

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO. <u> 1 </u>	DATED <u> 1/22/2026 </u>	ADDENDUM NO. <u> </u>	DATED <u> </u>
ADDENDUM NO. <u> </u>	DATED <u> </u>	ADDENDUM NO. <u> </u>	DATED <u> </u>
ADDENDUM NO. <u> </u>	DATED <u> </u>	ADDENDUM NO. <u> </u>	DATED <u> </u>

Number	Description
1	Revised Table of Contents; Deleted NTB No. 5750; Added NTB No. 7464; Revised Wage Rates; Revised Bid Items; Revised or Added Plan Sheet Nos. 6051, 6351, 6358-6359, 6363 & 6366, 8001-8002 & 8007; Amendment EBSx Download Required.

TOTAL ADDENDA: 1
(Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

Contractor

BY _____
Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

_____	President	Address
_____	Secretary	Address
_____	Treasurer	Address

The following is my (our) itemized proposal.
STBG-0044-01(036)/ 109672301000
Union County(ies)

Revised 01/26/2016

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION
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PROJECT: STBG-0044-01(036)/109672301 - Union

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(REVISIONS TO THE ABOVE WILL BE INDICATED ON THE SECOND SHEET
OF SECTION 905 AS ADDENDA)

01/21/2026 04:42 PM

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 7464

CODE: (SP)

DATE: 01/22/2026

SUBJECT: Additional Pay Item

PROJECT: STBG-0044-01(036) / 109672301 -- Union County

Bidders are hereby advised that the following pay item has been added from what is shown on the Summary of Quantities Plan Sheet:

Pay Item 203-EX023 Borrow Excavation, AH, LVM, Class B10 -per Cubic Yard

This pay item has been added to the quantities on the proposal's bid sheet.

"General Decision Number: MS20260118 01/02/2026

Superseded General Decision Number: MS20250118

State: Mississippi

Construction Type: Highway

County: Union County in Mississippi.

HIGHWAY CONSTRUCTION PROJECTS

Modification Number Publication Date
 0 01/02/2026

SUMS2010-041 08/04/2014

	Rates	Fringes
CARPENTER (Form Work Only).....	\$ 14.13	0.00
CARPENTER, Excludes Form Work....	\$ 13.49	0.00
CEMENT MASON/CONCRETE FINISHER...	\$ 13.70	0.00
ELECTRICIAN.....	\$ 21.80	7.93
HIGHWAY/PARKING LOT STRIPING: Truck Driver (Line Striping Truck).....	\$ 15.97	0.00
INSTALLER - GUARDRAIL.....	\$ 11.51	0.00
IRONWORKER, REINFORCING.....	\$ 14.82	0.00
LABORER: Common or General, Including Asphalt Raking, Shoveling, Spreading and Concrete Work.....	\$ 10.58	0.00
LABORER: Flagger.....	\$ 9.76	0.00
LABORER: Grade Checker.....	\$ 12.77	0.00
LABORER: Landscape.....	\$ 9.56	0.00
LABORER: Mason Tender - Cement/Concrete.....	\$ 11.08	0.00
LABORER: Pipelayer.....	\$ 10.76	0.00
LABORER: Laborer-Cones/ Barricades/Barrels - Setter/Mover/Sweeper.....	\$ 10.38	0.00
OPERATOR: Asphalt Spreader.....	\$ 16.03	0.00
OPERATOR: Backhoe/Excavator/Trackhoe.....	\$ 15.05	0.00
OPERATOR: Boring Machine.....	\$ 15.14	0.00
OPERATOR: Broom/Sweeper.....	\$ 10.94	0.00

OPERATOR: Bulldozer.....	\$ 14.34	0.00
OPERATOR: Concrete Saw.....	\$ 15.68	0.00
OPERATOR: Crane.....	\$ 18.32	0.00
OPERATOR: Distributor.....	\$ 12.59	0.00
OPERATOR: Drill.....	\$ 19.22	0.00
OPERATOR: Grader/Blade.....	\$ 14.50	0.00
OPERATOR: Loader.....	\$ 11.54	0.00
OPERATOR: Mechanic.....	\$ 15.13	0.00
OPERATOR: Milling Machine.....	\$ 15.12	0.00
OPERATOR: Oiler.....	\$ 12.33	0.00
OPERATOR: Paver (Asphalt, Aggregate, and Concrete).....	\$ 14.47	0.00
OPERATOR: Piledriver.....	\$ 15.13	0.00
OPERATOR: Roller (All Types)....	\$ 11.54	0.00
OPERATOR: Scraper.....	\$ 13.15	0.00
OPERATOR: Tractor.....	\$ 11.25	0.00
OPERATOR: Trencher.....	\$ 15.00	0.00
TRUCK DRIVER: Flatbed Truck.....	\$ 13.79	0.00
TRUCK DRIVER: Lowboy Truck.....	\$ 13.30	0.00
TRUCK DRIVER: Mechanic.....	\$ 14.23	0.00
TRUCK DRIVER: Off the Road Truck.....	\$ 12.29	0.00
TRUCK DRIVER: Water Truck.....	\$ 10.58	0.00
TRUCK DRIVER: Dump Truck (All Types).....	\$ 10.97	0.00
TRUCK DRIVER: Semi/Trailer Truck.....	\$ 15.81	0.00

WELDERS - Receive rate prescribed for craft performing operation to which welding is incidental.

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Note: Executive Order (EO) 13706, Establishing Paid Sick Leave for Federal Contractors applies to all contracts subject to the Davis-Bacon Act for which the contract is awarded (and any solicitation was issued) on or after January 1, 2017. If this contract is covered by the EO, the contractor must provide employees with 1 hour of paid sick leave for every 30 hours they work, up to 56 hours of paid sick leave each year. Employees must be permitted to use paid sick leave for their own illness, injury or other health-related needs, including

preventive care; to assist a family member (or person who is like family to the employee) who is ill, injured, or has other health-related needs, including preventive care; or for reasons resulting from, or to assist a family member (or person who is like family to the employee) who is a victim of, domestic violence, sexual assault, or stalking. Additional information on contractor requirements and worker protections under the EO is available at <https://www.dol.gov/agencies/whd/government-contracts>.

Note: Executive Order 13658 generally applies to contracts subject to the Davis-Bacon Act that were awarded on or between January 1, 2015 and January 29, 2022, and that have not been renewed or extended on or after January 30, 2022. Executive Order 13658 does not apply to contracts subject only to the Davis-Bacon Related Acts regardless of when they were awarded. If a contract is subject to Executive Order 13658, the contractor must pay all covered workers at least \$13.30 per hour (or the applicable wage rate listed on this wage determination, if it is higher) for all hours spent performing on the contract in 2025. The applicable Executive Order minimum wage rate will be adjusted annually. Additional information on contractor requirements and worker protections under Executive Order 13658 is available at www.dol.gov/whd/govcontracts.

Unlisted classifications needed for work not included within the scope of the classifications listed may be added after award only as provided in the labor standards contract clauses (29CFR 5.5 (a) (1) (iii)).

The body of each wage determination lists the classifications and wage rates that have been found to be prevailing for the type(s) of construction and geographic area covered by the wage determination. The classifications are listed in alphabetical order under rate identifiers indicating whether the particular rate is a union rate (current union negotiated rate), a survey rate, a weighted union average rate, a state adopted rate, or a supplemental classification rate.

Union Rate Identifiers

A four-letter identifier beginning with characters other than ""SU"", ""UAVG"", ?SA?, or ?SC? denotes that a union rate was prevailing for that classification in the survey. Example: PLUM0198-005 07/01/2024. PLUM is an identifier of the union whose collectively bargained rate prevailed in the survey for this classification, which in this example would be Plumbers. 0198 indicates the local union number or district council number where applicable, i.e., Plumbers Local 0198. The next number, 005 in the example, is an internal number used in processing the wage determination. The date, 07/01/2024 in the example, is the effective date of the most current negotiated rate.

Union prevailing wage rates are updated to reflect all changes over time that are reported to WHD in the rates in the collective bargaining agreement (CBA) governing the classification.

