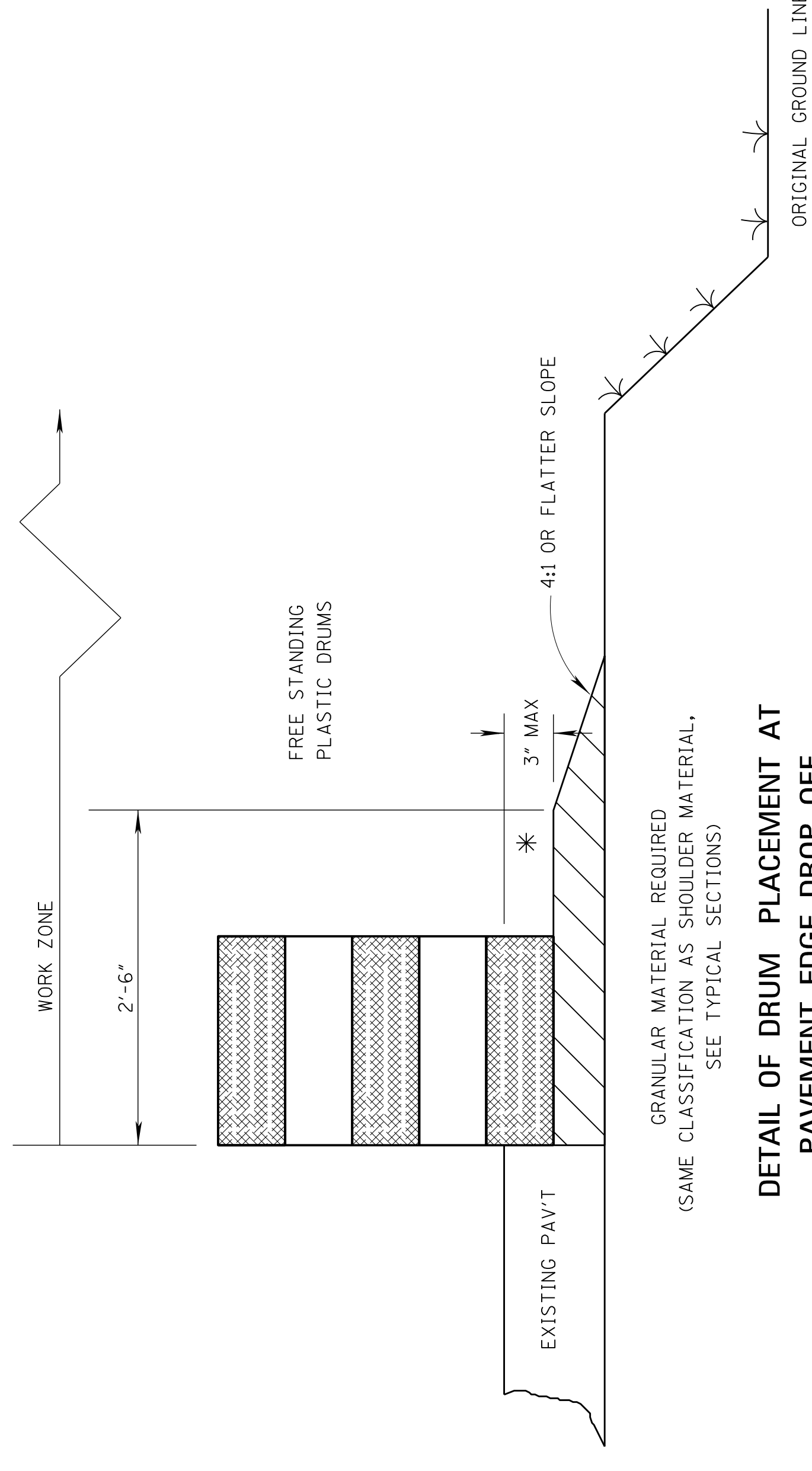
- GENERAL NOTES:
- * 1. 3" UNLESS SHOWN ELSEWHERE ON THE PLANS.
 - ** 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR TAPE AS INDICATED IN PAY ITEMS).
 - ▲ 3. RETROREFLECTIVE RAISED PAVEMENT MARKERS TO BE USED IF TEMPORARY MARKINGS ARE TO REMAIN IN PLACE OVER 3 MONTHS
 - ▲*** 4. SPACING OF RETROREFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

TANGENT SECTIONS	URBAN AREA (ft-in)	RURAL AREA (ft-in)
HORIZONTAL CURVES	40'-0"	50'-0"
INTERCHANGE LIMITS	40'-0"	+ 40'-0"

- ▲† NOTE: ON THE MAIN FACILITY, RETROREFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINES THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
- ▲ 5. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RETROREFLECTIVE RAISED PAVEMENT MARKERS FROM THE MDOT "APPROVED PRODUCTS LIST".

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**TEMPORARY STRIPING FOR
2-LANE AND 4-LANE
DIVIDED HIGHWAYS**



DETAIL OF DRUM PLACEMENT AT PAVEMENT EDGE DROP-OFF

GRANULAR MATERIAL REQUIRED (SAME CLASSIFICATION AS SHOULDER MATERIAL, SEE TYPICAL SECTIONS)

NOTES:

* A. PAVEMENT EDGE DROP-OFF

1. IF LESS THAN TWO AND ONE QUARTER (2.25) INCHES-NO PROTECTION REQUIRED. PLACE A SHOULDER WORK SIGN (W21-5) 500 FEET IN ADVANCE OF WORK ZONE SHOULDER AND A LOW SHOULDER SIGN (W8-9) AT THE BEGINNING AND THROUGHOUT THE WORK ZONE @ (750'±O.C.).
2. TWO AND ONE QUARTER TO THREE INCHES-PLACE DRUMS, VERTICAL PANELS OR BARRICADES EVERY 100 FEET ON TANGENT SECTIONS FOR SPEEDS OF 50 MILES PER HOUR OR GREATER. CONES MAY BE USED IN PLACE OF DRUMS, PANELS, AND BARRICADES DURING DAYLIGHT HOURS. FOR TANGENT SECTIONS WITH SPEEDS LESS THAN 50 MILES PER HOUR AND FOR CURVES, DEVICES SHOULD BE PLACED EVERY 50 FEET. SPACING FOR TAPERS SHOULD BE IN ACCORDANCE WITH THE MUTCD (1 / 3 L, WHERE L IS THE TAPER LENGTH IN FEET.)

3. GREATER THAN THREE (3) INCHES-POSITIVE SEPARATION OR WEDGE WITH 4:1 OR FLATTER SLOPE NEEDED. IF THERE IS EIGHT (8) FEET OR MORE DISTANCE BETWEEN THE EDGE OF TRAVEL LANE AND DROP-OFF, THEN DRUMS, PANELS OR BARRICADES MAY BE USED.
4. FOR TEMPORARY CONDITIONS, DROP-OFFS GREATER THAN THREE (3) INCHES MAY BE PROTECTED WITH DRUMS, VERTICAL PANELS OR BARRICADES FOR SHORT DISTANCES DURING DAYLIGHT HOURS WHILE WORK IS BEING DONE IN THE DROP-OFF AREA.
5. LESSER TREATMENTS THAN THOSE DESCRIBED ABOVE MAY BE CONSIDERED FOR LOW-VOLUME LOCAL STREETS.

△ B. DRUM, VERTICAL PANEL, OR BARRICADE SPACING

1. TANGENTS = $2 \times L / S$
2. TAPERS = $L / 3$

WHERE $L = S \times W$

L = TAPER LENGTH IN FEET

S = SPEED IN MPH (POSTED OR ANTICIPATED OPERATING SPEED)

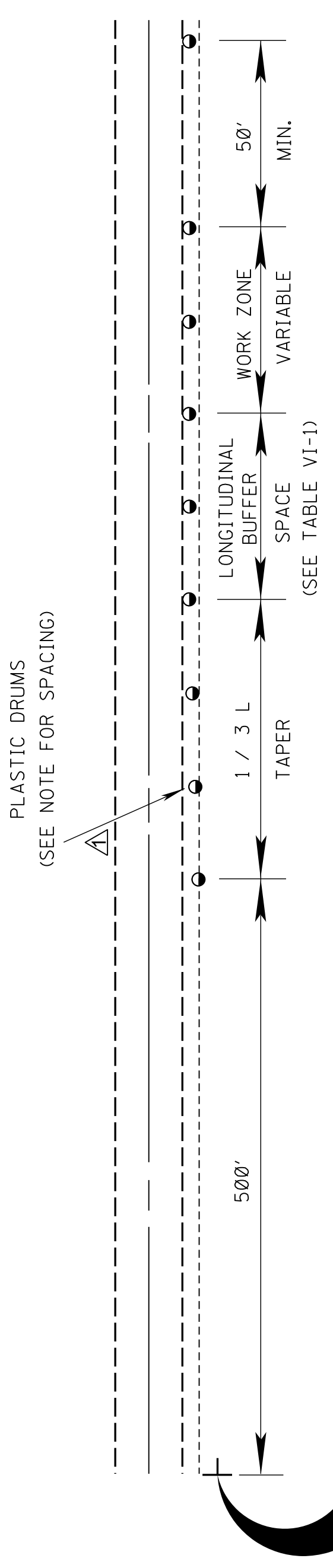
W = WIDTH OF OFFSET IN FEET

C. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.

TABLE VI-1. GUIDELINES FOR LENGTH OF LONGITUDINAL BUFFER SPACE

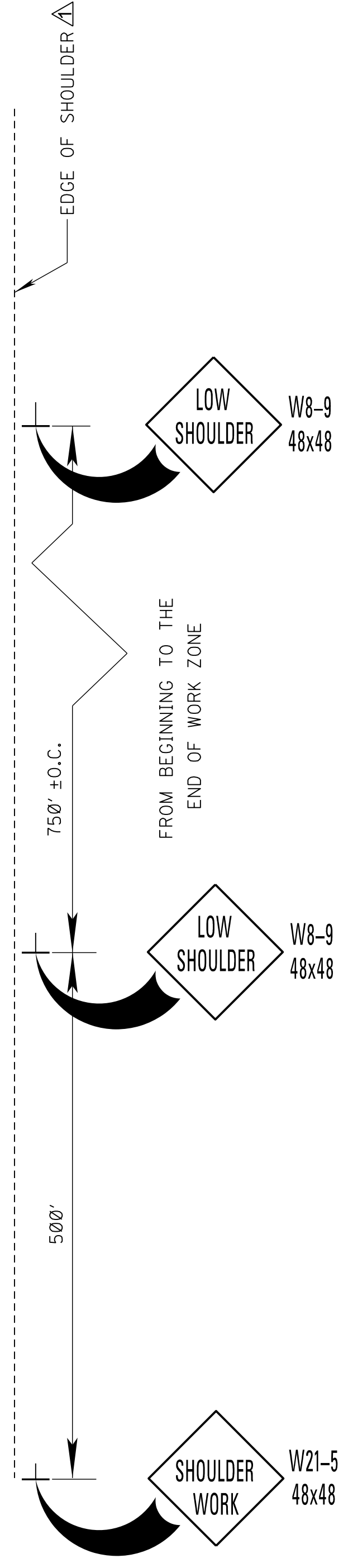
* SPEED (MPH)	LENGTH (FEET)
20	115
25	155
30	200
35	250
40	305
45	360
50	425
55	495
60	570
65	645

* * POSTED SPEED, OFF-PEAK 85 PERCENTILE SPEED PRIOR TO WORK STARTING, OR THE ANTICIPATED OPERATING SPEED IN MPH.

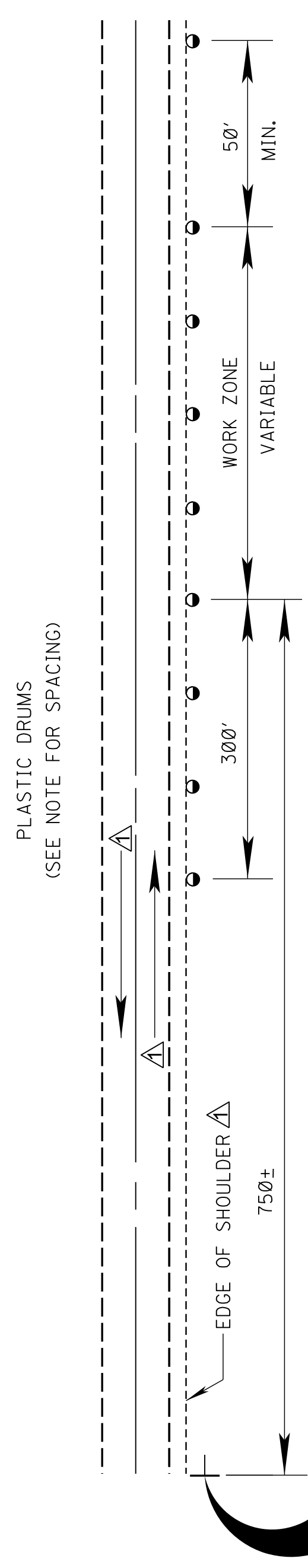


TYPICAL SHOULDER CLOSURE

- (1) TO BE USED WITH EIGHT (8) FOOT OR GREATER WIDTH IMPROVED SHOULDER.
- (2) TO BE USED WHEN CONSTRUCTION VEHICLES (EQUIPMENT) ENCROACHES ON OR WITHIN TWO (2) FEET OF THE SHOULDER BREAK.
- (3) FOR SHORT DURATION OPERATIONS OF 60 MINUTES OR LESS, ALL SIGNS AND CHANNELIZING DEVICES MAY BE ELIMINATED IF A VEHICLE WITH ACTIVATED HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS IS USED.

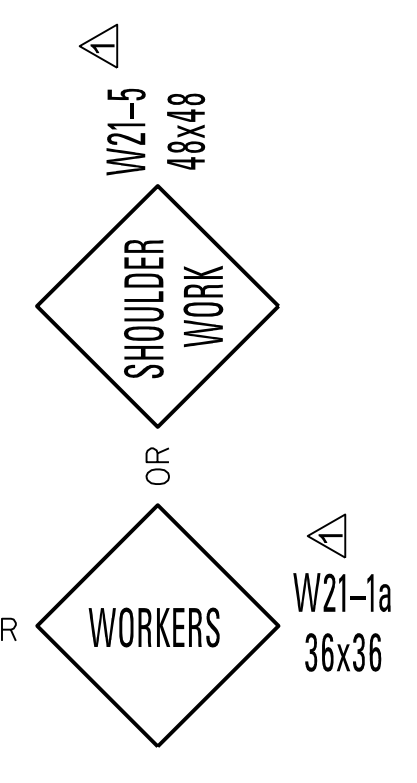


TYPICAL SHOULDER WORK #1 (SEE NOTE A-1 THIS SHEET)



TYPICAL SHOULDER WORK #2

NOTE: WORK OUTSIDE TWO (2) FOOT AND WITHIN TEN (10) FEET OF THE SHOULDER BREAK MAY BE PROTECTED BY PLACING DRUMS ALONG THE SHOULDER EDGE, 300 FEET PRIOR TO AND 50 FEET BEYOND THE WORK AREA, OR SEE NOTE A-3 THIS SHEET.



DATE	BY	REVISION
12/1/25	STN	REVISED TEXT

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**TRAFFIC CONTROL DETAILS
DRUM PLACEMENT
SHOULDER CLOSURE**

MDOT
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
WORKING NUMBER
TCP-16
SHEET NUMBER
6366

ISSUE DATE: AUGUST 01, 2017

ADDENDUM

STATE	PROJECT NO.
MISS.	STBC-0400-00(031)

WORKING SHEET NUMBER NUMBER

DESCRIPTION OF SHEETS

DETAILED INDEX (BRIDGE)	DI-BR-1	8001
SUMMARY OF QUANTITIES (BRIDGE)	SO-BR-1	8002
SR 465 OVER MUDDY BAYOU ~ BRIDGE AT STA. 417+44.47		
GENERAL NOTES AND ESTIMATED QUANTITIES	1 OF 20	8003
SR 465 OVER MUDDY BAYOU LAYOUT	2 OF 20	8004
SR 465 OVER MUDDY BAYOU FOUNDATION PLAN	3 OF 20	8005
END BENT NO. 1 DETAILS	4 OF 20	8006
END BENT NO. 6 DETAILS	5 OF 20	8007
ADDITIONAL END BENT DETAILS	6 OF 20	8008
INTERMEDIATE BENT NO. 2 DETAILS	7 OF 20	8009
INTERMEDIATE BENT NO. 3 DETAILS	8 OF 20	8010
INTERMEDIATE BENT NO. 4 DETAILS	9 OF 20	8011
INTERMEDIATE BENT NO. 5 DETAILS	10 OF 20	8012
ADDITIONAL INTERMEDIATE BENT AND STEEL PIPE PILE DETAILS	11 OF 20	8013
PLAN OF SPANS NO. 1 - 3	12 OF 20	8014
PLAN OF SPANS NO. 4 & 5	13 OF 20	8015
80'-0" & 100'-0" SPAN DETAILS (FIB-36)	14 OF 20	8016
ADDITIONAL SPAN DETAILS	15 OF 20	8017
MISCELLANEOUS SPAN DETAILS	16 OF 20	8018
80'-0" BEAM NO. 80-1 DETAILS (FIB-36)	17 OF 20	8019
100'-0" BEAM NO. 100-1 DETAILS (FIB-36)	18 OF 20	8020
45° L.F. SKEWED BEAM END DETAILS	19 OF 20	8021
BEARING PAD DETAILS	20 OF 20	8022
BILL OF REINFORCEMENT SHEETS		
BILL OF REINFORCEMENT (1)	BOR-1	8023
BILL OF REINFORCEMENT (2)	BOR-2	8024
BILL OF REINFORCEMENT (3)	BOR-3	8025
BILL OF REINFORCEMENT (4)	BOR-4	8026
GENERALIZED SOIL PROFILE SHEETS		
GENERALIZED SOIL PROFILE SR 465 OVER MUDDY BAYOU	GSP-1	8027
GENERALIZED SOIL PROFILE SR 465 OVER MUDDY BAYOU	GSP-2	8028
BRIDGE EROSION CONTROL PLANS		
EROSION CONTROL ELEVATION	ECBR-1	8029
EROSION CONTROL PLAN	ECBR-2	8030
BRIDGE STANDARD SHEETS		
3'-0" RAILING DETAILS (SLOTTED DRAINS)	RD-36	8031

BRIDGE DIVISION		
REVISIONS		
DATE	SHEET NO.	BY
10/16/2025	8012, 8013, 8015, 8016	JAF

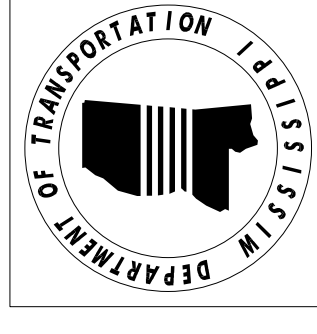
BY	REVISION

FMS: 106110 / 301000
COUNTY: WARREN
PROJECT NUMBER: STBC-0400-00(031)

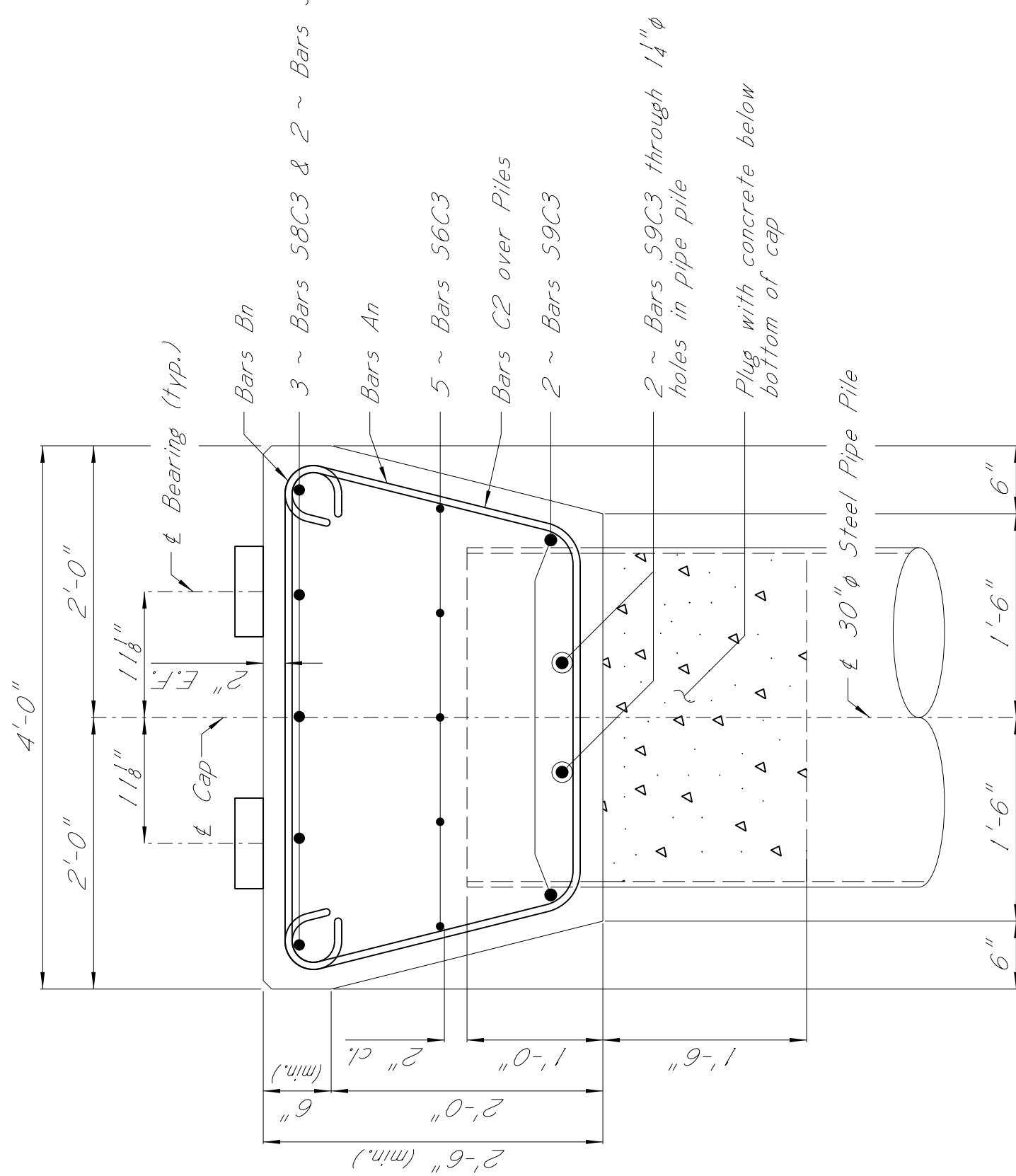
DESIGNER: Jacob Erass
DETAILER: Jacob Erass
CHECKER: Barrett Gammal
ISSUE DATE: 11/26/2025
PROJECT NO.: STBC-0400-00(031)
PROJECT NAME: MISSISSIPPI DEPARTMENT OF TRANSPORTATION
BRIDGE NUMBER: WARREN BRIDGE

