

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO. <u> 1 </u>	DATED <u> 1/23/2026 </u>	ADDENDUM NO. <u> </u>	DATED <u> </u>
ADDENDUM NO. <u> 2 </u>	DATED <u> 1/29/2026 </u>	ADDENDUM NO. <u> </u>	DATED <u> </u>
ADDENDUM NO. <u> </u>	DATED <u> </u>	ADDENDUM NO. <u> </u>	DATED <u> </u>

Number	Description
1	Postponed til February 03, 2026 Letting; Amendment EBSx Download Required.
2	Revised Table of Contents; Revised Advertisement; Deleted NTB No. 5750; Revised NTB No. 7562; NTB No. 7624 Replaces NTB No. 3599; Revised Wage Rates; Revised Special Provision 907-618-4; Amendment EBSx Download Required.

TOTAL ADDENDA: 2
(Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

Contractor

BY _____
Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

President Address

Secretary Address

Treasurer Address

The following is my (our) itemized proposal.

STP-0050-01(043)/ 109755301000

Calhoun County(ies)

Revised 01/26/2016

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION
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PROJECT: STP-0050-01(043)/109755301 - Calhoun

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OF SECTION 905 AS ADDENDA)

01/28/2026 03:12 PM

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 901 - ADVERTISEMENT

Electronic bids will be received by the Mississippi Transportation Commission at 10:00 o'clock A.M., Tuesday, February 03, 2026, from the Bid Express Service and shortly thereafter publicly read in the Construction Division For:

Mill & Overlay approximately 12.4 miles of SR 9 from SR 9W to the Pontotoc County Line, known as Federal Aid Project No. STP-0050-01(043) / 109755301 in Calhoun County.

The attention of bidders is directed to the Contract Provisions governing selection and employment of labor. Minimum wage rates have been predetermined by the Secretary of Labor and are subject to Public Law 87-581, Work Hours Act of 1962, as set forth in the Contract Provisions.

The specifications are on file in the offices of the Mississippi Department of Transportation.

Contractors may request permission to bid online at <http://shop.mdot.ms.gov> at no cost. Upon approval, Contractors shall be eligible to submit a bid using Bid Express at <http://bidx.com>. Specimen proposals may be viewed and downloaded online at no cost at <http://mdot.ms.gov> or purchased online at <http://shop.mdot.ms.gov> at a cost of Ten Dollars (\$10.00) per proposal plus a small convenience fee. Cash or checks will not be accepted as payment.

Bid bond, signed or countersigned by a Mississippi Agent or Qualified Nonresident Agent, with Power of Attorney attached, a Cashier's check or Certified Check for five (5%) percent of bid, payable to STATE OF MISSISSIPPI, must accompany each proposal.

The attention of bidders is directed to the provisions of Subsection 102.07 pertaining to irregular proposals and rejection of bids.

BRAD WHITE
EXECUTIVE DIRECTOR

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 7562

CODE: (SP)

DATE: 1/29/2025

SUBJECT: SCOPE OF WORK

PROJECT: STP-0050-01(043) –109755/301000 – Calhoun County

The contract documents do not include an official set of construction plans but may, by reference; include some Standard Drawings when so specified in a Notice to Bidders entitled, “Standard Drawings”.

The work to be accomplished using the Pay Items and corresponding specifications set forth in this contract, which is for scrub sealing and overlay of SR 9, beginning at SR 9W (Sta. 51+75) and going northerly for approximately 12.4 miles to the Pontotoc County Line (Sta. 702+15).

It shall be the responsibility of the contractor to protect the roadway and all existing structures, such as bridges and curbs, from damage occurring as a result of the contractor’s operations. Damages to existing features caused by the contractor’s operations shall be repaired or replaced at no cost to the Mississippi Department of Transportation.

At bridge ends and at the end of workday, a taper of one (1) vertical inch for each three (3) horizontal foot shall be provided.

The contractor shall make a utility location request to 811 prior to any excavation, except for trench widening or pavement removal/repair.