Union Average Rate Identifiers

The UAVG identifier indicates that no single rate prevailed for those classifications, but that 100% of the data reported for the classifications reflected union rates. EXAMPLE:

UAVG-OH-0010 01/01/2024. UAVG indicates that the rate is a weighted union average rate. OH indicates the State of Ohio. The next number, 0010 in the example, is an internal number used in producing the wage determination. The date, 01/01/2024 in the example, indicates the date the wage determination was updated to reflect the most current union average rate.

A UAVG rate will be updated once a year, usually in January, to reflect a weighted average of the current rates in the collective bargaining agreements on which the rate is based.

Survey Rate Identifiers

The ""SU"" identifier indicates that either a single non-union rate prevailed (as defined in 29 CFR 1.2) for this classification in the survey or that the rate was derived by computing a weighted average rate based on all the rates reported in the survey for that classification. As a weighted average rate includes all rates reported in the survey, it may include both union and non-union rates. Example: SUFL2022-007 6/27/2024. SU indicates the rate is a single non-union prevailing rate or a weighted average of survey data for that classification. FL indicates the State of Florida. 2022 is the year of the survey on which these classifications and rates are based. The next number, 007 in the example, is an internal number used in producing the wage determination. The date, 6/27/2024 in the example, indicates the survey completion date for the classifications and rates under that identifier.

?SU? wage rates typically remain in effect until a new survey is conducted. However, the Wage and Hour Division (WHD) has the discretion to update such rates under 29 CFR 1.6(c)(1).

State Adopted Rate Identifiers

The ""SA"" identifier indicates that the classifications and prevailing wage rates set by a state (or local) government were adopted under 29 C.F.R 1.3(g)-(h). Example: SAME2023-007 01/03/2024. SA reflects that the rates are state adopted. ME refers to the State of Maine. 2023 is the year during which the state completed the survey on which the listed classifications and rates are based. The next number, 007 in the example, is an internal number used in producing the wage determination. The date, 01/03/2024 in the example, reflects the date on which the classifications and rates under the ?SA? identifier took effect under state law in the state from which the rates were adopted.

WAGE DETERMINATION APPEALS PROCESS

1) Has there been an initial decision in the matter? This can be:

- a) a survey underlying a wage determination
- b) an existing published wage determination
- c) an initial WHD letter setting forth a position on a wage determination matter
- d) an initial conformance (additional classification and rate) determination

On survey related matters, initial contact, including requests for summaries of surveys, should be directed to the WHD Branch of Wage Surveys. Requests can be submitted via email to davisbaconinfo@dol.gov or by mail to:

Branch of Wage Surveys
Wage and Hour Division
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

Regarding any other wage determination matter such as conformance decisions, requests for initial decisions should be directed to the WHD Branch of Construction Wage Determinations. Requests can be submitted via email to BCWD-Office@dol.gov or by mail to:

Branch of Construction Wage Determinations
Wage and Hour Division
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

2) If an initial decision has been issued, then any interested party (those affected by the action) that disagrees with the decision can request review and reconsideration from the Wage and Hour Administrator (See 29 CFR Part 1.8 and 29 CFR Part 7). Requests for review and reconsideration can be submitted via email to dba.reconsideration@dol.gov or by mail to:

Wage and Hour Administrator
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

The request should be accompanied by a full statement of the interested party's position and any information (wage payment data, project description, area practice material, etc.) that the requestor considers relevant to the issue.

3) If the decision of the Administrator is not favorable, an interested party may appeal directly to the Administrative Review Board (formerly the Wage Appeals Board). Write to:

Administrative Review Board
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210.

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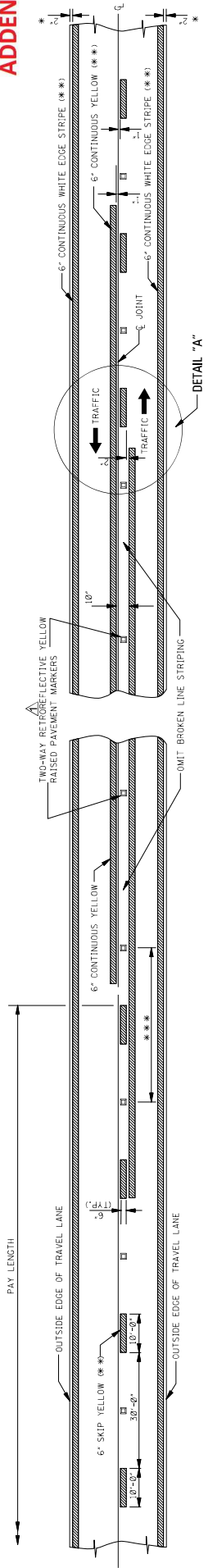
END OF GENERAL DECISION

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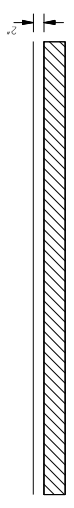
Bridge Repair on SR 30 over Tallahatchie River (Bridge No. 19.0), known as Federal Aid Project No. STBG-0044-01(036) / 109672301 in Union County.

Line No.	Item Code	Adj Code	Quantity	Units	Description [Fixed Unit Price]
Roadway Items					
0010	237-A002		200	Linear Feet	Wattles, 20"
0012	203-EX023	(E)	2,100	Cubic Yard	Borrow Excavation, AH, LVM, Class B10
0020	406-D001		267	Square Yard	Fine Milling of Bituminous Pavement, All Depths
0030	619-A1002		3,800	Linear Feet	Temporary Traffic Stripe, Continuous White
0040	619-A1007		4,080	Linear Feet	Temporary Traffic Stripe, Continuous White, Type 1 or 2 Tape
0050	619-A2002		450	Linear Feet	Temporary Traffic Stripe, Continuous Yellow
0060	619-A4001		1,900	Linear Feet	Temporary Traffic Stripe, Skip Yellow
0070	619-A6008		144	Linear Feet	Temporary Traffic Stripe, Legend, Type 1 or 2 Tape
0080	619-D1001		38	Square Feet	Standard Roadside Construction Signs, Less than 10 Square Feet
0090	619-D2001		152	Square Feet	Standard Roadside Construction Signs, 10 Square Feet or More
0100	619-F1001		1,500	Linear Feet	Concrete Median Barrier, Precast
0110	619-F2001		1,500	Linear Feet	Remove and Reset Concrete Median Barrier, Precast
0120	619-G4001		24	Linear Feet	Barricades, Type III, Double Faced
0130	619-G5001		20	Each	Free Standing Plastic Drums
0140	619-G7001		4	Each	Warning Lights, Type "B"
0150	619-J1002		2	Each	Impact Attenuator, 45 MPH
0160	619-J2002		2	Each	Impact Attenuator, 45 MPH, Replacement Package
0170	619-J3001		2	Each	Remove and Reset Impact Attenuator
0180	620-A001		1	Lump Sum	Mobilization
0190	907-234-A001		250	Linear Feet	Temporary Silt Fence
0200	907-234-F001		150	Linear Feet	Turbidity Barrier
0210	907-403-A015	(BA1)	22	Ton	9.5-mm, ST, Asphalt Pavement
0220	907-618-A001		1	Lump Sum	Maintenance of Traffic
0230	907-619-E3001		2	Each	Changeable Message Sign
0240	907-619-H2001		2	Each	Traffic Signal, Portable, Type 1
0250	907-626-B005		3,800	Linear Feet	6" Thermoplastic Traffic Stripe, Continuous White
0260	907-626-D006		1,900	Linear Feet	6" Thermoplastic Traffic Stripe, Skip Yellow
0270	907-626-E006		450	Linear Feet	6" Thermoplastic Traffic Stripe, Continuous Yellow
0280	907-627-L001		48	Each	Two-Way Yellow Reflective High Performance Raised Markers
Bridge Items					
0290	815-A009	(S)	1,922	Ton	Loose Riprap, Size 500
0300	815-E001	(S)	1,268	Square Yard	Geotextile under Riprap
0310	907-202-B001		5,931	Square Yard	Removal of Bridge Deck, Hydrodemolition