WORKING NUMBER
DI-BR-1
SHEET NUMBER
8001

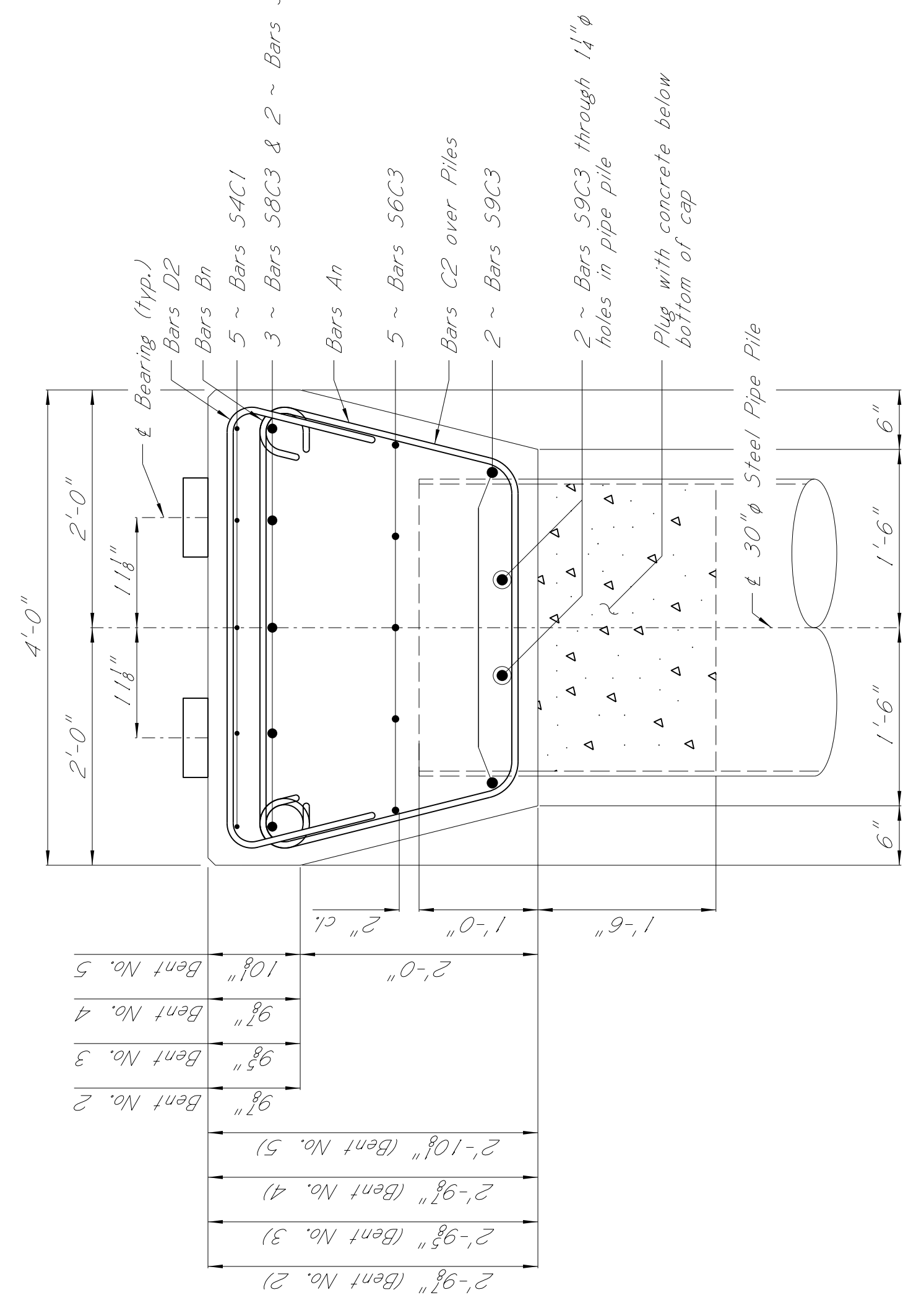
DETAILED INDEX (BRIDGE)



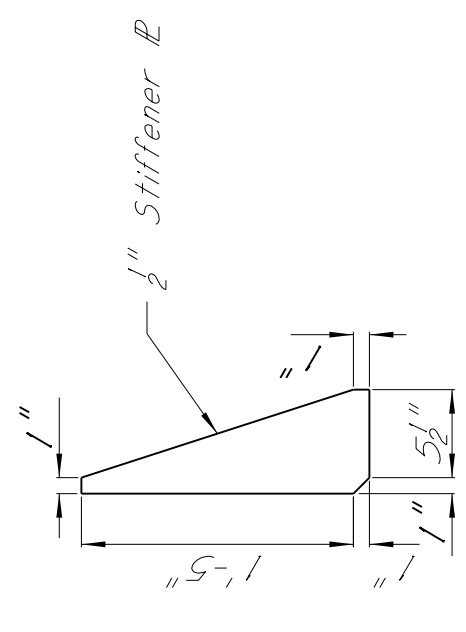
MISSISSIPPI DEPARTMENT OF TRANSPORTATION



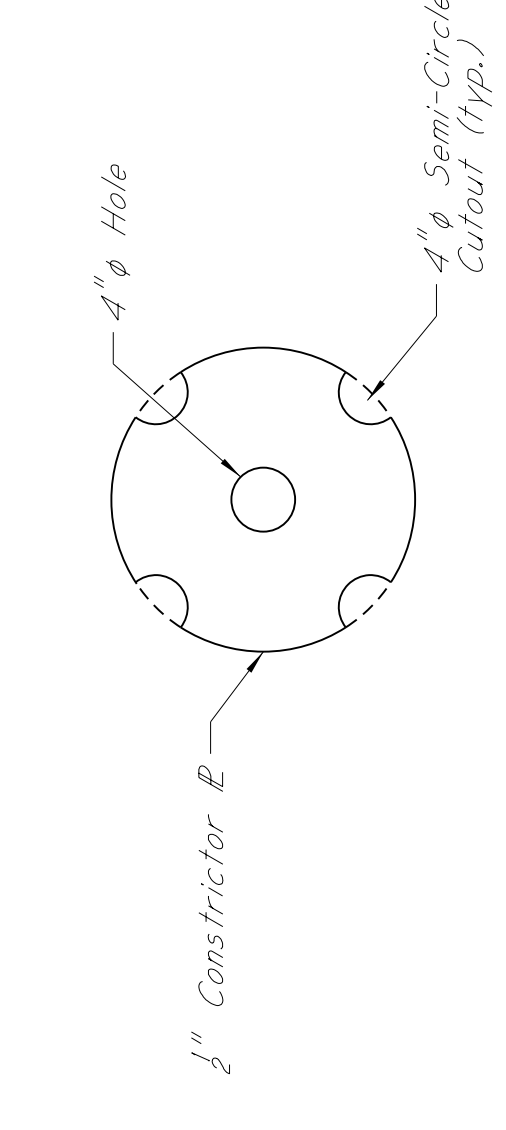
SECTION A-A



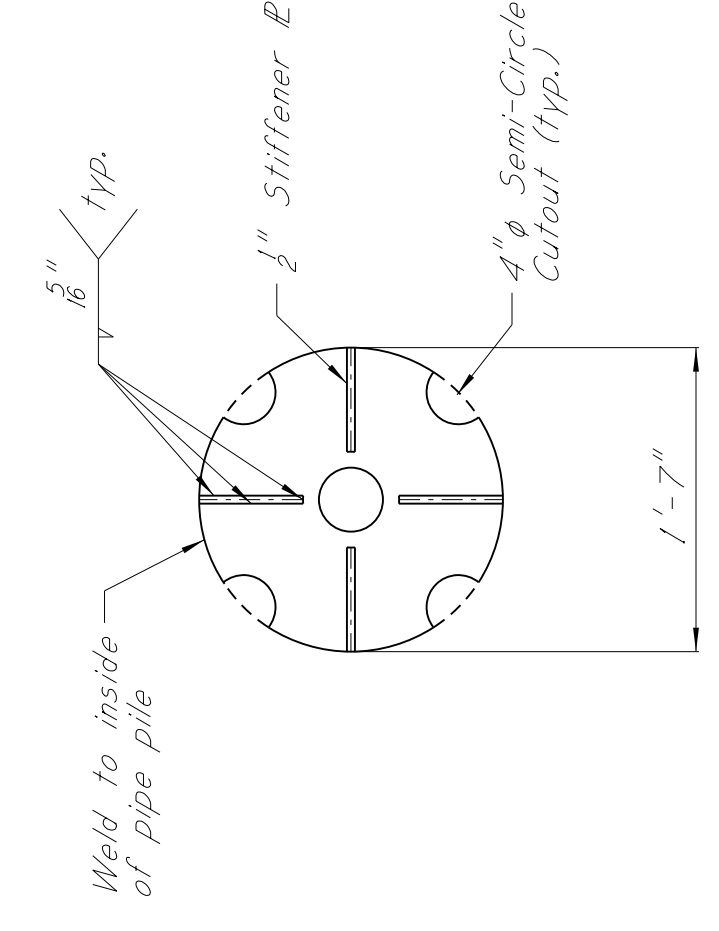
SECTION B-B



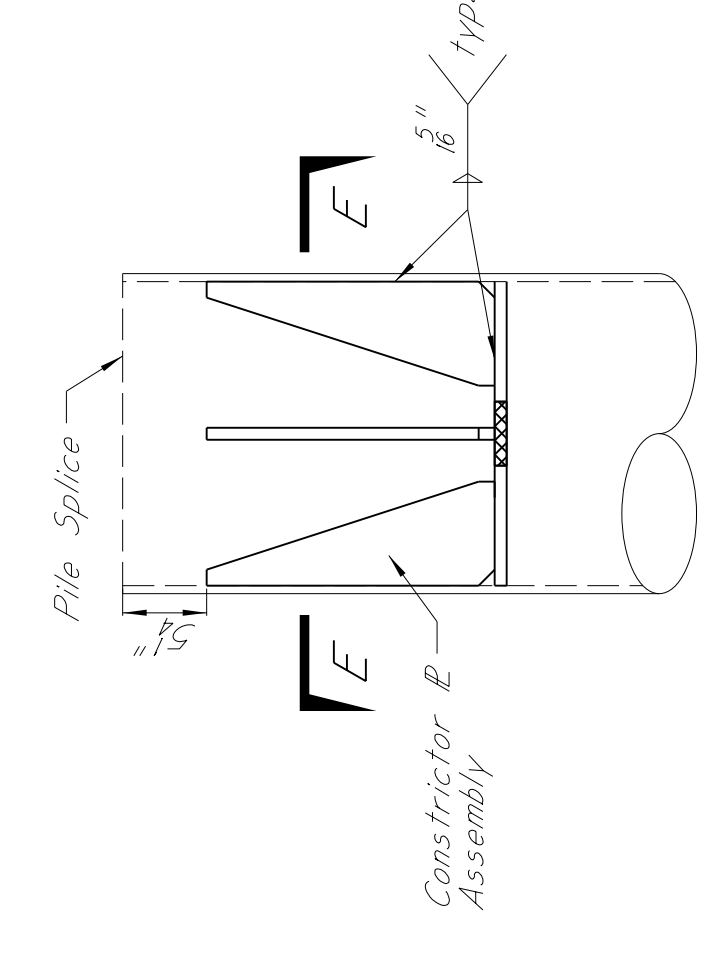
STIFFENER & DETAIL
20" ϕ Pipe Pile (End Bents No. 1 & 6)



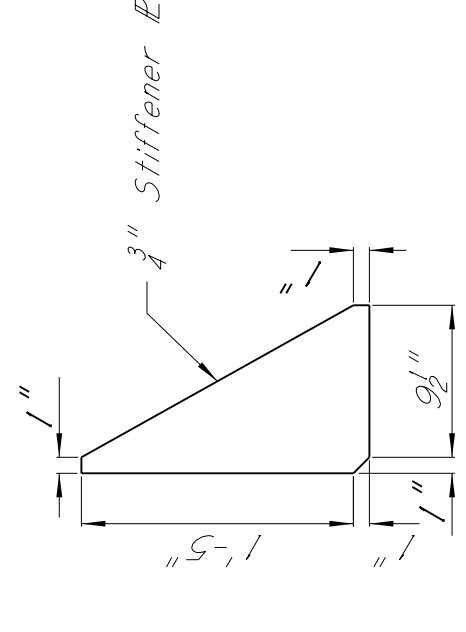
CONSTRUCTOR & DETAIL
20" ϕ Pipe Pile (End Bents No. 1 & 6)



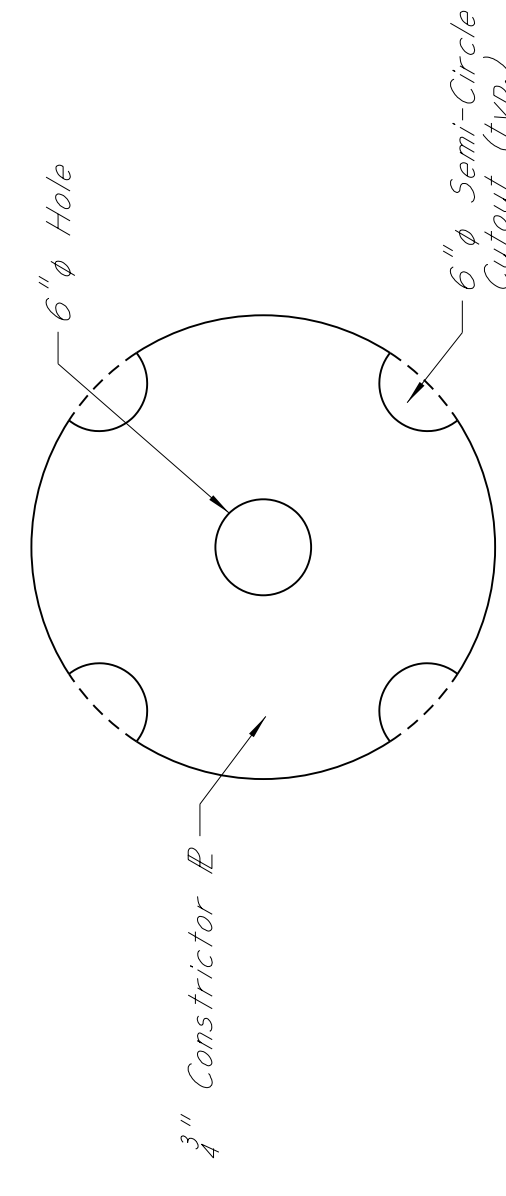
SECTION E-E
20" ϕ Pipe Pile (End Bents No. 1 & 6)



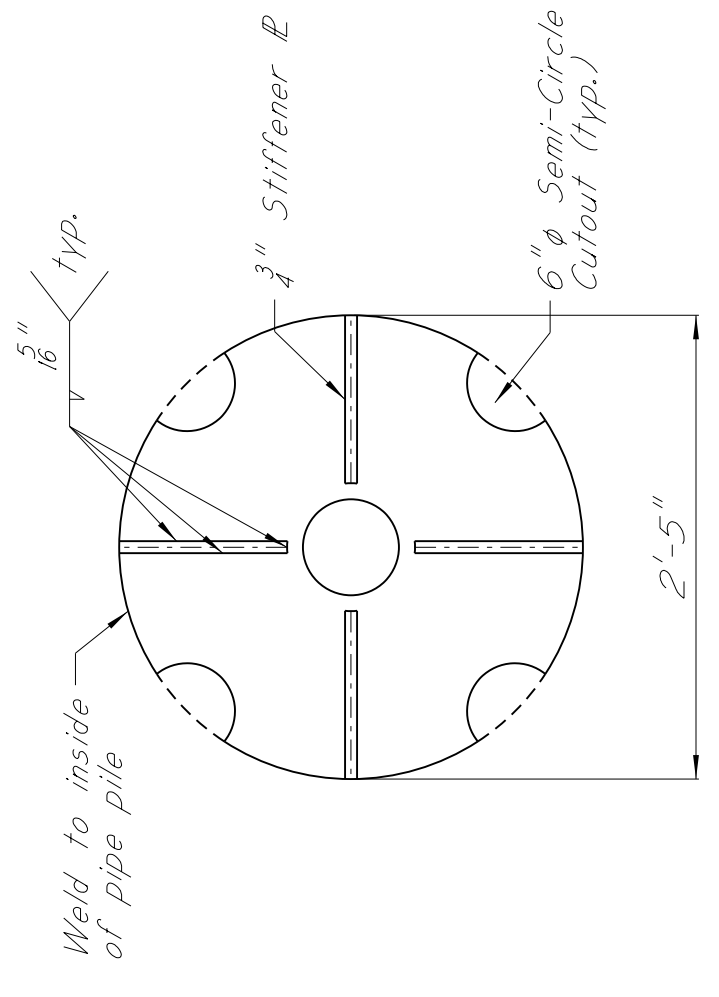
CONSTRUCTOR & ASSEMBLY DETAIL
Bottom Section of 20" ϕ Pipe Pile
(End Bents No. 1 & 6)



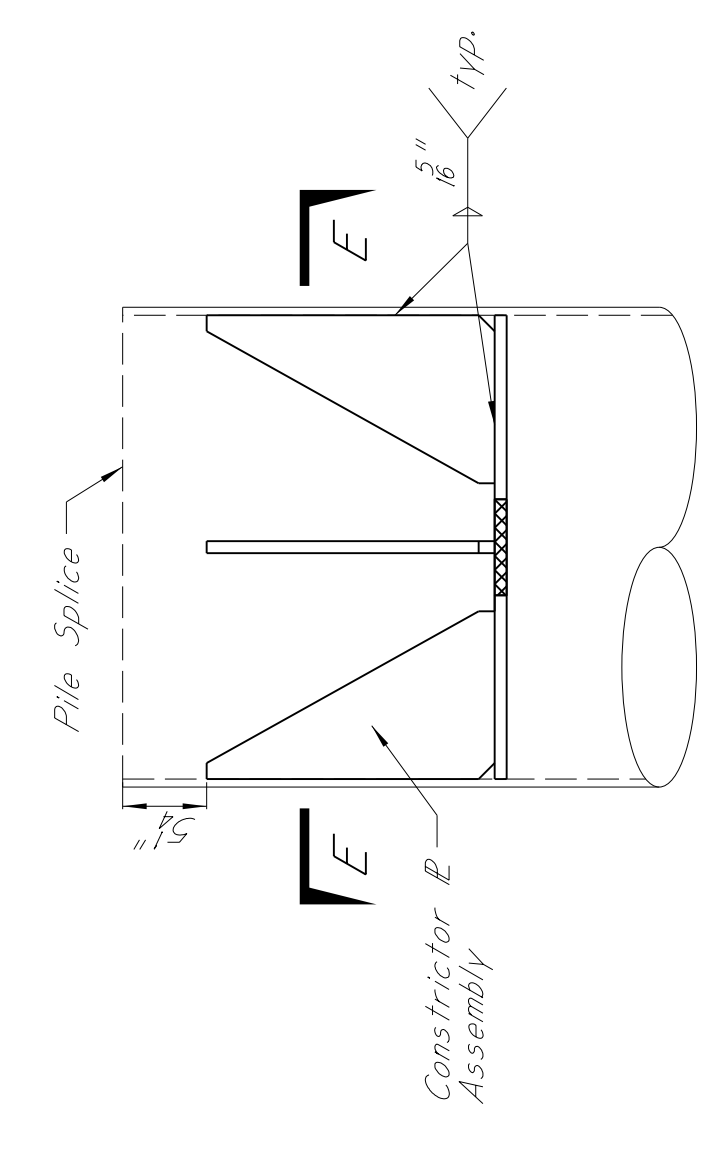
STIFFENER & DETAIL
30" ϕ Pipe Pile (Int. Bents No. 2 - 5)