In order to expedite the safe movement of traffic and to protect each phase of the work as it is performed, a firm sequence of operations is essential. The work shall begin and continually prosecuted.

The work shall consist of the following:

1. Random clearing shall be performed from the beginning of the project to the end of the project limits as directed by the Engineer. Following the MDOT Tree Management Policy, right-of-way shall be cleared from the edge of pavement or to the right-of-way line. Overhanging vegetation shall be trimmed to a minimum height of thirty feet (30’) above the ground elevation at the edge of the clearing limits. It is the intent of this Contract for vegetation and trees to be mulched in place at no more than 4” in depth to allow for full coverage of the ground. All trees that will result in a depth of more than 4” of coverage or affect drainage must be fully removed from the project site. This work shall be paid for under pay item 201-D: Random Clearing, per station. Each side of the roadway will be measured separately. Clearing within two feet (2’) of fences, utilities, and other obstructions as directed by the Project Engineer within the ROW is to be omitted in order to avoid damages.

If roadside foreslopes are steeper than 3:1 or include other roadway features that would make tree removal infeasible, risk assessment should be conducted by the Project Engineer and Highway Safety Division.

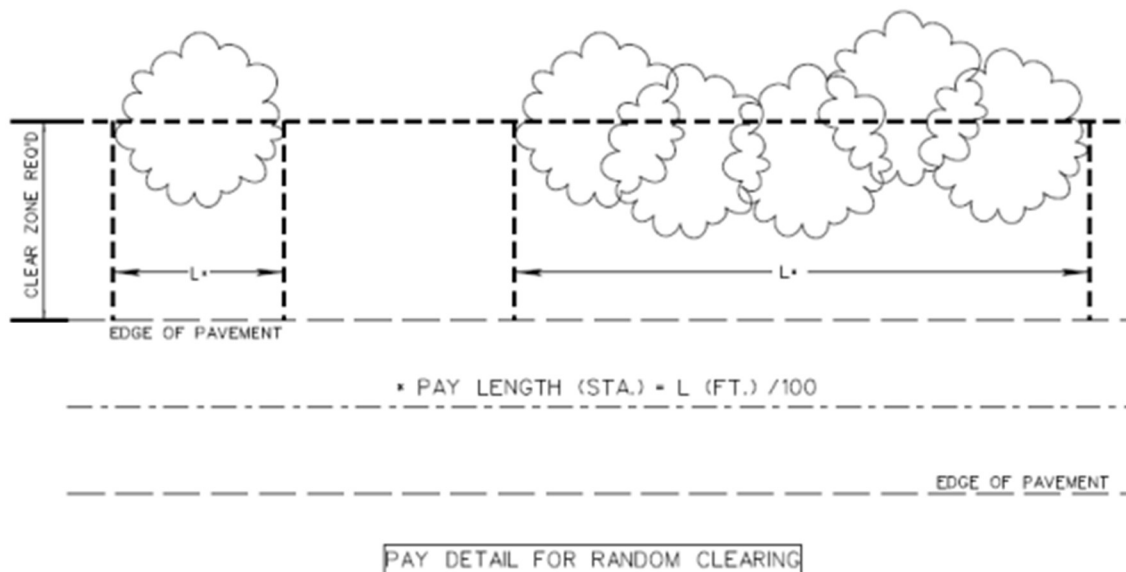
NOTE: No magnolia trees within the ROW limits are to be removed without prior approval from the Project Engineer.

NOTE: Due care should be taken to prevent damage in areas within the ROW that are outside the clearing limits as shown in the table. Any disturbed areas not shown above will not be measured for separate payment and shall be reestablished at no additional cost to the Department.

NOTE: Herbicide shall be used for sprout control of cut stumps. Paint or spray freshly cut stump surface thoroughly covering cambium area next to bark until the herbicide runs down around the root collar. Treat stump as soon as practical after cutting for more effective control but no later than day of cutting except when spraying must be postponed due to inclement weather. Pine stumps and all other stumps larger than 15 inches in diameter do not require spraying for control of sprouting. Permissible herbicides are 2,4-D (amine); picloram +2,4-D; ammonium sulfamate; and dicamba. Specific requirements such as mixing, diluting, rate, application, use restrictions, safety precautions, etc. will be in accordance with the manufacturer's printed container label.

