

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

| | | | |
|--------------------------------|----------------------------|--------------------------------|-------------------------|
| ADDENDUM NO. <u> 1 </u> | DATED <u> 1/22/2026 </u> | ADDENDUM NO. <u> </u> | DATED <u> </u> |
| ADDENDUM NO. <u> </u> | DATED <u> </u> | ADDENDUM NO. <u> </u> | DATED <u> </u> |
| ADDENDUM NO. <u> </u> | DATED <u> </u> | ADDENDUM NO. <u> </u> | DATED <u> </u> |

| Number | Description |
|--------|--|
| 1 | Revised Table of Contents; Revised Advertisement; Deleted NTB No. 5750; Revised NTB No. 7473; Added NTB No. 7474; Revised Wage Rates; Revised Bid Items; Amendment EBSx Download Required. |

TOTAL ADDENDA: 1
(Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

Contractor

BY _____
Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

| | | |
|-------|-----------|---------|
| _____ | President | Address |
| _____ | Secretary | Address |
| _____ | Treasurer | Address |

The following is my (our) itemized proposal.
STBG-0014-01(077)/ 108976301000
Pike County(ies)

Revised 01/26/2016

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION
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PROJECT: STBG-0014-01(077)/108976301 - Pike

| | |
|-----------|----------------------------------|
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OF SECTION 905 AS ADDENDA)

01/20/2026 01:35 PM

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 901 - ADVERTISEMENT

Electronic bids will be received by the Mississippi Transportation Commission at 10:00 o'clock A.M., Tuesday, January 27, 2026, from the Bid Express Service and shortly thereafter publicly read on the Sixth Floor For:

Seal & Overlay approximately 3.5 miles on MS 24 from Amite County Line to near I-55, known as Federal Aid Project No. STBG-0014-01(077) / 108976301 in Pike County.

The attention of bidders is directed to the Contract Provisions governing selection and employment of labor. Minimum wage rates have been predetermined by the Secretary of Labor and are subject to Public Law 87-581, Work Hours Act of 1962, as set forth in the Contract Provisions.

The specifications are on file in the offices of the Mississippi Department of Transportation.

Contractors may request permission to bid online at <http://shop.mdot.ms.gov> at no cost. Upon approval, Contractors shall be eligible to submit a bid using Bid Express at <http://bidx.com>. Specimen proposals may be viewed and downloaded online at no cost at <http://mdot.ms.gov> or purchased online at <http://shop.mdot.ms.gov> at a cost of Ten Dollars (\$10.00) per proposal plus a small convenience fee. Cash or checks will not be accepted as payment.

Bid bond, signed or countersigned by a Mississippi Agent or Qualified Nonresident Agent, with Power of Attorney attached, a Cashier's check or Certified Check for five (5%) percent of bid, payable to STATE OF MISSISSIPPI, must accompany each proposal.

The attention of bidders is directed to the provisions of Subsection 102.07 pertaining to irregular proposals and rejection of bids.

BRAD WHITE
EXECUTIVE DIRECTOR

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 – NOTICE TO BIDDERS NO. 7473

CODE: (SP)

DATE: 01/20/2026

SUBJECT: Scope of Work

PROJECT: STBG-0014-01(077) / 108976301 – Pike County

The contract documents do not include an official set of construction plans, but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, “Standard Drawings.”

The work to be accomplished using the pay items and corresponding specifications set forth in the contract is to scrub seal & overlay the following section of SR 24 in Pike County from Amite CL to I-55.

| Route | Length | Width | Scrub Seal | Surface Lift | Thickness |
|--------------|---------------|---------------|-------------------|---------------------|------------------|
| | (Mile) | (Feet) | (Inches) | | (Inches) |
| SR 24 | 3.5 | 28 | 0.375 | 9.5-mm, MT | 1.5 |

See sheet TS-1 for more details.

Work on the Project shall consist of the following:

1. The Contractor shall erect and maintain construction signing, provide all signs, set up nighttime lane closures (if needed) and traffic handling devices in accordance with the Traffic Control Plan. The cost for this work shall be included in the price bid for pay item 907-618-A: Maintenance of Traffic. All traffic control devices on this project shall comply with the latest version of the MUTCD. Fluorescent orange sheeting shall be used on all construction and traffic control signs except for those designated in the standards to be black legend and border on white background.
2. All MDOT owned right-of-way along SR 24 shall be cleared. Any limbs or portion of limbs extending onto the right-of-way shall be trimmed to a height of 70’. Careful consideration should be given to any above-ground utilities within the above-described limits, with clearing activities near those adhering to all applicable safety standards.

Cut trees may either be hauled away, mulched in place, or another method approved by the Engineer. Tree stumps shall be ground flush to the ground line unless directed otherwise by the Engineer. Material may not be mulched in residential areas. Mulched material shall be spread such that no more than 4” in depth of material is left in any location.

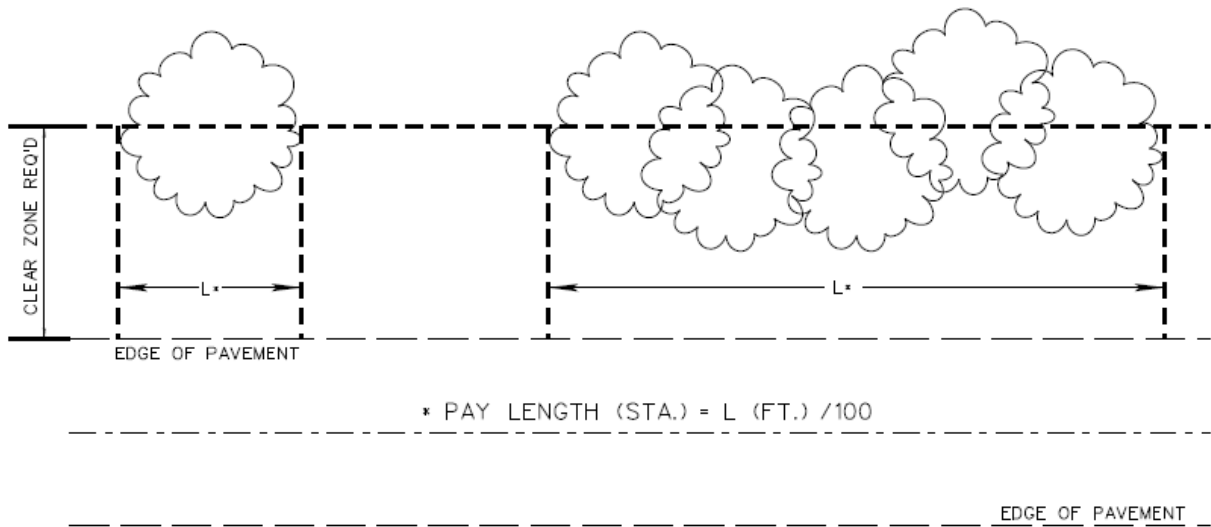
Removal should be done so as not to disturb below-ground root systems that may cause instability or slope failure, potentially damaging the roadway substructure. Any removal method alternatives to grinding mechanisms to remove remaining stumps to ground level should be pre-approved by the District Engineer with a plan to ensure substructure failure is

unlikely to occur. If roadside foreslopes are steeper than 3:1 or include other roadway features that would make tree removal infeasible, risk assessment should be conducted by the Engineer and Highway Safety Division.

This work shall be paid for as 201-D001: Random Clearing, per station. Each side of the roadway shall be measured separately.

Estimated Locations

| Station | Stations LT LN | Station | Stations RT LN |
|-------------------------|-------------------|-----------------|-------------------|
| 2+48 – 2+88 | 0.40 | 12+37 – 14+12 | 1.75 |
| 5+60 – 15+26 | 9.66 | 16+57 – 18+65 | 2.08 |
| 17+75 – 53+90 | 36.15 | 27+89 – 32+29 | 4.40 |
| 66+40 – 70+81 | 4.41 | 39+40 – 43+75 | 4.35 |
| 71+51 – 73+04 | 1.53 | 44+22 – 47+23 | 3.01 |
| 74+29 – 74+64 | 0.35 | 97+56 – 130+94 | 33.38 |
| 75+30 – 76+09 | 0.79 | 135+75 – 162+68 | 26.93 |
| 95+84 – 118+08 | 22.24 | 167+24 – 175+14 | 7.90 |
| 135+12 – 159+78 | 24.66 | 180+45 – 180+89 | 0.44 |
| 160+39 – 167+46 | 7.07 | 183+25 – 185+54 | 2.59 |
| 172+32 – 189+45 | 17.13 | 186+07 – 190+13 | 4.06 |
| | | 191+59 – 194+66 | 3.07 |
| LT LN Total | 124.39 | RT LN Total | 93.96 |
| Total (Stations) | 218.35 | | |



PAY DETAIL FOR RANDOM CLEARING

3. Prior to the overlay, the existing shoulders shall be clipped to provide proper drainage and surplus material shall be spread along the edge of the shoulders, fore slopes, or other adjacent areas as directed by the Engineer, and will be considered an absorbed item.
4. The Contractor shall fine mill at the following locations: See TS-1.

| <u>Location</u> | <u>Milling, SY</u> | <u>Remarks</u> |
|--------------------------------|--------------------|-------------------------------|
| BOP Sta. 0+00 – Sta. 1+50 | 467 | BOP Tie-In |
| EOP Sta. 193+16 – Sta 194+66 | 467 | EOP Tie-In |
| Bridge # 73.8 Bridge # 74.1 | 6,474 | Bridge Ends & between bridges |
| Local Roads | 3,745 | As directed – 9 Locations |
| Guardrail Pads | 1,604 | @ 2 Bridges |
| Total | 12,757 | |

5. The Contractor shall place scrub seal left and right of CL for a total width of 24' from the BOP to the EOP. See TS-1.
6. The Contractor shall perform pre-leveling operations by placing 0.75" and variable of 9.5-mm, MT Asphalt Pavement, Leveling as shown on TS-1.

The Contractor shall remove existing pavement markers prior to placing leveling and/or scrub seal. The cost of removing these pavement markers is to be absorbed in other items bid.

7. The Contractor shall place the top lift of asphalt on the roadway left and right of the centerline from BOP to EOP such that the overall pavement width is 28'. The finished cross-slope is to be 2% in tangent sections and match the existing super elevation rate in horizontal curves.

The Contractor shall remove existing pavement markers prior to placing asphalt. The cost of removing these pavement markers is to be absorbed in other items bid.

Asphalt surface shall be placed on all local roads, driveway aprons, and guardrail pads. Asphalt for this work shall be paid as 9.5-mm, MT, Asphalt Pavement, Leveling.

Driveway aprons shall be paved 10' wide or as directed by the Engineer. All local roads shall be paved to the normal right-of-way line or as directed by the Engineer.

Asphalt quantities were estimated as follows:

| | |
|----------------|------------|
| Mainline | 4,851 Tons |
| Local Roads | 309 Tons |
| Driveway Pads | 260 Tons |
| Guardrail Pads | 133 Tons |

| | |
|---------------|----------|
| Spot Leveling | 368 Tons |
|---------------|----------|

Note: The Contractor shall be responsible for traffic control while MDOT personnel conduct density testing on the asphalt. The cost shall be included in the bid price for pay item 907-618-A: Maintenance of Traffic.

8. The existing guardrail pads from the face of the rail extending outward shall be removed. The contractor shall then place crushed stone at these locations (6" depth). Excess excavation may be required. Existing guardrail items shall then be removed, and the following shall be installed, as per the standard drawings. A sawcut pay item has been included in the quantities for the removal of asphalt pavement. If the item is not utilized, no payment will be made under this item. See below for quantity estimations.

| Location Station | Terminal End | Bridge End Type H | Delineator White | W Beam | Removal | Remarks |
|--------------------|--------------|-------------------|------------------|------------|--------------|---------|
| Bridge 73.8 | | | | | | |
| 83+61 | 1 | 1 | 6 | 112.5 | 210 | RT |
| 83+61 | 1 | 1 | 5 | 62.5 | 110 | LT |
| 87+62 | 1 | 1 | 5 | 62.5 | 110 | RT |
| 87+62 | 1 | 1 | 6 | 112.5 | 210 | LT |
| Bridge 74.1 | | | | | | |
| 97+75 | 1 | 1 | 6 | 112.5 | 210 | RT |
| 97+75 | 1 | 1 | 5 | 62.5 | 110 | LT |
| 99+36 | 1 | 1 | 5 | 62.5 | 110 | RT |
| 99+36 | 1 | 1 | 6 | 112.5 | 210 | LT |
| Totals | 8 | 8 | 44 | 700 | 1,280 | |

9. The Contractor shall place granular material on the shoulders to raise the existing shoulders to the new grade. The shoulder shall be bladed, shaped, and compacted to a minimum slope of 4%. Granular material will not be allowed to be placed directly on the top lift of asphalt, but must be placed directly on the gravel shoulder by means of a road widener machine approved by the Engineer. Light blading or mowing of the shoulders will be required prior to placement of the granular material. Please refer to Subsection 618.03.3 of the Standard Specification as well as sheet SDTCP-16 for requirements related to pavement edge drop-offs.
10. The Contractor shall place rumble strips in the locations indicated in the typical sections on sheet RS-2 (Detail "B").
11. The Contractor shall place all permanent pavement markings, including stripe and raised pavement markers, throughout the project as required by the Standard Drawings or as directed by the Engineer. Existing pavement markings located on bridges shall be removed as required prior to placing new markings.

12. The Contractor shall sweep clean and remove all debris on all bridges within the project limits as part of final cleanup.

An asphalt taper shall be placed at the temporary joints caused by the milling or overlay in order to provide for the safe movement of traffic. The taper shall be three feet (3') in length per one inch of depth and will be an absorbed item.

Temporary striping shall be required after scrub seal, leveling and overlaying operations: Temporary striping shall be placed in the same location and layout as permanent stripe. All centerline, lane lines, edge lines, and no-passing stripes that have been removed during the day's operations shall be replaced with temporary stripe before work is discontinued for the day or as soon thereafter as weather conditions permit, except that:

- Replacement of no-passing stripes may be delayed for a period not to exceed three (3) days for a two or three lane road.
- Temporary edge lines on projects requiring shoulders constructed of granular material may be delayed for a period not to exceed three (3) days.

All asphalt and concrete curbs along local roads from BOP to EOP shall be painted (two applications) with traffic paint and traffic beads as shown on sheet DCIS-1; color to correspond to adjacent pavement stripe; such costs shall be included in other items bid.

It shall be the responsibility of the Contractor to protect the roadway and all existing structures, such as bridges, culverts, signs, and curbs, from damage occurring as a result of the Contractor's operations. Damages to existing structures caused by the Contractor's operations shall be repaired or replaced at no cost to the Department.

Incidental work such as removing vegetation, shaping and compaction of shoulders, removing excess asphalt material, project clean-up, and other incidental work necessary to complete the project will not be measured for separate payment. Such costs shall be included in the price of other items bid.

It is the Contractor's responsibility to insure the drainage of surface water from milled areas. Where applicable, existing shoulder material is to remain in place to be incorporated into final sloping of the shoulders. Temporary wedges (paper joints) of full lane width asphalt shall be placed by the Contractor immediately after the fine milling process to allow the safe transition of traffic. These wedges shall be maintained in a satisfactory condition by the Contractor until the permanent asphalt is placed, cost to be absorbed.



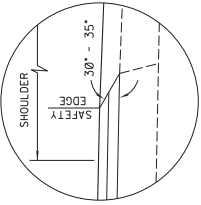
MDOT
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

DESIGNED BY:
CHECKED BY:
DATE:

FMS CON: 108976/301000
PROJECT NO.: STBG-0014-0027
COUNTY: PIKE

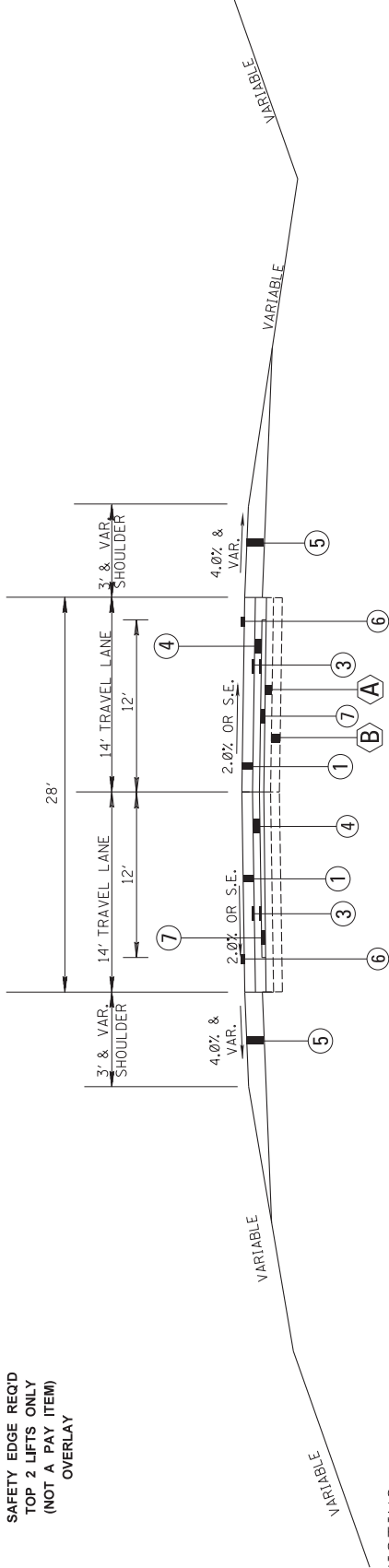
Notice to Bidders No. 7473 -- Cont'd

WV MO. 11
SHEET NO. 1



**SAFETY EDGE REQ'D
TOP 2 LIFTS ONLY
(NOT A PAY ITEM)
OVERLAY**

SR 24
C



TYPICAL SECTION

B.O.P. STA. 0+00 - E.O.P. STA. 194+66

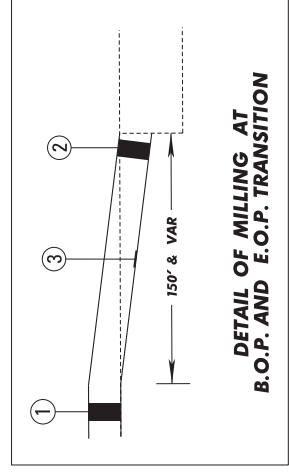
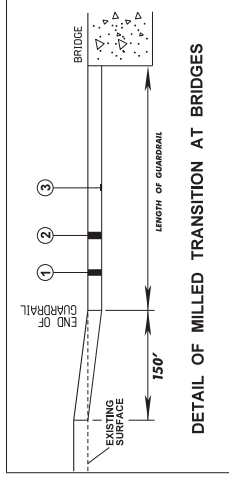
NOT TO SCALE

EXISTING

- A** BITUMINOUS PAVEMENT IN PLACE
- B** GRANULAR SUBGRADE IN PLACE

PROPOSED

- 1** 1.5" of 9.5MM MT ASPHALT PAVEMENT, REQ'D
- 2** 1.5" & VAR. FINE MILLING WHERE REQ'D
- 3** TACK COAT REQ'D
- 4** 0.75" & Var. of 9.5MM MT ASPHALT PAVEMENT, LEVELING (WHERE REQ'D)
- 5** 1.5" & VAR. OF CL. 6, GR. D GRANULAR MATERIAL REQ'D
- 6** RUMBLE STRIPE REQ'D. (See Sheet RS-2, Detail "B")
- 7** 0.375" of SCRUB SEAL REQ'D. (12' Lane Width)