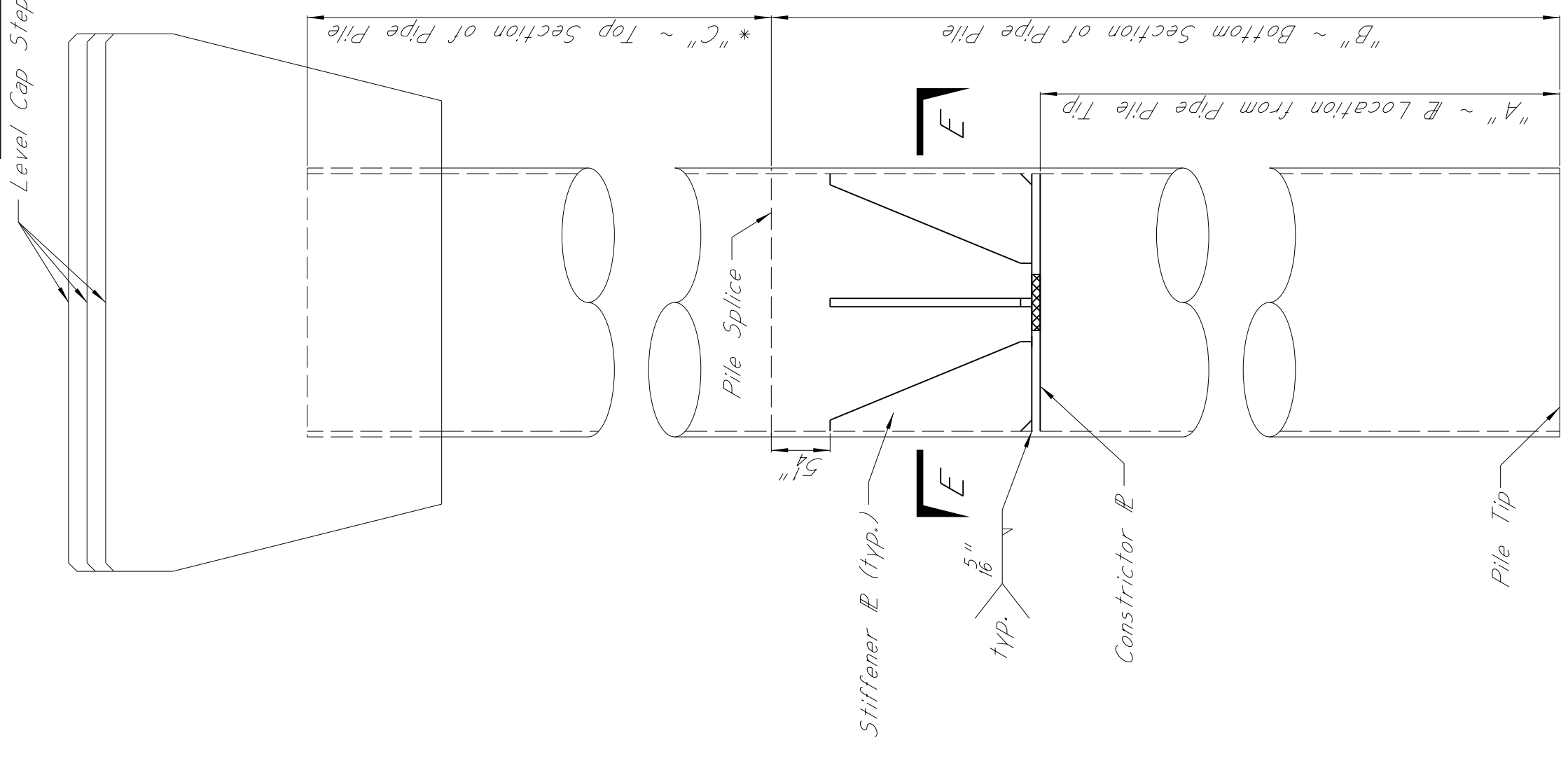
CONSTRUCTOR & DETAIL
30" ϕ Pipe Pile (Int. Bents No. 2 - 5)



SECTION E-E
30" ϕ Pipe Pile (Int. Bents No. 2 - 5)



CONSTRUCTOR & ASSEMBLY DETAIL
Bottom Section of 30" ϕ Pipe Pile
(Int. Bents No. 2 - 5)



ELEVATION OF PIPE PILE
N.T.S.

CONSTRUCTOR & DETAILS			
BENT	"A"	"B"	"C"
1	58'-0"	60'-0"	65'-0"
2	58'-0"	60'-0"	70'-0"
3	68'-0"	70'-0"	70'-0"
4	68'-0"	70'-0"	70'-0"
5	68'-0"	70'-0"	70'-0"
Out-of-Position	68'-0"	70'-0"	70'-0"
6	68'-0"	70'-0"	80'-0"

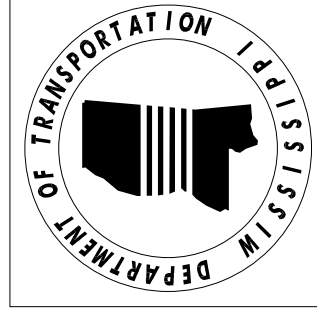
NOTE:
All concrete in bent caps shall be Class "AA" Chamfer all edges, unless otherwise stated. Dimensions from reinforcing steel to concrete faces are clear dimensions.

CONSTRUCTOR PLATE NOTE:
Constructor plates and stiffeners shall be shop welded to the inside of all test and production piles.

NOTE:
All material required for the constructor plate assemblies installed at Bents 1 & 6 shall be considered absolute under the 2015 Steel Piling, Wall Thickness 0.500" pay item. All material required for the constructor plate assemblies at Bents 2 - 5 shall be considered absolute under the 30" Steel Pipe Piling, Wall Thickness 0.500" pay item.

*NOTE:
An additional 20'-0" of pile is added for drivability purposes and is not quantified for under the piling pay items. Actual pile lengths will be provided based on the FDA test piles.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
BRIDGE AT STA. 417+44.47
ADDITIONAL INTERMEDIATE BENT
AND STEEL PIPE PILE DETAILS

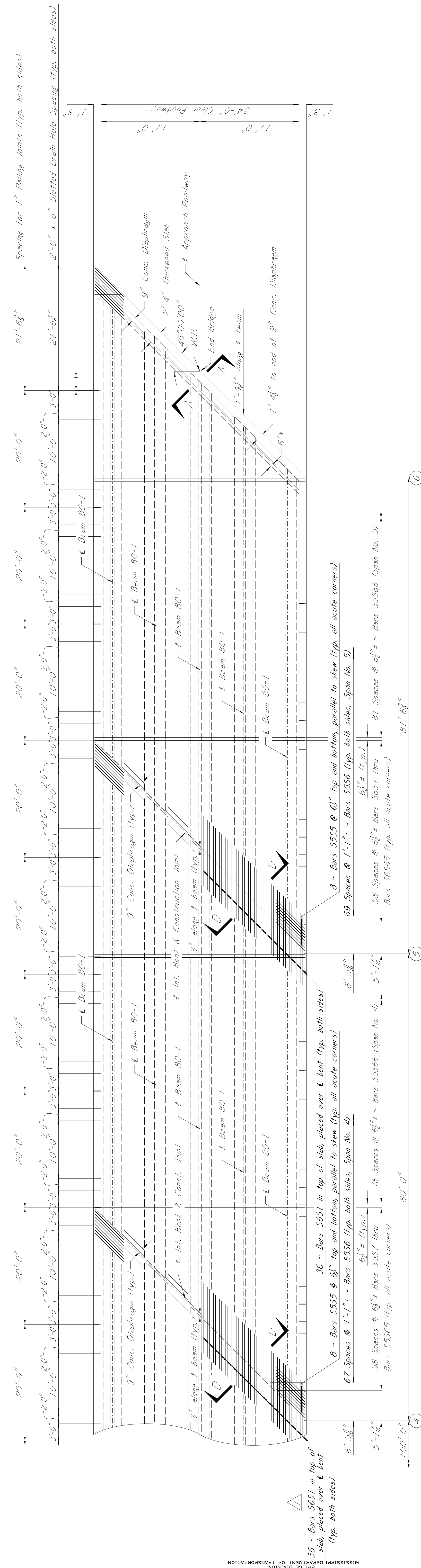


DATE	REVISION	BY
9-16-25	Revised Bor Callout	JAF

FMS: 106110 / 301000
COUNTY: WARREN
PROJECT NUMBER: STBC-0400-00(031)
DESIGNER: Jacob Erass
CHECKER: Barrett Gurnold
DATE: 11/26/2025
ISSUE DATE: 11/26/2025
DRAWN BY: STATE BRIDGE ENGINEER - WICHITAN, P.E.
REVIEWED BY: STATE BRIDGE ENGINEER - WICHITAN, P.E.

ADDENDUM

STATE PROJECT NO.
MISS. STBC-0400-00(031)



NOTE:
For General Notes, Railing Details, & other Typical Span Details see sheets no. 8003, 8016-8022 & RD-36.
PERMISSIBLE SPLICE LENGTH:
*4 ~ 1'-7"
*5 ~ 1'-11"

NOTE:
For slotted drain hole details and bar bending details for Bars D, R, & RX, see sheet no. RD-36.

NOTE:
Splices in longitudinal reinforcement shall be staggered in both top and bottom mats of steel in accordance with Mississippi Standard Specifications for Road and Bridge Construction Section 805 - Reinforcement.

NOTE:
Distance from full depth thickened slab tapered back to normal depth slab.
****NOTE:**
1" open joint in railing ~ no chamfer; install 6 x 1" expansion material per detail on sheet no. RD-36.
*****NOTE:**
Bars R & RX are not included in the Bill of Reinforcement and shall be absorbed under the cost of the bridge railing.

TABLE OF RAILING BARS

SPAN NO. 4		SPAN NO. 5	
Mark	No.	Mark	No.
D	336	D	339
***R	320	***R	319
***RX	0	***RX	4

PLAN OF 80'-0" SPAN NO. 4
Showing concrete dimensions and reinforcing steel in top of slab

PLAN OF 81'-6 3/8" SPAN NO. 5
Showing concrete dimensions and reinforcing steel in top of slab

PLAN OF 81'-6 3/8" SPAN NO. 6
Showing concrete dimensions and reinforcing steel in top of slab

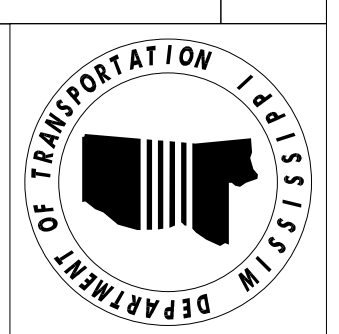
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
BRIDGE AT STA. 417+44.47
PLAN OF SPANS NO. 4 & 5

FMS: 106110 / 301000
COUNTY: WARREN
PROJECT NUMBER: STBC-0400-00(031)

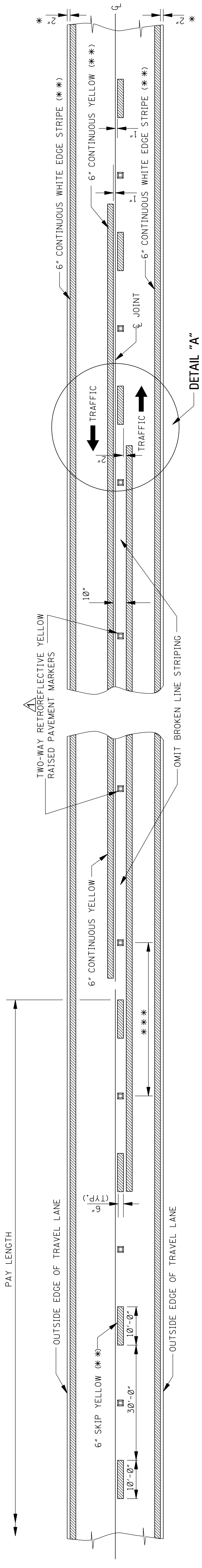
DESIGNER: Jacob Erass
CHECKER: Barrett Gorman
DATE: 9-16-25
REVISION: Revised Bar Collout

DATE: JAF
BY: JAF

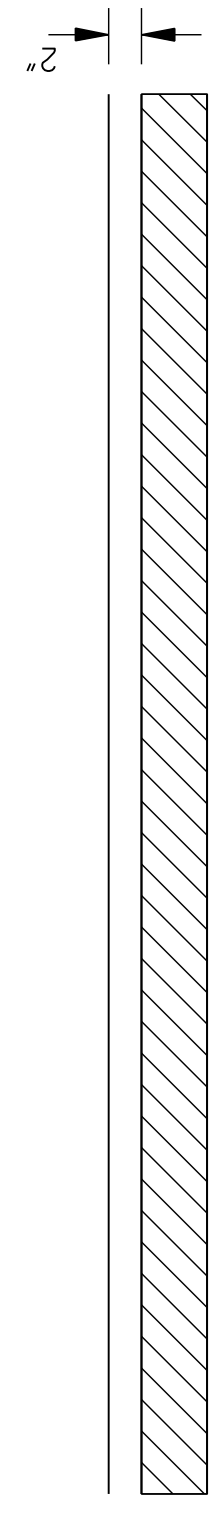
DESIGNER: Jacob Erass
CHECKER: Barrett Gorman
DATE: 11/25/2025
ISSUE DATE: 11/25/2025
PROJECT NO: STBC-0400-00(031)
PROJECT NAME: BRIDGE AT STA. 417+44.47
DRAWN BY: JACOB ERASS
SCALE: AS SHOWN
SHEET NUMBER: 8015
TOTAL SHEETS: 10



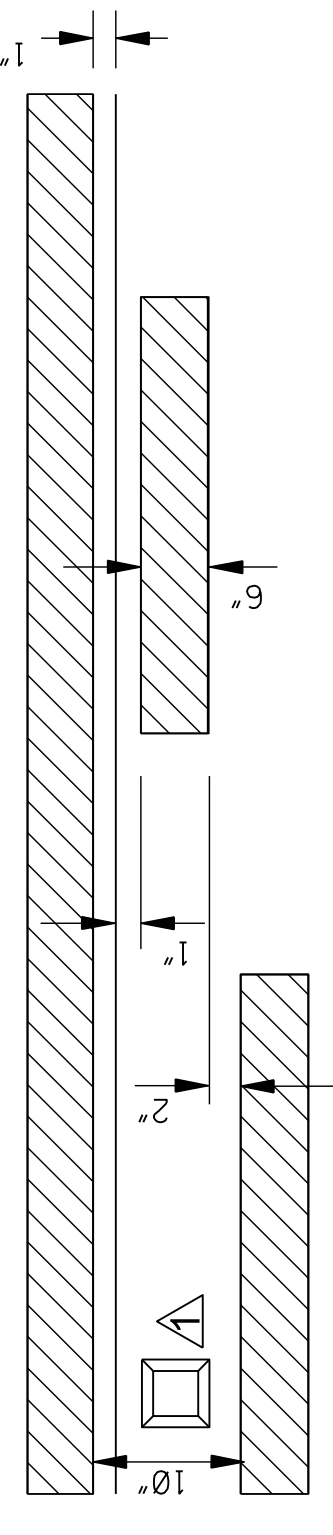
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
BRIDGE AT STA. 417+44.47
PLAN OF SPANS NO. 4 & 5



TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)



TRAFFIC ←



TRAFFIC →

DETAIL "A"

NOTE: THE CRITERIA FOR NO-PASSING ZONES CAN BE FOUND IN THE MDOT ROADWAY DESIGN MANUAL, SECTION II-1.01.

DIRECTION OF TRAFFIC
← →

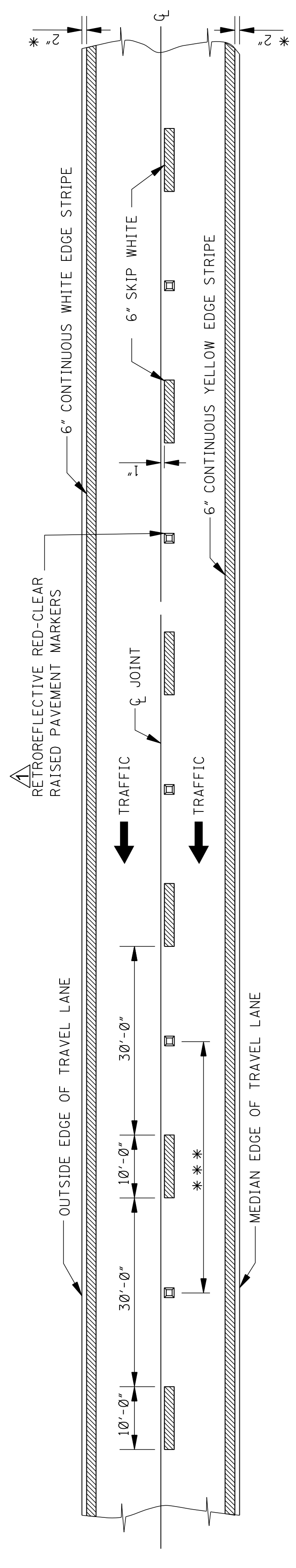
GENERAL NOTES:

- * 1. 2" UNLESS SHOWN ELSEWHERE ON THE PLANS, FOR STRIPING ON RUMBLE STRIP SECTIONS REFER TO WK. SHEETS RS-1, RS-2, AND RS-3.
- ** 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR PLASTIC AS INDICATED IN PAY ITEMS).
- *** 3. SPACING OF RETROREFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

	URBAN AREA (ft-in)	RURAL AREA (ft-in)
TANGENT SECTIONS	40'-0"	80'-0"
HORIZONTAL CURVES	40'-0"	40'-0"
INTERCHANGE LIMITS	40'-0"	+ 40'-0"

+ NOTE: ON THE MAIN FACILITY, RETROREFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINES THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.

▲ 4. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RETROREFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MDOT "APPROVED PRODUCTS LIST."