NOTE: Re-spraying will be required when the herbicide is washed off by rain within eight hours of application or diluted to such an extent that it is rendered ineffective.

NOTE: Grinding of all stumps will be required to be flush with the ground.



2. Repair failed areas on SR 9 as needed using the following pay items:

- 202-B, Removal of Asphalt Pavement, All Depths
- 203-G, Excess Excavation – for material below the pavement structure
- 304-F, Crushed Stone Base – to be used as directed by the Project Engineer
- 907-403-A, 19mm, ST, Asphalt Pavement
- 503-C, Saw Cut, Full Depth

Northbound Lanes								
Station Start	Station End	Length (ft)	Asphalt (ft)	Width	Asphalt (SY)	Area	Saw (LF)	Cuts
69+50	70+25	75	13		108.3		101	
83+75	84+25	50	13		72.2		76	
106+90	107+50	60	13		86.7		86	
153+90	154+25	35	13		50.6		61	
178+15	178+75	60	13		86.7		86	
188+50	189+00	50	13		72.2		76	
203+90	204+50	60	13		86.7		86	
209+00	211+10	210	13		303.3		236	
230+85	231+15	30	13		43.3		56	
243+50	245+00	150	13		216.7		176	
247+65	247+85	20	13		28.9		46	
291+45	292+10	65	13		93.9		91	
315+15	316+00	85	13		122.8		111	
322+40	324+00	160	13		231.1		186	
351+45	352+00	55	13		79.4		81	
355+50	356+25	75	13		108.3		101	
363+25	363+75	50	13		72.2		76	
390+00	391+10	110	13		158.9		136	
392+29	393+25	96	13		138.7		122	
396+50	397+50	100	13		144.4		126	
421+00	423+00	200	13		288.9		226	
432+00	434+00	200	13		288.9		226	
436+00	437+10	110	13		158.9		136	
438+50	439+50	100	13		144.4		126	
441+50	442+25	75	13		108.3		101	
442+45	442+75	30	13		43.3		56	
443+25	444+50	125	13		180.6		151	
445+35	447+25	190	13		274.4		216	
457+00	466+00	900	13		1300.0		926	
495+00	497+00	200	13		288.9		226	
507+15	507+75	60	13		86.7		86	

511+80	512+20	40	13	57.8	66
535+90	536+10	20	13	28.9	46
543+25	547+75	450	13	650.0	476
548+10	548+20	10	13	14.4	36
553+75	555+75	200	13	288.9	226
582+00	583+00	100	13	144.4	126
607+00	609+00	200	13	288.9	226
625+75	626+25	50	13	72.2	76

Southbound Lanes								
Station Start	Station End	Length (ft)	Asphalt (ft)	Width	Asphalt (SY)	Area	Saw (LF)	Cuts
85+35	86+10	75	13		108.3		163	
86+80	87+00	20	13		28.9		53	
87+90	88+25	35	13		50.6		83	
177+50	178+00	50	13		72.2		113	
187+15	188+00	85	13		122.8		183	
189+50	189+65	15	13		21.7		43	
210+00	210+50	50	13		72.2		113	
243+50	244+50	100	13		144.4		213	
245+25	245+50	25	13		36.1		63	
247+65	247+80	15	13		21.7		43	
390+00	391+10	110	13		158.9		233	
392+29	393+25	96	13		138.7		205	
438+50	439+50	100	13		144.4		213	
442+25	442+75	50	13		72.2		113	
443+25	444+50	125	13		180.6		263	
544+75	545+25	50	13		72.2		113	
546+50	548+00	150	13		216.7		313	
549+00	549+20	20	13		28.9		53	
553+25	555+25	200	13		288.9		413	
563+00	563+20	20	13		28.9		53	
573+55	573+75	20	13		28.9		53	
667+90	668+50	60	13		86.7		133	
691+50	692+10	60	13		86.7		133	

9226

9231

NOTE: Failed areas are estimated as one (1) foot of depth and backfilled with one (1) foot (maximum of 3 ½” lifts) of 19-mm, ST Asphalt. The removal to one (1) foot is paid under the Removal of Asphalt Pavement, All Depths. The asphalt shall be placed per the Project Engineer’s instructions. Saw cuts will be required and will be paid for separately.