Line No.	Item Code	Adj Code	Quantity	Units	Description [Fixed Unit Price]
0320	907-420-A001		2,000	Pounds	Undersealing
0330	907-804-O001	(S)	330	Cubic Yard	Bridge Deck Overlay Concrete
0340	907-808-A002	(S)	548	Linear Feet	Joint Repair
0350	907-823-A002		235	Linear Feet	Preformed Joint Seal, Type II
0360	907-823-A003		39	Linear Feet	Preformed Joint Seal, Type III
0370	907-823-B002		470	Linear Feet	Saw Cut, Type II
0380	907-823-B003		78	Linear Feet	Saw Cut, Type III
0390	907-824-A003		6	Square Feet	General Epoxy Repair
0400	907-824-C001		5	Each	Cap Cleaning
0410	907-824-PP003		912	Square Feet	Bridge Repair, Removal of Bridge Deck, Per Plans
0420	907-824-PP004		1	Lump Sum	Bridge Repair, Bank Reshaping, Per Plans
0430	907-824-PP006		21	Each	Bridge Repair, Extend Pile Encasements, Per Plans
0440	907-824-PP008		2,925	Linear Feet	Bridge Repair, Class 2 Spray Finish, Per Plans
0450	907-824-PP008		79	Linear Feet	Bridge Repair, Endwall Repair, Per Plans



TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)



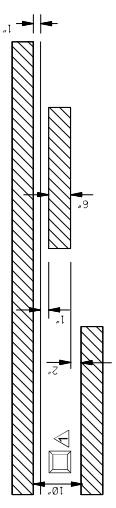
NOTE: THE CRITERIA FOR NO-PASSING ZONES CAN BE FOUND IN THE MOOT ROADWAY DESIGN MANUAL, SECTION 11-1.81.

GENERAL NOTES:

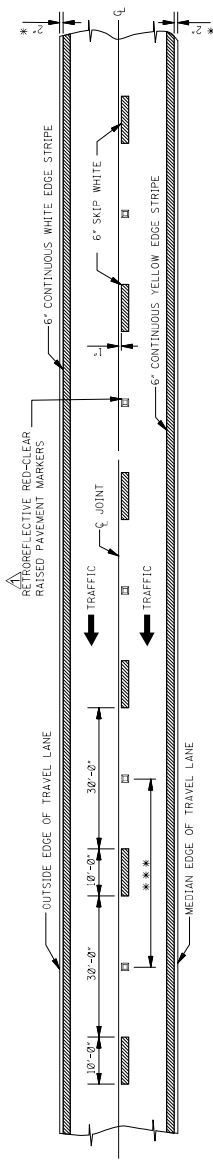
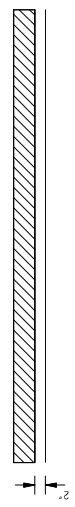
- * 1. 2' UNLESS SHOWN ELSEWHERE ON THE PLANS, FOR STRIPING ON RUMBLE STRIP SECTIONS REFER TO WK. SHEETS RS-1, RS-2, AND RS-3.
- ** 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR PLASTIC AS INDICATED IN PAY ITEMS).
- *** 3. SPACING OF RETROREFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

TANGENT SECTIONS	URBAN AREA	RURAL AREA
HORIZONTAL CURVES	40'-0"	80'-0"
INTERCHANGE LIMITS	40'-0"	+ 40'-0"

- † NOTE: ON THE MAIN FACILITY, RETROREFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON ALL THROUGH INTERCHANGES. RETROREFLECTIVE RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON 500'-0" LONG RAMP INTERCHANGES. RETROREFLECTIVE RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON RAMP AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
- △ 4. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RETROREFLECTIVE *APPROVED PRODUCTS LIST*.



DETAIL "A"



4-LANE WITH ONE-WAY TRAFFIC

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

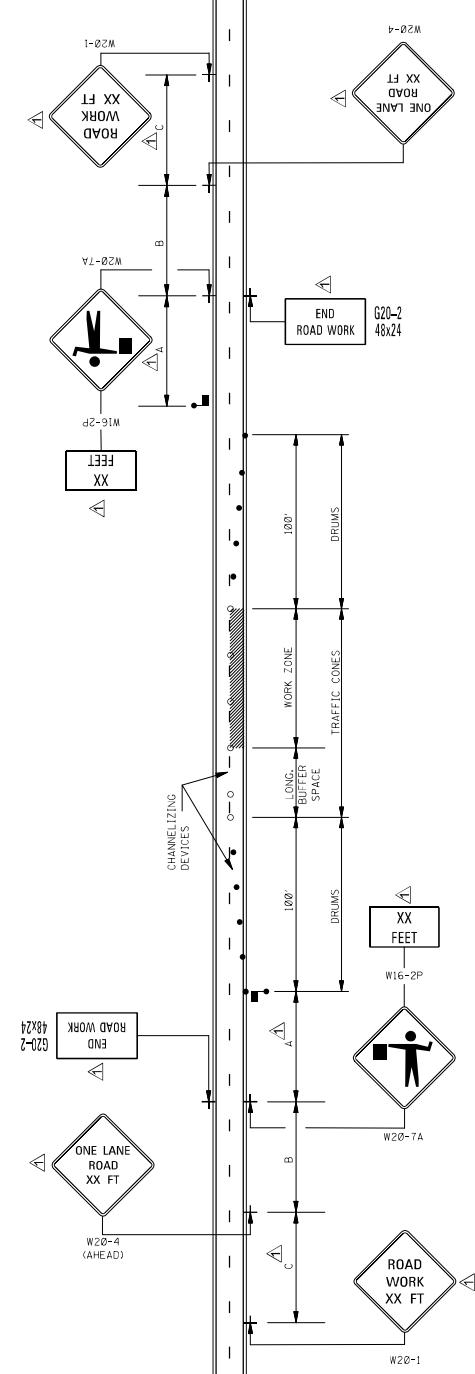
**PAVEMENT MARKING
DETAILS FOR
2-LANE AND 4-LANE
DIVIDED ROADWAYS**

DATE	REVISION
BY	REVISE REQS AND TEXT
STN	

ISSUE DATE: AUGUST 01, 2017

WORKING NUMBER
PM-1

SHEET NUMBER
6051



DISTANCE BETWEEN SIGNS

ROAD TYPE	A	B	C
URBAN (35 MPH OR LESS)	100 FT.	100 FT.	100 FT.
URBAN (40 - 70 MPH)	350 FT.	350 FT.	350 FT.
RURAL	500 FT.	500 FT.	500 FT.
EXPRESSWAY / FREEWAY	1000 FT.	1500 FT.	2640 FT.

- LEGEND
- FLAGGER
 - RETROREFLECTIVE FREE-STANDING PLASTIC DRUMS
 - TRAFFIC CONES (28" HEIGHT MINIMUM)

GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE. FLAGGER STATIONS SHALL BE LOCATED SUCH THAT APPROACH AND EXIT TAPPERS ARE LOCATED AT THE STOP. STOP VALUES IN STOPPING SIGHT DISTANCE COLUMN MAY BE USED AS A MINIMUM FOR THIS DISTANCE.
2. ALL CHANNELIZING DEVICES SHALL BE A MINIMUM OF 28" IN HEIGHT.
3. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 36" X 36" AND BLACK COPY ON FLUORESCENT ORANGE SHEETING.
4. WHEN WORK ZONE IS NO LONGER NEEDED, ALL SIGNS SHALL BE COVERED OR REMOVED AND ALL CHANNELIZING DEVICES SHALL BE MOVED TO THE SHOULDER EDGE.
5. ADDITIONAL FLAGGERS MAY BE NEEDED AS DIRECTED BY THE ENGINEER.
6. WHEN WORK IS REQUIRED AT NIGHT, FLAGGER STATIONS SHALL BE ILLUMINATED EXCEPT IN EMERGENCIES.
7. CHANNELIZING DEVICE TYPES FOR APPROACH AND EXIT TAPPERS:
 - a. ALONG LANE LINE AND WORK ZONE- TRAFFIC CONES (28" HEIGHT)
 - b. ALONG LANE LINE AND WORK ZONE- RETROREFLECTIVE PLASTIC DRUMS
8. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
9. AN OPTIONAL FLAGGER (W20-7A) WORD MESSAGE IS SHOWN IN THE "STANDARD HIGHWAY SIGNS" PUBLICATION.