4-LANE WITH ONE-WAY TRAFFIC

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

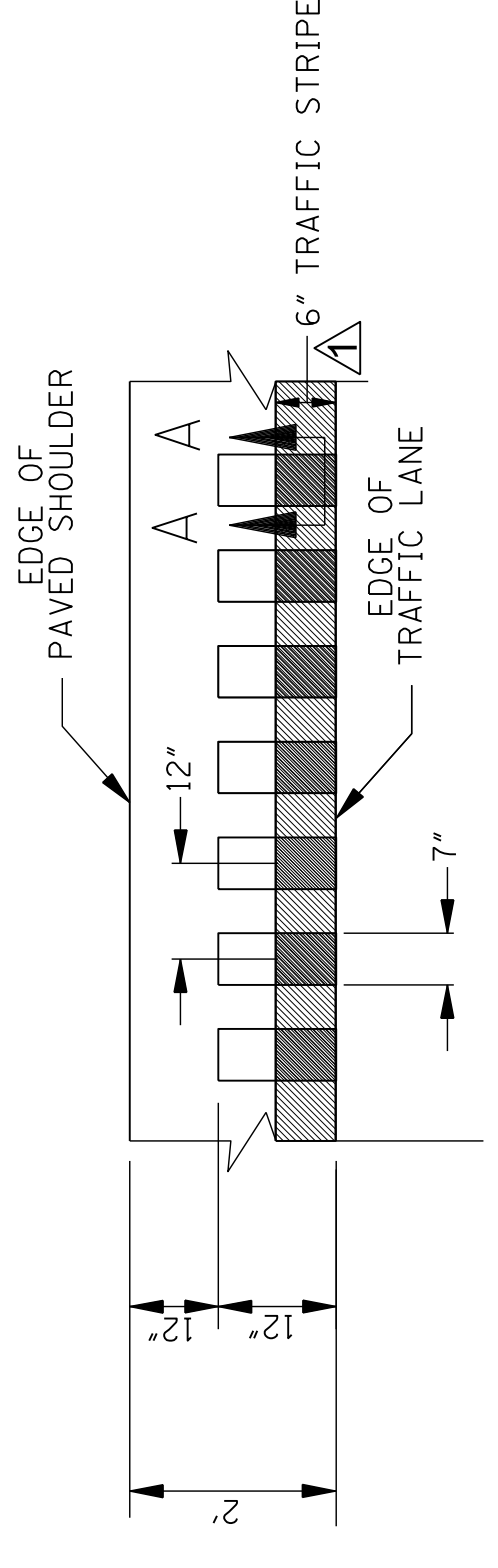
**PAVEMENT MARKING
DETAILS FOR
2-LANE AND 4-LANE
DIVIDED ROADWAYS**

DATE	REVISION
12/11/25	REVISE RPMS AND TEXT

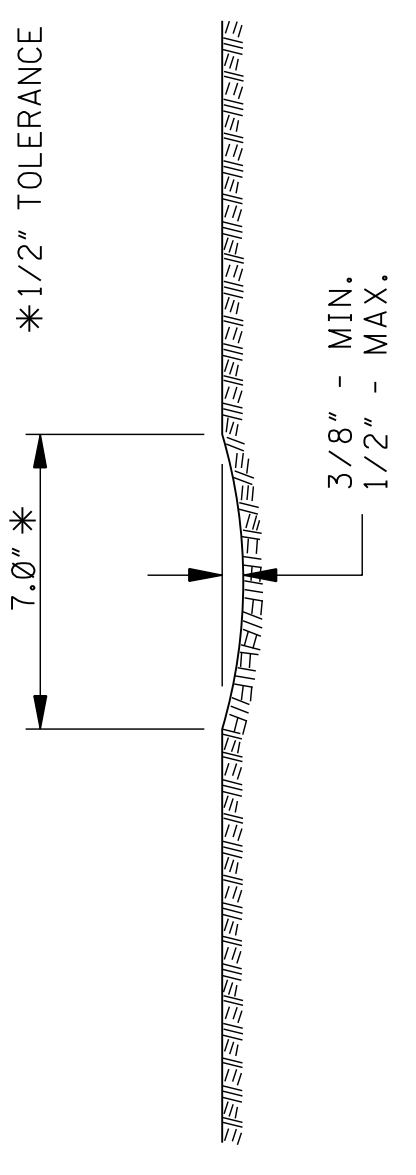
ISSUE DATE: AUGUST 01, 2017
SHEET NUMBER: PM-1
6051

GENERAL NOTES

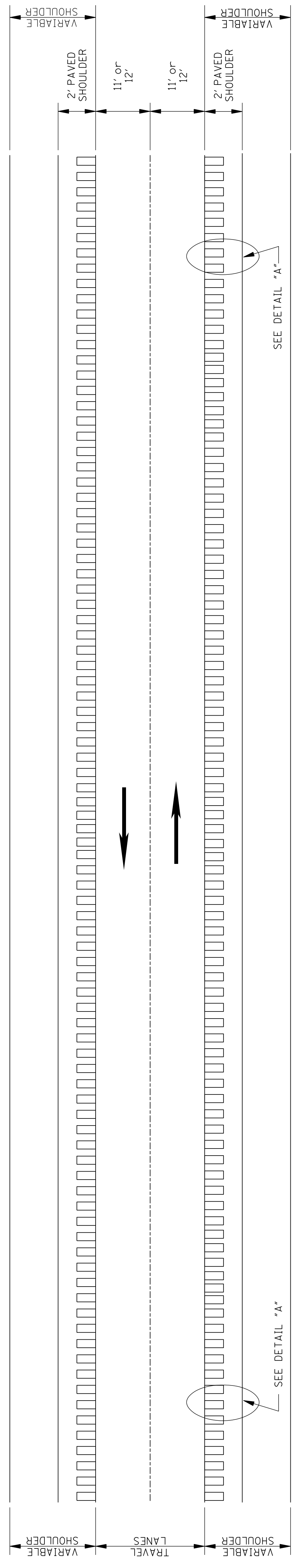
- GROUND-IN RUMBLE STRIPES SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT
- GROUND-IN RUMBLE STRIPES SHALL BE OMITTED ACROSS PUBLIC INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
- COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS
- GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO:
 - MAINLINE
 - INTERSECTING ROADWAY IF OVERLAID OR RECONSTRUCTED BEYOND NORMAL MAINLINE R.O.W.
 - ANY ROADWAY WITH EXISTING RUMBLE STRIPES PRIOR TO CONSTRUCTION.
- OMIT GROUND-IN RUMBLE STRIPS WHERE TRAFFIC LANE IS LESS THAN 11 FEET WIDE.



DETAIL "A"

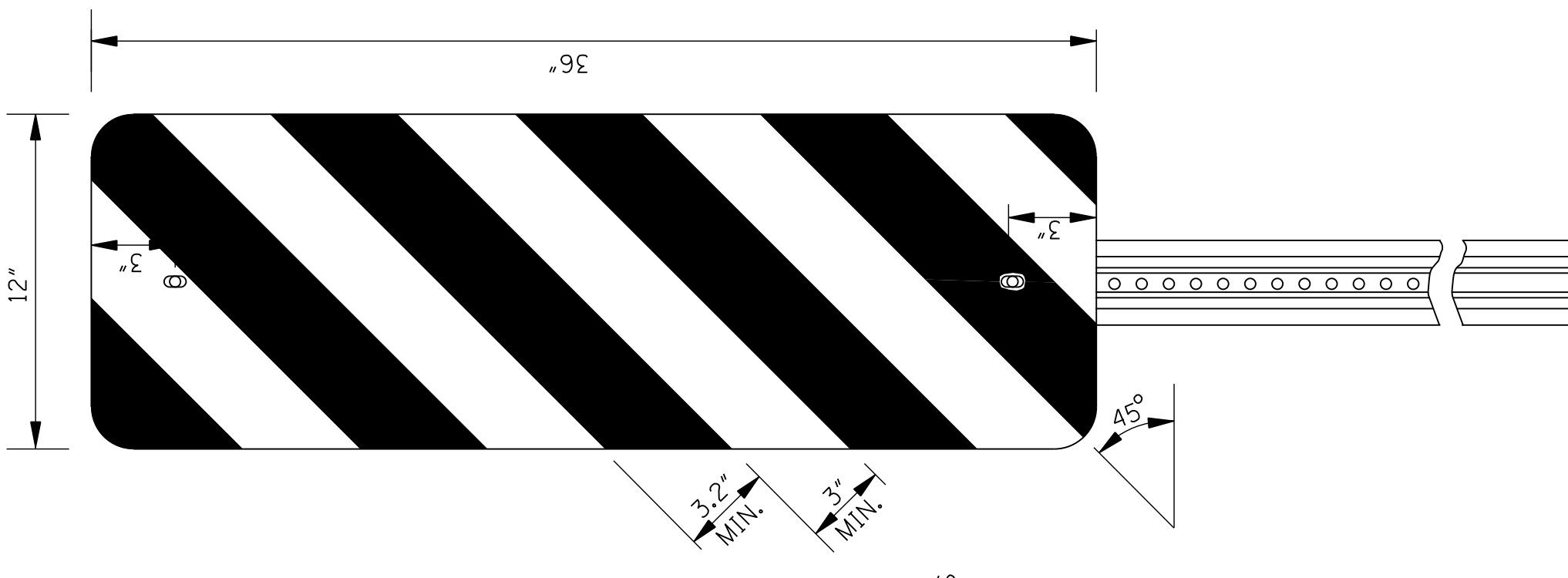


SECTION "A-A"



PLAN
NOT TO SCALE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
WORKING NUMBER RS-1	SHEET NUMBER 6064
RUMBLE STRIPES 2-LANE HIGHWAYS, (ASPHALT LANES, 2-FT ASPHALT SHOULDERS)	
ISSUE DATE: AUGUST 01, 2017	DATE: 12/11/25
BY: STN	REVISION: REVISED TEXT AND STRIPE DETAIL.

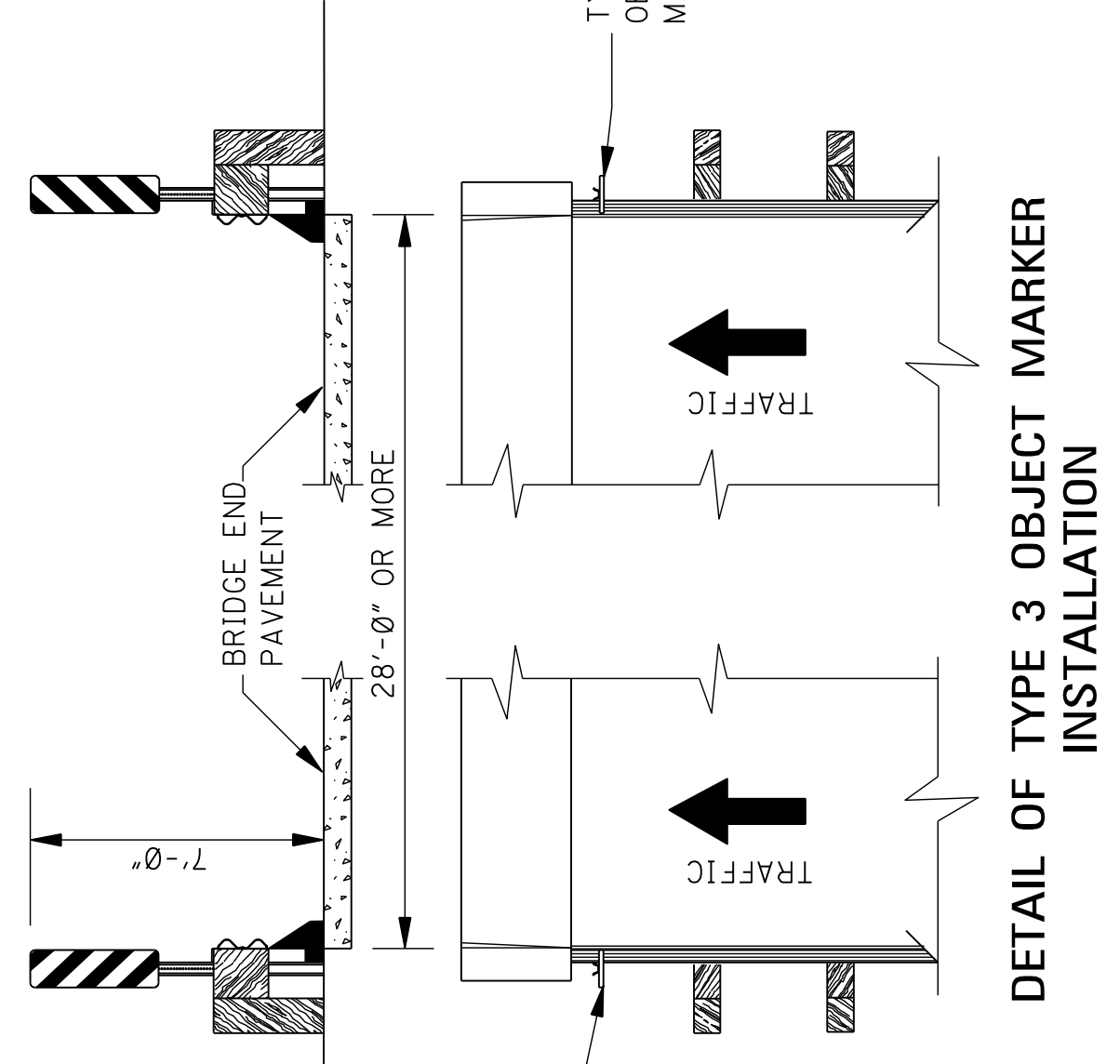


REAR VIEW OF TYPE 3 OBJECT MARKER OR DISTANCE REFERENCE SIGN ASSEMBLY

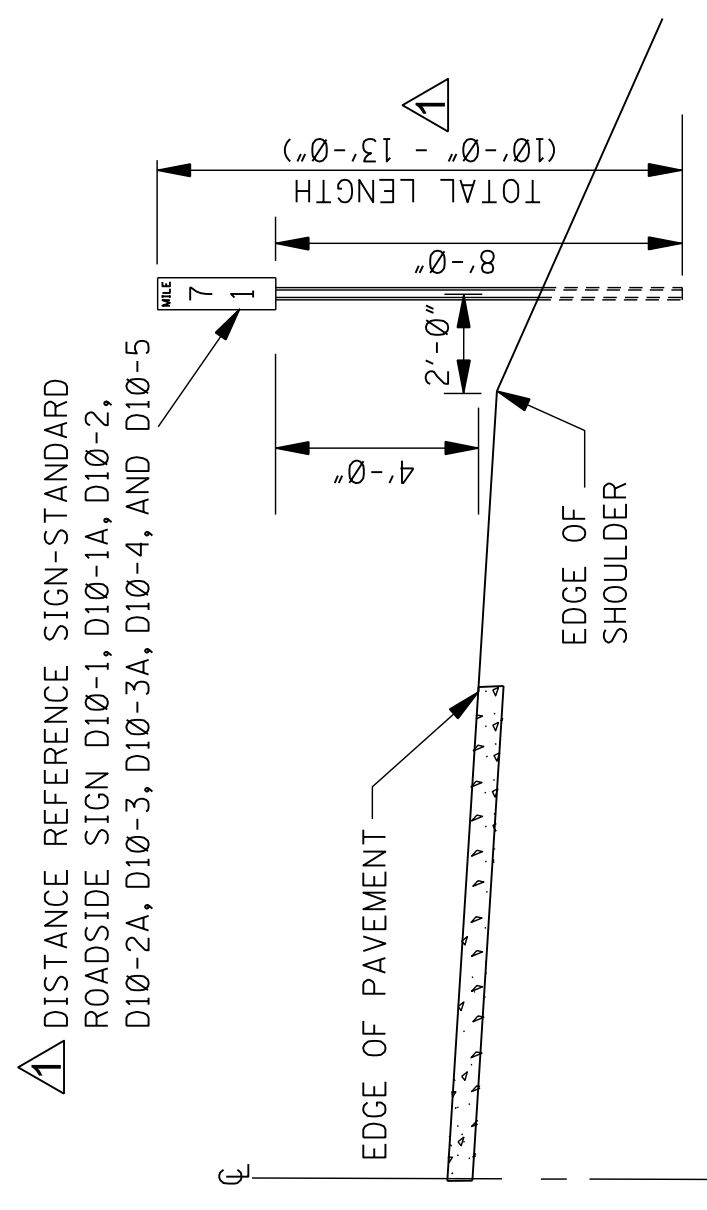
NOTE: TYPE 3 OBJECT MARKER AND DISTANCE REFERENCE SIGNS SHALL BE FASTENED TO U-SECTION POSTS WITH 3/8" DIA. BLIND FASTENERS OF THE COLLAR TYPE.

DETAIL OF TYPE 3 OBJECT MARKER

NOTE: COLORS - BLACK AND YELLOW. STRIPING SHOWN ABOVE FOR RIGHT SIDE ONLY. STRIPES SLANT DOWNWARD TO THE RIGHT FOR LEFT SIDE OF BRIDGE END. SEE DETAIL BELOW.

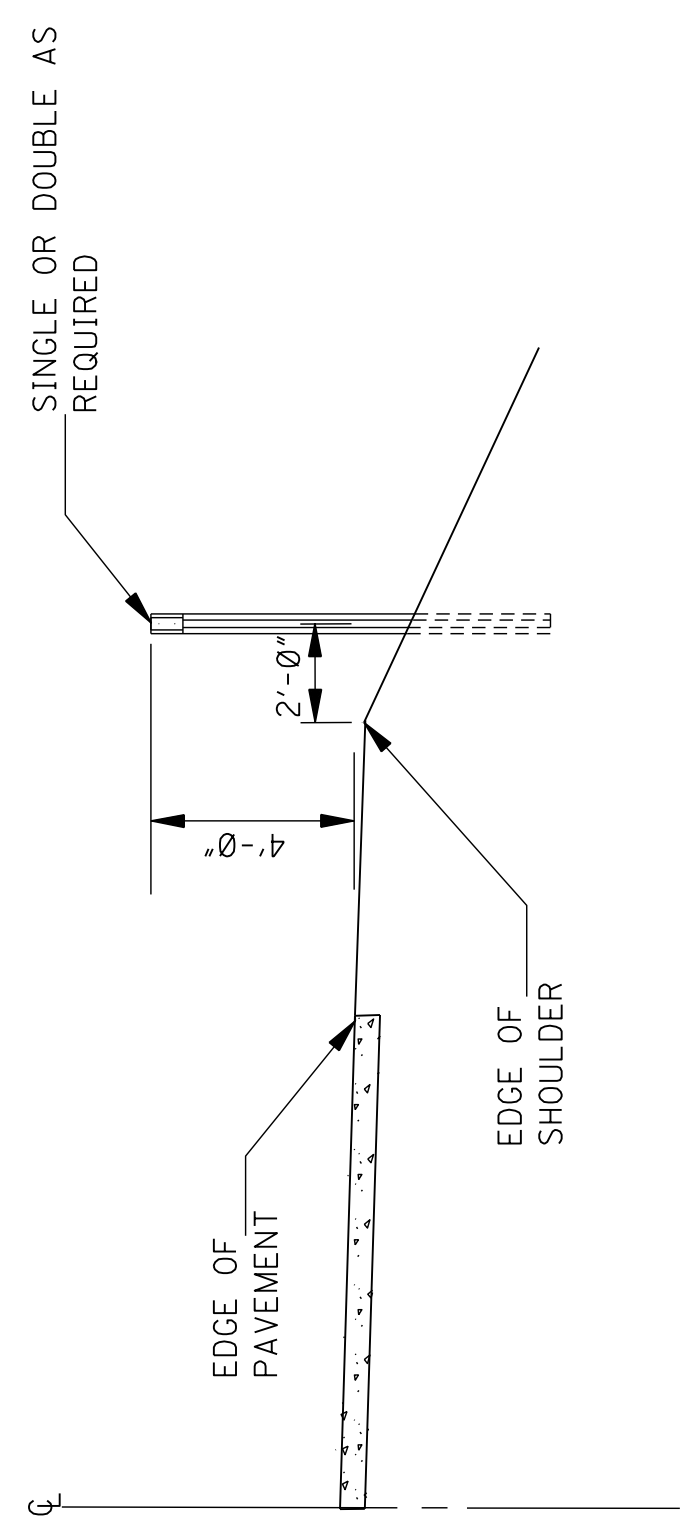


DETAIL OF TYPE 3 OBJECT MARKER INSTALLATION

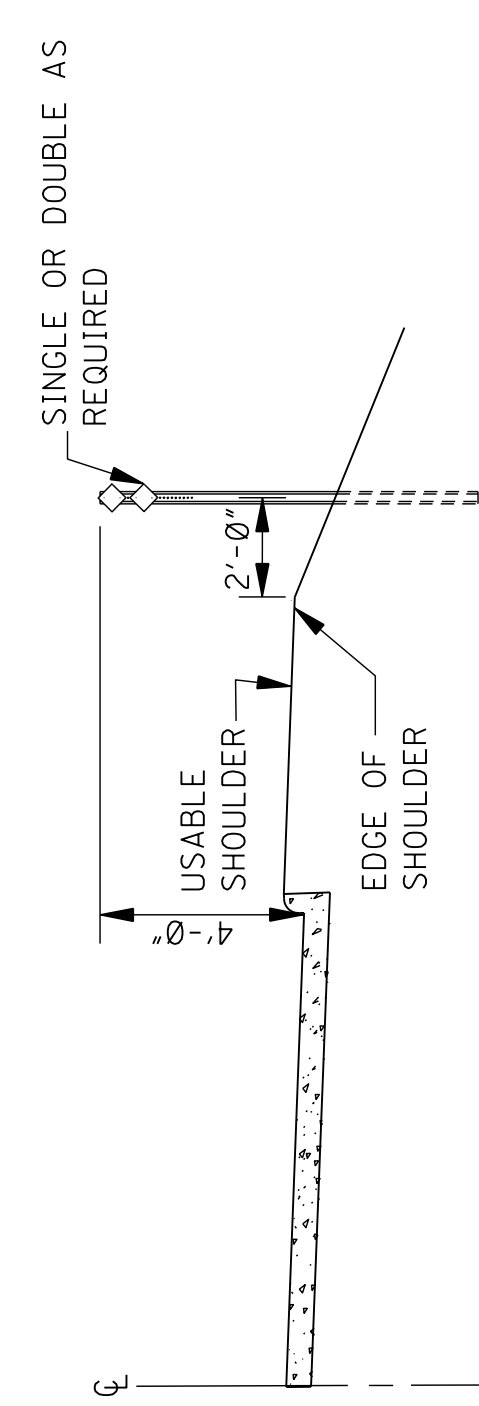


DISTANCE REFERENCE SIGN MOUNTING ON OUTSIDE SHOULDER ALONG MAIN FACILITY

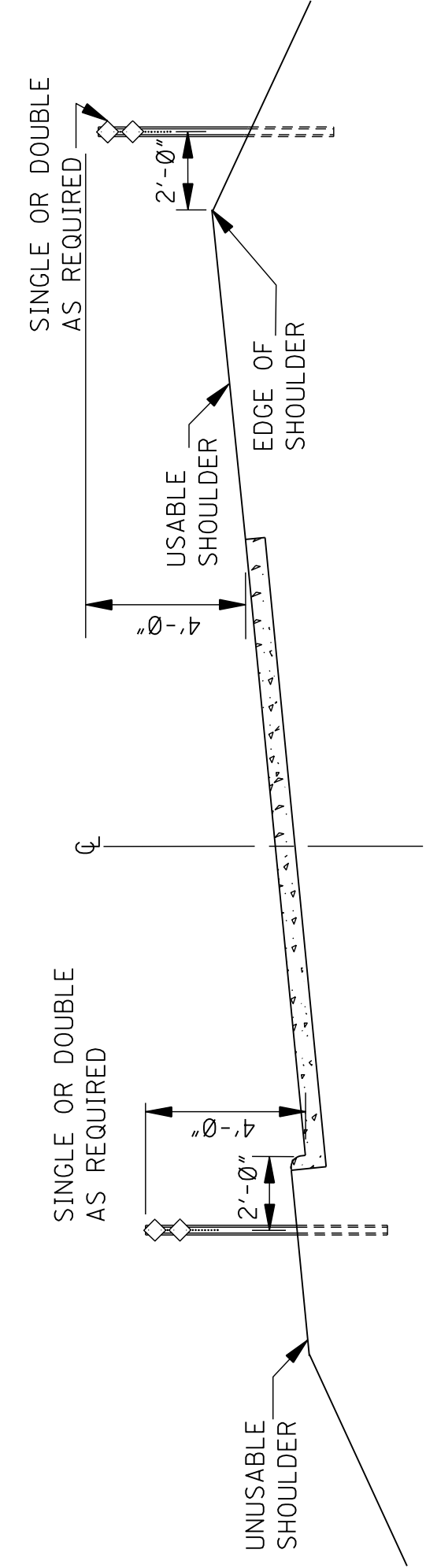
NOTE: SIGN MOUNTING ON LEFT LANE SHOULDER SHALL BE 90° OPPOSITE THE RIGHT LANE STATION. IF CONDITIONS ARE SUCH THAT MILE SIGN CANNOT BE LOCATED WITHIN 50 FEET OF ITS TRUE LOCATION, IT SHALL BE OMITTED ENTIRELY.