NOTE: Any extra excavation below one (1) foot depth required, as determined by the Project Engineer, will be paid for as Excess Excavation.

NOTE: Failed areas are to be backfilled the same day as excavation.

2. Cold milling of the existing asphalt pavement at local road tie-ins and bridge ends to a depth of one and one-half (1 1/2") inches and variable in order to provide a smooth transition will be required. The entire roadway section will not be milled.

The cold milling material obtained shall become the property of the contractor. Payment for Cold Milling of Pavement will be made under Pay Item No. 406-D, per square yard, and shall include all cost associated with the milling operation.

NOTE: During this operation and prior to placement of the asphalt, due care shall be required to keep surface water from ponding on the roadway surface; continuous monitoring of the project may be required.

NOTE: During this operation and prior to placement of the asphalt, contractor shall repair and maintain all potholes.

3. Placement of a scrub seal on the mainline. As per Special Provision 907-414, the Contractor shall submit to the Engineer a mix design that provides the project a target, or design, application rate for bituminous material in gal/yd² and cover material in lbs/yd².

NOTE: The scrub seal shall cover the entire roadway.

NOTE: County road intersections and bridges will not require a scrub seal

NOTE: Existing raised markers shall be removed prior to the scrub seal operations. The cost shall be absorbed in other bid items.

NOTE: All loose traffic stripe shall be removed prior to the scrub seal operation. The cost will be absorbed in other pay items.

NOTE: All sweeping/brooming operations will require that all excess aggregate will be removed using vacuum trucks or other approved methods. Excess aggregate will not be allowed to be swept off the roadway into adjacent areas. After final sweeping/brooming and prior to the asphalt overlay operations, any excess material will be removed from the roadway and gutter pans by vacuum trucks or other approved methods. This cost shall be absorbed in other pay items.

4. A quantity for 9.5mm, ST, Asphalt Pavement, Leveling has been set up for curve corrections and for leveling as directed by the Project Engineer.

9.5mm, ST, Asphalt Pavement, Leveling	
Area	Tons
Mainline	1300
Curve Corrections	750
Totals	2050

5. The contractor shall place one and one-half inches (1.5”) of ST 9.5mm for the surface course on the previously sealed and leveled mainline.
6. The contractor will be required to remove all cold mix from the bridge deck for BR # 60.8 and place three-quarters of an inch (3/4”) of ST 9.5mm on the bridge deck surface. The bridge deck shall not be milled. Any damages to the deck will be repaired at the Contractor’s expense. Costs associated with the removal of cold mix will be absorbed in other pay items.

NOTE: The use of dynamic rollers shall be prohibited for compaction of the asphalt lifts on the bridge deck. Only static compaction will be allowed. Due care shall be given to ensure asphalt thickness does not exceed a compacted thickness of three-quarters of an inch (3/4”). Asphalt cores may be used to verify this thickness. An allowable tolerance of +0” to -1/4” is permitted. If thicknesses have been verified to exceed the three quarters of an inch maximum tolerance, the entire bridge deck shall be milled with the approval of the Director of Structures and replaced at no additional cost to MDOT.

NOTE: Sawing and Sealing at the bridge ends will be required after asphalt pavement has been placed.

7. Placement of Granular Material Class 5, Group D on the shoulders as directed to raise the existing shoulders to the new surface course grade.

NOTE: Shoulders shall be bladed, shaped and compacted throughout the length of the project regardless of whether granular material is required.

NOTE: Granular material not required for the final shape of the shoulders may require removal under the pay item for excess excavation and may include small amounts of asphalt.

NOTE: Due care shall be taken during this operation to blade material to the roadway and away from the ditch line. Material inadvertently bladed to the roadway vegetation shall be removed at no cost to the Mississippi Department of Transportation.