POSTED SPEED AND/OR DECISION SPEED AND/OR ANTICIPATED OPERATING SPEED mph	MAXIMUM CHANNELIZING DEVICES (ft)		LONGITUDINAL BUFFER SPACE (ft)	STOPPING SIGHT DISTANCE
	TAPER	ALONG LANE LINE & WORK ZONE		
25	20	50	55	155
30	20	60	85	200
35	20	70	120	250
40	20	80	170	305
45	20	90	220	360
50	20	100	280	425
55	20	110	335	495
60	20	120	415	570
65	20	130	485	645

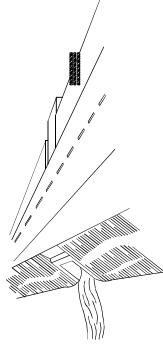
* NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**TRAFFIC CONTROL PLAN
WITH FLAGGER
(ONE-LANE CLOSURE OF
TWO-WAY TRAFFIC)**

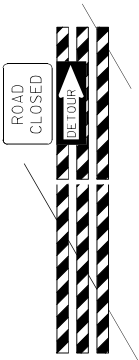
DATE	ISSUE DATE:	AUGUST 01, 2017
BY	REVISION	
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WORKING NUMBER: TCP-1
SHEET NUMBER: 6331



WING BARRICADES

- WING BARRICADES ARE TYPE III BARRICADES ERECTED ON THE SHOULDER OF A NARROWING OR RESTRICTED ROADWAY. WING BARRICADES MAY BE USED AS A MOUNTING FOR THE ADVANCE WARNING SIGNS OR FLASHERS.
 - IN ADVANCE OF A CONSTRUCTION PROJECT EVEN WHEN NO PART OF THE ROADWAY IS ACTUALLY CLOSED.
 - IN ADVANCE OF ALL BRIDGE OR CULVERT WIDENING OPERATIONS.
- WING BARRICADES SHOULD BE USED:
 - IN ADVANCE OF A CONSTRUCTION PROJECT EVEN WHEN NO PART OF THE ROADWAY IS ACTUALLY CLOSED.
 - IN ADVANCE OF ALL BRIDGE OR CULVERT WIDENING OPERATIONS.

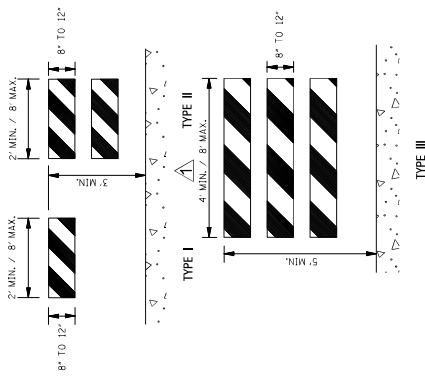


BARRICADE CLOSING A ROAD

BARRICADE CHARACTERISTICS

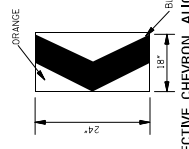
	I	II	III
WIDTH OF RAIL **	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.
LENGTH OF RAIL **	24" MIN. / 96" MAX.	24" MIN. / 96" MAX.	48" MIN. / 96" MAX.
WIDTH OF STRIPE *	6"	6"	6"
HEIGHT	36" MIN.	36" MIN.	60" MIN.
NUMBER OF RETROREFLECTORIZED RAIL FACES	2 (ONE EACH DIRECTION)	4 (TWO EACH DIRECTION)	3 IF FACING TRAFFIC IN ONE DIRECTION 6 IF FACING TRAFFIC IN TWO DIRECTIONS

- * 1. FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.
- ** 2. BARRICADES INTENDED FOR USE ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH SPEED ROADWAYS SHALL HAVE A MINIMUM OF 210 SQ. FT. OF REFLECTIVE AREA FACING TRAFFIC.



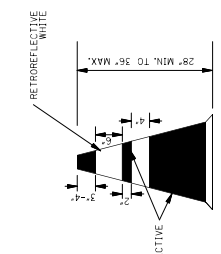
STANDARD BARRICADES

- THE RETROREFLECTIVE MARKING FOR BARRICADE RAILS SHALL BE ORANGE AND WHITE (SLOPING DOWNWARD AT AN ANGLE OF 45° IN THE DIRECTION THAT TRAFFIC IS TO PASS).
- RAIL STRIPE SHOULD BE 6 INCHES, EXCEPT THAT 4-INCH WIDE STRIPES MAY BE USED IF RAIL LENGTHS ARE LESS THAN 36 INCHES.
- DO NOT PLACE SANDBAGS OR OTHER DEVICES ON BARRICADE RAILS TO PROVIDE MASS OR BALLAST. HOWEVER, BALLAST MAY BE PLACED AS BALLAST TO THE LOWER PARTS OF THE FRAME OR THE STAYS TO BARRICADES.
- FOR ADDITIONAL INFORMATION OR DETAILS, SEE NOTED LATEST EDITION.
- BARRICADES ARE CLASSIFIED BY FHWA AS CATEGORY II WORK ZONE DEVICES. A LIST OF CRASHWORTHY BARRICADES AND OTHER CATEGORY II DEVICES CAN BE FOUND ON FHWA'S WEBSITE: <http://highways.dot.gov/safety/rwd/reduce-crash-severity>
- WHERE ROAD USERS INCLUDE PEDESTRIANS, THE PROVISION OF SUPPLEMENTAL AUDIBLE INFORMATION OR DETECTABLE BARRIERS OR BARRICADES SHOULD BE PROVIDED FOR PEOPLE WITH VISION DISABILITIES.
- BARRICADE RAIL SUPPORTS SHOULD NOT PROJECT INTO PEDESTRIAN CIRCULATION ROUTES MORE THAN 4 INCHES FOR THE SUPPORTS LOCATED BETWEEN 27 INCHES TO 48 INCHES ABOVE THE EXISTING SURFACE.

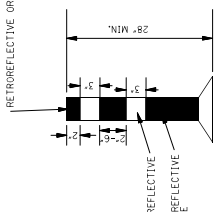


RETROREFLECTIVE CHEVRON ALIGNMENT SIGN (W1-8)

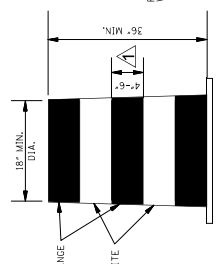
- CHEVRON SIGN CONSISTS OF A BLACK CHEVRON TYPE MARKING ON AN ORANGE BACKGROUND AND SHALL POINT IN THE DIRECTION OF TRAFFIC FLOW.
- THE CHEVRON SIGN SHALL BE MOUNTED ON CRASHWORTHY SUPPORT.
- CHEVRON SIGNS MAY BE USED TO SUPPLEMENT OTHER STANDARD DEVICES WHERE ONE OR MORE LANES ARE CLOSED FOR CONSTRUCTION OR MAINTENANCE. THEY SHOULD BE PLACED APPROXIMATELY 2'-8" BEHIND THE LANE TRANSITION STRIPE.
- CHEVRON SIGNS SHALL BE INSTALLED AT A MINIMUM HEIGHT OF 4 FEET MEASURED VERTICALLY FROM THE BOTTOM OF THE SIGN TO THE ELEVATION OF THE NEAR EDGE OF THE TRAVELWAY.