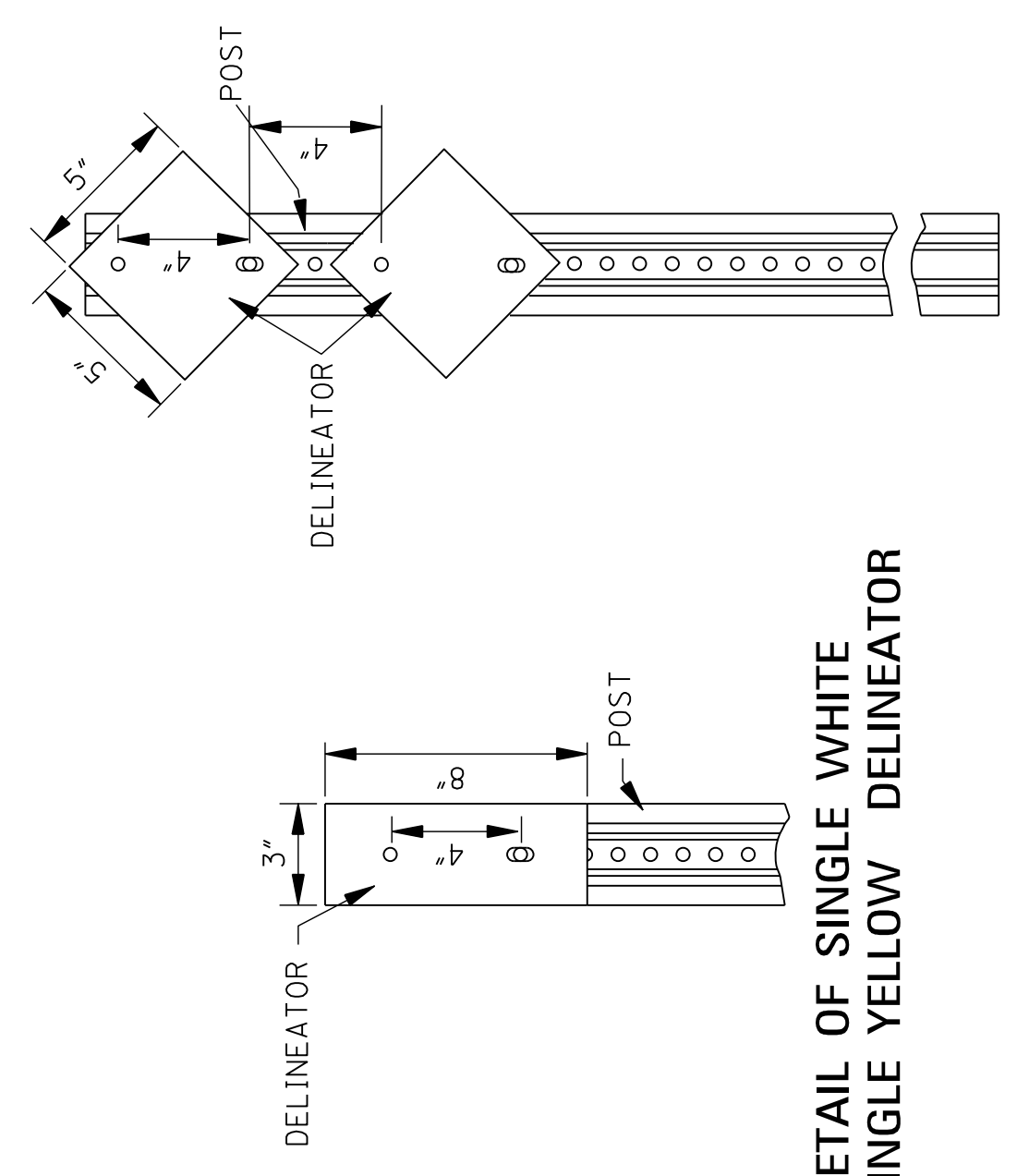
DELINEATOR MOUNTING ON OUTSIDE SHOULDER ALONG MAIN FACILITY OR RAMP



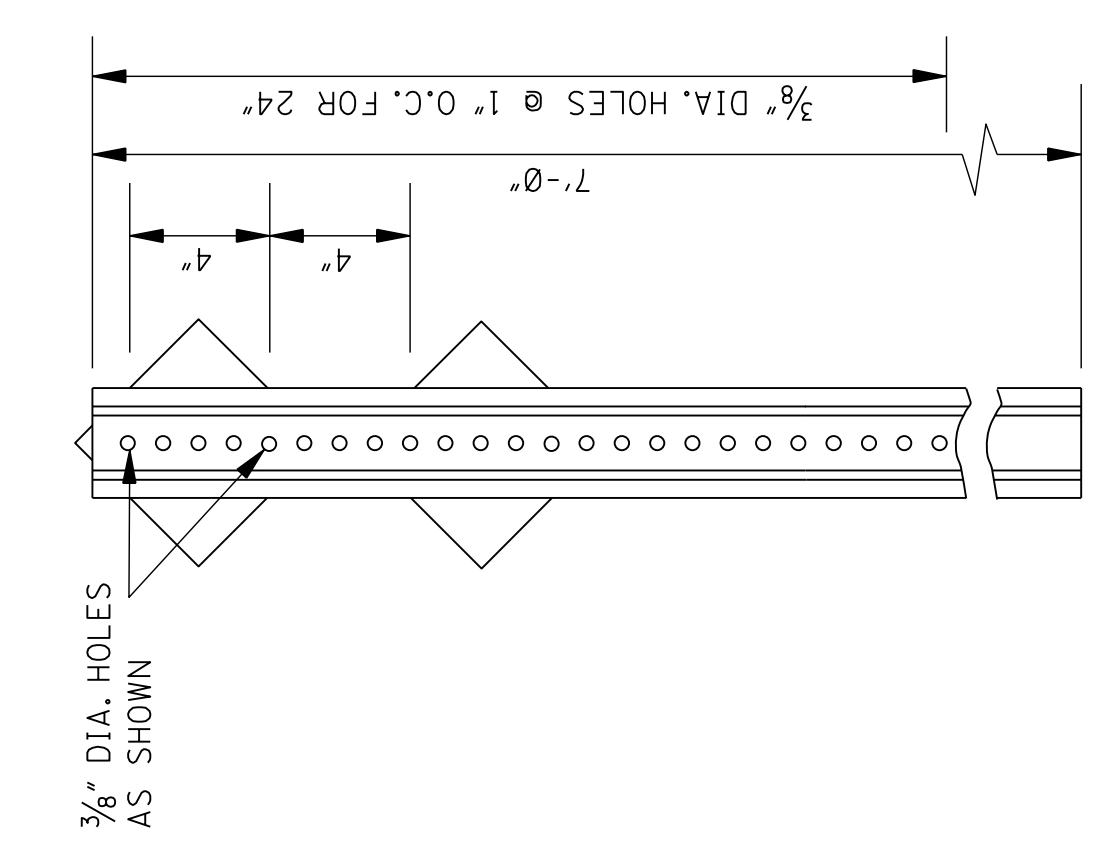
DELINEATOR MOUNTING ON OUTSIDE SHOULDER WITH MOUNTABLE CURB ALONG MAIN FACILITY OR RAMP



DELINEATOR MOUNTING ON INTERCHANGE LOOPS WITH UNMOUNTABLE CURB ON INSIDE

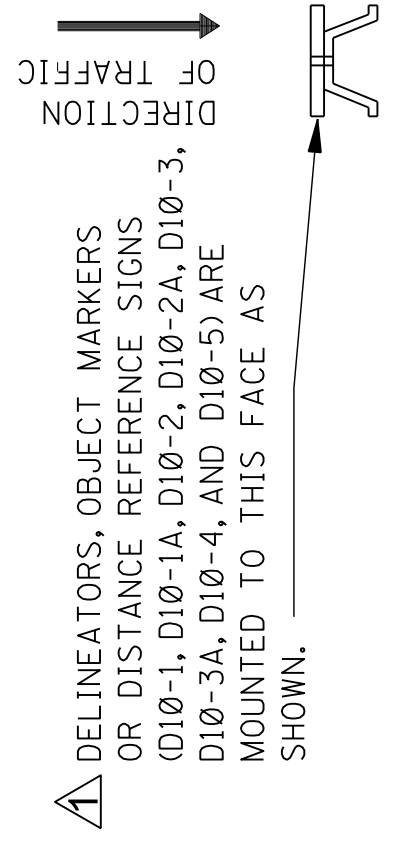


DETAIL OF SINGLE WHITE OR SINGLE YELLOW DELINEATOR



DETAIL OF DOUBLE WHITE OR DOUBLE YELLOW DELINEATOR

REAR VIEW OF DELINEATOR ASSEMBLY



MOUNTING DETAIL

NOTE: DELINEATORS SHALL BE FASTENED TO U-SECTION POSTS WITH 1/4" DIA. BLIND FASTENERS OF THE COLLAR TYPE.

DELINEATORS, OBJECT MARKERS OR DISTANCE REFERENCE SIGNS (D10-1, D10-1A, D10-2, D10-2A, D10-3, D10-3A, D10-4, AND D10-5) ARE MOUNTED TO THIS FACE AS SHOWN.

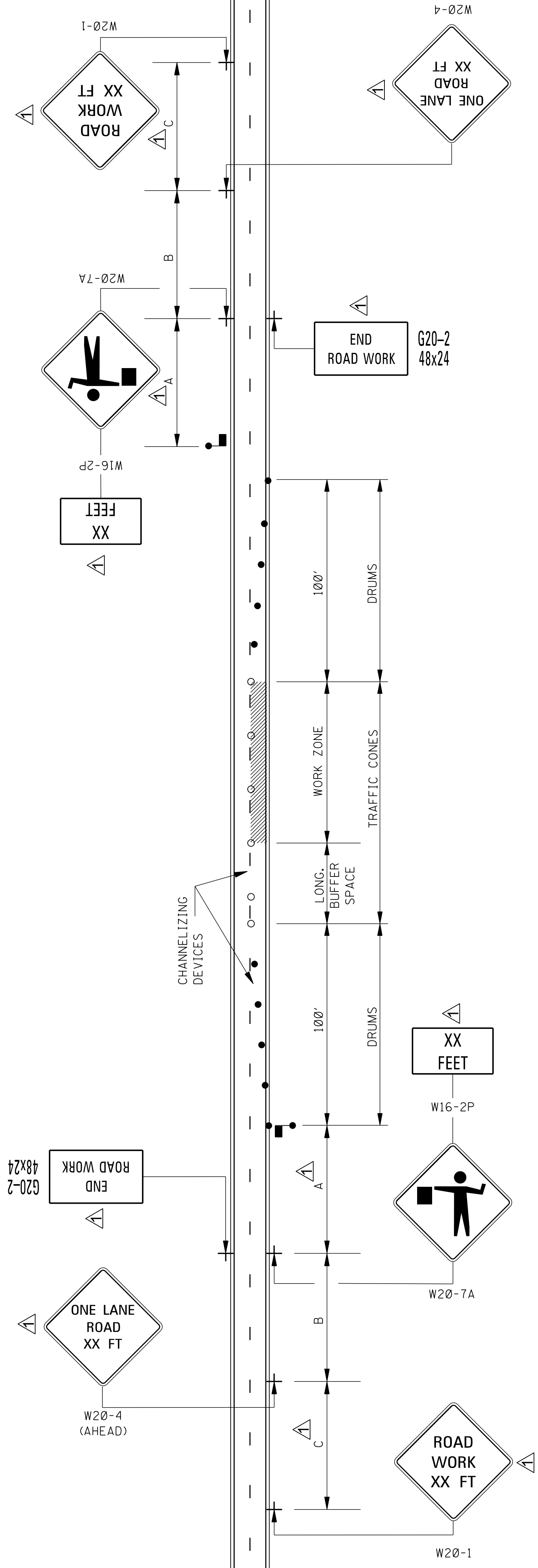
GENERAL NOTES:

- DELINEATORS AND TYPE 3 OBJECT MARKER SHALL BE RETROREFLECTIVE SHEETING ON 0.080" THICK ALUMINUM SHEET OR 14 GAGE GALVANIZED SHEET STEEL.
- DELINEATOR, TYPE 3 OBJECT MARKER AND DISTANCE REFERENCE SIGN POSTS SHALL BE GALVANIZED STEEL. THE POSTS SHALL BE FABRICATED BEFORE THE METAL IS GALVANIZED.
- WEIGHT WITHOUT GROUND PLATES:
 - A. DELINEATOR POST 7'-0" - 2.0 lb/ft TO 2.5 lb/ft
 - B. TYPE 3 OBJECT MARKER POST 9'-0" - 2.5 lb/ft TO 3.0 lb/ft
 - C. DISTANCE REFERENCE SIGN POST 10'-0", 11'-0", & 12'-0" - 3.0 lb/ft TO 3.5 lb/ft
- UNIT PRICE OF DELINEATORS AND TYPE 3 OBJECT MARKERS SHALL INCLUDE COST OF POST. DISTANCE REFERENCE SIGN POST WILL BE PAID FOR PER FOOT.
- RADIUS IN BENDS OF POST CROSS SECTION NOT TO EXCEED 1/8" FOR HOT ROLLED SECTION.
- GROUND PLATE NOT REQUIRED ON U-SECTION POST.

NO.	REVISION	DATE
1	REVISED TEXT	12/11/25
2	REVISED TEXT	
3	REVISED TEXT	
4	REVISED TEXT	
5	REVISED TEXT	
6	REVISED TEXT	

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

TYPICAL INSTALLATION AND DETAILS OF DELINEATORS AND DISTANCE REFERENCE SIGNS



- LEGEND
- ▲ FLAGGER
 - RETROREFLECTIVE FREE-STANDING PLASTIC DRUMS
 - TRAFFIC CONES (28" HEIGHT MINIMUM)

DISTANCE BETWEEN SIGNS

ROAD TYPE	A	B	C
URBAN (35 MPH OR LESS)	100 FT.	100 FT.	100 FT.
URBAN (40 - 70 MPH)	350 FT.	350 FT.	350 FT.
RURAL	500 FT.	500 FT.	500 FT.
EXPRESSWAY / FREEWAY	1000 FT.	1500 FT.	2640 FT.

GENERAL NOTES:

- THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE. FLAGGER STATIONS SHALL BE LOCATED SUCH THAT APPROACHING VEHICLES WILL HAVE SUFFICIENT DISTANCE TO STOP. VALUES IN STOPPING SIGHT DISTANCE COLUMN MAY BE USED AS A MINIMUM FOR THIS DISTANCE.
- ALL CHANNELIZING DEVICES SHALL BE A MINIMUM OF 28" IN HEIGHT.
- DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 36" x 36" AND BLACK COPY ON FLUORESCENT ORANGE SHEETING.
- WHEN WORK ZONE IS NO LONGER NEEDED, ALL SIGNS SHALL BE COVERED OR REMOVED AND ALL CHANNELIZING DEVICES SHALL BE MOVED TO THE SHOULDER EDGE.
- ADDITIONAL FLAGGERS MAY BE NEEDED AS DIRECTED BY THE ENGINEER.
- WHEN WORK IS REQUIRED AT NIGHT, FLAGGER STATIONS SHALL BE ILLUMINATED EXCEPT IN EMERGENCIES.
- CHANNELIZING DEVICE TYPES FOR:
 - APPROACH AND EXIT TAPERS- RETROREFLECTIVE PLASTIC DRUMS
 - ALONG LANE LINE AND WORK ZONE- TRAFFIC CONES (28" HEIGHT)
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
- AN OPTIONAL FLAGGER (W20-7A) WORD MESSAGE IS SHOWN IN THE "STANDARD HIGHWAY SIGNS" PUBLICATION.

POSTED SPEED AND/OR DESIGN SPEED AND/OR ANTICIPATED OPERATING SPEED ▲ mph	MAXIMUM CHANNELIZING DEVICE SPACING (ft)		LONGITUDINAL BUFFER SPACE (ft) †	STOPPING SIGHT DISTANCE
	TAPER	ALONG LANE LINE & WORK ZONE		
25	20	50	55	155
30	20	60	85	200
35	20	70	120	250
40	20	80	170	305
45	20	90	220	360
50	20	100	280	425
55	20	110	335	495
60	20	120	415	570
65	20	130	485	645

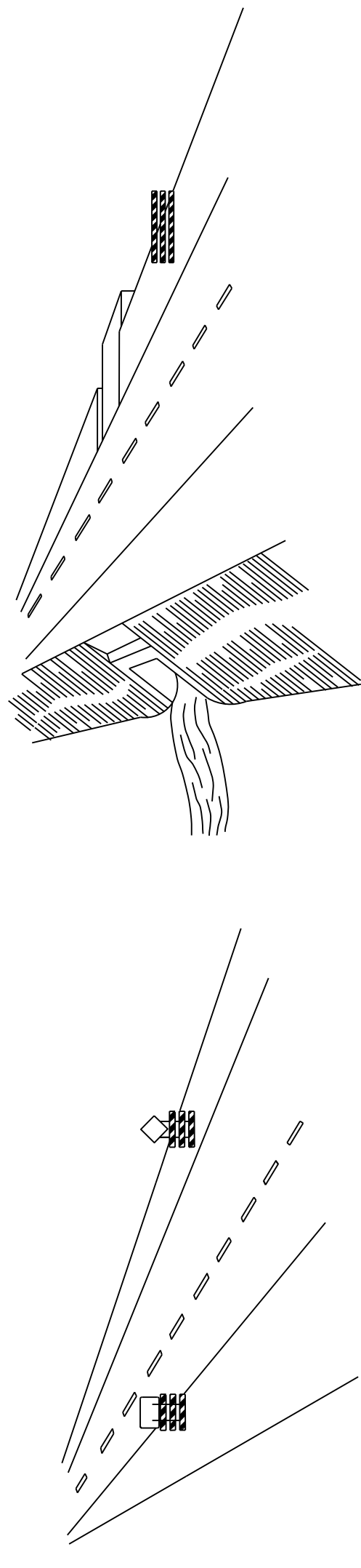
† NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**TRAFFIC CONTROL PLAN
WITH FLAGGER
(ONE-LANE CLOSURE OF
TWO-WAY TRAFFIC)**

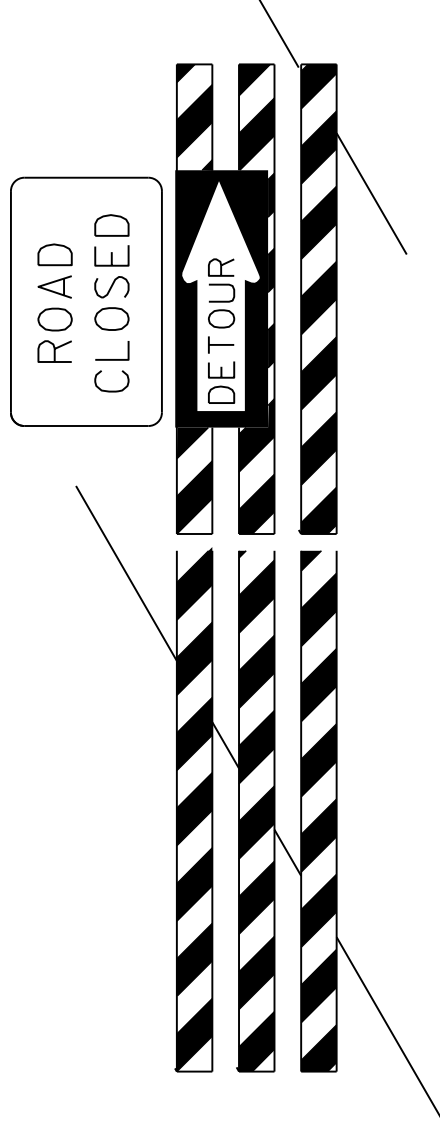
DATE	REVISION	BY
12/11/25	REVISED TEXT	SIN

ISSUE DATE: AUGUST 01, 2017
SHEET NUMBER: 6351
WORKING NUMBER: TCP-1



WING BARRICADES

1. WING BARRICADES ARE TYPE III BARRICADES ERECTED ON THE SHOULDER ON ONE OR BOTH SIDES OF THE PAVEMENT TO GIVE THE SENSATION OF A NARROWING OR RESTRICTED ROADWAY. WING BARRICADES MAY BE USED AS A MOUNTING FOR THE ADVANCE WARNING SIGNS OR FLASHERS.
2. WING BARRICADES SHOULD BE USED:
 - A. IN ADVANCE OF A CONSTRUCTION PROJECT EVEN WHEN NO PART OF THE ROADWAY IS ACTUALLY CLOSED.
 - B. IN ADVANCE OF ALL BRIDGE OR CULVERT WIDENING OPERATIONS.