DESIGNED BY: _____
 CHECKED BY: _____
 DATE: _____

FMS CN: 108976/301000
 PROJECT NO.: STBG-0014 (07)
 COUNTY: PIKE

TRAFFIC CONTROL SIGNS
 NOTICE TO CONTRACTORS

WV MO. TCRD
 SHEET NO. 3

| SIGNS REQUIRED (CONT'D) | | | |
|---|-----------------|----------------------------|---------------------------|
| SIGN NO. | SIZE | UNIT AREA REQ'D. (SQ. FT.) | TOTAL SIGN AREA (SQ. FT.) |
| W8-7 | 48" X 48" | 16.00 | 16.00 |
| W8-9 | 48" X 48" | 16.00 | 16.00 |
| W8-11 | 36" X 36" | 9.00 | 9.00 |
| W8-12 | 48" X 48" | 16.00 | 16.00 |
| W10-1 | 36" DIA. | 7.07 | 7.07 |
| W10-1 | 24" X 24" | 4.00 | 4.00 |
| W14-3 | 36" X 48" X 48" | 5.56 | 8 |
| W14-3 | 48" X 48" X 64" | 9.89 | 9.89 |
| W16-2 | 24" X 18" | 3.00 | 3.00 |
| W19-2 | 48" X 48" | 16.00 | 16.00 |
| W20 - 1 | 48" X 48" | 16.00 | 17 |
| W20 - 1 | 36" X 36" | 9.00 | 9.00 |
| W20 - 2 | 48" X 48" | 16.00 | 16.00 |
| W20 - 3 | 48" X 48" | 16.00 | 16.00 |
| W20 - 4 | 48" X 48" | 16.00 | 16.00 |
| W20 - 4b | 48" X 48" | 16.00 | 16.00 |
| W20 - 5L | 48" X 48" | 16.00 | 16.00 |
| W20 - 5R | 48" X 48" | 16.00 | 16.00 |
| W20 - 7a | 48" X 48" | 16.00 | 16.00 |
| W21 - 1 | 36" X 36" | 9.00 | 9.00 |
| W21 - 1a | 36" X 36" | 9.00 | 9.00 |
| W21-2 | 36" X 36" | 9.00 | 9.00 |
| W21-3 | 48" X 48" | 16.00 | 16.00 |
| W21-5 | 48" X 48" | 9.00 | 9.00 |
| W21-6 | 36" X 36" | 9.00 | 9.00 |
| W24-1L | 48" X 48" | 16.00 | 16.00 |
| W24-1R | 48" X 48" | 16.00 | 16.00 |
| W24-1aR | 48" X 48" | 16.00 | 16.00 |
| W24-1bL | 48" X 48" | 16.00 | 16.00 |
| W24-1bR | 48" X 48" | 16.00 | 16.00 |
| VP-1L | 12" X 36" | 3.00 | 3.00 |
| VP-1R | 12" X 36" | 3.00 | 3.00 |
| OM-3L | 12" X 36" | 3.00 | 3.00 |
| OM-3R | 12" X 36" | 3.00 | 3.00 |
| TOTAL SIGN AREA LESS THAN 10 SQ. FT. | | | |
| TOTAL SIGN AREA GREATER THAN 10 SQ. FT. 292 SQ. FT. | | | |

| SIGNS REQUIRED (CONT'D) | | | |
|-------------------------|-----------|----------------------------|---------------------------|
| SIGN NO. | SIZE | UNIT AREA REQ'D. (SQ. FT.) | TOTAL SIGN AREA (SQ. FT.) |
| W1 - 1L | 48" X 48" | 16.00 | 16.00 |
| W1 - 1R | 48" X 48" | 16.00 | 16.00 |
| W1 - 2L | 48" X 48" | 16.00 | 16.00 |
| W1 - 2R | 48" X 48" | 16.00 | 16.00 |
| W1 - 3L | 48" X 48" | 16.00 | 16.00 |
| W1 - 3R | 48" X 48" | 16.00 | 16.00 |
| W1 - 4aL | 48" X 48" | 16.00 | 16.00 |
| W1 - 4aR | 48" X 48" | 16.00 | 16.00 |
| W1 - 5L | 48" X 48" | 16.00 | 16.00 |
| W1 - 5R | 48" X 48" | 16.00 | 16.00 |
| W1 - 6L | 48" X 24" | 9.00 | 9.00 |
| W1 - 6L | 60" X 30" | 12.50 | 12.50 |
| W1 - 6R | 48" X 24" | 9.00 | 9.00 |
| W1 - 6R | 60" X 30" | 12.50 | 12.50 |
| W1 - 7 | 48" X 24" | 9.00 | 9.00 |
| W1 - 7 | 60" X 30" | 12.50 | 12.50 |
| W1 - 8L | 18" X 24" | 3.00 | 3.00 |
| W1 - 8L | 36" X 48" | 12.00 | 12.00 |
| W1 - 8R | 18" X 24" | 3.00 | 3.00 |
| W1 - 8R | 36" X 48" | 12.00 | 12.00 |
| W1-9L | 48" X 48" | 16.00 | 16.00 |
| W1-9L | 48" X 48" | 16.00 | 16.00 |
| W2-6 | 36" X 36" | 9.00 | 9.00 |
| W3-2a | 48" X 48" | 16.00 | 16.00 |
| W3-2a | 48" X 48" | 16.00 | 16.00 |
| W3-3 | 48" X 48" | 16.00 | 16.00 |
| W3-5 | 48" X 48" | 16.00 | 16.00 |
| W4-1L | 48" X 48" | 16.00 | 16.00 |
| W4-1R | 48" X 48" | 16.00 | 16.00 |
| W4-2L | 48" X 48" | 16.00 | 16.00 |
| W4-2R | 48" X 48" | 16.00 | 16.00 |
| W4-3L | 48" X 48" | 16.00 | 16.00 |
| W4-3R | 48" X 48" | 16.00 | 16.00 |
| W4-6 | 48" X 48" | 16.00 | 16.00 |
| W5-1a | 48" X 48" | 16.00 | 16.00 |
| W6-1 | 48" X 48" | 16.00 | 16.00 |
| W6-2 | 48" X 48" | 16.00 | 16.00 |
| W6-3 | 48" X 48" | 16.00 | 16.00 |
| W8-1 | 48" X 48" | 16.00 | 16.00 |
| W8-4 | 48" X 48" | 16.00 | 16.00 |
| W8-6 | 48" X 48" | 16.00 | 16.00 |

| SIGNS REQUIRED (CONT'D) | | | |
|-------------------------|-----------------|----------------------------|---------------------------|
| SIGN NO. | SIZE | UNIT AREA REQ'D. (SQ. FT.) | TOTAL SIGN AREA (SQ. FT.) |
| R1 - 1 | 36" OCTAGON | 7.46 | 7.46 |
| R1 - 1 | 48" OCTAGON | 13.25 | 13.25 |
| R1 - 2 | 36" X 36" X 36" | 3.90 | 3.90 |
| R1 - 2 | 48" X 48" X 48" | 6.93 | 6.93 |
| R1 - 2 | 60" X 60" X 60" | 10.83 | 10.83 |
| R1 - 3 | 18" X 9" | 1.13 | 1.13 |
| R1 - 3 | 24" X 12" | 2.00 | 2.00 |
| R2-1 | 24" X 30" | 5.00 | 5.00 |
| R2-1 | 36" X 48" | 12.00 | 12.00 |
| R2-1 | 48" X 60" | 20.00 | 20.00 |
| R3-1 | 36" X 36" | 9.00 | 9.00 |
| R3-1 | 48" X 48" | 16.00 | 16.00 |
| R3-2 | 36" X 36" | 9.00 | 9.00 |
| R3-2 | 48" X 48" | 16.00 | 16.00 |
| R3-4 | 36" X 36" | 9.00 | 9.00 |
| R3-4 | 48" X 48" | 16.00 | 16.00 |
| R3-5L | 30" X 36" | 7.50 | 7.50 |
| R3-5R | 30" X 36" | 7.50 | 7.50 |
| R3-6L | 30" X 36" | 7.50 | 7.50 |
| R3-6R | 30" X 36" | 7.50 | 7.50 |
| R3-7L | 30" X 30" | 6.25 | 6.25 |
| R3-7R | 30" X 30" | 6.25 | 6.25 |
| R4-1 | 24" X 30" | 5.00 | 25 |
| R4-1 | 48" X 60" | 20.00 | 20.00 |
| R4-2 | 24" X 30" | 5.00 | 8 |
| R4-2 | 48" X 60" | 20.00 | 20.00 |
| R4-7 | 48" X 60" | 20.00 | 20.00 |
| R4-8 | 48" X 60" | 20.00 | 20.00 |
| R5-1 | 48" X 48" | 16.00 | 16.00 |
| R5-1a | 42" X 30" | 8.75 | 8.75 |
| R6-1L | 36" X 12" | 3.00 | 3.00 |
| R6-1R | 36" X 12" | 3.00 | 3.00 |
| R6-2L | 24" X 30" | 5.00 | 5.00 |
| R6-2R | 24" X 30" | 5.00 | 5.00 |
| R11-2 | 48" X 30" | 10.00 | 10.00 |
| R11-3a | 60" X 30" | 12.50 | 12.50 |
| R11-3b | 60" X 30" | 12.50 | 12.50 |
| R11-4 | 60" X 30" | 12.50 | 12.50 |
| R12-1 | 36" X 48" | 12.00 | 12.00 |
| R16-3 | 36" X 48" | 12.00 | 12.00 |
| R16-3 | 48" X 60" | 20.00 | 20.00 |

| SIGNS REQUIRED | | | |
|----------------|-----------|----------------------------|---------------------------|
| SIGN NO. | SIZE | UNIT AREA REQ'D. (SQ. FT.) | TOTAL SIGN AREA (SQ. FT.) |
| G20 - 1 | 60" X 24" | 10.00 | 2 |
| G20 - 2 | 48" X 24" | 8.00 | 2 |
| G20 - 4 | 36" X 18" | 4.50 | 16 |
| M1 - 1 | 24" X 24" | 4.00 | 1 OR 2 DIGIT |
| M1 - 1 | 30" X 24" | 5.00 | 3 DIGIT |
| M1 - 4 | 24" X 24" | 4.00 | 1 OR 2 DIGIT |
| M1 - 4 | 30" X 24" | 5.00 | 3 DIGIT |
| M1 - 5 | 24" X 24" | 4.00 | 1 OR 2 DIGIT |
| M1 - 5 | 30" X 24" | 5.00 | 3 DIGIT |
| M3 - 1 | 24" X 12" | 2.00 | 1 OR 2 DIGIT |
| M3 - 1 | 30" X 15" | 3.13 | 3 DIGIT |
| M3 - 2 | 24" X 12" | 2.00 | DIGIT RATE MARKER |
| M3 - 2 | 30" X 15" | 3.13 | DIGIT RATE MARKER |
| M3 - 3 | 24" X 12" | 2.00 | DIGIT RATE MARKER |
| M3 - 3 | 30" X 15" | 3.13 | DIGIT RATE MARKER |
| M3 - 4 | 24" X 12" | 2.00 | DIGIT RATE MARKER |
| M3 - 4 | 30" X 15" | 3.13 | DIGIT RATE MARKER |
| M4 - 8 | 24" X 12" | 2.00 | DIGIT RATE MARKER |
| M4 - 8 | 30" X 15" | 3.13 | DIGIT RATE MARKER |
| M4 - 9 | 48" X 36" | 12.00 | DETOUR ↑ |
| M4 - 9L | 48" X 36" | 12.00 | DETOUR ← |
| M4 - 9R | 48" X 36" | 12.00 | DETOUR ↗ |
| M4 - 9S1 | 48" X 36" | 12.00 | DETOUR ↘ |
| M4 - 9S2 | 48" X 36" | 12.00 | DETOUR ↖ |
| M4 - 9S3 | 48" X 36" | 12.00 | DETOUR ↙ |
| M4 - 9S4 | 48" X 36" | 12.00 | DETOUR ↘ |
| M4 - 9S5 | 48" X 36" | 12.00 | DETOUR ↙ |
| M4 - 9S6 | 48" X 36" | 12.00 | DETOUR ↘ |
| M4 - 9S7 | 48" X 36" | 12.00 | DETOUR ↙ |
| M4 - 9S8 | 48" X 36" | 12.00 | DETOUR ↘ |
| M4 - 10L | 48" X 18" | 6.00 | DETOUR ↘ |
| M4 - 10R | 48" X 18" | 6.00 | DETOUR ↙ |
| M4 - 5 | 24" X 12" | 2.00 | TO |
| M5 - 1R | 21" X 15" | 2.19 | ↑ |
| M5 - 2L | 21" X 15" | 2.19 | ↖ |
| M5 - 2R | 21" X 15" | 2.19 | ↗ |
| M6 - 1L | 21" X 15" | 2.19 | ← |
| M6 - 1R | 21" X 15" | 2.19 | → |
| M6 - 2L | 21" X 15" | 2.19 | ↖ |
| M6 - 2R | 21" X 15" | 2.19 | ↗ |
| M6 - 3 | 21" X 15" | 2.19 | ↑ |

NOTES

- 1 STANDARD
 - 2 SPECIAL (USE WHERE WARRANTED)
 - 3 INTERSTATE ROUTE MARKER
 - 4 STATE ROUTE MARKER
 - 5 COLORS OF CARDINAL DIRECTION MARKERS AND DIRECTIONAL ROUTE MARKERS.
 - 6 BLACK STRIPES ON YELLOW BACKGROUND
 - 7 INTERSTATE USE ONLY
 - 8 TOP OF SIGN - BLACK LETTERING ON ORANGE BACKGROUND.
 - 9 BOTTOM OF SIGN - BLACK LETTERING ON WHITE BACKGROUND.
- THE BACKGROUND OF ALL WARNING SIGNS ("W" SERIES) EXCEPT W10-1 SHALL BE ORANGE. THE W10-1 BACKGROUND SHALL BE YELLOW IN ALL CASES.



MISSISSIPPI DEPARTMENT OF TRANSPORTATION

DESIGNED BY:
 CHECKED BY:
 DATE:

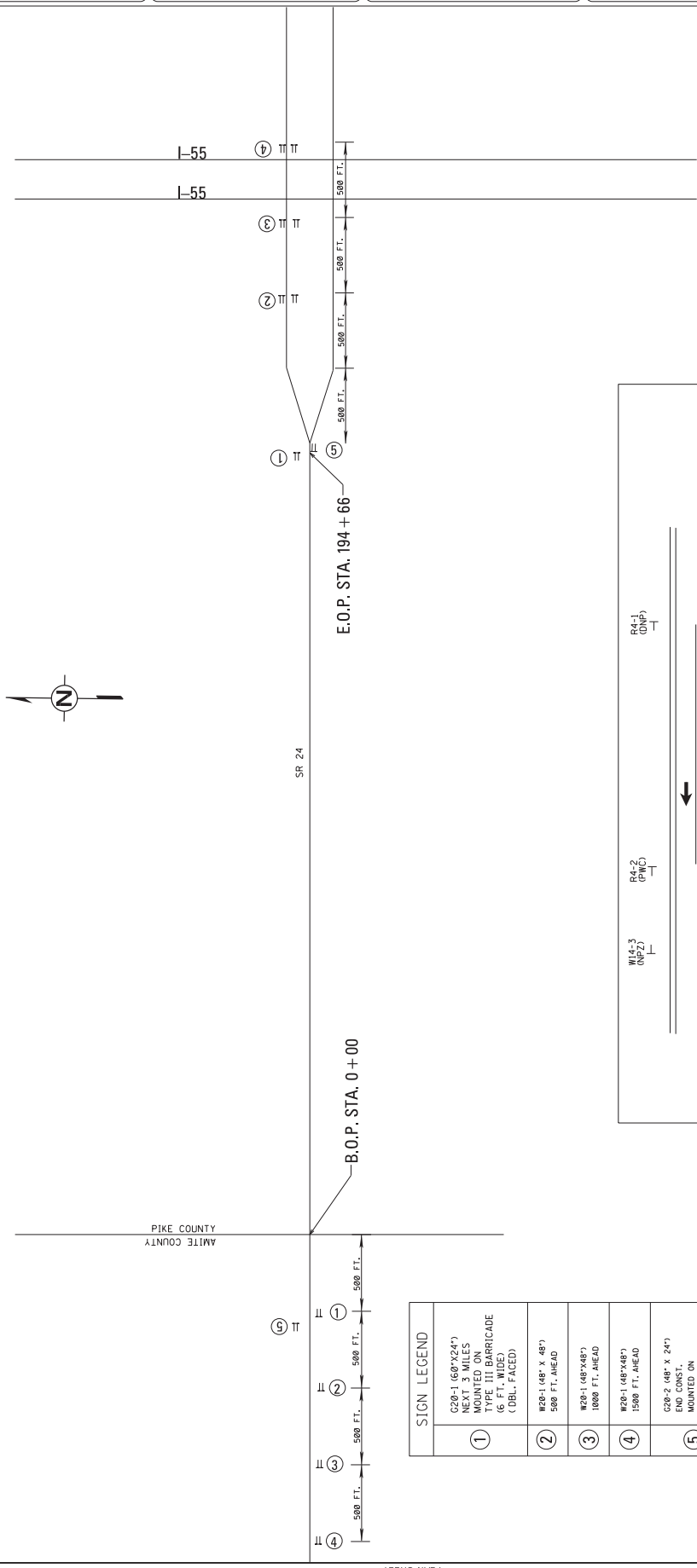
FMS CON: 108976/301000
 PROJECT NO.: STBG-0014 (0077)
 COUNTY: PIKE

Notice to Bidders No. 7473

W.P.O. DES.
 SHEET NO. 4

NOTE: W20-1 (48"x48") SHALL BE REQ'D ON ALL LOCAL ROADS.

*** NOT TO SCALE ***



| SIGN LEGEND | |
|-------------|--|
| ① | G20-1 (60"x24") NEXT 3 MILES MOUNTED ON TYPE III BARRICADE (6 FT. WIDE) (DBL.-FACED) |
| ② | W20-1 (48" X 48") 500 FT. AHEAD |
| ③ | W20-1 (48"x48") 1000 FT. AHEAD |
| ④ | W20-1 (48"x48") 1500 FT. AHEAD |
| ⑤ | G20-2 (48" X 24") END CONST. MOUNTED ON TYPE III BARRICADE (6 FT. WIDE) (DBL.-FACED) |
| ⑥ | G20-1 (60"x24") NEXT 3 MILES MOUNTED ON TYPE III BARRICADE (6 FT. WIDE) (DBL.-FACED) |

ADDITIONAL TRAFFIC CONTROL SIGNS REQUIRED:

R4-1 (R.P.)
 W14-3 (W.P.)
 R4-2 (R.P.)
 R4-1 (R.P.)
 W14-3 (W.P.)

ADDITIONAL TRAFFIC CONTROL SIGNS REQUIRED:

R4-1 "DO NOT PASS", R4-2 "PASS WITH CARE", AND W14-3 "NO PASSING ZONE" SIGNS ARE REQUIRED IN ACCORDANCE WITH SUB-SECTION 507-618 AND AS SPECIFIED IN THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES. IF "NO PASSING ZONES" ARE 1,000' OR MORE, INSTALL ADDITIONAL "DO NOT PASS" SIGNS ON MAXIMUM SPACING OF 750'.

ALL TRAFFIC CONTROL ITEMS ON THIS SHEET SHALL BE INCLUDED IN BID FOR PAY ITEM 618-4001, MAINTENANCE OF TRAFFIC CONTROL SIGNS. THESE ITEMS SHALL BE USED ON ALL CONSTRUCTION AND TRAFFIC CONTROL SIGNS EXCEPT FOR THOSE DESIGNATED ON THE PLANS TO BE BLACK LEGEND AND BORDER ON WHITE BACKGROUND.