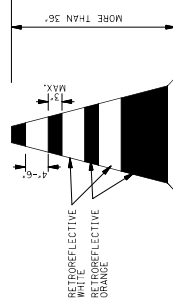
PLASTIC CONE DETAIL (28 - 36")



TUBULAR MARKER DETAIL



PLASTIC DRUM DETAIL



PLASTIC CONE DETAIL (36" OR GREATER)

- PLASTIC DRUMS, TUBULAR MARKERS, OR PLASTIC CONES SHALL BE ON, AND USED AS AN EXPEDITIOUS FORWARD TRAFFIC CHANNELIZATION. THE COLOR AND MARKING OF THE DEVICE USED SHALL BE CONSISTENT WITH MARKING STANDARDS FOR BARRICADE. THE PREDOMINANT COLOR ON THE DEVICE USED SHALL BE ORANGE WITH RETROREFLECTIVE, HORIZONTAL, CIRCUMFERENTIAL WHITE STRIPES AS SHOWN ON THE DETAILS.
- DRUMS, TUBULAR MARKERS, OR PLASTIC CONES SHOULD NEVER BE PLACED IN THE ROADWAY WITHOUT WARNING SIGNS.
- WHERE PRACTICAL, PLASTIC DRUMS, TUBULAR MARKERS, OR PLASTIC CONES SHOULD BE PLACED NO CLOSER THAN 3'-8" FROM THE EDGE OF TRAVELED LANE.
- BALLAST SHALL NOT BE PLACED ON THE TOP OF THE DRUM.

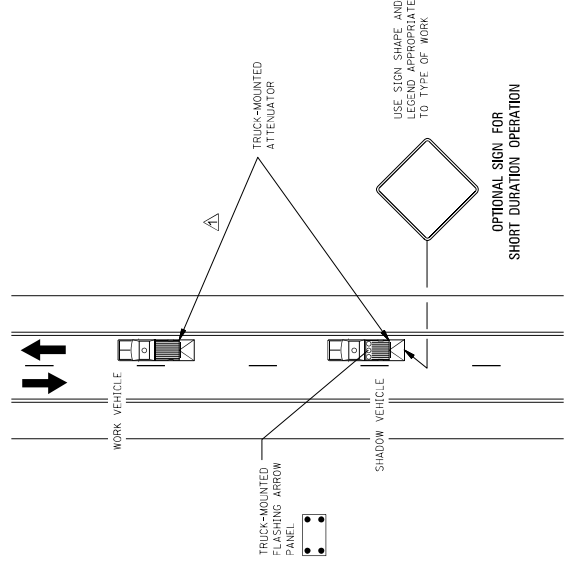
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

HIGHWAY SIGN AND BARRICADE DETAILS FOR CONSTRUCTION PROJECTS

NO.	DATE	BY	CHKD.	REVISION
1				REVISED TEXT AND DETAILS
2				
3				
4				

WORKING NUMBER: TCP-8
SHEET NUMBER: 6358
ISSUE DATE: AUGUST 01, 2017

MOBILE OPERATIONS ON TWO-LANE ROAD

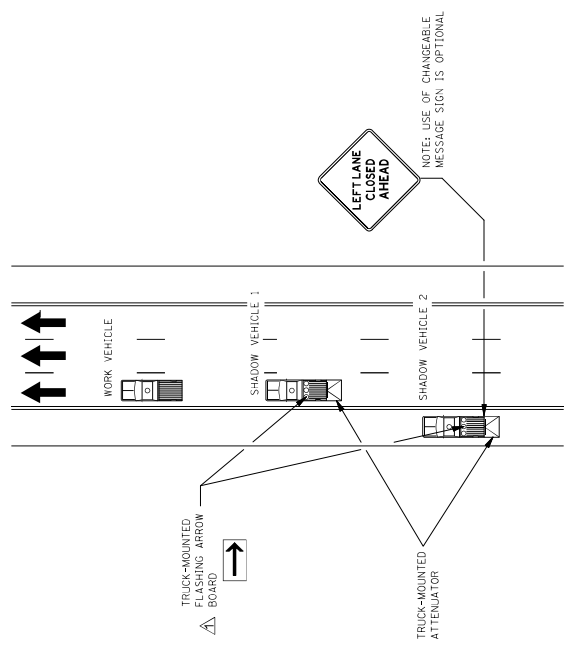


MOBILE OPERATIONS ON TWO-LANE ROAD

NOTES FOR TWO-LANE OPERATION:

- WHERE PRACTICAL AND WHEN NEEDED, THE WORK AND SHADOW VEHICLES SHOULD BE MOVED PERIODICALLY TO ALLOW TRAFFIC TO PASS. IF THIS CAN NOT BE DONE FREQUENTLY AS AN ALTERNATIVE, A "DO NOT PASS" SIGN MAY BE PLACED ON THE REAR OF THE VEHICLE BLOCKING THE LANE.
- THE DISTANCE BETWEEN THE WORK AND SHADOW VEHICLES MAY VARY ACCORDING TO THE TYPE OF ROADWAY. THE SHADOW VEHICLE SHOULD BE PLACED TO WARN TRAFFIC OF THE OPERATION AHEAD, WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR. THE SHADOW VEHICLE SHOULD MAINTAIN THE MINIMUM DISTANCE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. THE SHADOW VEHICLE SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- ADDITIONAL SHADOW VEHICLES TO WARN AND REDUCE THE SPEED OF ONCOMING OR OPPOSING TRAFFIC MAY BE USED. POLICE PATROL CARS MAY BE USED FOR THIS PURPOSE.
- A TRUCK-MOUNTED ATTENUATOR (TMA) SHOULD BE USED ON THE SHADOW VEHICLE AND MAY BE USED ON THE WORK VEHICLE.
- VEHICLES SHALL BE EQUIPPED WITH TWO HIGH-INTENSITY FLASHING LIGHTS MOUNTED ON THE REAR, ADJACENT TO THE SIGN, SHADOW AND WORK VEHICLES SHALL DISPLAY FLASHING OR ROTATING BEACONS BOTH FORWARD AND TO THE REAR.
- VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBTURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
- ARROW BOARD TO BE USED IN CAUTION MODE.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

MOBILE OPERATIONS ON MULTILANE ROAD

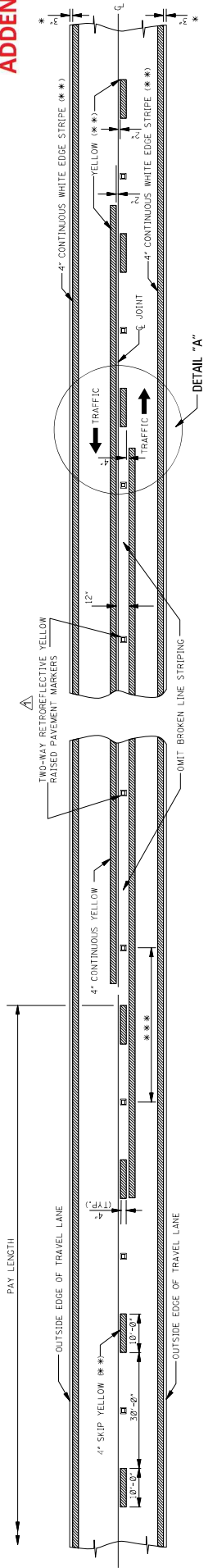


MOBILE OPERATIONS ON MULTILANE ROAD

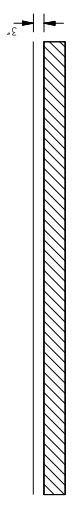
NOTES FOR MULTILANE LANE OPERATION:

- SHADOW AND WORK VEHICLES SHALL DISPLAY HIGH-INTENSITY FLASHING LIGHTS ON THE REAR. THE SHADOW VEHICLE SHALL BE EQUIPPED WITH AN ARROW BOARD THAT IS USED WHEN THE FREEMWAY LANE IS CLOSED. WHEN MORE THAN ONE FREEMWAY LANE IS CLOSED, A SEPARATE ARROW BOARD SHALL BE USED FOR EACH CLOSED LANE.
- SHADOW VEHICLE 2 SHOULD BE EQUIPPED WITH AN ARROW BOARD AND TRUCK MOUNTED ATTENUATOR (TMA). AN APPROPRIATE LANE CLOSURE SIGN SHOULD BE PLACED ON SHADOW VEHICLE 2 SO AS NOT TO OBTURD THE ARROW BOARD.
- SHADOW VEHICLE 1 SHOULD BE EQUIPPED WITH AN ARROW BOARD AND TRUCK-MOUNTED ATTENUATOR (TMA).
- SHADOW VEHICLE 2 SHOULD TRAVEL AT A VARYING DISTANCE FROM SHADOW VEHICLE 1 TO MAINTAIN ADEQUATE SIGHT DISTANCE FOR TRAFFIC APPROACHING FROM THE REAR.
- THE SPACING BETWEEN THE WORK VEHICLES AND THE SHADOW VEHICLES, AND BETWEEN EACH SHADOW VEHICLE, SHOULD BE MINIMIZED TO DETER ROAD USERS FROM DRIVING IN BETWEEN.
- ON HIGH-SPEED ROADWAYS, A THIRD SHADOW VEHICLE (NOT SHOWN) MAY BE USED WITH SHADOW VEHICLE 1 IN THE CLOSED LANE, SHADOW VEHICLE 2 STRADDLING THE EDGE LINE, AND SHADOW VEHICLE 3 ON THE SHOULDER.
- ARROW BOARD SHALL BE AS A MINIMUM TYPE B, 60" X 30", IN ACCORDANCE WITH THE CRITERIA PRESENTED IN THE MUTCD.
- WORK SHOULD NORMALLY BE DONE DURING OFF-PEAK HOURS.
- VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBTURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
- WHERE PRACTICAL AND WHEN NEEDED, THE WORK AND SHADOW VEHICLES SHOULD BE MOVED PERIODICALLY TO ALLOW TRAFFIC TO PASS. IF THIS CAN NOT BE DONE FREQUENTLY AS AN ALTERNATIVE, A "DO NOT PASS" SIGN MAY BE PLACED ON THE REAR OF THE VEHICLE BLOCKING THE LANE.
- THE DISTANCE BETWEEN THE WORK AND SHADOW VEHICLES MAY VARY ACCORDING TO THE TYPE OF ROADWAY. THE SHADOW VEHICLE SHOULD BE PLACED TO WARN TRAFFIC OF THE OPERATION AHEAD, WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR. THE SHADOW VEHICLE SHOULD MAINTAIN THE MINIMUM DISTANCE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. THE SHADOW VEHICLE SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- ADDITIONAL SHADOW VEHICLES TO WARN AND REDUCE THE SPEED OF ONCOMING OR OPPOSING TRAFFIC MAY BE USED. POLICE PATROL CARS MAY BE USED FOR THIS PURPOSE.
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- VEHICLES SHALL BE EQUIPPED WITH TWO HIGH-INTENSITY FLASHING LIGHTS MOUNTED ON THE REAR, ADJACENT TO THE SIGN, SHADOW AND WORK VEHICLES SHALL DISPLAY FLASHING OR ROTATING BEACONS BOTH FORWARD AND TO THE REAR.
- VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBTURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
- ARROW BOARD TO BE USED IN CAUTION MODE.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
ROADWAY DESIGN DIVISION	
STANDARD PLAN	
TRAFFIC CONTROL PLAN	
MOBILE OPERATIONS	
MULTILANE ROADS	
AND	
TWO-LANE ROADS	
WORKING NUMBER	ICP-9
SHEET NUMBER	6359
ISSUE DATE:	AUGUST 01, 2017
BY	REVISION
DATE	REVISION
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TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)

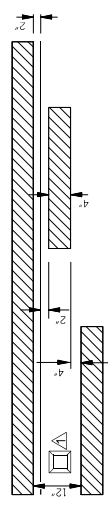
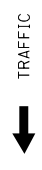


GENERAL NOTES:

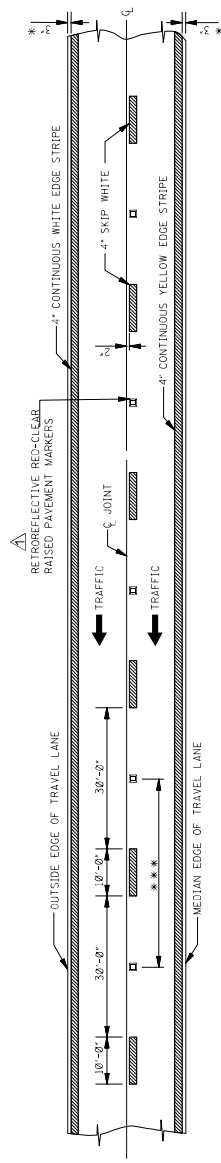
- * 1. 3" UNLESS SHOWN ELSEWHERE ON THE PLANS.
- ** 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR TAPE AS INDICATED IN PAY ITEMS).
- ▲ 3. RETROREFLECTIVE RAISED PAVEMENT MARKERS TO BE USED IF TEMPORARY MARKINGS ARE TO REMAIN IN PLACE OVER 3 MONTHS.
- ▲▲▲ 4. SPACING OF RETROREFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

TANGENT SECTIONS	URBAN AREA (F+H)	RURAL AREA (F+H)
HORIZONTAL CURVES	40'-0"	80'-0"
INTERCHANGE LIMITS	40'-0"	+ 40'-0"

- ▲▲ NOTE: ON THE MAIN FACILITY, RETROREFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINES THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP AND ENDING 1000' BEFORE THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
- ▲ 5. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RETROREFLECTIVE POLYMER BEAD TYPE MARKERS FROM THE MOST APPROVED PRODUCTS LIST.



DETAIL "A"



4-LANE WITH ONE-WAY TRAFFIC

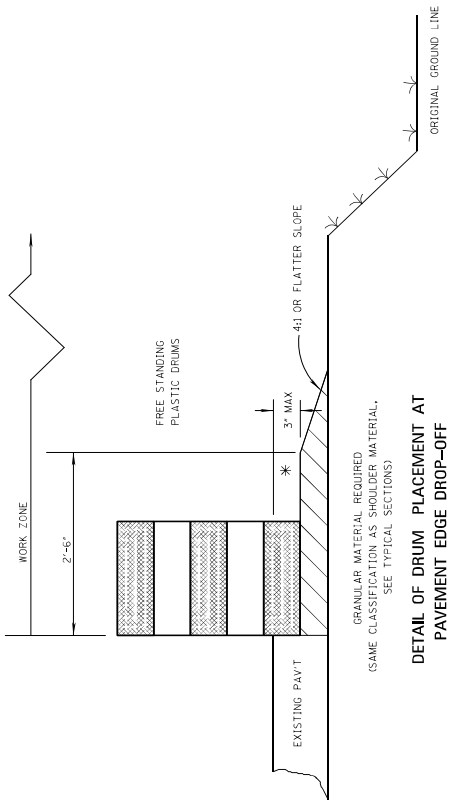
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

TEMPORARY STRIPING FOR TRAFFIC CONTROL 2-LANE AND 4-LANE DIVIDED HIGHWAYS

WORKING NUMBER: TCP-13
SHEET NUMBER: 63/63

ISSUE DATE: AUGUST 01, 2017

NO.	DATE	BY	REVISION
1			REVISED TEXT



DETAIL OF DRUM PLACEMENT AT PAVEMENT EDGE DROP-OFF

NOTES:

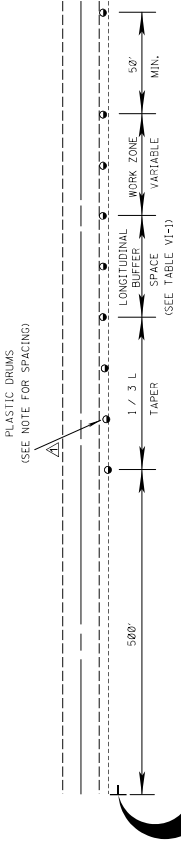
- * A. PAVEMENT EDGE DROP-OFF
 1. IF LESS THAN TWO AND ONE QUARTER (2.25) INCHES-NO PROTECTION REQUIRED. PLACE A SHOULDER WORK SIGN (W21-5) 500 FEET IN ADVANCE OF WORK ZONE SHOULDER AND A LOW SHOULDER SIGN (WB-9) AT THE BEGINNING AND THROUGHOUT THE WORK ZONE @ (750'±) O.C.
 2. TWO AND ONE QUARTER TO THREE INCHES-PLACE DRUMS, VERTICAL PANELS OR BARRICADES EVERY 100 FEET ON TANGENT SECTIONS FOR SPEEDS OF 50 MILES PER HOUR OR GREATER. CONES MAY BE USED IN PLACE OF DRUMS, PANELS, AND BARRICADES DURING DAYLIGHT HOURS. FOR TANGENT SECTIONS WITH SPEEDS LESS THAN 50 MILES PER HOUR AND FOR CURVES, DEVICES SHOULD BE PLACED EVERY 50 FEET. SPACING FOR TAPERS SHOULD BE IN ACCORDANCE WITH THE MUTCD (1 / 3 L, WHERE L IS THE TAPER LENGTH IN FEET).
 3. GREATER THAN THREE (3) INCHES-POSITIVE SEPARATION OR WEDGE WITH 4:1 OR FLATTER SLOPE NEEDED. IF THERE IS EIGHT (8) FEET OR MORE DISTANCE BETWEEN THE EDGE OF TRAVEL LANE AND DROP-OFF, THEN DRUMS, PANELS OR BARRICADES MAY BE USED.
 4. FOR TEMPORARY CONDITIONS, DROP-OFFS GREATER THAN THREE (3) INCHES MAY BE PROTECTED WITH DRUMS, VERTICAL PANELS OR BARRICADES FOR SHORT DISTANCES DURING DAYLIGHT HOURS WHILE WORK IS BEING DONE IN THE DROP-OFF AREA.
 5. LESSEER TREATMENTS THAN THOSE DESCRIBED ABOVE MAY BE CONSIDERED FOR LOW-VOLUME LOCAL STREETS.
- △ B. DRUM, VERTICAL PANEL, OR BARRICADE SPACING
 1. TANGENTS = 2 X S
 2. TAPERS = L / 3
 - WHERE L = L / 3
 - WHERE L = S X W
 - L = TAPER LENGTH IN FEET
 - S = SPEED IN MPH (POSTED OR ANTICIPATED OPERATING SPEED)
 - W = WIDTH OF OFFSET IN FEET

BY	DATE	REVISION

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

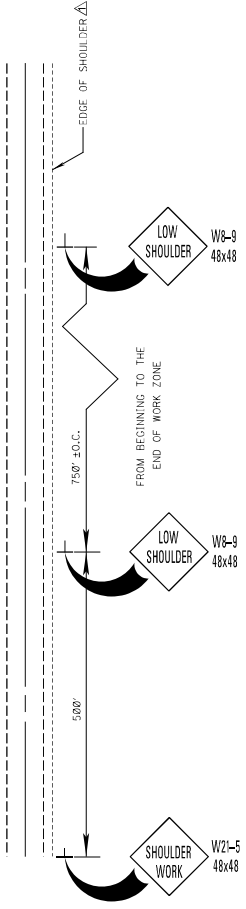
**TRAFFIC CONTROL DETAILS
DRUM PLACEMENT
AND
SHOULDER CLOSURE**

ISSUE DATE: AUGUST 01, 2017
SHEET NUMBER 63/66

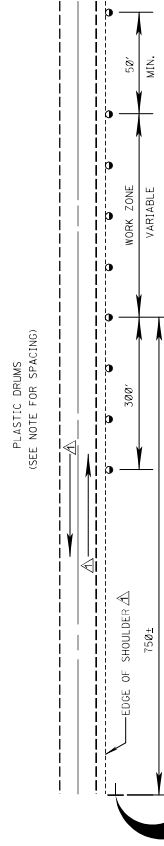


TYPICAL SHOULDER CLOSURE

- (1) TO BE USED WITH EIGHT (8) FOOT OR GREATER WIDTH IMPROVED SHOULDER.
- (2) TO BE USED WHEN CONSTRUCTION VEHICLES EQUIPMENT ENCLOSES ON OR WITHIN TWO (2) FEET OF THE SHOULDER BREAK.
- △ (3) FOR SHORT DURATION OPERATIONS OF 60 MINUTES OR LESS, ALL SIGNS AND CHANNELIZING DEVICES MAY BE ELIMINATED IF A VEHICLE WITH ACTIVATED HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS IS USED.



**TYPICAL SHOULDER WORK #1
(SEE NOTE A-1 THIS SHEET)**



TYPICAL SHOULDER WORK #2

- NOTE:
WORK OUTSIDE TWO (2) FOOT AND WITHIN TEN (10) FEET OF THE SHOULDER BREAK MAY BE PROTECTED BY PLACING DRUMS ALONG THE SHOULDER EDGE, 300 FEET PRIOR TO AND 50 FEET BEYOND THE WORK AREA, OR SEE NOTE A-3 THIS SHEET.

TABLE VI-1. GUIDELINES FOR LENGTH OF LONGITUDINAL BUFFER SPACE

PERCENTILE SPEED (MPH)	PERCENTILE SPEED (MPH)	PERCENTILE SPEED (MPH)
20	115	115
25	155	155
30	200	200
35	250	250
40	305	305
45	360	360
50	425	425
55	500	500
60	575	575
65	645	645

* POSTED SPEED, OFF-PEAK. *5 PERCENTILE SPEED (MPH) *60 PERCENTILE SPEED (MPH) *75 PERCENTILE SPEED (MPH)



ADDENDUM

DESIGNED BY: ALEX HAWKINS
 CHECKED BY: BARRETT GERMOND
 DATE: 2025-11-12

FMS CON: 109672/301000
 PROJECT NO.: STBG-0044-01(036)
 COUNTY: UNION

BRIDGE AT STA. 61+58.83
 DETAILED INDEX (BRIDGE)
 DEP. DIR OF STRUCTURES, STATE BRIDGE ENGINEER - SCOTT WESTERLO, P.E.
 MICHAEL BOWEN, P.E.

WK. NO.
 DL-BR-1
 SHEET NO.
 8001

DESCRIPTION OF SHEETS	WORKING NUMBER	SHEET NUMBER
DETAILED INDEX (BRIDGE)	D1-BR-1	8002
SR 30 OVER TALLAHATCHIE RIVER BRIDGE AT STA. 61+58.83	1 OF 7	8002
ESTIMATED QUANTITIES AND GENERAL NOTES	2 OF 7	8003
HYDRODEMOLITION DETAILS	3 OF 7	8004
PLAN OF HYDRODEMOLITION	4 OF 7	8005
END WALL REPAIR DETAILS	5 OF 7	8006
JOINT REPAIR DETAILS	6 OF 7	8007
PILE REPAIR DETAILS	7 OF 7	8008
APRAPH PLACEMENT DETAILS	*****	8009 - 8023
INFORMATION PLANS		

BRIDGE DIVISION		
REVISIONS		
DATE	SHEET NO.	DESCRIPTION BY
01/20/2026	8002	Revised quantities and epoxy repair locations AWH
01/20/2026	8007	Revised encasement locations, details, and notes AWH