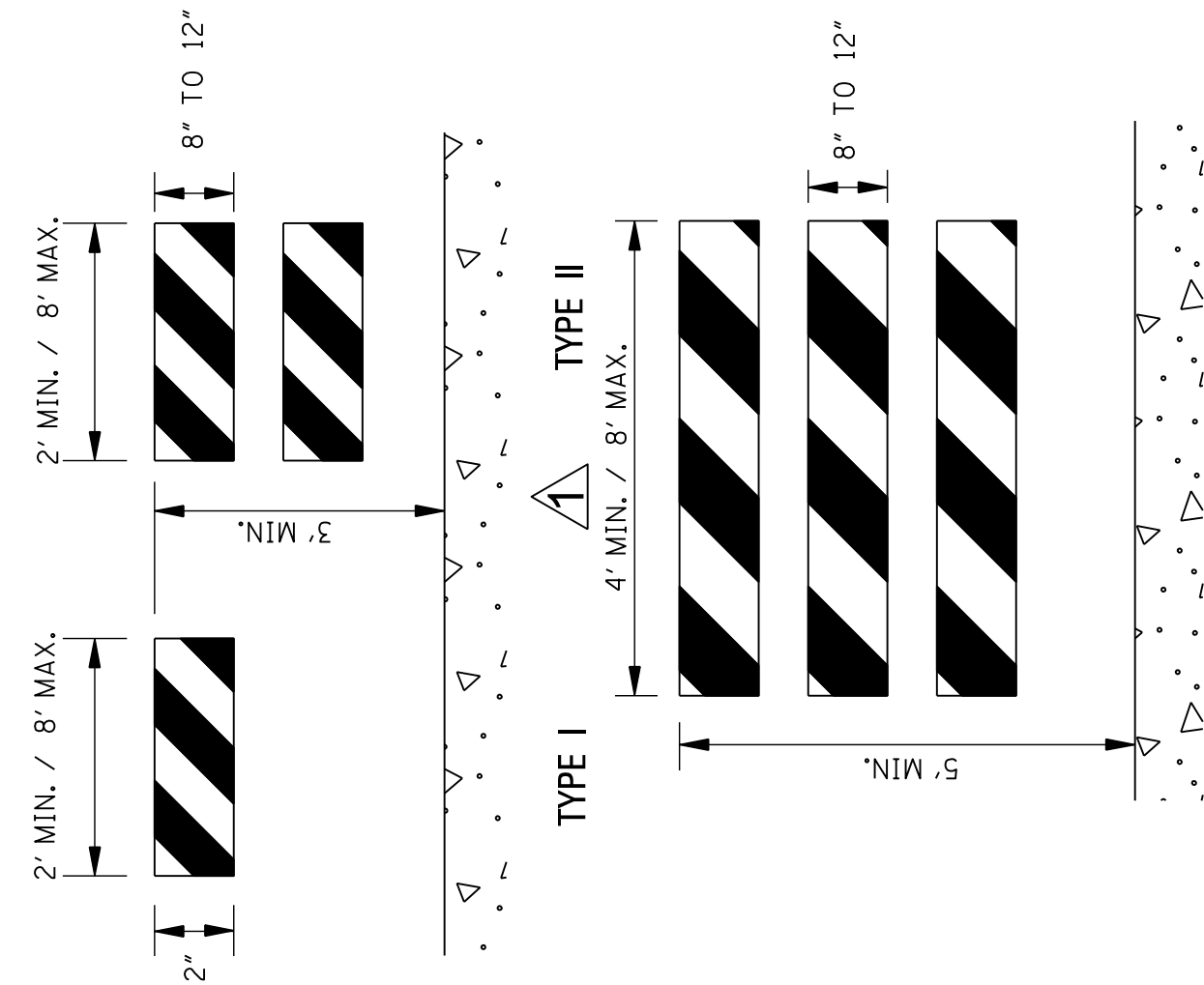
BARRICADE CLOSING A ROAD

BARRICADE CHARACTERISTICS

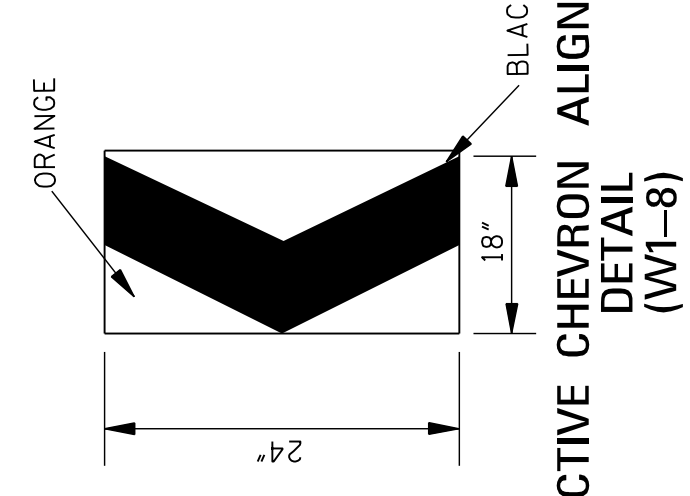
	I	II	III
WIDTH OF RAIL **	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.
LENGTH OF RAIL **	24" MIN. / 96" MAX.	24" MIN. / 96" MAX.	48" MIN. / 96" MAX.
WIDTH OF STRIPE *	6"	6"	6"
HEIGHT	36" MIN.	36" MIN.	60" MIN.
NUMBER OF RETROREFLECTORIZED RAIL FACES	2 (ONE EACH DIRECTION)	4 (TWO EACH DIRECTION)	3 IF FACING TRAFFIC IN ONE DIRECTION 6 IF FACING TRAFFIC IN TWO DIRECTIONS

- * 1. FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.
- ** 2. BARRICADES INTENDED FOR USE ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH SPEED ROADWAYS, SHALL HAVE A MINIMUM OF 270 IN² OF REFLECTIVE AREA FACING TRAFFIC.

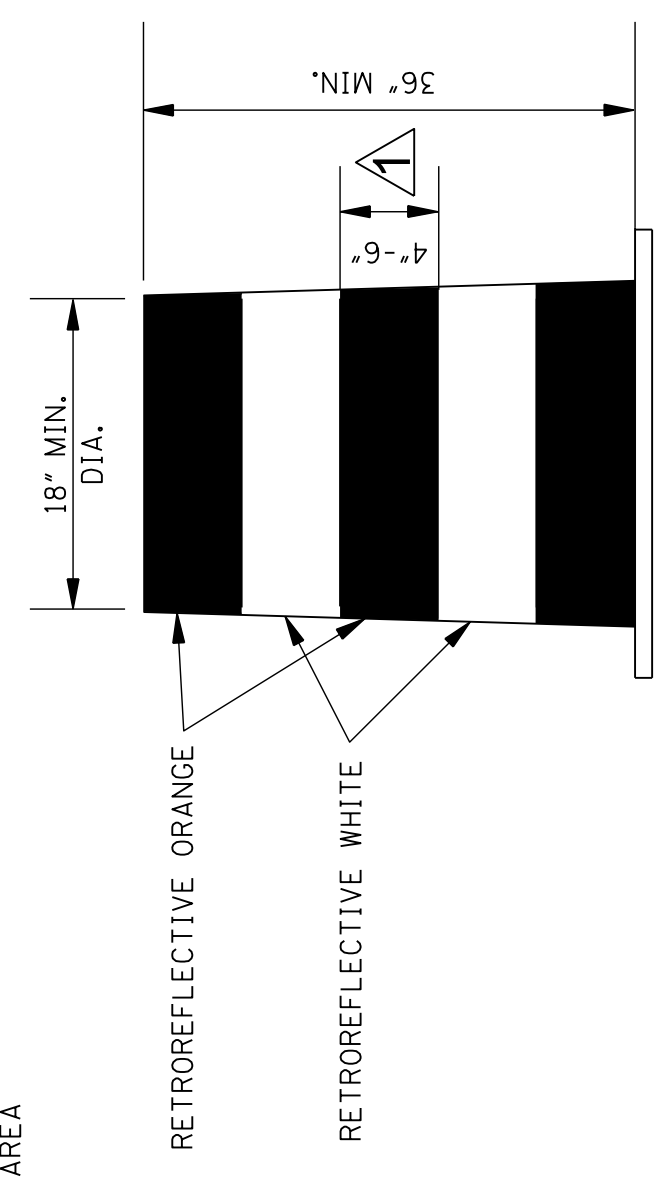
STANDARD BARRICADES



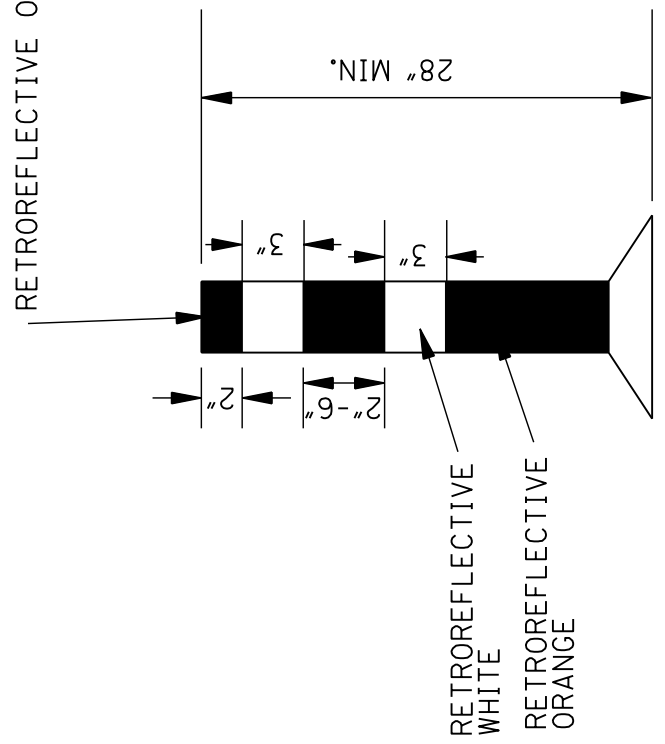
1. THE RETROREFLECTIVE MARKING FOR BARRICADE RAILS SHALL BE ORANGE AND WHITE (SLOPING DOWNWARD AT AN ANGLE OF 45° IN THE DIRECTION THAT TRAFFIC IS TO PASS).
2. RAIL STRIPE SHOULD BE 6 INCHES, EXCEPT THAT 4-INCH WIDE STRIPES MAY BE USED IF RAIL LENGTHS ARE LESS THAN 36 INCHES.
3. DO NOT PLACE SANDBAGS OR OTHER DEVICES ON BARRICADE RAILS TO PROVIDE MASS SANDBAGS, HOWEVER, MAY BE PLACED AS BALLAST TO THE LOWER PARTS OF THE FRAME OR THE STAYS TO BARRICADES.
4. FOR ADDITIONAL INFORMATION OR DETAILS, SEE MUTCD, LATEST EDITION.
5. BARRICADES ARE CLASSIFIED BY FHWA AS CATEGORY II WORK ZONE DEVICES. A LIST OF CRASHWORTHY BARRICADES AND OTHER CATEGORY II DEVICES CAN BE FOUND ON FHWA'S WEBSITE: <http://highways.dot.gov/safety/Rwd/Reduce-crash-severity>
6. WHERE ROAD USERS INCLUDE PEDESTRIANS, THE PROVISION OF SUPPLEMENTAL AUDIBLE INFORMATION OR DETECTABLE BARRIERS OR BARRICADES SHOULD BE PROVIDED FOR PEOPLE WITH VISION DISABILITIES.
7. BARRICADE RAIL SUPPORTS SHOULD NOT PROJECT INTO PEDESTRIAN CIRCULATION ROUTES MORE THAN 4 INCHES FOR THE SUPPORTS LOCATED BETWEEN 27 INCHES TO 80 INCHES ABOVE THE EXISTING SURFACE.



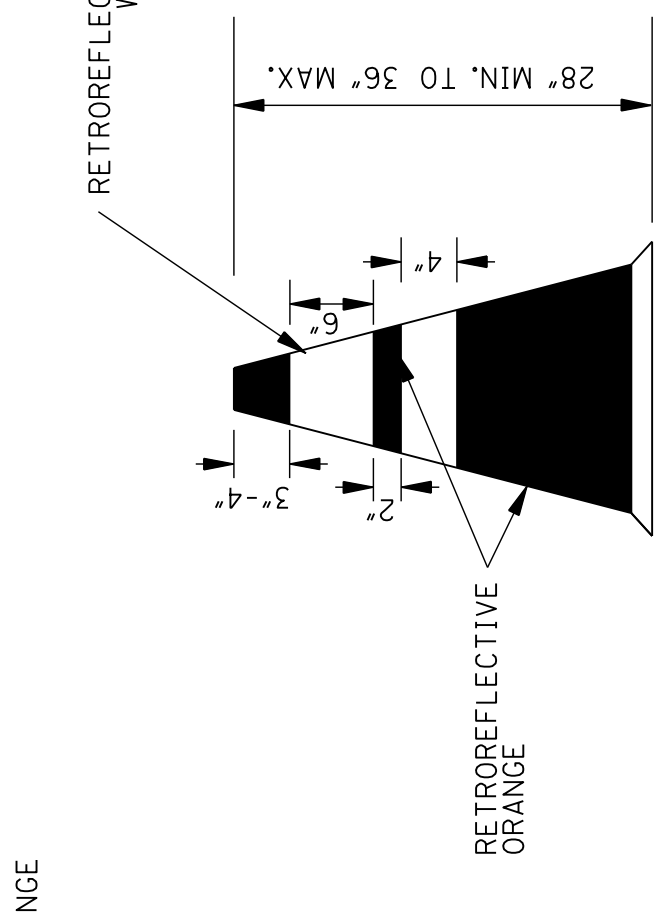
1. A CHEVRON SIGN CONSISTS OF A BLACK CHEVRON TYPE MARKING ON AN ORANGE BACKGROUND AND SHALL POINT IN THE DIRECTION OF TRAFFIC FLOW.
2. THE CHEVRON SIGN SHALL BE MOUNTED ON CRASHWORTHY SUPPORT.
3. CHEVRON SIGNS MAY BE USED TO SUPPLEMENT OTHER STANDARD DEVICES WHERE ONE OR MORE LANES ARE CLOSED FOR CONSTRUCTION OR MAINTENANCE. THEY SHOULD BE PLACED APPROXIMATELY 2'-0" BEHIND THE LANE TRANSITION STRIPE.
4. CHEVRON SIGNS SHALL BE INSTALLED AT A MINIMUM HEIGHT OF 4 FEET MEASURED VERTICALLY FROM THE BOTTOM OF THE SIGN TO THE ELEVATION OF THE NEAR EDGE OF THE TRAVELWAY.



PLASTIC DRUM DETAIL

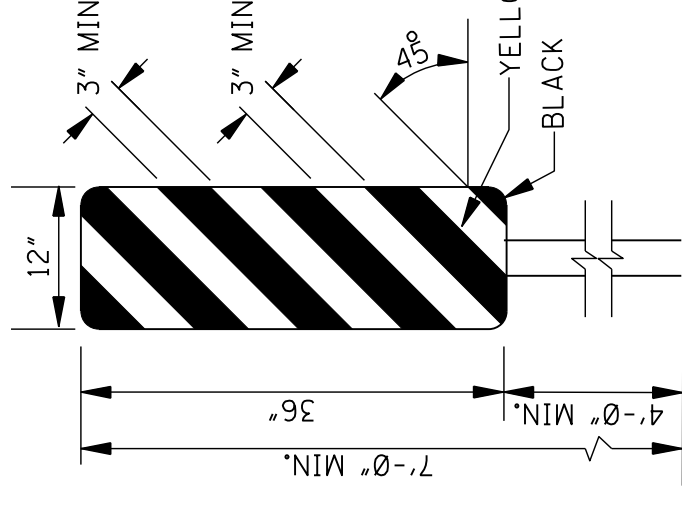


TUBULAR MARKER DETAIL

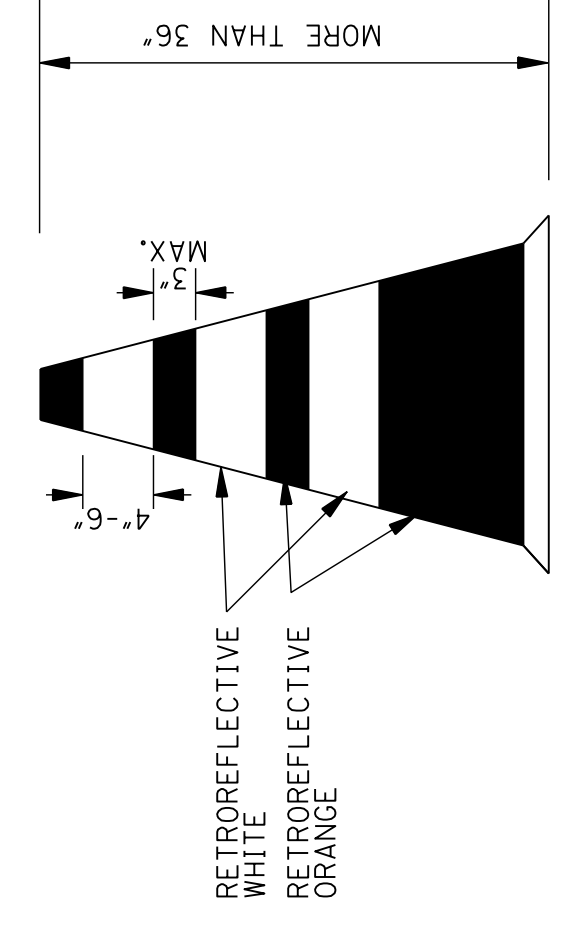


PLASTIC CONE DETAIL (28" - 36")

TYPE 3 OBJECT MARKER (OM3-R)



1. TYPE 3 OBJECT MARKERS SHALL BE USED AT ALL EXPOSED BRIDGE ABUTMENTS AND AT OTHER LOCATIONS AS DEEMED NECESSARY BY THE ENGINEER.
2. THE OM3-R IS SHOWN. THE OM3-L IS SIMILAR EXCEPT THE STRIPES SLOPE DOWNWARD FROM THE UPPER LEFT SIDE TO THE LOWER RIGHT SIDE AND SHALL BE PLACED ON THE LEFT SIDE OF THE OBJECT.
3. THE INSIDE EDGE OF THE MARKER SHALL BE IN LINE WITH THE INNER EDGE OF THE OBSTRUCTION.
4. THE STRIPING SHALL CONSIST OF ALTERNATING BLACK AND RETROREFLECTIVE YELLOW SLOPING DOWNWARD TOWARD THE SIDE ON WHICH TRAFFIC IS THE PASS.