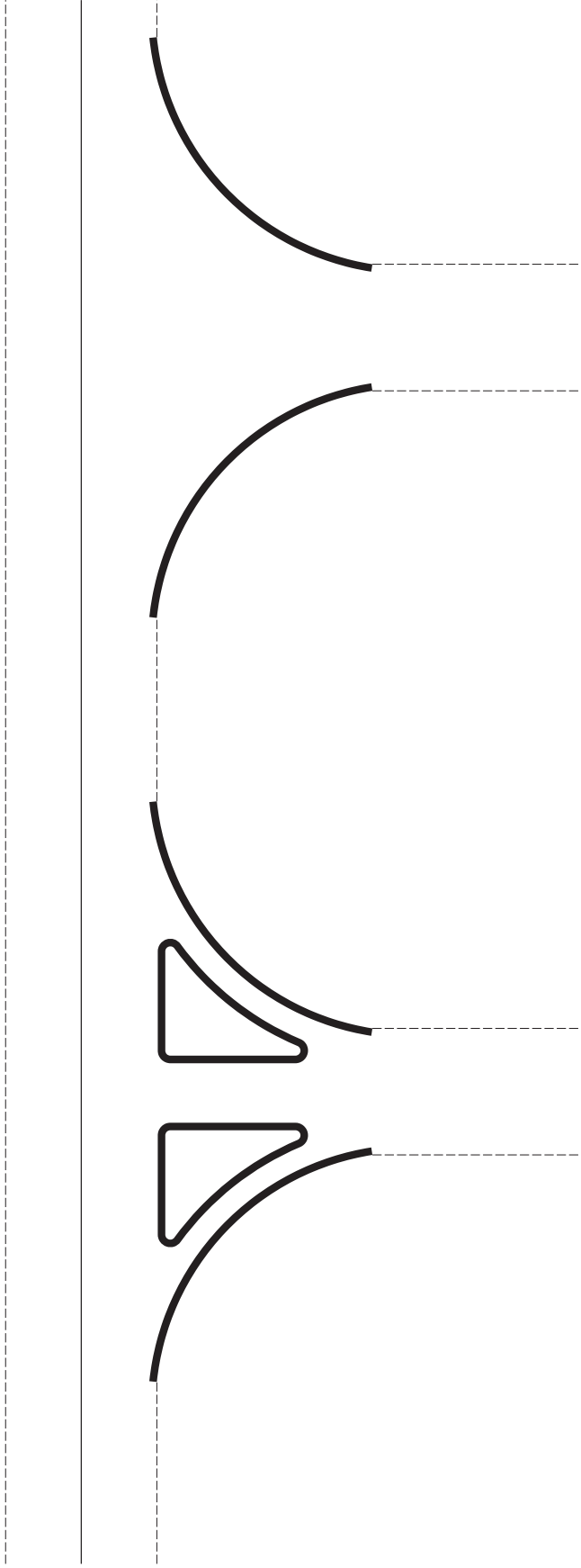
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
MDOT

| | |
|--------------|--|
| DESIGNED BY: | |
| DETAILED BY: | |
| CHECKED BY: | |
| DATE: | |

| |
|------------------------------|
| FMS CON: 108976/301000 |
| PROJECT NO.: STBG-0014-00027 |
| COUNTY: PIKE |

DETAIL FOR
 0-4
 Notice to Bidders No. 7473
 AND REVISIONS

| |
|-------------------|
| WATERLOO DISTRICT |
| SHEET NO. 5 |



** ALL ASPHALT AND CONCRETE CURBS ALONG RAMPS, LOCAL ROADS, ETC. FROM B.O.P. TO E.O.P. SHALL BE PAINTED (TWO APPLICATIONS) WITH TRAFFIC PAINT AND TRAFFIC BEADS; COLOR TO CORRESPOND TO ADJACENT PAVEMENT STRIPE; COST TO BE ABSORBED IN OTHER PAY ITEMS.

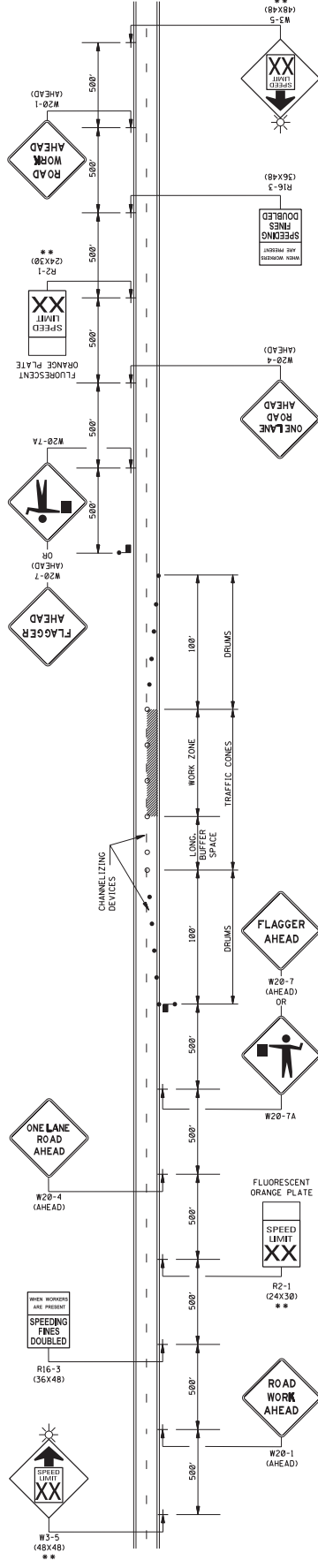


MISSISSIPPI DEPARTMENT OF TRANSPORTATION
MDOT

DESIGNED BY:
CHECKED BY:
DATE:

FMS CON: 108976/301000
PROJECT NO.: STBG-0014-0172
COUNTY: PIKE

TRAFFIC CONTROL PLAN WITH FLAGGER
ONE LANE ROAD WITH FLAGGER
WY: MO. SD: 16P-1
SHEET NO. 6



- LEGEND**
- FLAGGER
 - RETROREFLECTIVE FREE-STANDING PLASTIC DRUMS
 - TRAFFIC CONES (28" HEIGHT MINIMUM)
 - THE SPEED ON R2-1 AND W3-5 SIGNS SHALL BE 10 MPH LESS THAN THE POSTED SPEED LIMIT UNLESS OTHERWISE DIRECTED BY COMMISSION ORDER.

DISTANCE BETWEEN SIGNS

| ROAD TYPE | A | B | C |
|------------------------|----------|----------|----------|
| URBAN (35 MPH OR LESS) | 100 FT. | 100 FT. | 100 FT. |
| URBAN (40 - 70 MPH) | 350 FT. | 350 FT. | 350 FT. |
| RURAL | 500 FT. | 500 FT. | 500 FT. |
| EXPRESSWAY / FREEWAY | 1000 FT. | 1500 FT. | 2640 FT. |

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE. APPROACHING VEHICLES WILL HAVE SUFFICIENT DISTANCE TO STOP. VALUES IN STOPPING SIGHT DISTANCE COLUMN MAY BE USED AS A MINIMUM FOR THIS DISTANCE.
2. ALL CHANNELIZING DEVICES SHALL BE A MINIMUM OF 28" IN HEIGHT.
3. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 36" x 36" AND BLACK COPY ON FLUORESCENT ORANGE SHEETING.
4. WHEN WORK ZONE IS NO LONGER NEEDED, ALL SIGNS SHALL BE COVERED OR REMOVED AND ALL CHANNELIZING DEVICES SHALL BE MOVED TO THE SHOULDER EDGE.
5. ADDITIONAL FLAGGERS MAY BE NEEDED AS DIRECTED BY THE ENGINEER.
6. WHEN WORK IS REQUIRED AT NIGHT, FLAGGER STATIONS SHALL BE ILLUMINATED.
7. CHANNELIZING DEVICE TYPES FOR:
 - a. ALONG LANE LINE AND WORK ZONE - RETROREFLECTIVE PLASTIC DRUMS
 - b. ALONG LANE LINE AND WORK ZONE - TRAFFIC CONES (28" HEIGHT)
8. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

| POSTED SPEED AND/OR DESIGN SPEED (mph) | MAXIMUM CHANNELIZING DEVICES ALONG TAPER (ft) | | LONGITUDINAL BUFFER SPACE (ft) | STOPPING SIGHT DISTANCE |
|--|---|-----------|--------------------------------|-------------------------|
| | ALONG LANE LINE & WORK ZONE | WORK ZONE | | |
| 25 | 20 | 60 | 55 | 155 |
| 30 | 20 | 60 | 60 | 200 |
| 35 | 20 | 70 | 120 | 250 |
| 40 | 20 | 80 | 170 | 305 |
| 45 | 20 | 90 | 220 | 360 |
| 50 | 20 | 100 | 280 | 425 |
| 55 | 20 | 110 | 335 | 495 |
| 60 | 20 | 120 | 415 | 570 |
| 65 | 20 | 130 | 485 | 645 |

† NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE. APPROACHING VEHICLES WILL HAVE SUFFICIENT DISTANCE TO STOP. VALUES IN STOPPING SIGHT DISTANCE COLUMN MAY BE USED AS A MINIMUM FOR THIS DISTANCE.

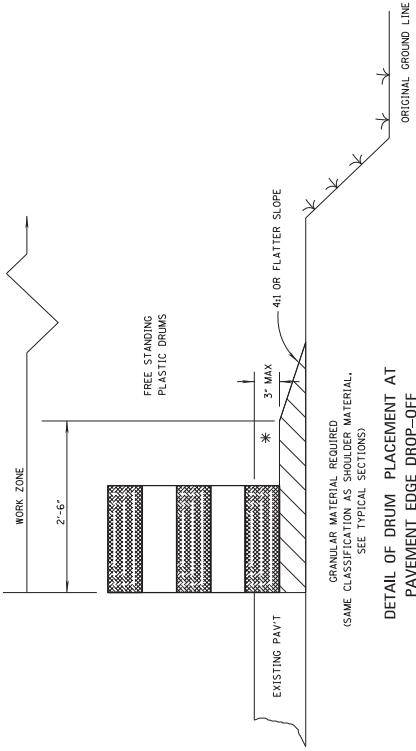


DESIGNED BY: _____
 CHECKED BY: _____
 DATE: _____

FMS CON: 108976/301000
 PROJECT NO.: STBG-0014-1(17)
 COUNTY: PIKE

TRAFFIC CONTROL DETAILS
 DRUM PLACEMENT AND SPACING GUIDELINES

WY 2016
 SBT 07.16
 SHEET NO. 7



DETAIL OF DRUM PLACEMENT AT PAVEMENT EDGE DROP-OFF
 GRANULAR MATERIAL REQUIRED (SAME CLASSIFICATION AS SHOULDER MATERIAL, SEE TYPICAL SECTIONS)

NOTES

- * A. PAVEMENT EDGE DROP-OFF
 1. IF LESS THAN TWO AND ONE QUARTER (2.25) INCHES-NO PROTECTION REQUIRED; PLACE A SHOULDER WORK SIGN (W21-5) 500 FEET IN ADVANCE OF WORK ZONE SHOULDER AND A LOW SHOULDER SIGN (W8-9) AT THE BEGINNING AND THROUGHOUT THE WORK ZONE @ (1 MILE ±0.C.).
 2. IF 2.25 INCHES OR GREATER, CONES OR WEDGES SHALL BE USED IN PLACE OF DRUMS, PANELS, AND BARRICADES DURING DAYLIGHT HOURS. FOR TANGENT SECTIONS WITH SPEEDS LESS THAN 50 MPH PER HOUR AND FOR CURVES, DEVICES SHOULD BE PLACED EVERY 50 FEET. SPACING FOR TAPERS SHOULD BE IN ACCORDANCE WITH THE MUTCD. 11.7.3 L, WHERE L IS THE TAPER LENGTH IN FEET.
 3. GREATER THAN THREE (3) INCHES-POSITIVE SEPARATION OR WEDGE WITH 4:1 OR FLATTER SLOPE NEEDED, IF THERE IS EIGHT (8) FEET OR MORE DISTANCE BETWEEN THE EDGE OF TRAVEL LANE AND DROP-OFF, THEN DRUMS, PANELS OR BARRICADES MAY BE USED.
 4. FOR TEMPORARY CONDITIONS, DROP-OFFS GREATER THAN THREE (3) INCHES MAY BE PROTECTED WITH DRUMS, VERTICAL PANELS OR BARRICADES FOR SHORT DISTANCES DURING DAYLIGHT HOURS WHILE WORK IS BEING DONE IN THE DROP-OFF AREA.
 5. LESSER TREATMENTS THAN THOSE DESCRIBED ABOVE MAY BE CONSIDERED FOR LOW-VOLUME LOCAL STREETS.

B. DRUM SPACING

- 1. TANGENTS = $2 \times S$
- 2. TAPERS = $L / 3$
- WHERE L = S X W
- S = SPEED IN MPH (POSTED OR 85 PERCENTILE)
- W = WIDTH OF OFFSET IN FEET

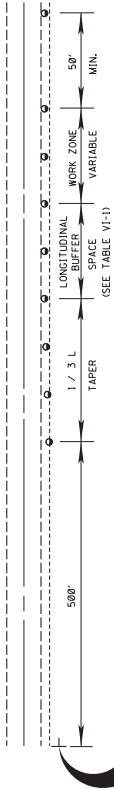
C. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.

TABLE V1-1 GUIDELINES FOR LENGTH OF LONGITUDINAL BUFFER SPACE

| POSTED SPEED (MPH) | LENGTH (FEET) |
|--------------------|---------------|
| 20 | 25 |
| 25 | 35 |
| 30 | 45 |
| 35 | 55 |
| 40 | 65 |
| 45 | 75 |
| 50 | 85 |
| 55 | 95 |
| 60 | 105 |
| 65 | 115 |
| 70 | 125 |
| 75 | 135 |
| 80 | 145 |
| 85 | 155 |

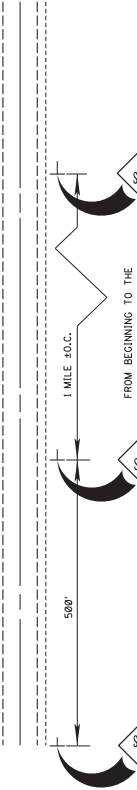
* * * POSTED SPEED, OFF-PEAK 85 PERCENTILE SPEED PRIOR TO WORK STARTING, OR THE ANTICIPATED OPERATING SPEED IN MPH.

PLASTIC DRUMS
 (SEE NOTE FOR SPACING)



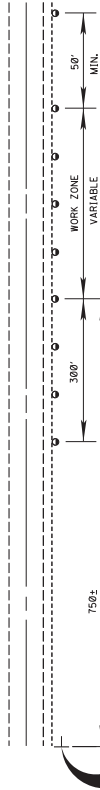
TYPICAL SHOULDER CLOSURE

- (1) TO BE USED WITH EIGHT (8) FOOT OR GREATER WIDTH IMPROVED SHOULDER.
- (2) TO BE USED WHEN CONSTRUCTION VEHICLES EQUIPMENT ENCROACHES ON OR WITHIN TWO (2) FEET OF THE SHOULDER BREAK.



TYPICAL SHOULDER WORK #1
 (SEE NOTE A-1 THIS SHEET)

PLASTIC DRUMS
 (SEE NOTE FOR SPACING)



TYPICAL SHOULDER WORK #2

NOTE:
 WORK OUTSIDE TWO (2) FOOT AND WITHIN TEN (10) FEET OF THE SHOULDER BREAK MAY BE PROTECTED BY PLACING DRUMS ALONG THE SHOULDER EDGE, 300 FEET PRIOR TO AND 50 FEET BEYOND THE WORK AREA, OR SEE NOTE A-3 THIS SHEET.



MISSISSIPPI DEPARTMENT OF TRANSPORTATION

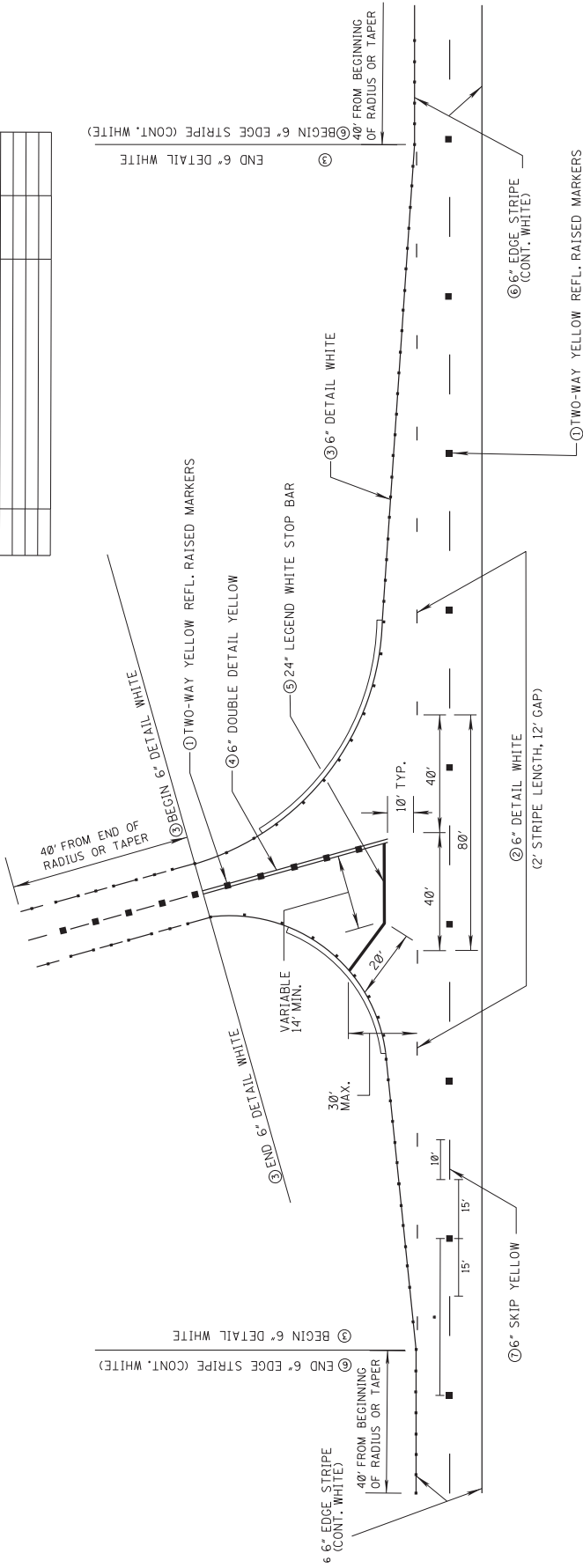
DESIGNED BY:
 CHECKED BY:
 DATE:

FMS CON: 108976/301000
 PROJECT NO.: STB8-0014-2(27)
 COUNTY: PIKE

DETAIL OF STRIPING
 2-LANE HIGHWAY
 7473

WY 2010
 PILES
 SHEET NO. 8

| PAVEMENT MARKING | | | |
|------------------|-------------------------------------|-----------|----------|
| SYMBOL | DESCRIPTION | PIPE FLAG | QUANTITY |
| ① | TWO-WAY YELLOW REFL. RAISED MARKERS | PLASTIC | |
| ② | 6" SKIP YELLOW (2'-12") | PLASTIC | |
| ③ | 6" DETAIL STRIPE WHITE (SOLID) | PLASTIC | |
| ④ | 6" DETAIL STRIPE YELLOW (SOLID) | PLASTIC | |
| ⑤ | 24" THERMO LEGEND WHITE (STOP BAR) | PLASTIC | |
| ⑥ | 6" CONTINUOUS EDGE WHITE (SOLID) | PLASTIC | |
| ⑦ | 6" TRAFFIC SKIP YELLOW (10'-30') | PLASTIC | |



GENERAL NOTES:
 • SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

| TANGENT SECTIONS | HORIZONTAL CURVES | INTERCHANGE LIMITS |
|------------------|-------------------|--------------------|
| 40' - 0" | 40' - 0" | 40' - 0" |
| 80' - 0" | 40' - 0" | 40' - 0" |
| 80' - 0" | 40' - 0" | 40' - 0" |



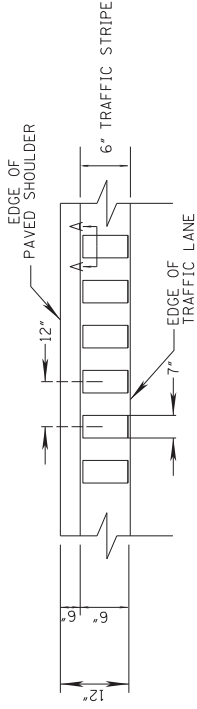
DESIGNED BY:
 DETAILED BY:
 CHECKED BY:
 DATE:

FMS CON: 307908/301000
 PROJECT NO.: MP-7042-20(01) - 13
 COUNTY: LAWRENCE

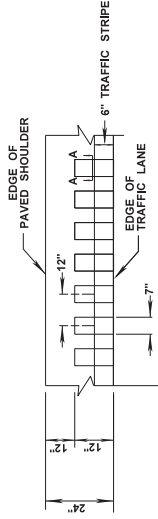
Notice to Bidders No. 7473 -- Cont.