8. Placement of Temporary Traffic Stripe daily as per Special Provisions Nos. 907-618.

9. Remove and replace guardrails as per the attached table. Existing guardrail is not located in asphalt and will be driven in the Granular Material.

BR #	Guardrail Removal (LF)	Guardrail Installation (LF)	Terminal End Section, Flared (EA)	Bridge End Section (Type C)	Bridge End Section (Type C Mod)	Delineators White (EA)
60.8	747	550	4	2	2	26

NOTE: The contractor is responsible for verifying the bridge end sections.

10. Install a six (6) inch rumble strip for rumble stripe.
11. Place permanent pavement markings as required (Thermoplastic Striping, Reflective High Performance Raised Markers).
12. All existing post-mounted standard roadside signs estimated in the attached table shall be replaced. The Contractor shall deliver the removed signs to the Calhoun County Maintenance Lot located at 132 Shannon Street, Calhoun. All signs and hardware shall be removed from post prior to delivery. The Contractor is required to verify the sign quantity prior to ordering materials. All hardware and footings required for the erection of new signs and posts shall be absorbed in other items of work.

Sign Quantity			
Pay Item	Description	Unit	Quantity
202-B	Removal of Sign	EA	19
202-B	Removal of Sign, Including Post and Footing	EA	107
630-A	Standard Roadside Signs, Sheet Aluminum, 0.080" Thickness	SF	70.94
630-A	Standard Roadside Signs, Sheet Aluminum, .1" Thickness	SF	35
630-A	Standard Roadside Signs, Sheet Aluminum, 0.125" Thickness	SF	745.46
630-C	Square Tube Post, 2.0 lb/ft	LF	1770

NOTE: Any Existing Pipe Post will remain in place for the new signs to be attached as shown in the Sign Table attached to the Scope of Work.

The contractor shall provide all signs and traffic handling devices necessary to safely maintain traffic around or through the work areas.

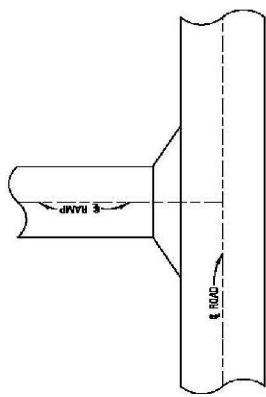
The Engineer may direct the use of additional cones at County roads or intersections within lane closures and will be absorbed in Maintenance of Traffic.

Incidental work such as removing vegetation, shaping and compaction of shoulder, necessary and incidental grading of roadway ditches, and other incidental work that is necessary to

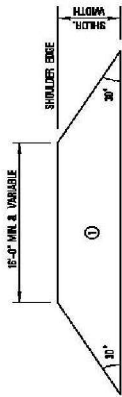
complete the work will not be measured for separate payment and the cost will be included in the bid items provided.

Prior to grinding rumble strips, the Contractor shall mark the beginning and ending of these rumble strips. Temporary paint shall not be used solely as a final location of begin and end of rumble strip.

STATE	PROJECT NO.
MISS.	



PLAN VIEW OF RAMP



DETAIL OF PAVED APRON

① 1'-0\" & VARIABLE DEPTH ASPHALT PAVEMENT REQUIRED



MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
MISCELLANEOUS DETAIL	
PROJECT NUMBER	MISS-1
SHEET NUMBER	
TITLE NAME	MIS-1
DATE	06/05/10
BY	JNT