PLASTIC CONE DETAIL (36" OR GREATER)

1. PLASTIC DRUMS, TUBULAR MARKERS, OR PLASTIC CONES SHALL BE ON END AND USED AS AN EXPEDIENT METHOD FOR TRAFFIC CHANNELIZATION. THE COLOR AND MARKING OF THE DEVICE USED SHALL BE CONSISTENT WITH MARKING STANDARDS FOR BARRICADE. THE PREDOMINANT COLOR ON THE DEVICE USED SHALL BE ORANGE WITH RETROREFLECTIVE, HORIZONTAL, CIRCUMFERENTIAL WHITE STRIPES AS SHOWN ON THE DETAILS.
2. DRUMS, TUBULAR MARKERS, OR PLASTIC CONES SHOULD NEVER BE PLACED IN THE ROADWAY WITHOUT WARNING SIGNS.
3. WHERE PRACTICAL PLASTIC DRUMS, TUBULAR MARKERS, OR PLASTIC CONES SHOULD BE PLACED NO CLOSER THAN 3'-0" FROM THE EDGE OF TRAVELED LANE.
4. BALLAST SHALL NOT BE PLACED ON THE TOP OF THE DRUM.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

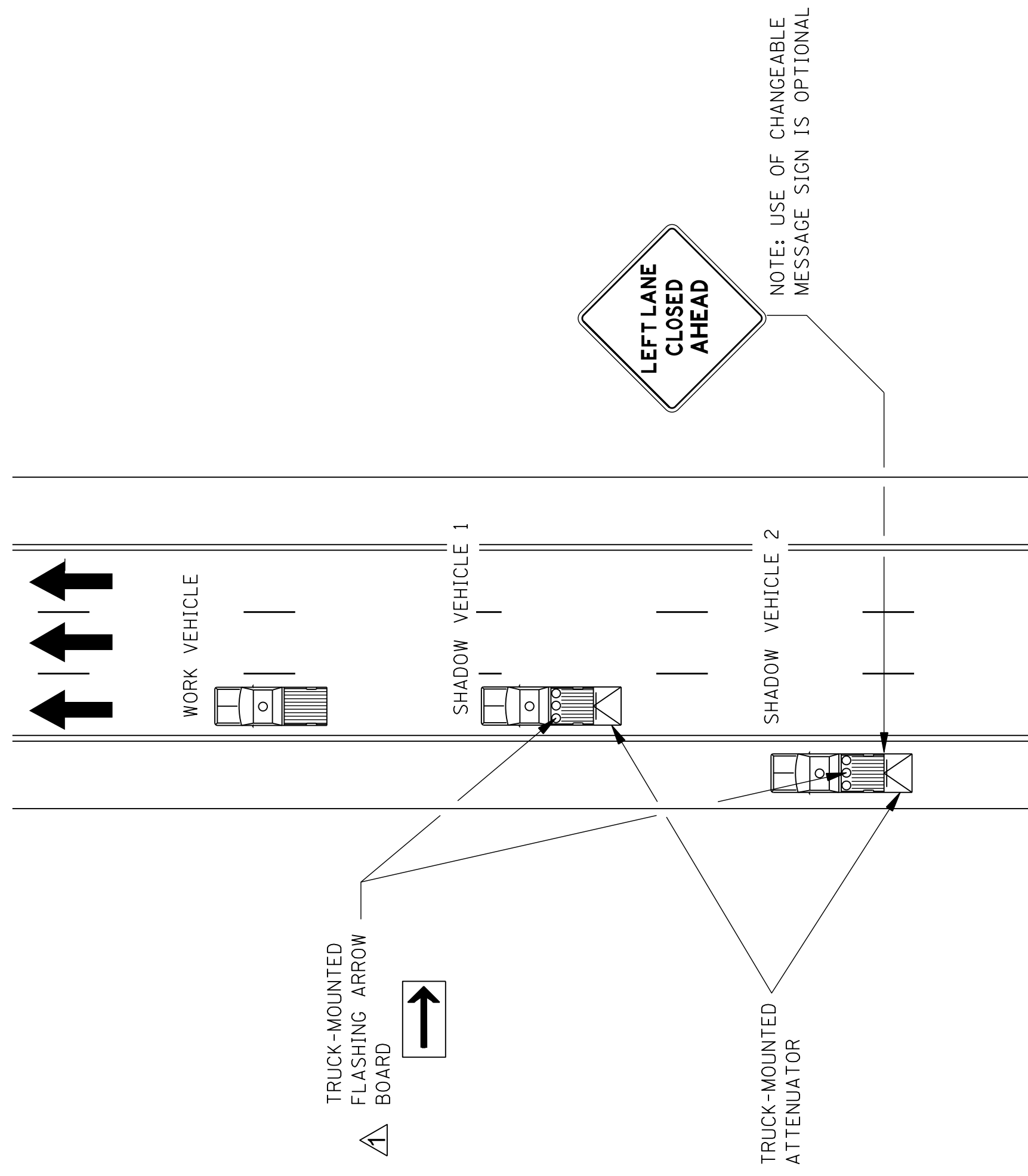
HIGHWAY SIGN AND BARRICADE DETAILS FOR CONSTRUCTION PROJECTS

DATE	12/1/25	REVISION	STN
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ISSUE DATE: AUGUST 01, 2017

WORKING NUMBER: TCP-8
SHEET NUMBER: 6358

MOBILE OPERATIONS ON MULTILANE ROAD

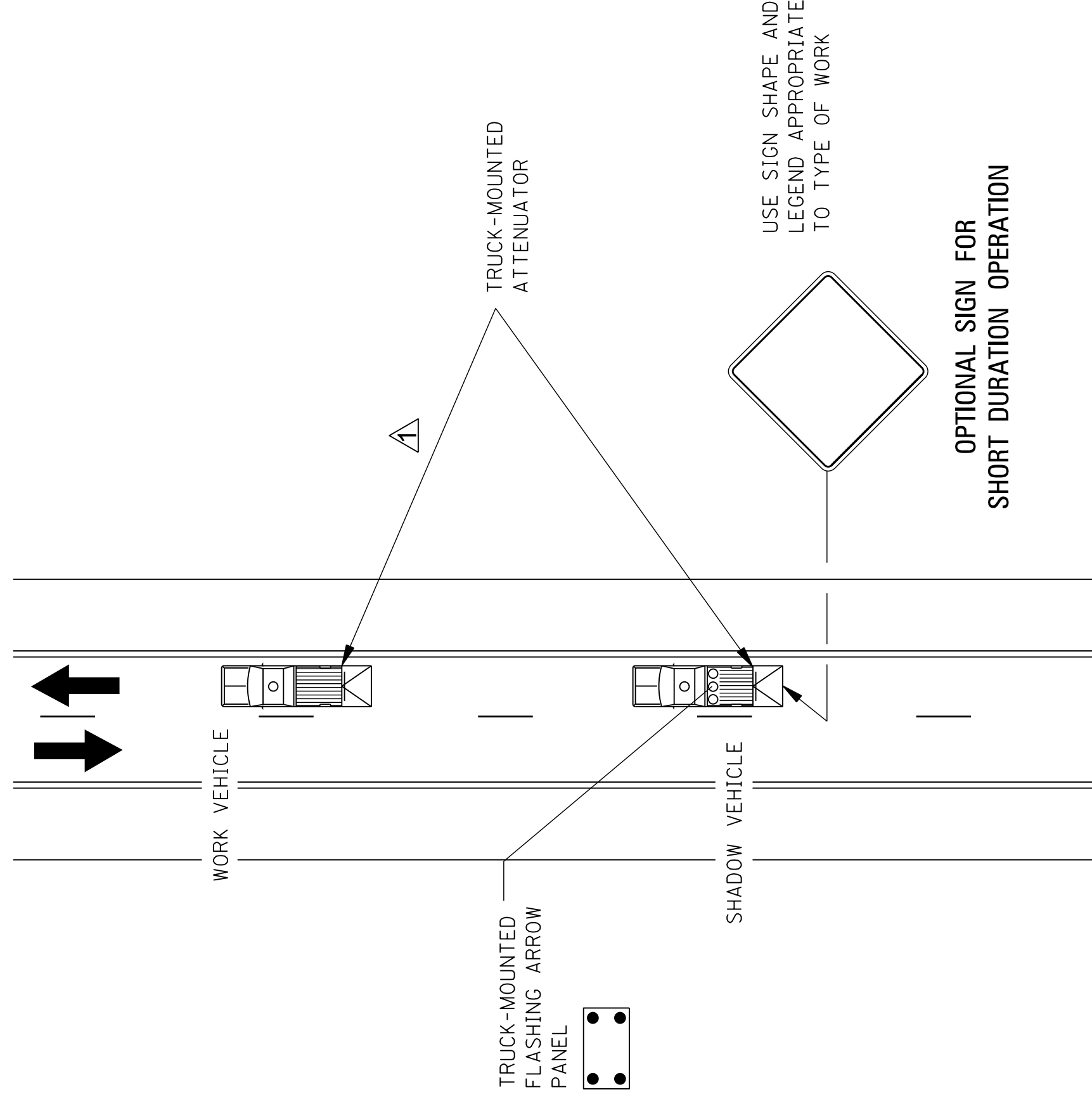


MOBILE OPERATIONS ON MULTILANE ROAD

NOTES FOR MULTILANE LANE OPERATION:

1. SHADOW AND WORK VEHICLES SHALL DISPLAY HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS. AN ARROW BOARD SHALL BE USED WHEN A FREEWAY LANE IS CLOSED. WHEN MORE THAN ONE FREEWAY LANE IS CLOSED, A SEPARATE ARROW BOARD SHALL BE USED FOR EACH CLOSED LANE.
2. SHADOW VEHICLE 2 SHOULD BE EQUIPPED WITH AN ARROW BOARD AND TRUCK MOUNTED ATTENUATOR (TMA). AN APPROPRIATE LANE CLOSURE SIGN SHOULD BE PLACED ON SHADOW VEHICLE 2 SO AS NOT TO OBSCURE THE ARROW BOARD.
3. SHADOW VEHICLE 1 SHOULD BE EQUIPPED WITH AN ARROW BOARD AND TRUCK-MOUNTED ATTENUATOR (TMA).
4. SHADOW VEHICLE 2 SHOULD TRAVEL AT A VARYING DISTANCE FROM THE WORK OPERATION SO AS TO PROVIDE ADEQUATE SIGHT DISTANCE FOR TRAFFIC APPROACHING FROM THE REAR.
5. THE SPACING BETWEEN THE WORK VEHICLES AND THE SHADOW VEHICLES, AND BETWEEN EACH SHADOW VEHICLE, SHOULD BE MINIMIZED TO DETERMINE ROAD USERS FROM DRIVING IN BETWEEN.
6. ON HIGH-SPEED ROADWAYS, A THIRD SHADOW VEHICLE (NOT SHOWN) MAY BE USED WITH SHADOW VEHICLE 1 IN THE CLOSED LANE, SHADOW VEHICLE 2 STRADDLING THE EDGE LINE, AND SHADOW VEHICLE 3 ON THE SHOULDER.
7. ARROW BOARD SHALL BE AS A MINIMUM TYPE B, 60" X 30" IN ACCORDANCE WITH THE CRITERIA PRESENTED IN THE MUTCD.
8. WORK SHOULD NORMALLY BE DONE DURING OFF-PEAK HOURS.
9. VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBSCURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
10. WHEN THE WORK VEHICLE OCCUPIES AN INTERIOR LANE (A LANE OTHER THAN THE FAR RIGHT OR FAR LEFT) OF A DIRECTIONAL ROADWAY HAVING A RIGHT-HAND SHOULDER, 10 FEET OR MORE IN WIDTH, SHADOW VEHICLE 2 SHOULD DRIVE ON THE RIGHT-HAND SHOULDER WITH A SIGN INDICATING THAT WORK IS TAKING PLACE IN THE INTERIOR LANE.
11. ON HIGH-SPEED ROADWAYS, A THIRD SHADOW VEHICLE (NOT SHOWN) MAY BE USED WITH SHADOW VEHICLE 1 IN THE CLOSED LANE, SHADOW VEHICLE 2 STRADDLING THE EDGE LINE, AND SHADOW VEHICLE 3 ON THE SHOULDER.
12. WHERE ADEQUATE SHOULDER WIDTH IS NOT AVAILABLE, SHADOW VEHICLE 3 MAY ALSO STRADDLE THE EDGE LINE.
13. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

MOBILE OPERATIONS ON TWO-LANE ROAD



MOBILE OPERATIONS ON TWO-LANE ROAD

NOTES FOR TWO-LANE OPERATION:

1. WHERE PRACTICAL AND WHEN NEEDED, THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS. IF THIS CAN NOT BE DONE FREQUENTLY, AS AN ALTERNATIVE, A "DO NOT PASS" SIGN MAY BE PLACED ON THE REAR OF THE VEHICLE BLOCKING THE LANE.
2. THE DISTANCE BETWEEN THE WORK AND SHADOW VEHICLES MAY VARY ACCORDING TO TERRAIN, PAINT DRYING TIME, AND OTHER FACTORS. SHADOW VEHICLES ARE USED TO WARN TRAFFIC OF THE OPERATION AHEAD. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, THE SHADOW VEHICLE SHOULD MAINTAIN THE MINIMUM DISTANCE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. THE SHADOW VEHICLE SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
3. ADDITIONAL SHADOW VEHICLES TO WARN AND REDUCE THE SPEED OF ONCOMING OR OPPOSING TRAFFIC MAY BE USED. POLICE PATROL CARS MAY BE USED FOR THIS PURPOSE.
4. A TRUCK-MOUNTED ATTENUATOR (TMA) SHOULD BE USED ON THE SHADOW VEHICLE AND MAY BE USED ON THE WORK VEHICLE.
5. THE WORK VEHICLE SHALL BE EQUIPPED WITH BEACONS, AND THE SHADOW VEHICLES SHALL BE EQUIPPED WITH TWO HIGH-INTENSITY FLASHING LIGHTS MOUNTED ON THE REAR, ADJACENT TO THE SIGN. SHADOW AND WORK VEHICLES SHALL DISPLAY FLASHING OR ROTATING BEACONS BOTH FORWARD AND TO THE REAR.
6. VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBSCURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
7. ARROW BOARD TO BE USED IN CAUTION MODE.
8. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

DATE	BY	REVISION
12/1/25	STN	REVISED TEXT AND DETAILS.

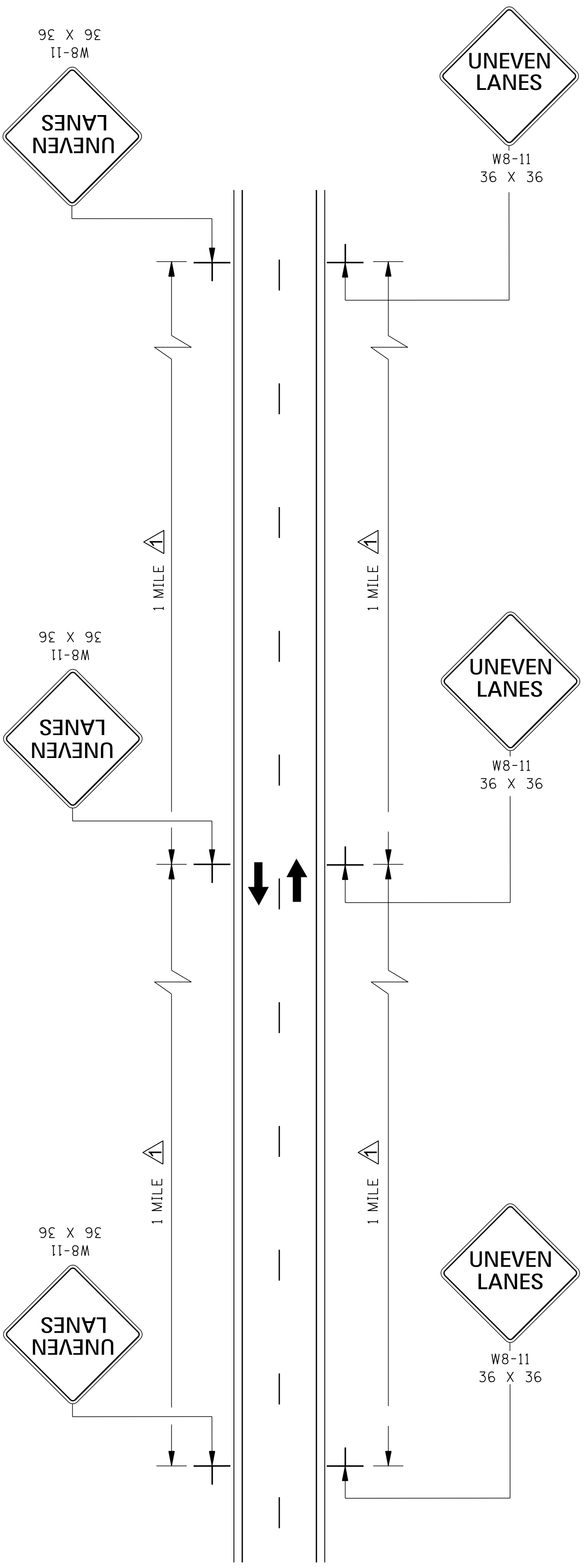
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**TRAFFIC CONTROL PLAN
MOBILE OPERATIONS
MULTILANE ROADS
AND
TWO-LANE ROADS**

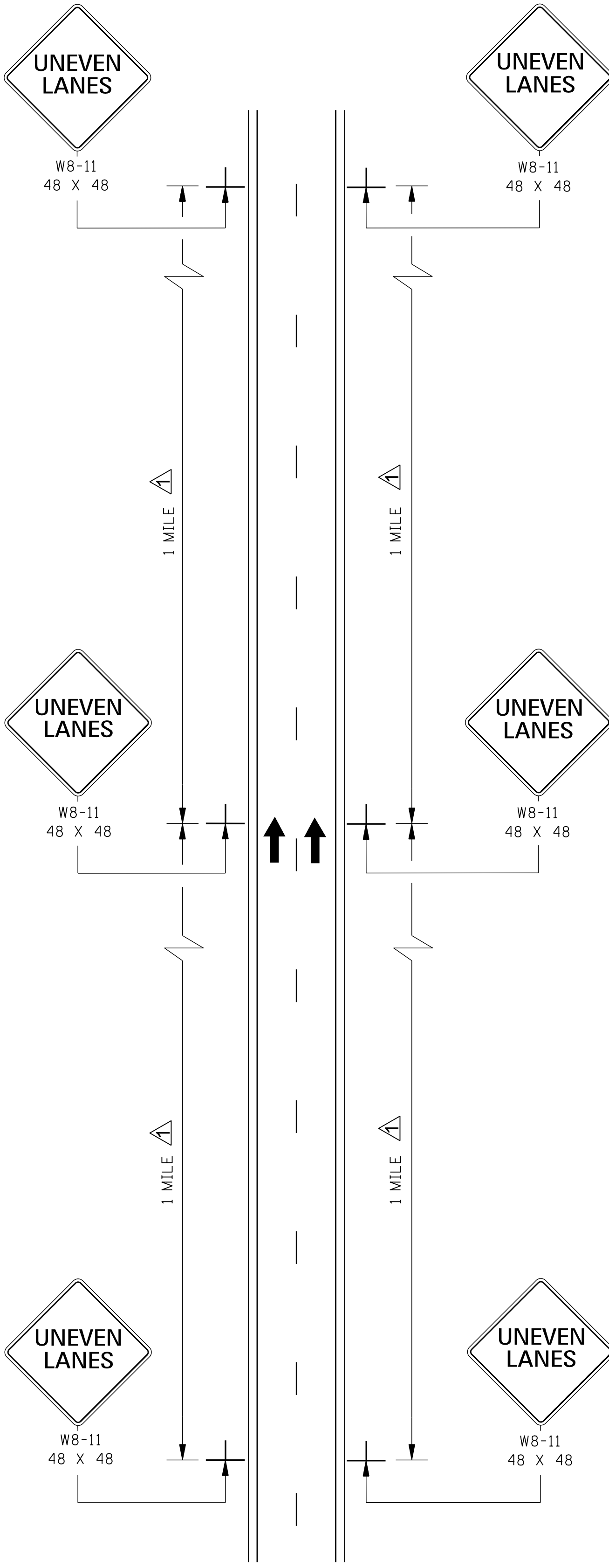
ISSUE DATE: AUGUST 01, 2017

WORKING NUMBER
TCP-9

SHEET NUMBER
6359



TWO-WAY TRAFFIC



ONE-WAY TRAFFIC

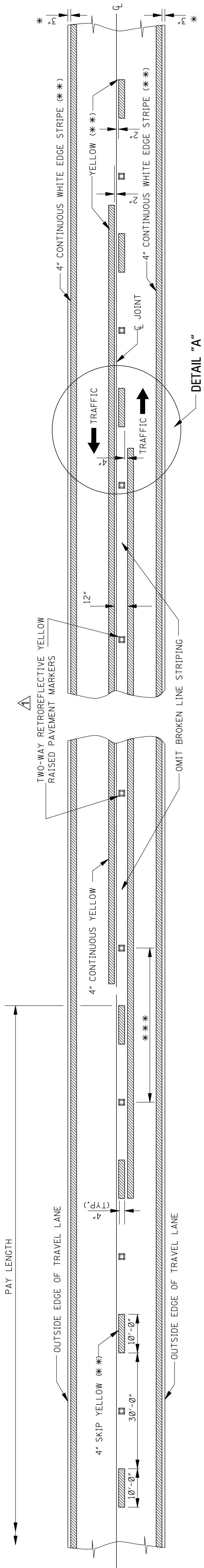
GENERAL NOTES:

- UNEVEN LANE LINE:
 - IF LESS THAN OR EQUAL TO 1/2", NO SIGNS REQUIRED.
 - IF GREATER THAN 1/2" AND LESS THAN OR EQUAL TO 2/4", PLACE SIGNS AS SHOWN ON THIS SHEET.
 - IF GREATER THAN 2/4", TRAFFIC SHOULD NOT BE ALLOWED TO CROSS UNEVEN LANE LINE.
- THE W8-11 SIGNS SHOULD BE SPACED AT 1 MILE INTERVALS THROUGHOUT UNEVEN LANE LINE LIMITS.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.
- SIGNS SHALL BE RETROREFLECTIVE WITH ORANGE BACKGROUND/BLACK COPY.