WY: 2022
 R: 02
 SHEET NO. 9

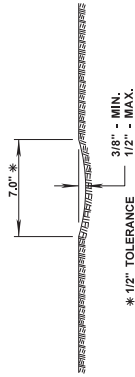
- GENERAL NOTES**
- GROUND-IN RUMBLE STRIPES SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT
 - GROUND-IN RUMBLE STRIPES SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
 - COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS
 - GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO MAINLINE ONLY



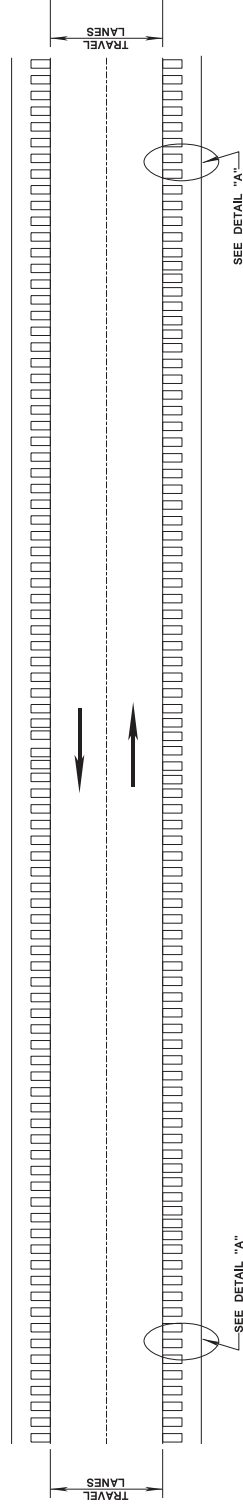
DETAIL "A"
12' PAVEMENT EDGE



DETAIL "B"
14' PAVEMENT EDGE



SECTION "A-A"



PLAN
 NOT TO SCALE



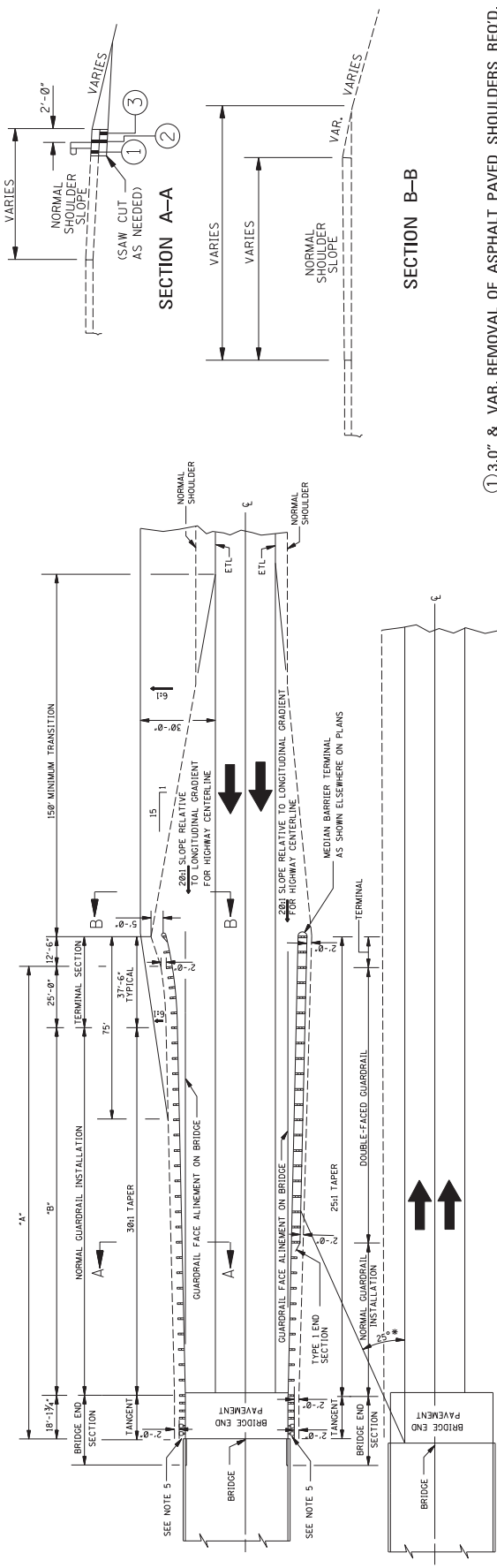
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

DESIGNED BY: DISTRICT 7
 CHECKED BY:
 DATE:

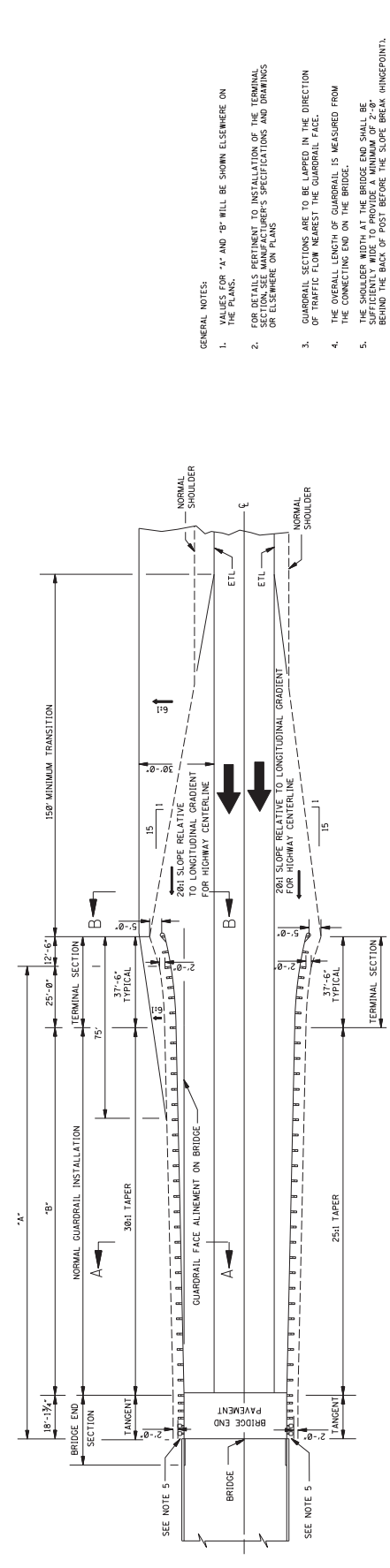
FMS CON: 108976/301000
 PROJECT NO.: STB-0014-1(17)
 COUNTY: PIKE

GUARRAIL TYPICAL INSTALLATION
 DIVIDED HIGHWAYS
 473 -- Co

SHEET ID: GR-41 MOD
 SHEET NO.: 10



PLAN SHEET



PLAN SHEET

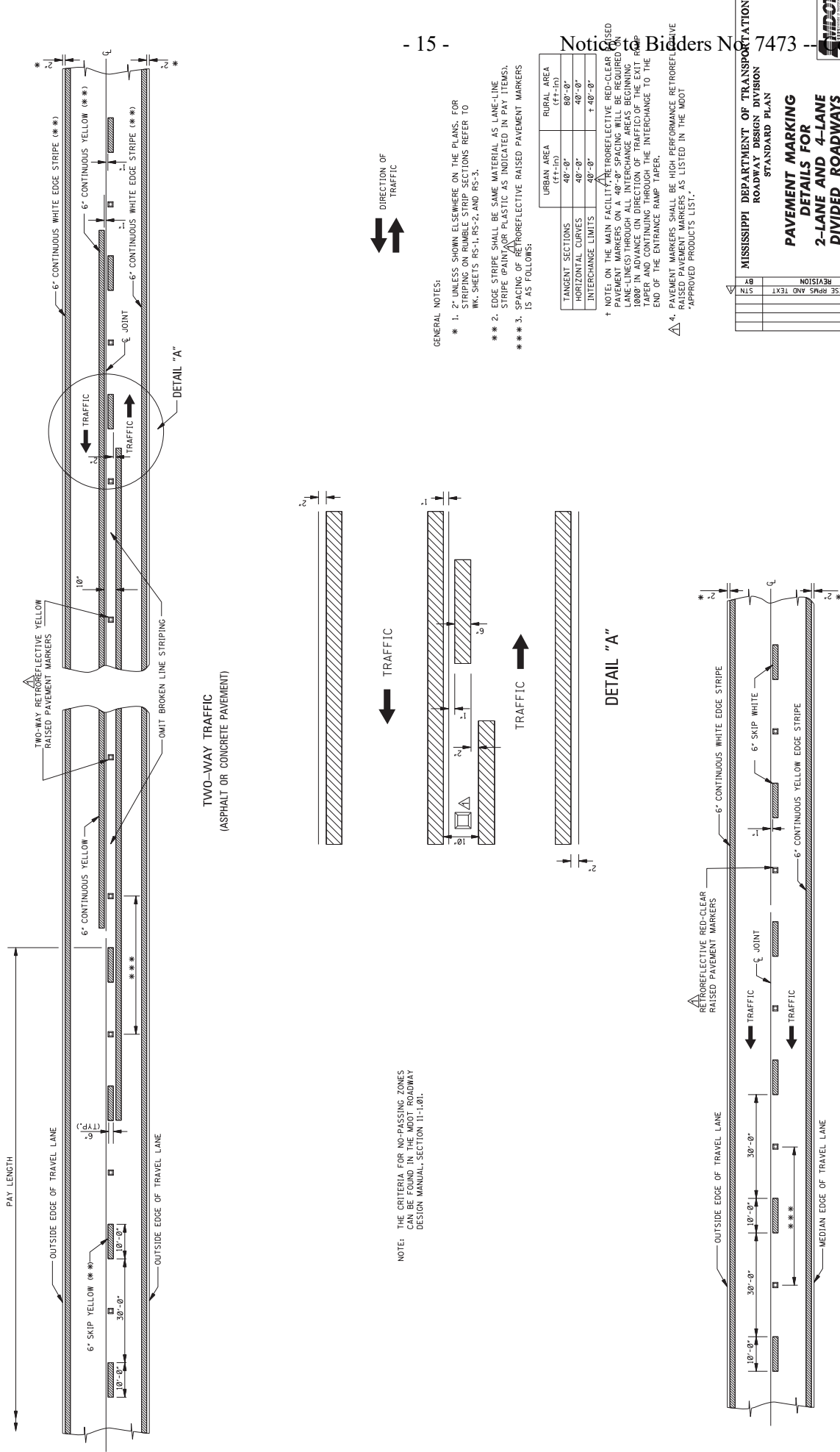
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SECTION A-A

SECTION B-B

- ① 3.0' & VAR. REMOVAL OF ASPHALT PAVED SHOULDERS REQ'D.
- ② 6.0' CRUSHED LIMESTONE REQ'D.
- ③ 3.0' & VAR. EXCESS EXCAVATION REQ'D.

- GENERAL NOTES:
- VALUES FOR "A" AND "B" WILL BE SHOWN ELSEWHERE ON THE PLANS.
 - FOR DETAILS PERTAINING TO INSTALLATION OF THE TERMINAL SECTION, SEE MANUFACTURER'S SPECIFICATIONS AND DRAWINGS OR ELSEWHERE ON PLANS.
 - GUARRAIL SECTIONS ARE TO BE LAPPED IN THE DIRECTION OF TRAFFIC FLOW NEAREST THE GUARRAIL FACE.
 - THE OVERALL LENGTH OF GUARRAIL IS MEASURED FROM THE CONNECTING END ON THE BRIDGE.
 - THE SHOULDER WIDTH AT THE BRIDGE END SHALL BE SUFFICIENTLY WIDE TO PROVIDE A MINIMUM OF 2'-0" BEHIND THE BACK OF POST BEFORE THE SLOPE BREAK (HINGEPOINT).
 - TYPE, DETAILS AND LIMITS OF GUARRAIL BRIDGE END SECTION WILL BE SHOWN ELSEWHERE ON THE PLANS.



NOTE: THE CRITERIA FOR NO-PASSING ZONES CAN BE FOUND IN THE MOOT ROADWAY DESIGN MANUAL, SECTION 11-1.01.



DETAIL "A"

GENERAL NOTES:

- * 1. UNLESS SHOWN ELSEWHERE ON THE PLANS, FOR STRIPING ON RUMBLE STRIP SECTIONS REFER TO WK. SHEETS RS-1, RS-2, AND RS-3.
- ** 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR PLASTIC AS INDICATED IN PAY ITEMS).
- *** 3. SPACING OF RETROREFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

| TANGENT SECTIONS | RURAL AREA (R-1, R-2) | URBAN AREA (U-1, U-2) |
|--------------------|-----------------------|-----------------------|
| HORIZONTAL CURVES | 40'-0" | 40'-0" |
| INTERCHANGE LIMITS | 40'-0" | 1-40'-0" |

- 1. NOTE: ON THE MAIN FACILITY, RETROREFLECTIVE RED-CLEAR PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON ALL INTERCHANGES THROUGH THE INTERCHANGE. RETROREFLECTIVE RED-CLEAR PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON ALL INTERCHANGES THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
- 2. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RETROREFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MOOT "APPROVED PRODUCTS LIST."

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**PAVEMENT MARKING
DETAILS FOR
2-LANE AND 4-LANE
DIVIDED ROADWAYS**

ISSUE DATE: AUGUST 01, 2017

PROJECT NUMBER: 6051

DATE: 08/01/17

BY: []

REVISION: []

DATE: []

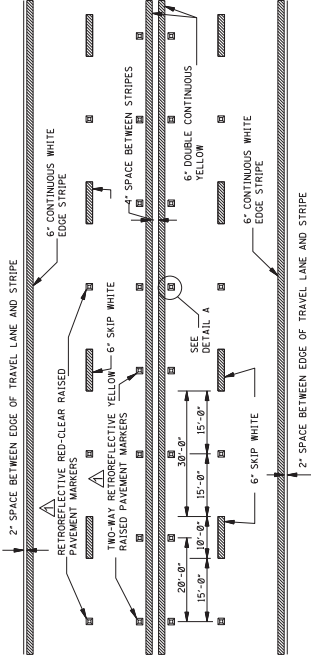
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REVISION: []

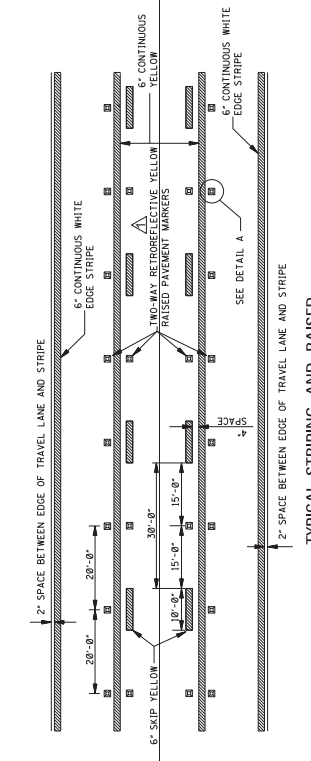
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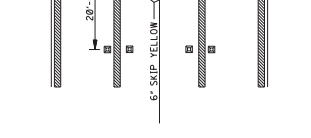
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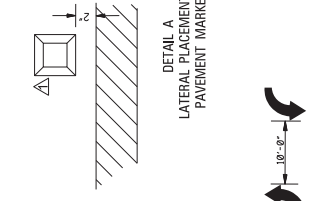
TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 3-LANE SECTION



TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 4-LANE SECTION



TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 5-LANE SECTION

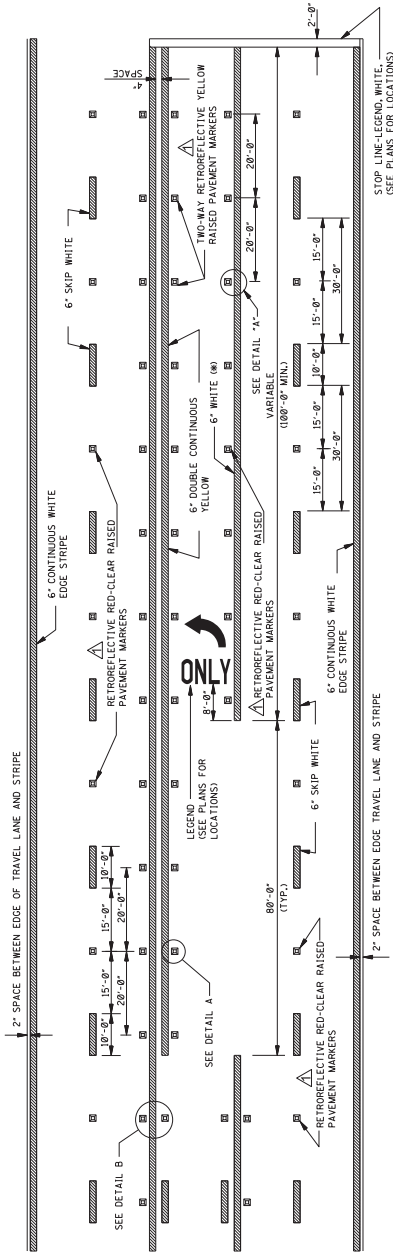


DETAIL A
LATERAL PLACEMENT OF PAVEMENT MARKERS



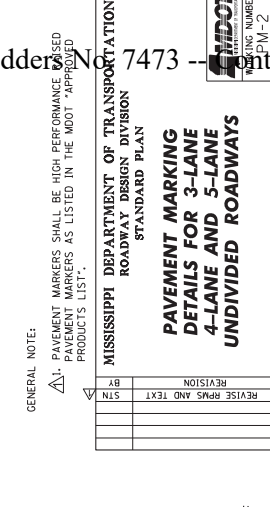
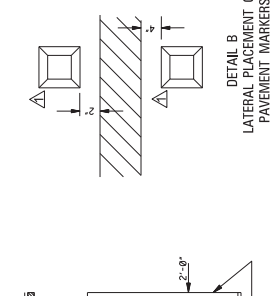
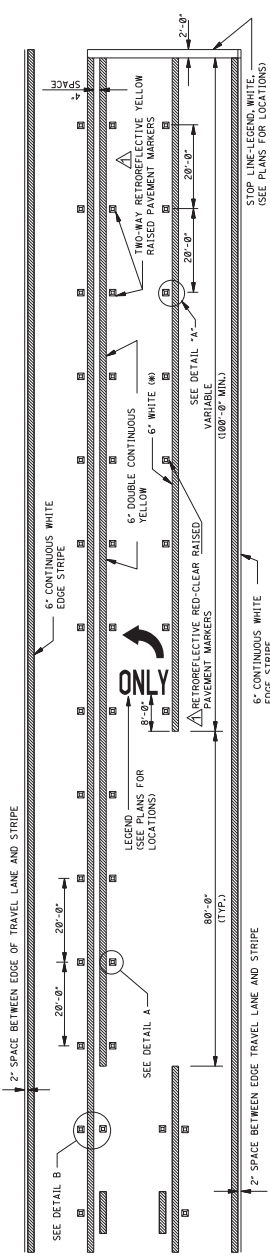
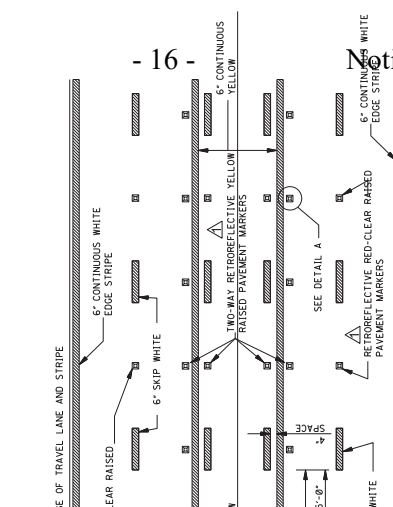
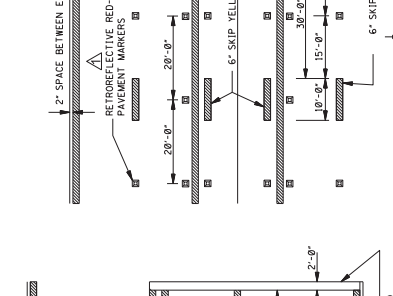
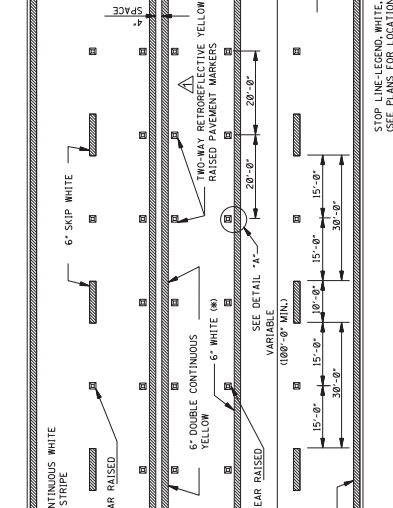
TYPICAL TWO-WAY ARROW INSTALLATION

NOTES: 1. CONSIDER EACH SEGMENT OF CONTINUOUS TWO-WAY LEFT TURN LANE SEPARATELY. 2. IF SEGMENT IS GREATER THAN 350', PLACE FIRST SET OF ARROWS 50' TO 100' FROM BEGINNING AND/OR END OF SEGMENT AND SPACE ADDITIONAL SETS OF ARROWS (250' O.C.).