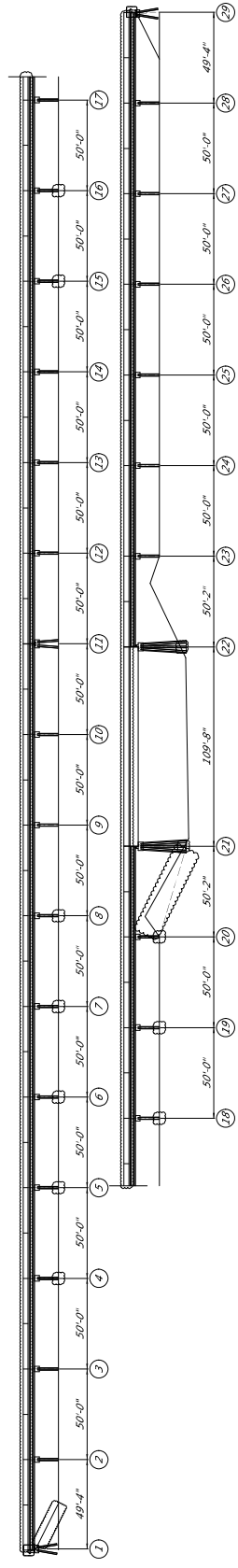
APPENDUM

DESIGNED BY: ALEX HAWKINS
 CHECKED BY: BARRETT GERMOND
 DATE: 2025-11-12

FMS CON: 109672/301000
 PROJECT NO.: STBG-0044-01(036)
 COUNTY: UNION

BRIDGE AT STA. 61+58.83
 ESTIMATED QUANTITIES
 AND GENERAL NOTES

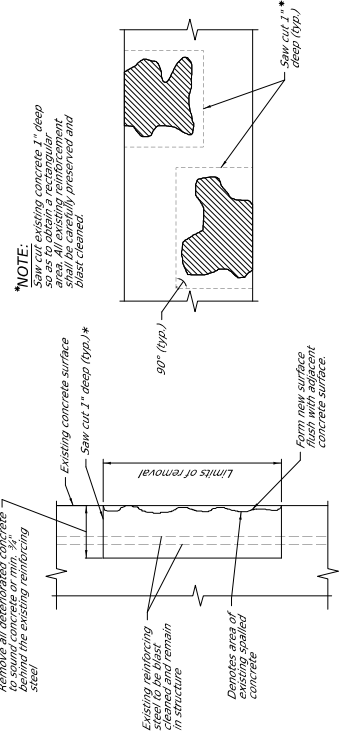
WK. NO. 1 OF 7
 SHEET NO. 8002



ELEVATION OF BRIDGE 14809 (19.0)
 Showing limits of work

PAY ITEM NO.	DESCRIPTION	UNIT	QUANTITY
815-A009	Losses Riprap, Size 300	TON	1922
815-E001	Grass/Gravel Under Riprap	SY	1268
907-202-8001	Removal of Bridge Deck, Hydrodemolition	SY	5931
907-220-A001	Undersealing	LBS	2000
907-804-C001	Bridge Deck Overlay Concrete	CY	330
907-808-A002	Joint Repair	LF	548
907-823-A002	Preformed Joint Seal, Type II	LF	235
907-823-A003	Preformed Joint Seal, Type III	LF	39
907-823-B002	Saw Cut, Type II	LF	470
907-823-B003	Saw Cut, Type III	LF	78
907-824-C001	General Epoxy Repair	SF	6
907-824-C002	Cap Cleaning	EA	5
907-824-P001	Bridge Repair, Removal of Bridge Deck, Per Plans	SF	912
907-824-P004	Bridge Repair, Bank Reshaping, Per Plans	LS	1
907-824-P006	Bridge Repair, Extend Pier Encasements, Per Plans	EA	21
907-824-P008	Bridge Repair, Endwall Repair, Per Plans	LF	79
907-824-P009	Bridge Repair, Class 2 Spray Finish, Per Plans	LF	2923

1" SAWCUT & REINFORCEMENT NOTES:
 1. All 1" sawcuts shall be considered an abraded item of work. The Contractor shall verify depth of reinforcing steel before making any cut of the reinforcing steel.
 2. All existing reinforcement shall remain in place. Any damage to reinforcement shall be repaired to the satisfaction of the Engineer at no cost to the State.



***NOTE:**
 Saw cut existing concrete 1" deep so as to obtain a rectangular area of the reinforcing steel. This sawcut shall be carefully preserved and blast cleaned.

Remove all deteriorated concrete behind the existing reinforcing steel

Existing reinforcing in structure

Existing concrete surface

Limits of removal

90° (typ)

Saw cut 1" deep (typ)

Form new surface flush with adjacent concrete surface

Denotes area of existing spalled concrete

DISTURBANCE NOTE:
 Any surrounding area that is disturbed in order to accomplish the work described herein shall be restored to its original condition. There will be no separate payment for this work.

SPECIAL PROVISIONS REQUIRED:
 907-202: Removal of Bridge Deck, Hydrodemolition
 907-804: Bridge Deck Overlay Concrete
 907-808: Joint Repair
 907-823: Preformed Joint Seal
 907-823-B: Reinforced Joint Seal

TEMPORARY PRECAST BARRIER NOTE:
 The Contractor shall submit proposed anchor details, including design calculations stamped by a Mississippi registered Professional Engineer, prior to beginning work to be approved by the Director of Structures, State Bridge Engineer.

EPOXY MORTAR SPALL REPAIR DETAILS

EPOXY MORTAR REPAIR NOTES:
 General Epoxy Repair shall be done in accordance with Special Provision Subsection 907-824.03.1 and with the approved materials outlined in Special Provision Subsection 907-824.03.2. All work and material required to perform this item of work shall be paid for under Pay Item No. 907-824-A003 General Epoxy Repair.
 It should be noted that hammers used for concrete removal should be limited to 15 lbs. for box girder or prestressed beams and 30 lbs. for all other epoxy repairs.
 This item shall be bid such that this item may be increased, decreased, or eliminated as directed by the Project Engineer.
 Total quantity increased to be used at the direction of the Project Engineer.

CONTRACTOR SUBMITTALS
 All dimensions of the existing structure and clearances shall be field verified by the Contractor. The Contractor shall be responsible for adjusting the work items necessary to perform hydrodemolition prior to beginning work associated with the work items described herein. This shall include, but is not limited to:
 a. Finish grades of existing bridge.
 b. Existing open joint widths.
 c. Any other element that will affect the work items described herein.

FIELD VERIFICATION SUBMITTAL
 The Contractor shall submit verification of the existing bridge elements associated with the work items described herein. This shall include, but is not limited to:
 a. Finish grades of existing bridge.
 b. Existing open joint widths.
 c. Any other element that will affect the work items described herein.

HYDRODEMOLITION PLAN SUBMITTAL
 The Contractor shall submit a proposed hydrodemolition plan associated with the work items necessary to perform hydrodemolition prior to beginning work to be approved by the Director of Structures, State Bridge Engineer.

CONTAINMENT PLAN SUBMITTAL
 The Contractor shall submit a containment plan associated with the work items for approval.

TEMPORARY PRECAST BARRIER SUBMITTAL
 The Contractor shall submit anchor details, including design calculations stamped by a Mississippi registered Professional Engineer, prior to beginning work to be approved by the Director of Structures, State Bridge Engineer.

GENERAL SCOPE OF WORK
 1. Hydrodemolition
 2. Joint sealing
 3. End wall repair
 4. Underseal end bents and approaches
 5. Piece repair at Bent 2 in the footprint of bent
 6. Re-establish spall bank between Bents 20 and 21 and extend pile encasements
 7. Rolling Class 2 spray finish
 8. Cap cleaning at open bents

UNDERSEALING
 Underseal compound shall be installed in strict accordance with Manufacturer's instructions. All labor, preparations, and materials associated with filling the voids underneath the bent caps shall be included in this item. The contractor shall be responsible for the required properties in Special Provision Subsection 907-220.82. Prior to construction, the contractor shall be required to submit the Manufacturer's instructions.

GENERAL NOTES:
 No change of plans will be permitted except by written approval of the Director of Structures, State Bridge Engineer. Provided such changes will not be cause for contract price adjustment. When changes are made to the original contract price adjustment, the contractor shall be responsible for adjusting the elements of the new construction to ensure a proper fit with the existing structure.
 Any damage to the existing structure during the duration of the work shall be the responsibility of the Contractor at no additional cost to the State.

CAP CLEANING NOTE:
 Cap cleaning shall be done in accordance with Special Provision Subsection 907-824.03.3. This item of work shall be paid for under Pay Item No. 907-824-C001: Cap Cleaning.

INFORMATION PLANS
 The Contractor's request for information can be provided by Bridge Division.

LOCATION	QUANTITY (SQ)	REMARKS
Bent 4	1	Bottom of cap between beams 1 and 2
Bent 20	1	Pile 4
Bent 21	1	Brush guard
Bent 22	3	Phas 9 - 11

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APPENDUM

DESIGNED BY: ALEX HAWKINS
 CHECKED BY: BARRETT GERMOND
 DATE: 2025-11-12

PROJECT NO.: ST8G-0044-01(036)
 COUNTY: UNION

FMS CON: 109672/301000

BRIDGE AT STA. 61+58.83
 PILE REPAIR DETAILS

DEPT. OF STRUCTURES, STATE BRIDGE ENGINEER - SCOTT WESTERLO, P.E.
 MICHAEL DEW, P.E.

WK. NO. 6 OF 7
 SHEET NO. 8007