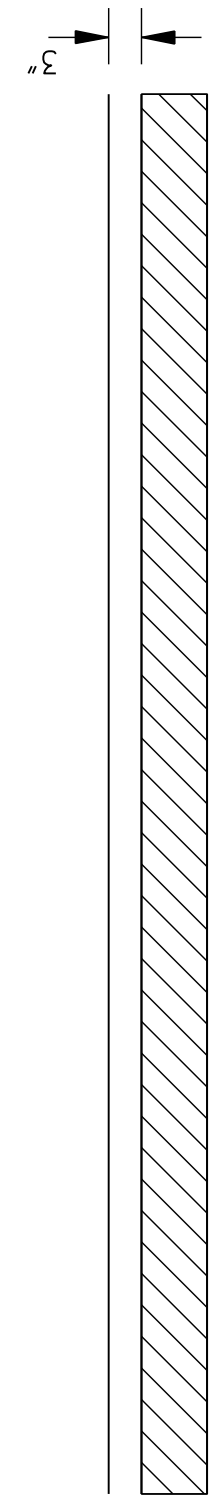
12/11/25	REVISED TEXT	STN	BY
	REVISION		

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

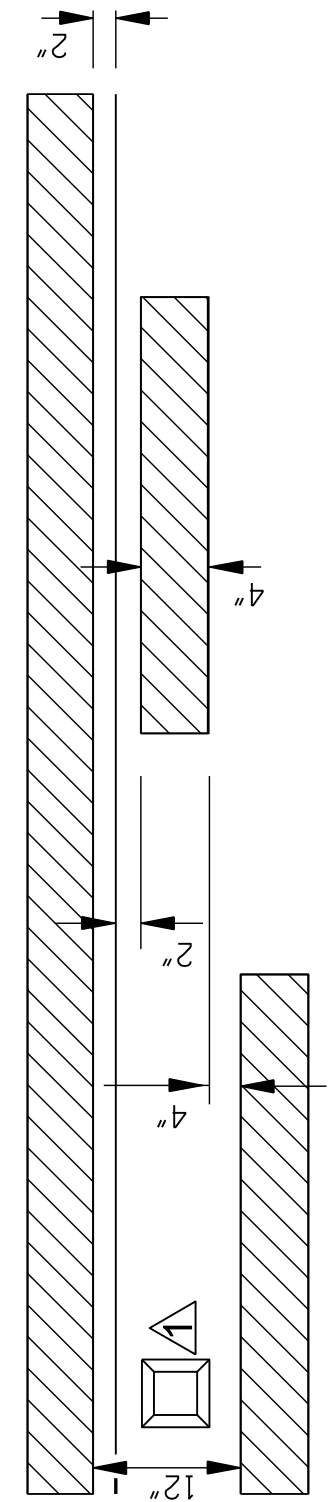
**TRAFFIC CONTROL PLANS
UNEVEN PAVEMENT
DETAILS**



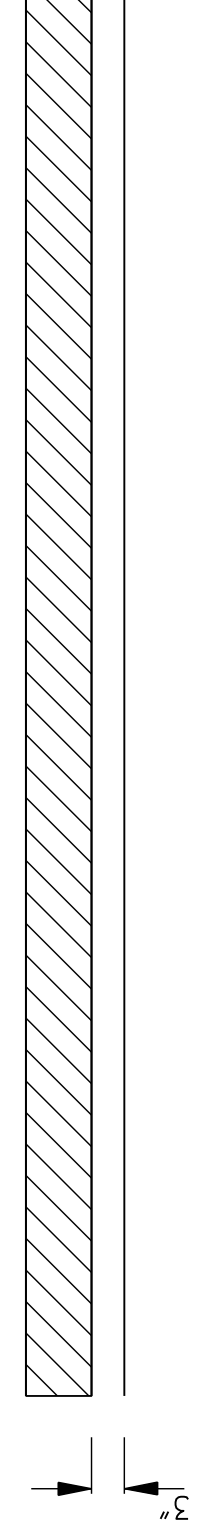
TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)



TRAFFIC



TRAFFIC



DETAIL "A"



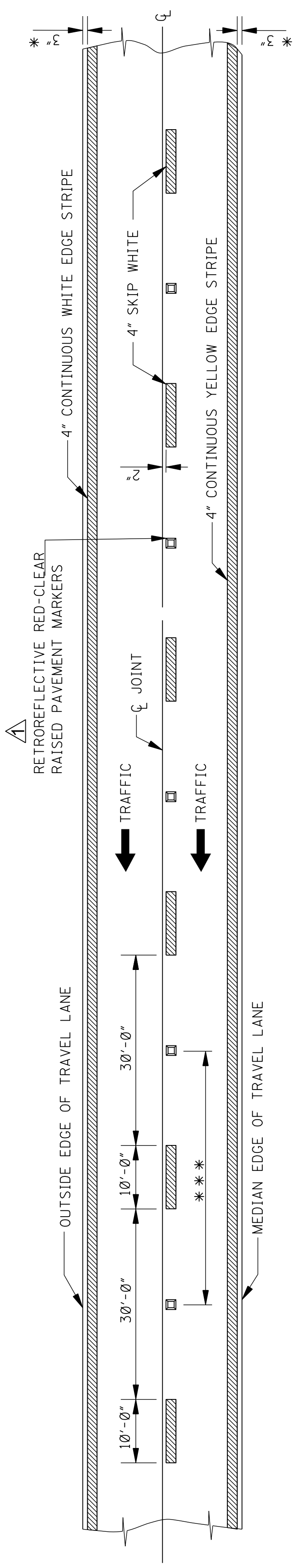
GENERAL NOTES:

- * 1. 3" UNLESS SHOWN ELSEWHERE ON THE PLANS.
- ** 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR TAPE AS INDICATED IN PAY ITEMS).
- ▲ 3. RETROREFLECTIVE RAISED PAVEMENT MARKERS TO BE USED IF TEMPORARY MARKINGS ARE TO REMAIN IN PLACE OVER 3 MONTHS
- ▲*** 4. SPACING OF RETROREFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

TANGENT SECTIONS	URBAN AREA (ft-in)	RURAL AREA (ft-in)
HORIZONTAL CURVES	40'-0"	50'-0"
INTERCHANGE LIMITS	40'-0"	+ 40'-0"

▲† NOTE: ON THE MAIN FACILITY, RETROREFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINES THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.

▲ 5. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RETROREFLECTIVE RAISED PAVEMENT MARKERS FROM THE MDOT "APPROVED PRODUCTS LIST".



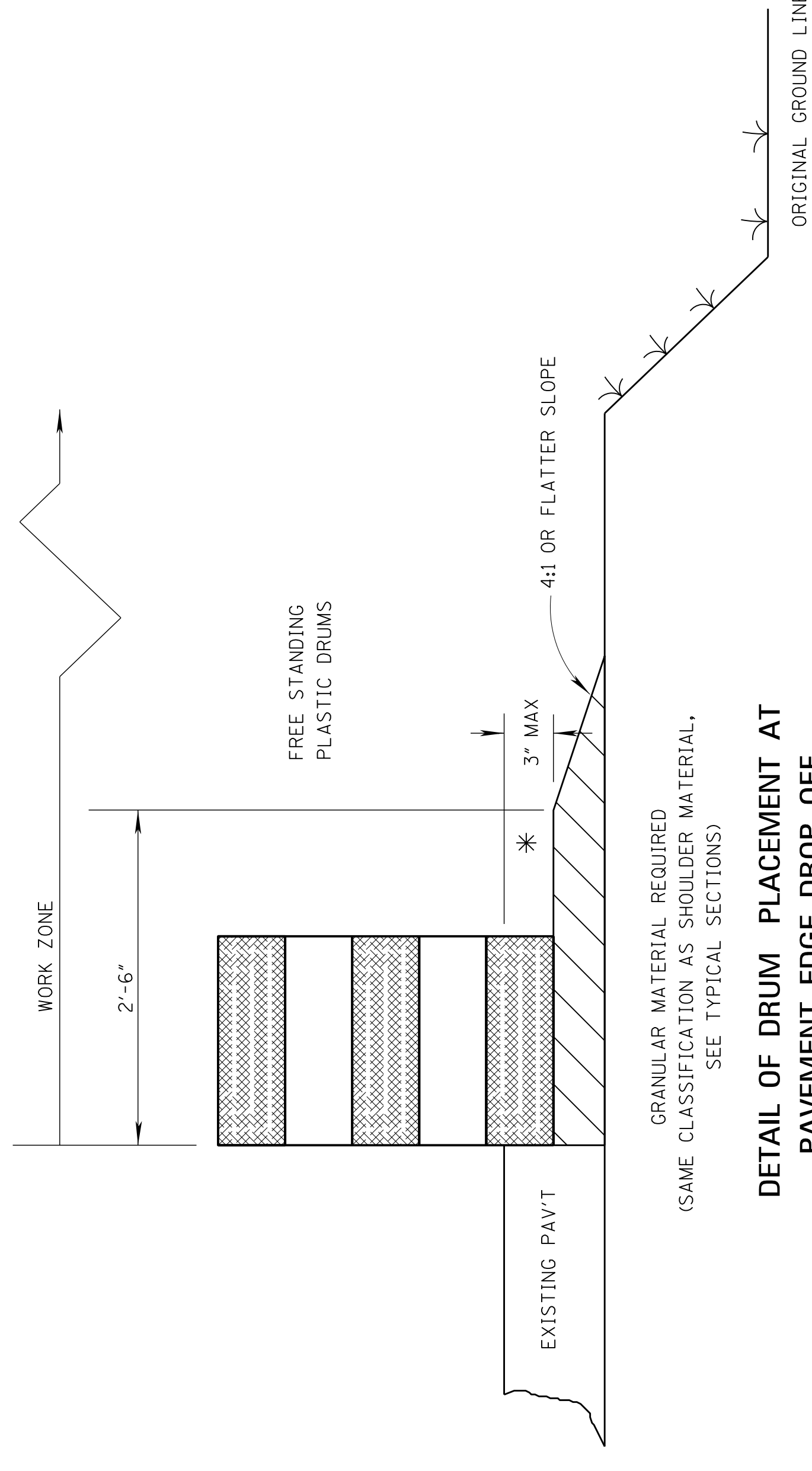
4-LANE WITH ONE-WAY TRAFFIC

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**TEMPORARY STRIPING FOR
2-LANE AND 4-LANE
DIVIDED HIGHWAYS**

DATE	REVISION
12/11/25	REVISED TEXT

ISSUE DATE: AUGUST 01, 2017
SHEET NUMBER: TCP-13
WORKING NUMBER: 6363
SHEET NUMBER: 6363



GRANULAR MATERIAL REQUIRED
(SAME CLASSIFICATION AS SHOULDER MATERIAL,
SEE TYPICAL SECTIONS)

**DETAIL OF DRUM PLACEMENT AT
PAVEMENT EDGE DROP-OFF**

NOTES:

* A. PAVEMENT EDGE DROP-OFF

1. IF LESS THAN TWO AND ONE QUARTER (2.25) INCHES-NO PROTECTION REQUIRED. PLACE A SHOULDER WORK SIGN (W21-5) 500 FEET IN ADVANCE OF WORK ZONE SHOULDER AND A LOW SHOULDER SIGN (W8-9) AT THE BEGINNING AND THROUGHOUT THE WORK ZONE @ (750'+0.C.).
2. TWO AND ONE QUARTER TO THREE INCHES-PLACE DRUMS, VERTICAL PANELS OR BARRICADES EVERY 100 FEET ON TANGENT SECTIONS FOR SPEEDS OF 50 MILES PER HOUR OR GREATER. CONES MAY BE USED IN PLACE OF DRUMS, PANELS, AND BARRICADES DURING DAYLIGHT HOURS. FOR TANGENT SECTIONS WITH SPEEDS LESS THAN 50 MILES PER HOUR AND FOR CURVES, DEVICES SHOULD BE PLACED EVERY 50 FEET. SPACING FOR TAPERS SHOULD BE IN ACCORDANCE WITH THE MUTCD (1 / 3 L, WHERE L IS THE TAPER LENGTH IN FEET.)
3. GREATER THAN THREE (3) INCHES-POSITIVE SEPARATION OR WEDGE WITH 4:1 OR FLATTER SLOPE NEEDED. IF THERE IS EIGHT (8) FEET OR MORE DISTANCE BETWEEN THE EDGE OF TRAVEL LANE AND DROP-OFF, THEN DRUMS, PANELS OR BARRICADES MAY BE USED.
4. FOR TEMPORARY CONDITIONS, DROP-OFFS GREATER THAN THREE (3) INCHES MAY BE PROTECTED WITH DRUMS, VERTICAL PANELS OR BARRICADES FOR SHORT DISTANCES DURING DAYLIGHT HOURS WHILE WORK IS BEING DONE IN THE DROP-OFF AREA.
5. LESSER TREATMENTS THAN THOSE DESCRIBED ABOVE MAY BE CONSIDERED FOR LOW-VOLUME LOCAL STREETS.

△ B. DRUM, VERTICAL PANEL, OR BARRICADE SPACING

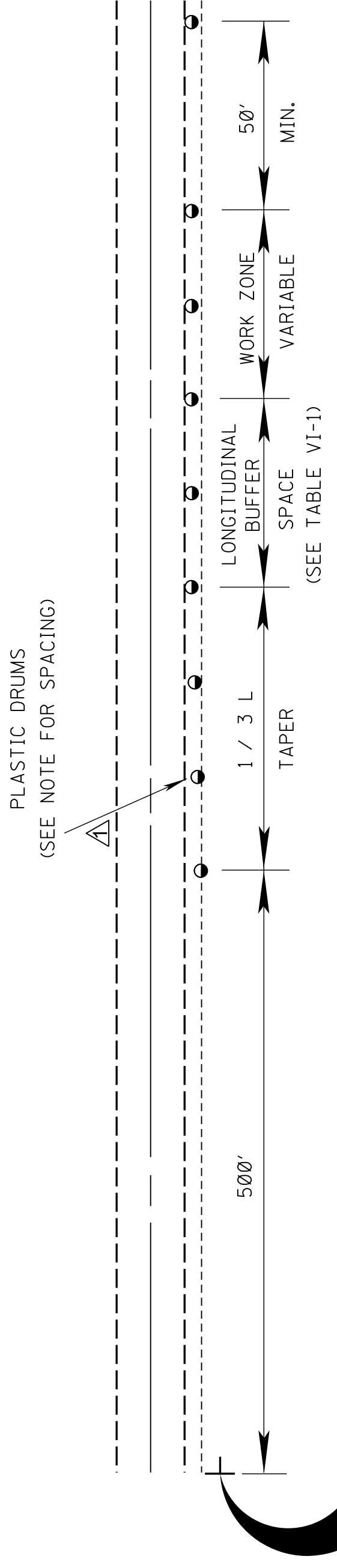
1. TANGENTS = $2 \times L / S$
 2. TAPERS = $L / 3$
- WHERE L = S X W
L = TAPER LENGTH IN FEET
S = SPEED IN MPH (POSTED OR ANTICIPATED OPERATING SPEED)
W = WIDTH OF OFFSET IN FEET

C. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.

TABLE VI-1. GUIDELINES FOR LENGTH OF
LONGITUDINAL BUFFER SPACE

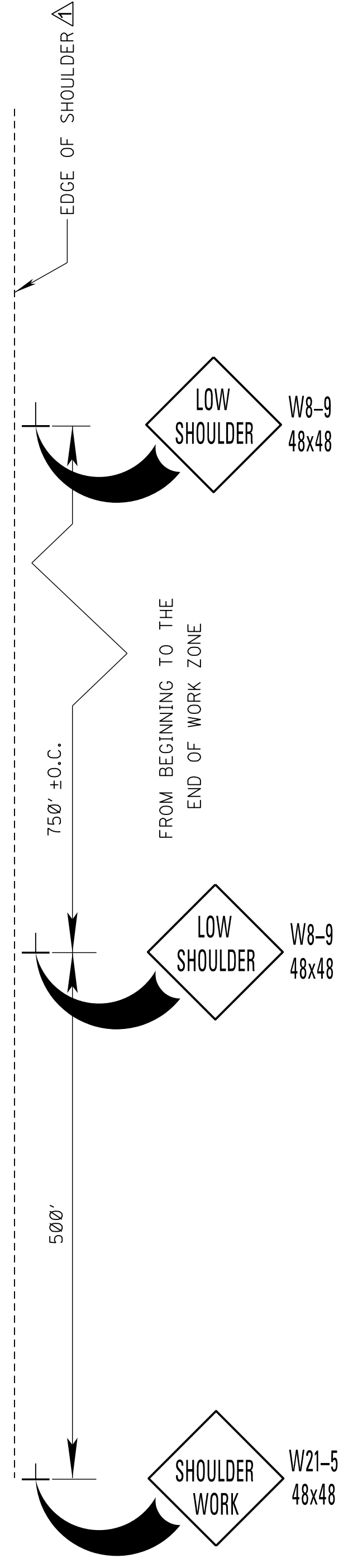
* SPEED (MPH)	LENGTH (FEET)
20	115
25	155
30	200
35	250
40	305
45	360
50	425
55	495
60	570
65	645

* * POSTED SPEED, OFF-PEAK 85 PERCENTILE SPEED
PRIOR TO WORK STARTING, OR THE ANTICIPATED
OPERATING SPEED IN MPH.



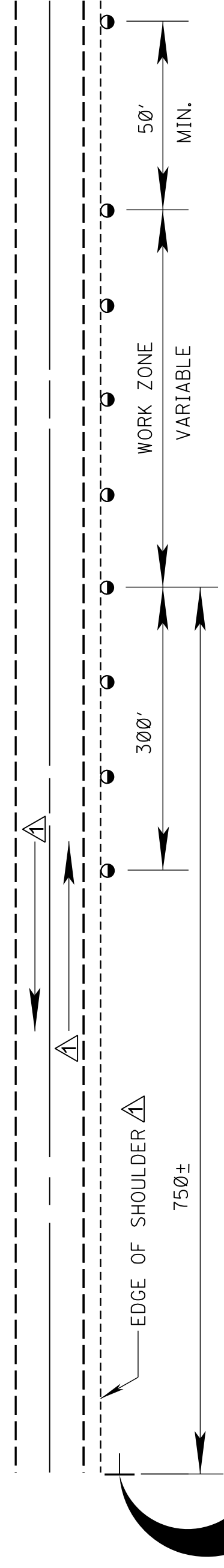
TYPICAL SHOULDER CLOSURE

- (1) TO BE USED WITH EIGHT (8) FOOT OR GREATER WIDTH IMPROVED SHOULDER.
- (2) TO BE USED WHEN CONSTRUCTION VEHICLES (EQUIPMENT) ENCROACHES ON OR WITHIN TWO (2) FEET OF THE SHOULDER BREAK.
- (3) FOR SHORT DURATION OPERATIONS OF 60 MINUTES OR LESS, ALL SIGNS AND CHANNELIZING DEVICES MAY BE ELIMINATED IF A VEHICLE WITH ACTIVATED HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS IS USED.



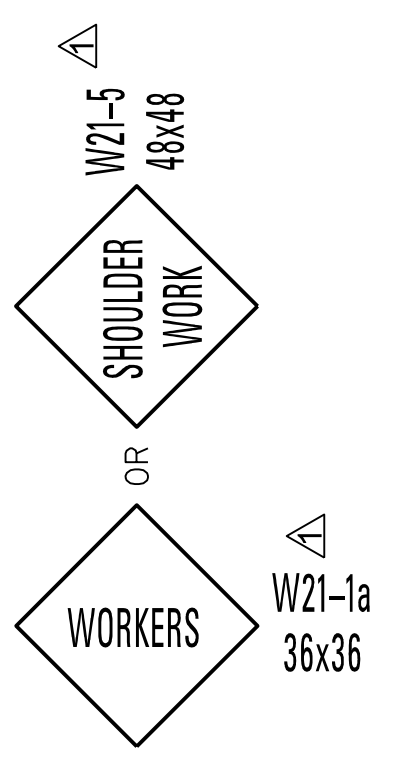
**TYPICAL SHOULDER WORK #1
(SEE NOTE A-1 THIS SHEET)**

PLASTIC DRUMS
(SEE NOTE FOR SPACING)



TYPICAL SHOULDER WORK #2

NOTE:
WORK OUTSIDE TWO (2) FOOT AND WITHIN TEN (10) FEET OF THE SHOULDER BREAK MAY BE PROTECTED BY
PLACING DRUMS ALONG THE SHOULDER EDGE, 300 FEET PRIOR TO AND 50 FEET BEYOND THE WORK AREA, OR
SEE NOTE A-3 THIS SHEET.



DATE	BY	REVISION
12/1/25	STN	REVISED TEXT

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**TRAFFIC CONTROL DETAILS
DRUM PLACEMENT
SHOULDER CLOSURE**