TYPICAL STRIPING AND RAISED PAVEMENT MARKERS AT LEFT TURN LANES

* NOTE: USE DETAIL STRIPING IF LENGTH < 150' AT THIS LOCATION, OTHERWISE USE CONTINUOUS STRIPING.



DETAIL B
LATERAL PLACEMENT OF PAVEMENT MARKERS

GENERAL NOTE:
▲ 1. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE PAVEMENT MARKERS AS LISTED IN THE MOOT *APPROVED PRODUCTS LIST*.

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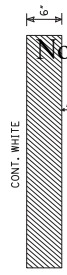
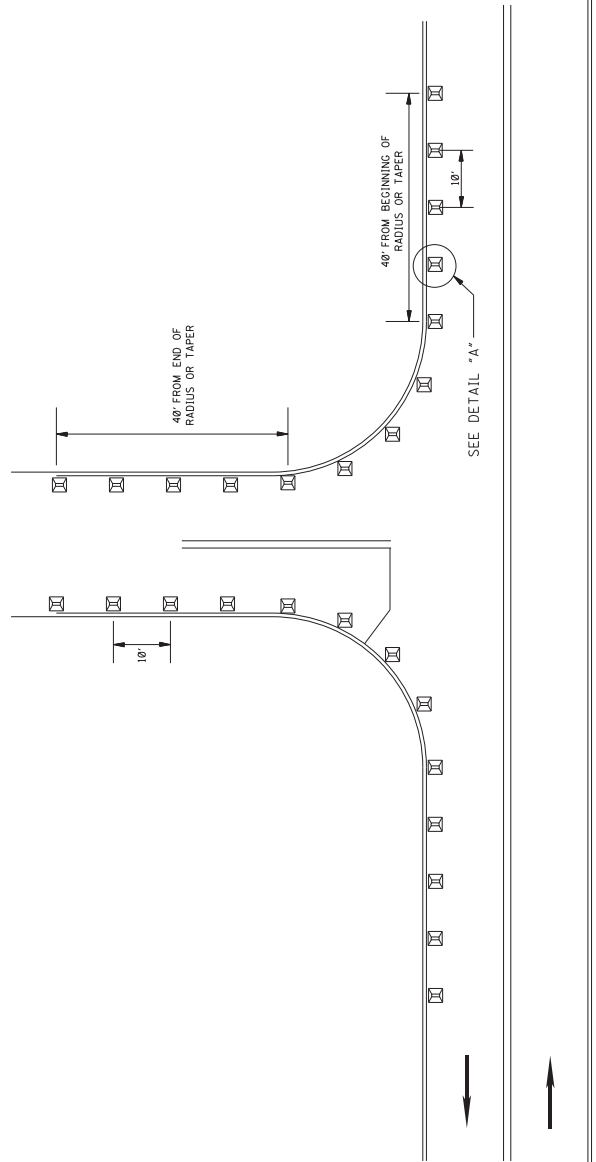
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MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN
**PAVEMENT MARKING
DETAILS FOR 3-LANE
4-LANE AND 5-LANE
UNDIVIDED ROADWAYS**

ISSUE DATE: AUGUST 01, 2017
DRAWING NUMBER: PM-2
PROJECT NUMBER: 6052

△ TYPICAL PLACEMENT OF RETROREFLECTIVE RAISED PAVEMENT MARKERS ON SIDE ROAD RADIUS 2-LANE, TWO WAY TRAFFIC



GENERAL NOTES:

- △ 1. RETROREFLECTIVE FACES OF MARKERS SHALL BE VISIBLE TO THE TRAVELING MOTORIST ON STATE DESIGNATED HIGHWAYS.
- △ 2. RETROREFLECTIVE FACES OF MARKERS SHALL BE HIGH PERFORMANCE TWO-WAY CLEAR.
- △ 3. RETROREFLECTIVE FACES OF MARKERS SHALL NOT BE ROTATED WHEN BEING PLACED ALONG RADIUS AND TANGENT SECTION OF LOCAL ROAD.
- △ 4. MARKERS SHALL BE INSTALLED TO THE LIMITS SHOWN ABOVE.

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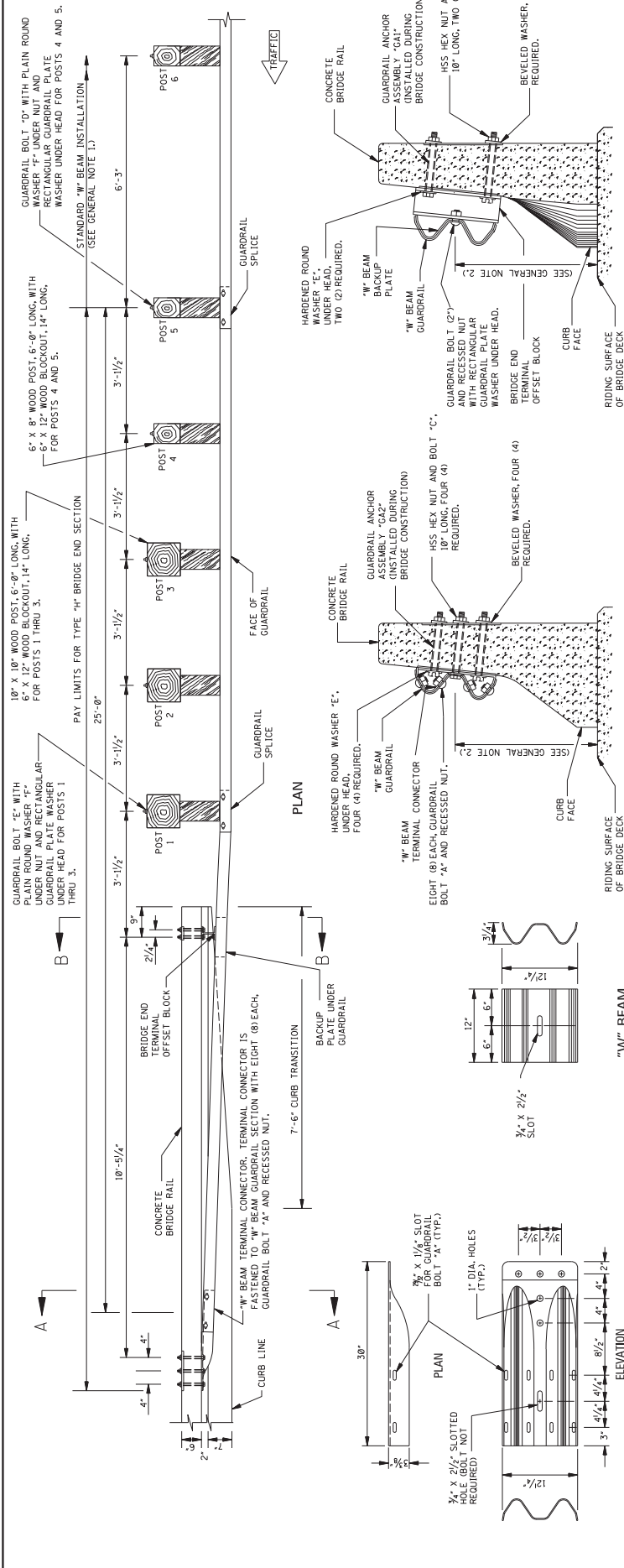
ISSUE DATE: AUGUST 01, 2017

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

2-WAY RETROREFLECTIVE RAISED PAVEMENT MARKERS AT INTERSECTING ROADS (2-LANE)

PLAN NUMBER: 7473-11
SHEET NUMBER: 6061

**GUARDRAIL:
BRIDGE END SECTION
TYPE "H"
(WOOD POSTS)**

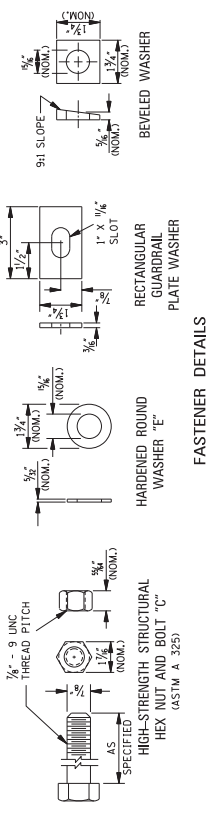


SECTION B-B

SECTION A-A

GENERAL NOTES:

1. ALL NOTES AND DETAILS PERTAINING TO NORMAL "W" BEAM GUARDRAIL INSTALLATION NOT SPECIFICALLY MODIFIED ON THIS SHEET WILL BE FOUND ON SHEET GR-1 (WOOD POSTS).
2. GUARDRAIL END IS 21' AND WILL BE TRANSITIONED TO 25' AT POST 5.
3. GUARDRAIL ELEMENTS SHALL BE LAPPED IN THE DIRECTION OF TRAFFIC. THE ONLY EXCEPTION NOTED IS THAT GUARDRAIL SHALL BE LAPPED FOR APPROACHING TRAFFIC ON A BRIDGE WITH 2-WAY TRAFFIC.



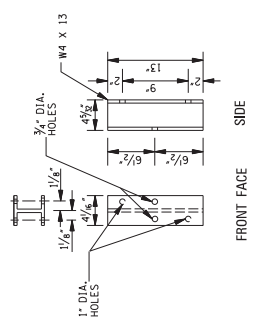
FASTENER DETAILS

"W" BEAM BACKUP PLATE

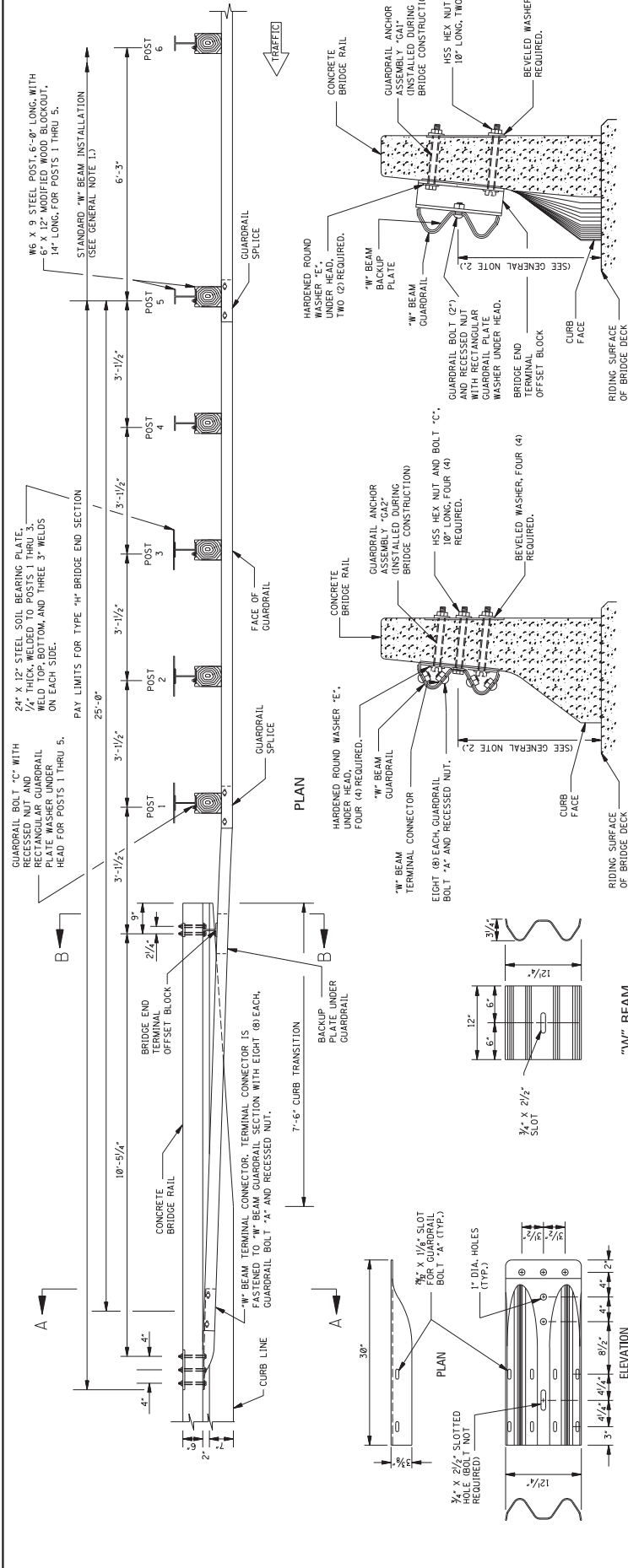
NOTE: THE "W" BEAM BACKUP PLATE IS USED UNDER THE "W" BEAM RAIL SECTION WHERE INDICATED ON THIS SHEET. THE CROSS-SECTIONAL DIMENSIONS OF THIS PART ARE IDENTICAL TO THOSE OF THE STANDARD "W" BEAM RAIL SECTION.

"W" BEAM TERMINAL CONNECTOR

NOTE: THE "W" BEAM TERMINAL CONNECTOR IS USED WITH THE TYPE "H" BRIDGE END SECTION. THE CROSS-SECTIONAL DIMENSIONS OF THIS PART ARE IDENTICAL TO THE STANDARD "W" BEAM SECTION GA8570 M 180 CLASS B, TYPE 11A.



FRONT FACE
SIDE
DETAIL OF BRIDGE END
TERMINAL OFFSET BLOCK



SECTION B-B

SECTION A-A

"W" BEAM BACKUP PLATE

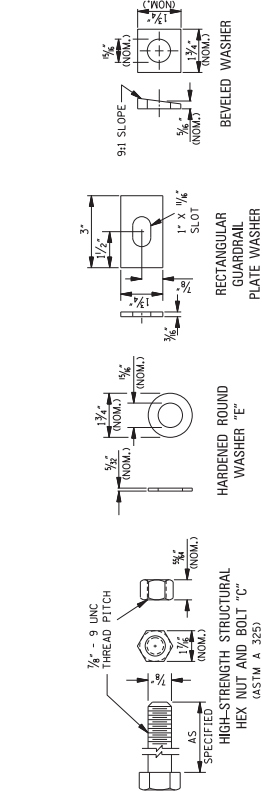
"W" BEAM TERMINAL CONNECTOR

GENERAL NOTES:

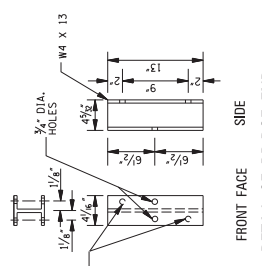
1. ALL NOTES AND DETAILS PERTAINING TO NORMAL "W" BEAM GUARDRAIL INSTALLATION NOT SPECIFICALLY MODIFIED ON THIS SHEET WILL BE FOUND ON SHEET CR-1B (STEEL POSTS).
2. GUARDRAIL END IS 21' AND WILL BE TRANSITIONED TO 25' AT POST 5.
3. GUARDRAIL ELEMENTS SHALL BE LAPPED IN THE DIRECTION OF TRAFFIC. THE ONLY EXCEPTION NOTED IS THAT GUARDRAIL SHALL BE LAPPED FOR APPROACHING TRAFFIC ON A BRIDGE WITH 2-WAY TRAFFIC.

NOTE: THE "W" BEAM BACKUP PLATE IS USED UNDER THE "W" BEAM RAIL SECTION WHERE INDICATED ON THIS SHEET. THE CROSS-SECTIONAL DIMENSIONS OF THIS PART ARE IDENTICAL TO THOSE OF THE STANDARD "W" BEAM RAIL SECTION.

NOTE: THE "W" BEAM TERMINAL CONNECTOR IS USED WITH THE TYPE "H" BRIDGE END SECTION. THE CROSS-SECTIONAL DIMENSIONS OF THIS PART ARE IDENTICAL TO THE STANDARD "W" BEAM SECTION GA8SH10 M 180 CLASS B, TYPE 11A.



FASTENER DETAILS



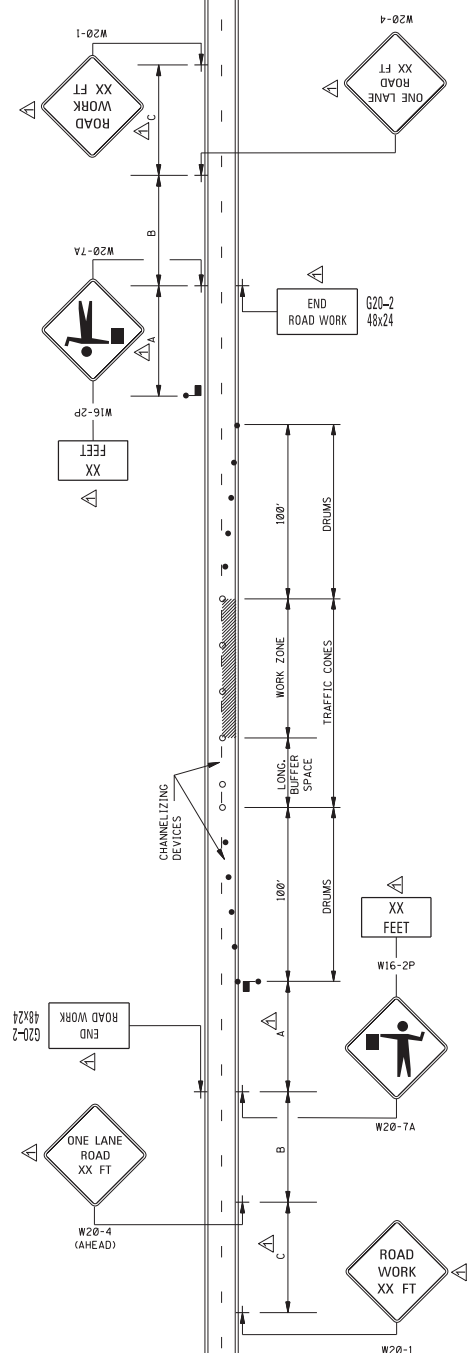
DETAIL OF BRIDGE END TERMINAL OFFSET BLOCK

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**GUARDRAIL:
BRIDGE END SECTION
TYPE "H"
(STEEL POSTS)**

| DATE | REVISION |
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ISSUE DATE: AUGUST 01, 2017
DRAWING NUMBER: CR-2D
SHEET NUMBER: 6208



LEGEND
 FLAGGER
 RETROREFLECTIVE FREE-STANDING PLASTIC DRUMS
 TRAFFIC CONES (28" HEIGHT MINIMUM)

DISTANCE BETWEEN SIGNS

| ROAD TYPE | A | B | C |
|------------------------|----------|----------|----------|
| URBAN (35 MPH OR LESS) | 100 FT. | 100 FT. | 100 FT. |
| URBAN (40 - 70 MPH) | 350 FT. | 350 FT. | 350 FT. |
| RURAL | 500 FT. | 500 FT. | 500 FT. |
| EXPRESSWAY / FREEWAY | 1000 FT. | 1500 FT. | 2640 FT. |

- GENERAL NOTES:**
1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE. FLAGGER STATIONS SHALL BE LOCATED SUCH THAT APPROACH AND EXIT TAPERS ARE NOT NEARER TO THE STOPPING SPACE THAN THE STOPPING DISTANCE COLUMN MAY BE USED AS A MINIMUM FOR THIS DISTANCE.
 2. ALL CHANNELIZING DEVICES SHALL BE A MINIMUM OF 28" IN HEIGHT.
 3. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 36" x 36" AND BLACK COPY ON FLUORESCENT ORANGE SHEETING.
 4. WHEN WORK ZONE IS NO LONGER NEEDED, ALL SIGNS SHALL BE COVERED OR REMOVED AND ALL CHANNELIZING DEVICES SHALL BE MOVED TO THE SHOULDER EDGE.
 5. ADDITIONAL FLAGGERS MAY BE NEEDED AS DIRECTED BY THE ENGINEER.
 6. WHEN WORK IS REQUIRED AT NIGHT, FLAGGER STATIONS SHALL BE ILLUMINATED EXCEPT IN EMERGENCIES.
 7. CHANNELIZING DEVICE TYPES FOR APPROACH AND EXIT TAPERS:
 - a. ALONG LANE LINE AND WORK ZONE - TRAFFIC CONES (28" HEIGHT)
 - b. ALONG LANE LINE AND WORK ZONE - TRAFFIC CONES (28" HEIGHT)
 8. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
 9. AN OPTIONAL FLAGGER (W20-7A) WORD MESSAGE IS SHOWN IN THE "STANDARD HIGHWAY SIGNS" PUBLICATION.

| POSTED SPEED AND/OR DESIGN SPEED AND/OR ANTICIPATED OPERATING SPEED mph | MAXIMUM CHANNELIZING DEVICES (ft) | | LONGITUDINAL BUFFER SPACE (ft) | STOPPING SIGHT DISTANCE |
|--|-----------------------------------|-----------------------------|--------------------------------|-------------------------|
| | TAPER | ALONG LANE LINE & WORK ZONE | | |
| 25 | 20 | 50 | 55 | 155 |
| 30 | 20 | 60 | 85 | 200 |
| 35 | 20 | 70 | 120 | 250 |
| 40 | 20 | 80 | 170 | 305 |
| 45 | 20 | 90 | 220 | 360 |
| 50 | 20 | 100 | 280 | 425 |
| 55 | 20 | 110 | 335 | 495 |
| 60 | 20 | 120 | 415 | 570 |
| 65 | 20 | 130 | 485 | 645 |

* NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 ROADWAY DESIGN DIVISION
 STANDARD PLAN

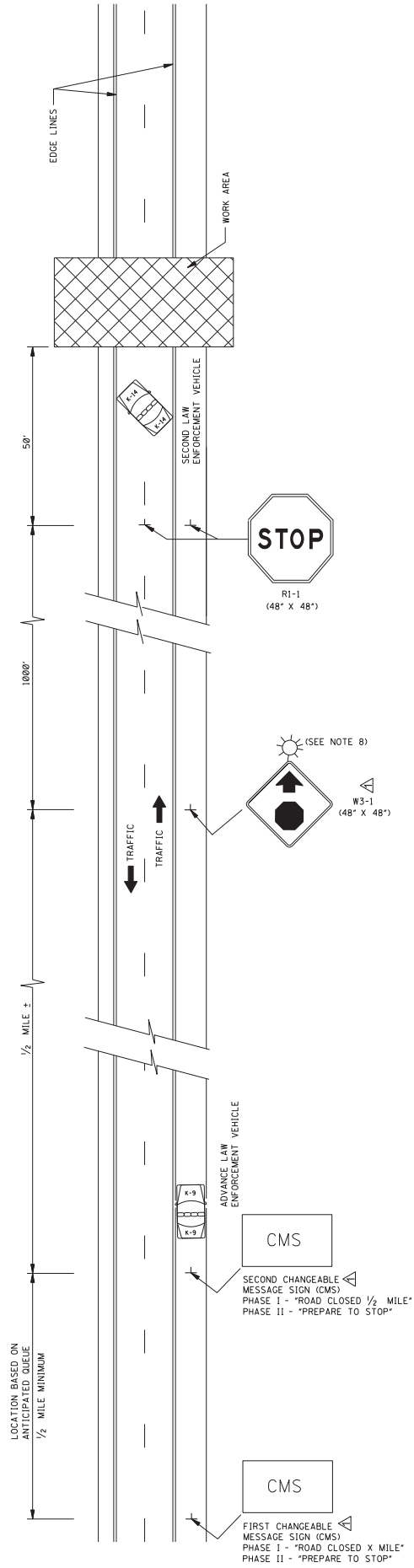
**TRAFFIC CONTROL PLAN
 WITH FLAGGER
 (ONE-LANE CLOSURE OF
 TWO-WAY TRAFFIC)**

ISSUE DATE: AUGUST 01, 2017
 SHEET NUMBER: 6351

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STATE PROJECT NO.
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GENERAL NOTES:

1. THIS TYPE OF HIGHWAY CLOSURE SHOULD ONLY BE USED FOR CONSTRUCTION OPERATIONS THAT WILL BE COMPLETED WITHIN 30 MINUTES. AFTER THE HIGHWAY HAS BEEN CLOSED AND REOPENED VIA THE CONTRACT DOCUMENTS, THE CONTRACTOR SHALL REMOVE THE CLOSURE AND REOPEN THE HIGHWAY WITHIN 30 MINUTES. THE CONTRACTOR SHALL PROVIDE ANOTHER SHORT DURATION CLOSURE, EXCEPT WITH THE APPROVAL OF THE ENGINEER.
2. AT LEAST TWO LAW ENFORCEMENT OFFICERS AND TWO LAW ENFORCEMENT VEHICLES SHOULD BE PROVIDED ON EACH APPROACH TO THE CLOSURE. EACH LAW ENFORCEMENT VEHICLE SHOULD HAVE A PROTRUDING OPERATING FLASHING BLUE LIGHT ON LIGHT BAR.
3. RESTRICTIONS ON ROAD CLOSURES ARE SPECIFIED IN THE CONTRACT DOCUMENT.
4. THE ADVANCE LAW ENFORCEMENT VEHICLE SHOULD BE MOVED BACK AS REQUIRED BY THE QUEUING OF STOPPED VEHICLES.
5. IF QUEUE EXCEEDS THE FIRST CHANGEABLE MESSAGE SIGN (CMS) AT ANY TIME DURING A CLOSURE, THE TRAFFIC CONTROL PLAN SHOULD BE ADJUSTED AS NECESSARY, WITH APPROVAL OF THE ENGINEER.
6. TRAFFIC CONTROL FOR THE CLOSURE SHOULD BE ACCOMPLISHED IN THE FOLLOWING ORDER:
 - A. FIRST CHANGEABLE MESSAGE SIGN (CMS)
 - B. SECOND CHANGEABLE MESSAGE SIGN (CMS)
 - C. ADVANCE LAW ENFORCEMENT VEHICLE, LIGHTS AND FLASHERS ON.
 - D. "W3-1 (48" X 48") (ORANGE/BLACK TYPE XI RETROREFLECTIVE SHEETING) SIGNS ERRECTED.
 - E. "R1-1 (48" X 48") SIGNS ERRECTED OR INSTALLED ON TEMPORARY STANDS TO STOP TRAFFIC. THE ORDER OF ERECTION SHOULD BE IN THE FOLLOWING ORDER: RIGHT SHOULDER THEN CENTER, AND FLASHERS ON.
 - F. SECOND LAW ENFORCEMENT VEHICLE, LIGHTS AND FLASHERS ON.
7. TRAFFIC CONTROL SHOULD BE REMOVED IN THE FOLLOWING ORDER:
 - A. WITH TRAFFIC STOPPED REMOVE THE "R1-1 SHOULDER THEN SIGN ON THE RIGHT CENTER THEN SIGN ON THE RIGHT SHOULDER. SECOND LAW ENFORCEMENT VEHICLE LEADS TRAFFIC THROUGH WORK AREA.
 - B. AFTER ALL STOPPED VEHICLES HAVE STARTED MOVING THE "W3-1 (48" X 48") SIGNS SHALL BE REMOVED. THESE SIGNS MAY BE COVERED IF RE-USE IS IMMINENT.
 - C. AFTER ALL VEHICLES HAVE RESUMED APPROXIMATELY NORMAL SPEED, THE CHANGEABLE MESSAGE SIGNS SHALL BE TURNED OFF.
8. UNILLUMINATED SECTIONS OF HIGHWAYS SHOULD NOT BE CLOSED DURING HOURS OF DARKNESS EXCEPT FOR EMERGENCIES OR WITH THE APPROVAL OF THE ENGINEER. IF DARKNESS OCCURS DURING HOURS OF DARKNESS, A TYPE B HIGH INTENSITY FLASHING BARRICADE WARNING LIGHT SHALL BE USED ON EACH W3-1 SIGN.
9. IF AN ENTRANCE RAMP IS LOCATED BETWEEN THE SECOND CMS AND R1-1, THE CMS AND W3-1 SIGNS SHALL ALSO BE ERRECTED ON THE RAMP SHOULDER.
10. THE ABOVE DURATION WILL APPLY TO EACH APPROACH TO THE CLOSURE.
11. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC, INCLUDING SECURING LAW ENFORCEMENT SERVICES.

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ISSUE DATE: AUGUST 01, 2017

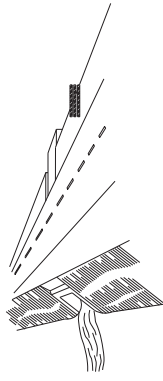
PROJECT NUMBER: 6356

PLAN NUMBER: 1

REVISION NUMBER: LCP-6

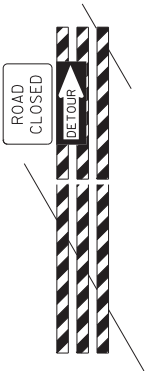
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**SHORT DURATION
CLOSING OF TWO-LANE
TWO-WAY HIGHWAYS**



WING BARRICADES

1. WING BARRICADES ARE TYPE III BARRICADES ERECTED ON THE SHOULDER OF A ROAD TO PROTECT THE SHOULDER FROM THE SPILLING OF A NARROWING OR RESTRICTED ROADWAY. WING BARRICADES MAY BE USED AS A MOUNTING FOR THE ADVANCE WARNING SIGNS OR FLASHERS.
 - A. IN ADVANCE OF A CONSTRUCTION PROJECT EVEN WHEN NO PART OF THE ROADWAY IS ACTUALLY CLOSED.
 - B. IN ADVANCE OF ALL BRIDGE OR CULVERT WIDENING OPERATIONS.
2. WING BARRICADES SHOULD BE USED:
 - A. IN ADVANCE OF A CONSTRUCTION PROJECT EVEN WHEN NO PART OF THE ROADWAY IS ACTUALLY CLOSED.
 - B. IN ADVANCE OF ALL BRIDGE OR CULVERT WIDENING OPERATIONS.

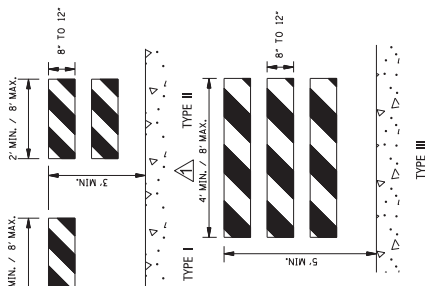


BARRICADE CLOSING A ROAD

BARRICADE CHARACTERISTICS

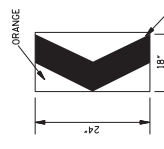
| | I | II | III |
|---|------------------------|------------------------|--|
| WIDTH OF RAIL ** | 8" MIN. - 12" MAX. | 8" MIN. - 12" MAX. | 8" MIN. - 12" MAX. |
| LENGTH OF RAIL ** | 24" MIN. / 95" MAX. | 24" MIN. / 95" MAX. | 48" MIN. / 95" MAX. |
| WIDTH OF STRIPE * | 6" | 6" | 6" |
| HEIGHT | 36" MIN. | 36" MIN. | 60" MIN. |
| NUMBER OF RETROREFLECTORIZED RAIL FACES | 2 (ONE EACH DIRECTION) | 4 (TWO EACH DIRECTION) | 3 IF FACING TRAFFIC IN TWO DIRECTIONS 6 IF FACING TRAFFIC IN TWO DIRECTIONS |

- ** 1. FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.
- ** 2. BARRICADES INTENDED FOR USE ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH SPEED FACILITIES SHALL HAVE A MINIMUM OF 210 IP OF REFLECTIVE AREA FACING TRAFFIC.



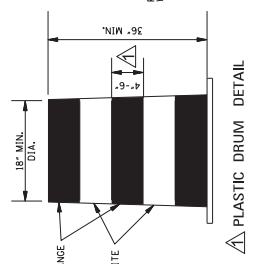
STANDARD BARRICADES

1. THE RETROREFLECTIVE MARKING FOR BARRICADE RAILS SHALL BE ORANGE AND WHITE (SLOPING DOWNWARD AT AN ANGLE OF 45° IN THE DIRECTION THAT TRAFFIC IS TO PASS).
2. RAIL STRIPE SHOULD BE 6 INCHES, EXCEPT THAT 4-INCH WIDE STRIPES MAY BE USED IF RAIL LENGTHS ARE LESS THAN 36 INCHES.
3. DO NOT PLACE SANDBARS OR OTHER DEVICES ON BARRICADE RAILS TO PROVIDE MASS SLOWDOWN. SANDBARS MAY BE PLACED AS BALLAST TO THE LOWER PARTS OF THE FRAME OR THE STAYS TO BARRICADES.
4. FOR ADDITIONAL INFORMATION OR DETAILS, SEE MUTCD, LATEST EDITION.
5. BARRICADES ARE CLASSIFIED BY FHWA AS CATEGORY II WORK ZONE DEVICES. A LIST OF CRASHWORTHY BARRICADES AND OTHER CATEGORY II DEVICES CAN BE FOUND ON FHWA'S WEBSITE: <http://highways.dot.gov/safety/fhwadot/road-reduce-crash-severity>
6. WHERE ROAD USERS INCLUDE PEDESTRIANS, THE PROVISION OF SUPPLEMENTAL AUDIBLE INFORMATION OR DETECTABLE BARRIERS OR BARRICADES SHOULD BE PROVIDED FOR PEOPLE WITH VISION DISABILITIES.
7. BARRICADE RAIL SUPPORTS SHOULD NOT PROJECT INTO PEDESTRIAN CIRCULATION ROUTES MORE THAN 4 INCHES FOR THE SUPPORTS LOCATED BETWEEN 27 INCHES TO 88 INCHES ABOVE THE EXISTING SURFACE.

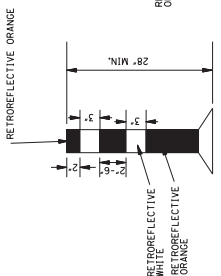


RETROREFLECTIVE CHEVRON ALIGNMENT SIGN DETAIL (W1-8)

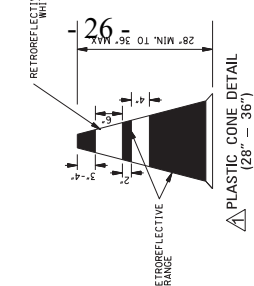
1. CHEVRON SIGN CONSISTS OF A BLACK CHEVRON TYPE MARKING ON AN ORANGE BACKGROUND AND SHALL POINT IN THE DIRECTION OF TRAFFIC FLOW.
2. THE CHEVRON SIGN SHALL BE MOUNTED ON CRASHWORTHY SUPPORT.
3. CHEVRON SIGNS MAY BE USED TO SUPPLEMENT OTHER STANDARD DEVICES WHERE ONE OR MORE LANES ARE CLOSED FOR CONSTRUCTION OR MAINTENANCE. THEY SHOULD BE PLACED APPROXIMATELY 2'-0" BEHIND THE LANE TRANSITION STRIPE.
4. CHEVRON SIGNS SHALL BE INSTALLED AT A MINIMUM HEIGHT OF 4 FEET MEASURED VERTICALLY FROM THE BOTTOM OF THE SIGN TO THE ELEVATION OF THE NEAR EDGE OF THE TRAVELWAY.



PLASTIC DRUM DETAIL (36" OR GREATER)



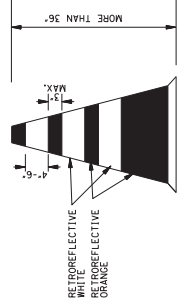
TUBULAR MARKER DETAIL



PLASTIC CONE DETAIL (28" - 36")

TYPE 3 OBJECT MARKER (OM3-R)

1. TYPE 3 OBJECT MARKERS SHALL BE USED AT ALL EXPOSED BRIDGE ABUTMENTS AND AT OTHER LOCATIONS AS DEEMED NECESSARY BY THE ENGINEER.
2. THE OM3-R IS SIMILAR EXCEPT THE STRIPES ARE PLACED ON THE LOWER RISE SIDE AND SHALL BE PLACED ON THE LEFT SIDE OF THE OBJECT.
3. THE INSIDE EDGE OF THE MARKER SHALL BE IN LINE WITH THE INNER EDGE OF THE OBSTRUCTION.
4. THE STRIPING SHALL CONSIST OF ALTERNATING BLACK AND RETROREFLECTIVE YELLOW SLOPING DOWNWARD TOWARD THE SIDE ON WHICH TRAFFIC IS THE PASS.



PLASTIC CONE DETAIL (36" OR GREATER)

1. PLASTIC DRUMS, TUBULAR MARKERS, OR PLASTIC CONES SHALL BE ON BIG AND SMALL AS AN EXPEDITIOUS METHOD FOR TRAFFIC CHANNELIZATION. THE COLOR AND MARKING OF THE DEVICE USED SHALL BE CONSISTENT WITH MARKING STANDARDS FOR BARRICADE. THE PREDOMINANT COLOR ON THE DEVICE USED SHALL BE ORANGE WITH RETROREFLECTIVE, HORIZONTAL, CIRCUMFERENTIAL WHITE STRIPES AS SHOWN ON THE DETAILS.
2. DRUMS, TUBULAR MARKERS, OR PLASTIC CONES SHOULD NEVER BE PLACED IN THE ROADWAY WITHOUT WARNING SIGNS.
3. WHERE PRACTICAL, PLASTIC DRUMS, TUBULAR MARKERS, OR PLASTIC CONES SHOULD BE PLACED NO CLOSER THAN 3'-0" FROM THE EDGE OF TRAVELED LANE.
4. BALLAST SHALL NOT BE PLACED ON THE TOP OF THE DRUM.

Notice to Bidders No. 7473 -

| DATE | REVISION | BY |
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MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

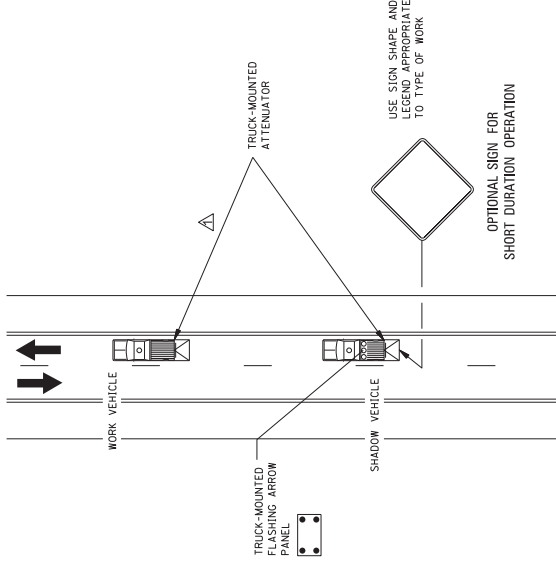
HIGHWAY SIGN AND BARRICADE DETAILS FOR CONSTRUCTION PROJECTS

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

ISSUE DATE: AUGUST 01, 2017

PROJECT NUMBER: 63358

MOBILE OPERATIONS ON TWO-LANE ROAD

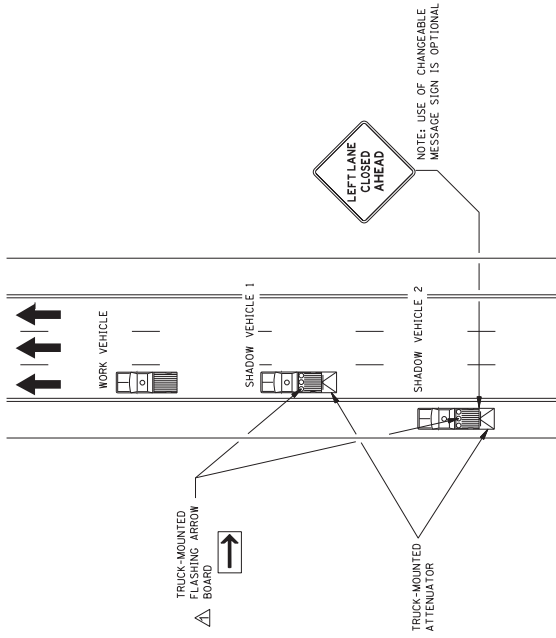


MOBILE OPERATIONS ON TWO-LANE ROAD

NOTES FOR TWO-LANE OPERATION:

- WHERE PRACTICAL AND WHEN NEEDED, THE WORK AND SHADOW VEHICLES SHOULD BE POSITIONED PERIODICALLY TO ALLOW TRAFFIC TO PASS. THIS CAN NOT BE DONE FREQUENTLY AS AN ALTERNATIVE, A "DO NOT PASS" SIGN MAY BE PLACED ON THE REAR OF THE VEHICLE BLOCKING THE LANE.
- THE DISTANCE BETWEEN THE WORK AND SHADOW VEHICLES MAY VARY ACCORDING TO THE TYPE OF ROADWAY. SHADOW VEHICLES SHOULD BE USED TO WARN TRAFFIC OF THE OPERATION AHEAD, WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, THE SHADOW VEHICLE SHOULD MAINTAIN THE MINIMUM DISTANCE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. THE SHADOW VEHICLE SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- ADDITIONAL SHADOW VEHICLES TO WARN AND REDUCE THE SPEED OF ONCOMING OR OPPOSING TRAFFIC MAY BE USED. POLICE PATROL CARS MAY BE USED FOR THIS PURPOSE.
- A TRUCK-MOUNTED ATTENUATOR (TMA) SHOULD BE USED ON THE SHADOW VEHICLE AND MAY BE USED ON THE WORK VEHICLE.
- VEHICLES SHALL BE EQUIPPED WITH BEACONS, AND THE SHADOW LIGHTS MOUNTED ON THE REAR, ADJACENT TO THE SIGN, SHADOW AND WORK VEHICLES SHALL DISPLAY FLASHING OR ROTATING BEACONS BOTH FORWARD AND TO THE REAR.
- VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBSCURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
- ARROW BOARD TO BE USED IN CAUTION MODE.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

MOBILE OPERATIONS ON MULTILANE ROAD



MOBILE OPERATIONS ON MULTILANE ROAD

NOTES FOR MULTILANE OPERATION:

- SHADOW AND WORK VEHICLES SHALL DISPLAY HIGH-INTENSITY FLASHING BEACONS. SIGN LEGENDS SHALL NOT BE COVERED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
- SHADOW VEHICLE 2 SHOULD BE EQUIPPED WITH AN ARROW BOARD AND TRUCK MOUNTED ATTENUATOR (TMA). AN APPROPRIATE LANE CLOSURE SIGN SHOULD BE PLACED ON SHADOW VEHICLE 2 SO AS NOT TO OBSCURE THE ARROW BOARD.
- SHADOW VEHICLE 1 SHOULD BE EQUIPPED WITH AN ARROW BOARD AND TRUCK-MOUNTED ATTENUATOR (TMA).
- SHADOW VEHICLE 2 SHOULD TRAVEL AT A VARYING DISTANCE FROM THE WORK VEHICLE TO MAINTAIN ADEQUATE SIGHT DISTANCE FOR TRAFFIC APPROACHING FROM THE REAR.
- THE SPACING BETWEEN THE WORK VEHICLES AND THE SHADOW VEHICLES, AND BETWEEN EACH SHADOW VEHICLE, SHOULD BE MINIMIZED TO DETERMINE ROAD USERS FROM DRIVING IN BETWEEN.
- ON HIGH-SPEED ROADWAYS, A THIRD SHADOW VEHICLE (NOT SHOWN) MAY BE USED WITH SHADOW VEHICLE 1 IN THE CLOSED LANE, SHADOW VEHICLE 2 STRADDLING THE EDGE LINE, AND SHADOW VEHICLE 3 ON THE SHOULDER.
- ARROW BOARD SHALL BE AS A MINIMUM TYPE B, 60" X 30" IN ACCORDANCE WITH THE CRITERIA PRESENTED IN THE MUTCD.
- WORK SHOULD NORMALLY BE DONE DURING OFF-PEAK HOURS.
- VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBSCURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
- WHERE PRACTICAL AND WHEN NEEDED, THE WORK AND SHADOW VEHICLES SHOULD BE POSITIONED PERIODICALLY TO ALLOW TRAFFIC TO PASS. THIS CAN NOT BE DONE FREQUENTLY AS AN ALTERNATIVE, A "DO NOT PASS" SIGN MAY BE PLACED ON THE REAR OF THE VEHICLE BLOCKING THE LANE.
- THE DISTANCE BETWEEN THE WORK AND SHADOW VEHICLES MAY VARY ACCORDING TO THE TYPE OF ROADWAY. SHADOW VEHICLES SHOULD BE USED TO WARN TRAFFIC OF THE OPERATION AHEAD, WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, THE SHADOW VEHICLE SHOULD MAINTAIN THE MINIMUM DISTANCE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. THE SHADOW VEHICLE SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- ADDITIONAL SHADOW VEHICLES TO WARN AND REDUCE THE SPEED OF ONCOMING OR OPPOSING TRAFFIC MAY BE USED. POLICE PATROL CARS MAY BE USED FOR THIS PURPOSE.
- A TRUCK-MOUNTED ATTENUATOR (TMA) SHOULD BE USED ON THE SHADOW VEHICLE AND MAY BE USED ON THE WORK VEHICLE.
- VEHICLES SHALL BE EQUIPPED WITH TWO HIGH-INTENSITY FLASHING LIGHTS MOUNTED ON THE REAR, ADJACENT TO THE SIGN, SHADOW AND WORK VEHICLES SHALL DISPLAY FLASHING OR ROTATING BEACONS BOTH FORWARD AND TO THE REAR.
- VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBSCURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
- ARROW BOARD TO BE USED IN CAUTION MODE.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

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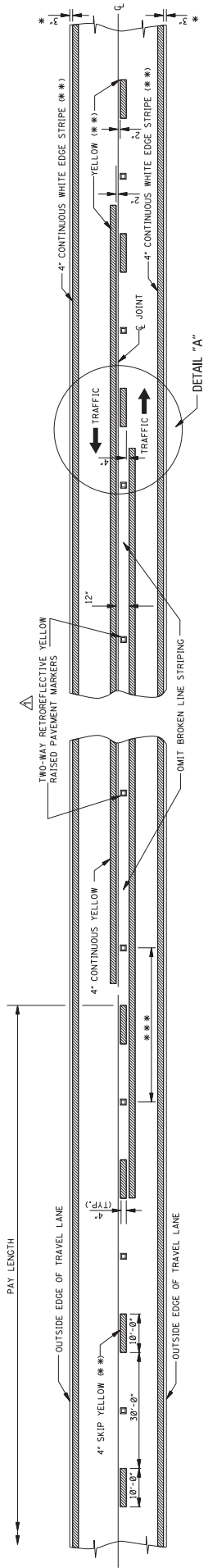
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

TRAFFIC CONTROL PLAN
MOBILE OPERATIONS
MULTILANE ROADS
AND
TWO-LANE ROADS

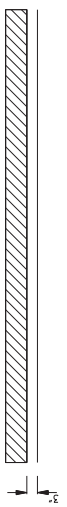
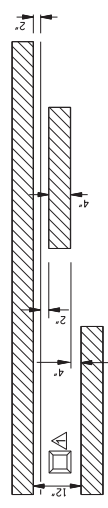
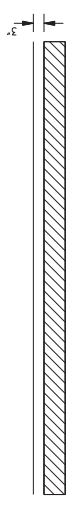
ISSUE DATE: AUGUST 01, 2017

PROJECT NUMBER: 6359

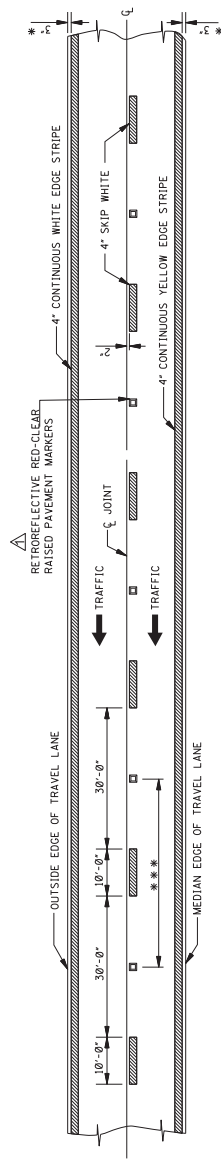




TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)



DETAIL "A"



4-LANE WITH ONE-WAY TRAFFIC

GENERAL NOTES:

- * 1. 3" UNLESS SHOWN ELSEWHERE ON THE PLANS.
- ** 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR TAPE AS INDICATED IN PAY ITEMS).
- ▲ 3. RETROREFLECTIVE RAISED PAVEMENT MARKERS TO BE USED IF TEMPORARY MARKINGS ARE TO REMAIN IN PLACE OVER 3 MONTHS.
- ▲▲▲ 4. SPACING OF RETROREFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

| TANGENT SECTIONS | URBAN AREA (ft-in) | RURAL AREA (ft-in) |
|--------------------|--------------------|--------------------|
| HORIZONTAL CURVES | 40'-0" | 80'-0" |
| INTERCHANGE LIMITS | 40'-0" | + 40'-0" |

- ▲▲ NOTE: ON THE MAIN FACILITY, RETROREFLECTIVE RED-CLEAR PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED IN LANE-LINES THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP AND ENDING 1000' BEFORE THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
- ▲ 5. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RETROREFLECTIVE RAISED PAVEMENT MARKERS FROM THE MOST APPROVED PRODUCTS LIST.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

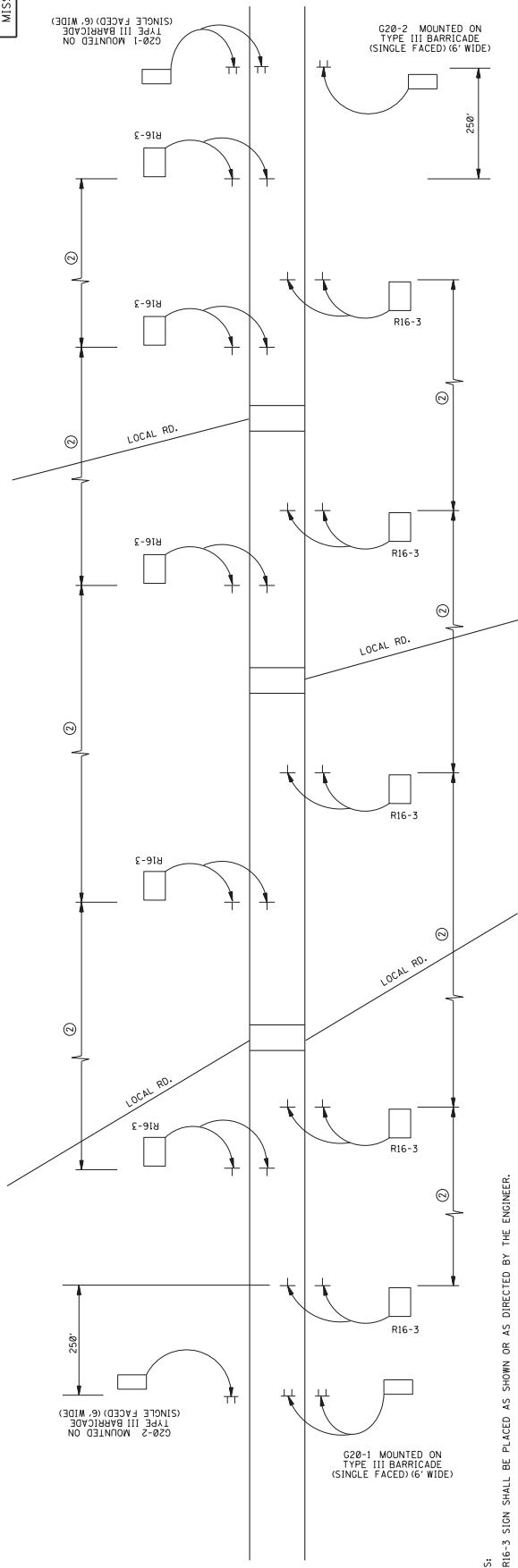
TEMPORARY STRIPING FOR TRAFFIC CONTROL 2-LANE AND 4-LANE DIVIDED HIGHWAYS

ISSUE DATE: AUGUST 01, 2017

PROJECT NUMBER: 6363

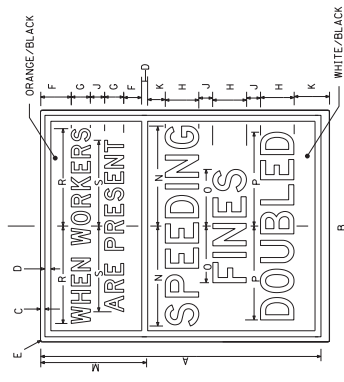
CP-13

MDOT



DIVIDED HIGHWAY SHOWN
(2 LANE - 2 WAY ROADWAY SIMILAR)
(PROJECT MORE THAN 1 MILE LENGTH)

- NOTES:
1. R16-3 SIGN SHALL BE SHOWN OR AS DIRECTED BY THE ENGINEER.
 2. R16-3 SIGN SHOULD BE SPACED AT A MAXIMUM OF 2 MILES THROUGHOUT LENGTH OF PROJECT.
 3. THIS SHEET WILL ONLY APPLY TO SPEED REDUCTION SECTIONS.



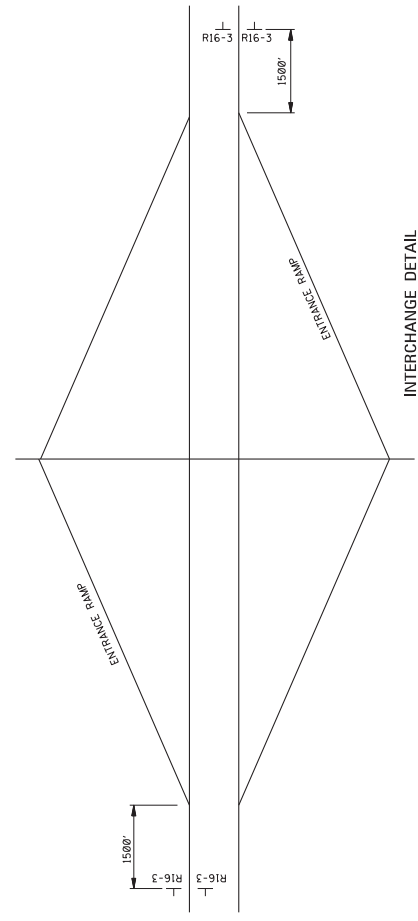
| SIGN | DIMENSIONS (INCHES) | | | | | | | | | | | | |
|------|---------------------|-------|--------|-------|--------|--------|--------|----|---|---|---|---|---|
| | A | B | C | D | E | F | G | H | I | J | K | L | M |
| STD. | 60 | 48 | 3/4 | 1 1/4 | 3 | 3 3/4 | 4 | 0m | 7 | D | P | R | S |
| STD. | 3 | 6 5/8 | 1 25/8 | 2 1/8 | 11 7/8 | 19 5/8 | 20 5/8 | 18 | | | | | |

| SIGN | DIMENSIONS (INCHES) | | | | | | | | | | | | |
|------|---------------------|-------|-------|-------|----|-------|--------|--------|----|---|---|---|---|
| | A | B | C | D | E | F | G | H | I | J | K | L | M |
| STD. | 48 | 36 | 3/4 | 1 1/4 | 3 | 2 3/4 | 3 | 0m | 6 | D | P | R | S |
| STD. | 3 | 4 7/8 | 1 5/8 | 1 3/4 | 14 | 7 7/8 | 13 3/8 | 13 3/8 | 12 | | | | |

48" x 60"
(INTERSTATE USE)

36" x 48"
(ALL OTHER HIGHWAYS)

R16-3



INTERCHANGE DETAIL

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**LOCATION OF
R16-3 SIGNS
(SPEEDING FINES
DOUBLED)**

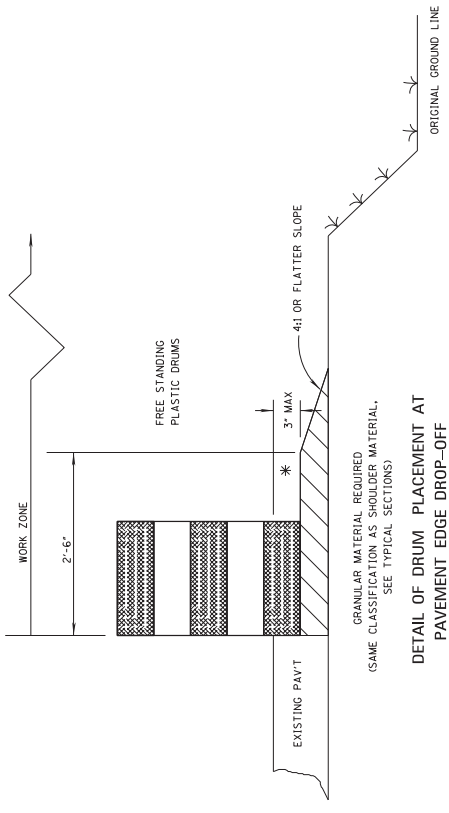
DATE: _____ BY: _____

REVISION: _____

ISSUE DATE: AUGUST 01, 2017

PROJECT NUMBER: 63365



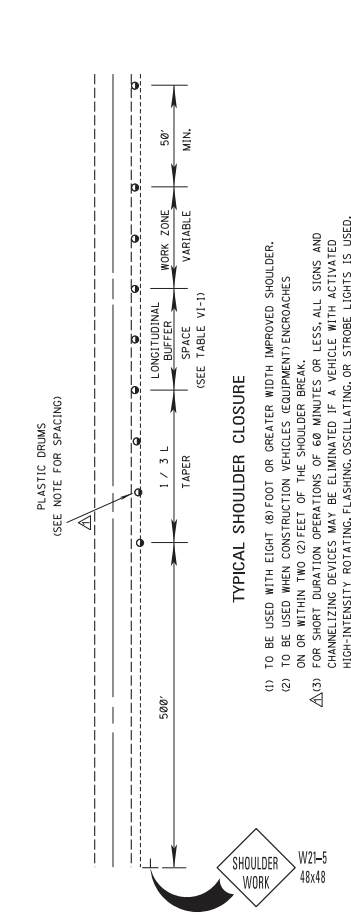


DETAIL OF DRUM PLACEMENT AT PAVEMENT EDGE DROP-OFF

GRANULAR MATERIAL REQUIRED AS SHOULDER MATERIAL, (SAME SPECIFICATIONS AS SHOULDER MATERIAL, SEE TYPICAL SECTIONS)

NOTES:

- * A. PAVEMENT EDGE DROP-OFF
- 1. IF LESS THAN TWO AND ONE QUARTER (2.25) INCHES-NO PROTECTION REQUIRED. PLACE A SHOULDER WORK SIGN (W21-5) 500 FEET IN ADVANCE OF WORK ZONE SHOULDER AND A LOW SHOULDER SIGN (W8-9) AT THE BEGINNING AND THROUGHOUT THE WORK ZONE @ (750'±0.C.).
- 2. TWO AND ONE QUARTER TO THREE INCHES-PLACE DRUMS, VERTICAL PANELS OR BARRICADES EVERY 100 FEET ON TANGENT SECTIONS FOR SPEEDS OF 50 MILES PER HOUR OR GREATER, CONES MAY BE USED IN PLACE OF DRUMS, PANELS, AND BARRICADES DURING DAYLIGHT HOURS, FOR TANGENT SECTIONS WITH SPEEDS LESS THAN 50 MILES PER HOUR AND FOR CURVES, DEVICES SHOULD BE PLACED EVERY 50 FEET, SPACING FOR TAPERS SHOULD BE IN ACCORDANCE WITH THE MUTCD (1 / 3 L, WHERE L IS THE TAPER LENGTH IN FEET).
- 3. GREATER THAN THREE (3) INCHES-POSITIVE SEPARATION OR WEDGE WITH 4:1 OR FLATTER SLOPE NEEDED, IF THERE IS EIGHT (8) FEET OR MORE DISTANCE BETWEEN THE EDGE OF TRAVEL LANE AND DROP-OFF, THEN DRUMS, PANELS OR BARRICADES MAY BE USED.
- 4. FOR TEMPORARY CONDITIONS, DROP-OFFS GREATER THAN THREE (3) INCHES MAY BE PROTECTED WITH DRUMS, VERTICAL PANELS OR BARRICADES FOR SHORT DISTANCES DURING DAYLIGHT HOURS WHILE WORK IS BEING DONE IN THE DROP-OFF AREA.
- 5. LESSER TREATMENTS THAN THOSE DESCRIBED ABOVE MAY BE CONSIDERED FOR LOW-VOLUME LOCAL STREETS.



TYPICAL SHOULDER CLOSURE

(1) TO BE USED WITH EIGHT (8) FOOT OR GREATER WIDTH IMPROVED SHOULDER.
 (2) TO BE USED WHEN CONSTRUCTION VEHICLES (EQUIPMENT) ENCROACHES ON OR WITHIN TWO (2) FEET OF THE SHOULDER BREAK.
 (3) FOR SHORT DURATION OPERATIONS OF 60 MINUTES OR LESS, ALL SIGNS AND CHANNELIZING DEVICES MAY BE ELIMINATED IF A VEHICLE WITH ACTIVATED HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS IS USED.

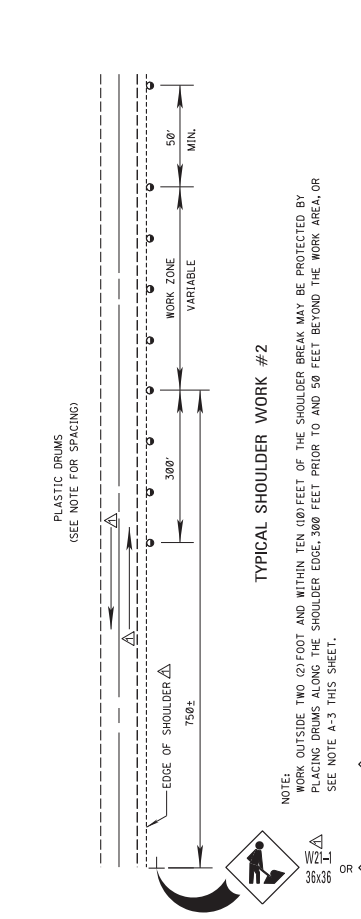
TYPICAL SHOULDER WORK #1 (SEE NOTE A-1 THIS SHEET)

500' TAPER 1 / 3 L LONGITUDINAL BUFFER SPACE 300' WORK ZONE VARIABLE EDGE OF SHOULDER 50' MIN.

500' FROM BEGINNING TO THE END OF WORK ZONE

750' ±0.C. LOW SHOULDER W8-9 48x48

SHOULDER WORK W21-5 48x48



TYPICAL SHOULDER WORK #2

NOTE: WORK OUTSIDE TWO (2) FOOT AND WITHIN TEN (10) FEET OF THE SHOULDER BREAK MAY BE PROTECTED BY PLACING DRUMS ALONG THE SHOULDER EDGE, 300 FEET PRIOR TO AND 50 FEET BEYOND THE WORK AREA, OR SEE NOTE A-3 THIS SHEET.

500' TAPER 1 / 3 L LONGITUDINAL BUFFER SPACE 300' WORK ZONE VARIABLE EDGE OF SHOULDER 50' MIN.

SHOULDER WORK W21-5 48x48

OR WORKERS W21-1a 36x36

OR SHOULDER WORK W21-5 48x48

TABLE VI-1. GUIDELINES FOR LENGTH OF LONGITUDINAL BUFFER SPACE

| PERCENTILE SPEED (MPH) | LENGTH (FEET) |
|------------------------|---------------|
| 20 | 115 |
| 25 | 155 |
| 30 | 200 |
| 35 | 260 |
| 40 | 305 |
| 45 | 360 |
| 50 | 425 |
| 55 | 475 |
| 60 | 510 |
| 65 | 645 |

- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.
- 1. TANGENTS = 2 X S
 - 2. TAPERS = L / 3
 - WHERE L = S X W
 - L = TAPER LENGTH IN FEET
 - S = SPEED IN MPH (POSTED OR ANTICIPATED OPERATING SPEED)
 - W = WIDTH OF OFFSET IN FEET

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**TRAFFIC CONTROL DETAILS
DRUM PLACEMENT
AND
SHOULDER CLOSURE**

ISSUE DATE: AUGUST 01, 2017

PROJECT NUMBER: 63366

REVISIONS:

| NO. | DATE | DESCRIPTION |
|-----|------|-------------|
| 1 | | ISSUE |

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 7474

CODE: (SP)

DATE: 1/20/2026

SUBJECT: Scope of Work Revision

PROJECT: STBG-0014-01(077) / 108976301 – Pike County

Bidders are hereby advised of the following changes to the scope of work and bid items:

- MDOT Roadway Design Standard Drawing revisions dated 12/11/2025 have been included in the revised scope of work.
- Quantity changes to allow for removal of traffic stripes prior to scrub seal.
- Quantity increased for 202-B240, Removal of Traffic Stripe, 1,260 LF to 21,060 LF.
- Quantity increased for 619-A2001, Temp. Traffic Stripe, Cont. Yellow, 6 MI to 9 MI.
- Quantity increased for 619-A4002, Temp. Traffic Stripe, Skip Yellow, 6 MI to 9 MI.

| | | |
|--|----------|------|
| OPERATOR: Broom/Sweeper..... | \$ 10.17 | 0.00 |
| OPERATOR: Bulldozer..... | \$ 13.40 | 0.00 |
| OPERATOR: Concrete Saw..... | \$ 13.60 | 0.00 |
| OPERATOR: Crane..... | \$ 16.00 | 0.00 |
| OPERATOR: Distributor..... | \$ 11.70 | 0.00 |
| OPERATOR: Drill..... | \$ 19.22 | 0.00 |
| OPERATOR: Grader/Blade..... | \$ 13.59 | 0.00 |
| OPERATOR: Loader..... | \$ 10.15 | 0.00 |
| OPERATOR: Mechanic..... | \$ 16.28 | 0.00 |
| OPERATOR: Milling Machine..... | \$ 15.38 | 0.00 |
| OPERATOR: Mixer..... | \$ 14.85 | 0.00 |
| OPERATOR: Oiler..... | \$ 13.08 | 0.48 |
| OPERATOR: Paver (Asphalt, Aggregate, and Concrete)..... | \$ 10.00 | 0.00 |
| OPERATOR: Piledriver..... | \$ 15.13 | 0.00 |
| OPERATOR: Roller (All Types).... | \$ 11.05 | 0.00 |
| OPERATOR: Scraper..... | \$ 12.63 | 0.00 |
| OPERATOR: Tractor..... | \$ 13.29 | 0.00 |
| OPERATOR: Trencher..... | \$ 15.00 | 0.00 |
| TRUCK DRIVER: Flatbed Truck..... | \$ 13.29 | 0.00 |
| TRUCK DRIVER: Lowboy Truck..... | \$ 12.49 | 0.00 |
| TRUCK DRIVER: Mechanic..... | \$ 12.35 | 0.00 |
| TRUCK DRIVER: Off the Road Truck..... | \$ 12.31 | 0.00 |
| TRUCK DRIVER: Water Truck..... | \$ 13.15 | 0.00 |
| TRUCK DRIVER: Dump Truck (All Types)..... | \$ 11.63 | 0.00 |
| TRUCK DRIVER: Semi/Trailer Truck..... | \$ 17.34 | 0.00 |

WELDERS - Receive rate prescribed for craft performing operation to which welding is incidental.

=====
Note: Executive Order (EO) 13706, Establishing Paid Sick Leave for Federal Contractors applies to all contracts subject to the Davis-Bacon Act for which the contract is awarded (and any solicitation was issued) on or after January 1, 2017. If this contract is covered by the EO, the contractor must provide

employees with 1 hour of paid sick leave for every 30 hours they work, up to 56 hours of paid sick leave each year. Employees must be permitted to use paid sick leave for their own illness, injury or other health-related needs, including preventive care; to assist a family member (or person who is like family to the employee) who is ill, injured, or has other health-related needs, including preventive care; or for reasons resulting from, or to assist a family member (or person who is like family to the employee) who is a victim of, domestic violence, sexual assault, or stalking. Additional information on contractor requirements and worker protections under the EO is available at <https://www.dol.gov/agencies/whd/government-contracts>.

Note: Executive Order 13658 generally applies to contracts subject to the Davis-Bacon Act that were awarded on or between January 1, 2015 and January 29, 2022, and that have not been renewed or extended on or after January 30, 2022. Executive Order 13658 does not apply to contracts subject only to the Davis-Bacon Related Acts regardless of when they were awarded. If a contract is subject to Executive Order 13658, the contractor must pay all covered workers at least \$13.30 per hour (or the applicable wage rate listed on this wage determination, if it is higher) for all hours spent performing on the contract in 2025. The applicable Executive Order minimum wage rate will be adjusted annually. Additional information on contractor requirements and worker protections under Executive Order 13658 is available at www.dol.gov/whd/govcontracts.

Unlisted classifications needed for work not included within the scope of the classifications listed may be added after award only as provided in the labor standards contract clauses (29CFR 5.5 (a) (1) (iii)).

The body of each wage determination lists the classifications and wage rates that have been found to be prevailing for the type(s) of construction and geographic area covered by the wage determination. The classifications are listed in alphabetical order under rate identifiers indicating whether the particular rate is a union rate (current union negotiated rate), a survey rate, a weighted union average rate, a state adopted rate, or a supplemental classification rate.

Union Rate Identifiers

A four-letter identifier beginning with characters other than ""SU"", ""UAVG"", ?SA?, or ?SC? denotes that a union rate was prevailing for that classification in the survey. Example: PLUM0198-005 07/01/2024. PLUM is an identifier of the union whose collectively bargained rate prevailed in the survey for this classification, which in this example would be Plumbers. 0198 indicates the local union number or district council number where applicable, i.e., Plumbers Local 0198. The next number, 005 in the example, is an internal number used in processing the wage determination. The date, 07/01/2024 in the example, is the effective date of the most current negotiated rate.

Union prevailing wage rates are updated to reflect all changes over time that are reported to WHD in the rates in the collective bargaining agreement (CBA) governing the

classification.

Union Average Rate Identifiers

The UAVG identifier indicates that no single rate prevailed for those classifications, but that 100% of the data reported for the classifications reflected union rates. EXAMPLE:

UAVG-OH-0010 01/01/2024. UAVG indicates that the rate is a weighted union average rate. OH indicates the State of Ohio. The next number, 0010 in the example, is an internal number used in producing the wage determination. The date, 01/01/2024 in the example, indicates the date the wage determination was updated to reflect the most current union average rate.

A UAVG rate will be updated once a year, usually in January, to reflect a weighted average of the current rates in the collective bargaining agreements on which the rate is based.

Survey Rate Identifiers

The "SU" identifier indicates that either a single non-union rate prevailed (as defined in 29 CFR 1.2) for this classification in the survey or that the rate was derived by computing a weighted average rate based on all the rates reported in the survey for that classification. As a weighted average rate includes all rates reported in the survey, it may include both union and non-union rates. Example: SUFL2022-007 6/27/2024. SU indicates the rate is a single non-union prevailing rate or a weighted average of survey data for that classification. FL indicates the State of Florida. 2022 is the year of the survey on which these classifications and rates are based. The next number, 007 in the example, is an internal number used in producing the wage determination. The date, 6/27/2024 in the example, indicates the survey completion date for the classifications and rates under that identifier.

?SU? wage rates typically remain in effect until a new survey is conducted. However, the Wage and Hour Division (WHD) has the discretion to update such rates under 29 CFR 1.6(c)(1).

State Adopted Rate Identifiers

The "SA" identifier indicates that the classifications and prevailing wage rates set by a state (or local) government were adopted under 29 C.F.R 1.3(g)-(h). Example: SAME2023-007 01/03/2024. SA reflects that the rates are state adopted. ME refers to the State of Maine. 2023 is the year during which the state completed the survey on which the listed classifications and rates are based. The next number, 007 in the example, is an internal number used in producing the wage determination. The date, 01/03/2024 in the example, reflects the date on which the classifications and rates under the ?SA? identifier took effect under state law in the state from which the rates were adopted.

WAGE DETERMINATION APPEALS PROCESS

1) Has there been an initial decision in the matter? This can be:

- a) a survey underlying a wage determination
- b) an existing published wage determination
- c) an initial WHD letter setting forth a position on

a wage determination matter
d) an initial conformance (additional classification
and rate) determination

On survey related matters, initial contact, including requests
for summaries of surveys, should be directed to the WHD Branch
of Wage Surveys. Requests can be submitted via email to
davisbaconinfo@dol.gov or by mail to:

Branch of Wage Surveys
Wage and Hour Division
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

Regarding any other wage determination matter such as
conformance decisions, requests for initial decisions should be
directed to the WHD Branch of Construction Wage Determinations.
Requests can be submitted via email to BCWD-Office@dol.gov or
by mail to:

Branch of Construction Wage Determinations
Wage and Hour Division
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

2) If an initial decision has been issued, then any interested
party (those affected by the action) that disagrees with the
decision can request review and reconsideration from the Wage
and Hour Administrator (See 29 CFR Part 1.8 and 29 CFR Part 7).
Requests for review and reconsideration can be submitted via
email to dba.reconsideration@dol.gov or by mail to:

Wage and Hour Administrator
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

The request should be accompanied by a full statement of the
interested party's position and any information (wage payment
data, project description, area practice material, etc.) that
the requestor considers relevant to the issue.

3) If the decision of the Administrator is not favorable, an
interested party may appeal directly to the Administrative
Review Board (formerly the Wage Appeals Board). Write to:

Administrative Review Board
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210.

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END OF GENERAL DECISION

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Seal & Overlay approximately 3.5 miles on MS 24 from Amite County Line to near I-55, known as Federal Aid Project No. STBG-0014-01(077) / 108976301 in Pike County.

| Line No. | Item Code | Adj Code | Quantity | Units | Description [Fixed Unit Price] |
|----------------------|--------------|----------|----------|-------------|---|
| Roadway Items | | | | | |
| 0010 | 201-D001 | | 218 | Station | Random Clearing |
| 0020 | 202-B006 | | 560 | Square Yard | Removal of Asphalt Paved Shoulders, All Depths |
| 0030 | 202-B158 | | 1,280 | Linear Feet | Removal of Guard Rail, Including Rails, Posts and Terminal Ends |
| 0040 | 202-B240 | | 21,060 | Linear Feet | Removal of Traffic Stripe |
| 0050 | 203-G002 | (E) | 280 | Cubic Yard | Excess Excavation, LVM, AH |
| 0060 | 304-A008 | (GY) | 1,134 | Cubic Yard | Granular Material, LVM, Class 6, Group D |
| 0070 | 406-D001 | | 12,757 | Square Yard | Fine Milling of Bituminous Pavement, All Depths |
| 0080 | 407-A001 | (A2) | 5,048 | Gallon | Asphalt for Tack Coat |
| 0090 | 423-A001 | | 7 | Mile | Rumble Strips, Ground In |
| 0100 | 503-C010 | | 1,280 | Linear Feet | Saw Cut, Full Depth |
| 0110 | 606-B002 | | 700 | Linear Feet | Guard Rail, Class A, Type 1, 'W' Beam |
| 0120 | 606-D019 | | 8 | Each | Guard Rail, Bridge End Section, Type H |
| 0130 | 606-E005 | | 8 | Each | Guard Rail, Terminal End Section, Flared |
| 0140 | 618-B001 | | 1 | Square Feet | Additional Construction Signs [\$10.00] |
| 0150 | 619-A1001 | | 7 | Mile | Temporary Traffic Stripe, Continuous White |
| 0160 | 619-A2001 | | 9 | Mile | Temporary Traffic Stripe, Continuous Yellow |
| 0170 | 619-A4002 | | 9 | Mile | Temporary Traffic Stripe, Skip Yellow |
| 0180 | 619-A5001 | | 3,695 | Linear Feet | Temporary Traffic Stripe, Detail |
| 0190 | 619-A6002 | | 1,086 | Linear Feet | Temporary Traffic Stripe, Legend |
| 0200 | 620-A001 | | 1 | Lump Sum | Mobilization |
| 0210 | 630-F006 | | 44 | Each | Delineators, Guard Rail, White |
| 0220 | 907-403-A014 | (BA1) | 4,851 | Ton | 9.5-mm, MT, Asphalt Pavement |
| 0230 | 907-403-B011 | (BA1) | 1,070 | Ton | 9.5-mm, MT, Asphalt Pavement, Leveling |
| 0240 | 907-414-A001 | | 51,909 | Square Yard | Scrub Seal |
| 0250 | 907-618-A001 | | 1 | Lump Sum | Maintenance of Traffic |
| 0260 | 907-619-B001 | | 66 | Linear Feet | Temporary Portable Rumble Strips |
| 0270 | 907-626-C014 | | 7 | Mile | 6" Thermoplastic Edge Stripe, Continuous White |
| 0280 | 907-626-D005 | | 3 | Mile | 6" Thermoplastic Traffic Stripe, Skip Yellow |
| 0290 | 907-626-E005 | | 3 | Mile | 6" Thermoplastic Traffic Stripe, Continuous Yellow |
| 0300 | 907-626-G004 | | 2,379 | Linear Feet | Thermoplastic Detail Stripe, White |
| 0310 | 907-626-G005 | | 1,316 | Linear Feet | Thermoplastic Detail Stripe, Yellow |
| 0320 | 907-626-H010 | | 1,086 | Linear Feet | Thermoplastic Legend, White |

| Line No. | Item Code | Adj Code | Quantity | Units | Description [Fixed Unit Price] |
|------------------------------------|------------------|-----------------|-----------------|--------------|---|
| 0330 | 907-627-H001 | | 324 | Each | Chip Seal Reflective Raised Markers |
| 0340 | 907-627-J001 | | 309 | Each | Two-Way Clear Reflective High Performance Raised Markers |
| 0350 | 907-627-L001 | | 485 | Each | Two-Way Yellow Reflective High Performance Raised Markers |
| ALTERNATE GROUP AA NUMBER 1 | | | | | |
| 0360 | 304-H001 | (GY) | 280 | Cubic Yard | 3/4" and Down Crushed Stone Base, LVM |
| ALTERNATE GROUP AA NUMBER 2 | | | | | |
| 0370 | 304-H002 | (GY) | 280 | Cubic Yard | Size 610 Crushed Stone Base, LVM |
| ALTERNATE GROUP AA NUMBER 3 | | | | | |
| 0380 | 304-H003 | (GY) | 280 | Cubic Yard | Size 825B Crushed Stone Base, LVM |