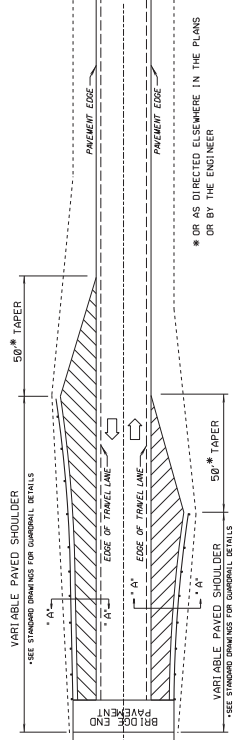
Random Clearing NB						
Starting Station	Ending Station	Length (ft)	No. of Stations	Location	> 3:1 Slope	ROW width
56+00	58+00	200	2	NB Outside		60'
59+00	61+00	200	2	NB Outside	Yes	60'
65+00	66+00	100	1	NB Outside		60'
68+00	70+00	200	2	NB Outside		60'
70+00	73+00	300	3	NB Outside	Yes	60'
76+00	76+96	96	1	NB Outside		60'
77+10	78+00	90	1	NB Outside		60'
79+00	80+00	100	1	NB Outside		85'
81+00	84+00	300	3	NB Outside	Yes	81 to 83 = 85' 83 to 84 = 60'
85+00	86+00	100	1	NB Outside		60'
86+00	87+00	100	1	NB Outside	Yes	60'
89+00	91+00	200	2	NB Outside		60'
93+00	94+00	100	1	NB Outside		60'
94+00	98+00	400	4	NB Outside	Yes	60'
98+00	100+00	200	2	NB Outside		60'
101+00	103+00	200	2	NB Outside	Yes	60'
103+00	104+00	100	1	NB Outside		60'
106+00	109+00	300	3	NB Outside	Yes	60'
109+00	116+00	700	7	NB Outside		109 to 113 = 60' 113 to 116 = 75'
116+00	117+00	100	1	NB Outside	Yes	75'
119+00	123+00	400	4	NB Outside	Yes	60'
123+00	126+00	300	3	NB Outside		60'
126+00	131+00	500	5	NB Outside	Yes	60'
131+00	132+00	100	1	NB Outside		60'
132+00	137+00	500	5	NB Outside	Yes	60'
137+00	144+00	700	7	NB Outside		60'
144+00	146+00	200	2	NB Outside	Yes	60'
146+00	150+00	400	4	NB Outside		60'
150+00	156+00	600	6	NB Outside	Yes	60'
156+00	167+00	1100	11	NB Outside		60'
167+00	171+00	400	4	NB Outside	Yes	60'
171+00	172+00	100	1	NB Outside		60'
174+00	177+00	300	3	NB Outside		80'
177+00	179+00	200	2	NB Outside	Yes	80'
179+00	180+00	100	1	NB Outside		80'
180+00	181+00	100	1	NB Outside	Yes	80'
181+00	186+00	500	5	NB Outside		80'
186+00	189+00	300	3	NB Outside	Yes	80'
189+00	199+00	1000	10	NB Outside		85'
199+00	204+00	500	5	NB Outside	Yes	60'
206+00	209+00	300	3	NB Outside		60'
209+00	211+00	200	2	NB Outside	Yes	85'
211+00	214+00	300	3	NB Outside		60'
226+00	232+00	600	6	NB Outside		226 to 229 = 75' 229 to 232 = 60'
232+00	236+00	400	4	NB Outside	Yes	60'

236+00	248+00	1200	12	NB Outside		60'
248+00	249+00	100	1	NB Outside	Yes	60'
249+00	252+00	300	3	NB Outside		60'
253+00	256+00	300	3	NB Outside	Yes	60'
256+00	264+00	800	8	NB Outside		60'
264+00	266+00	200	2	NB Outside	Yes	60'
266+00	277+00	1100	11	NB Outside		266 to 271 = 60' 266 to 277 = 85'
297+00	303+00	600	6	NB Outside		60'
304+00	305+00	100	1	NB Outside		60'
308+00	309+00	100	1	NB Outside		60'
311+00	323+00	1200	12	NB Outside		60'
323+00	336+00	1300	13	NB Outside	Yes	60'
338+00	340+00	200	2	NB Outside		60'
340+00	344+00	400	4	NB Outside	Yes	60'
345+00	348+00	300	3	NB Outside	Yes	60'
348+00	352+00	400	4	NB Outside		60' with sight flare
352+00	403+00	5100	51	NB Outside	Yes	60'
403+00	405+00	200	2	NB Outside		60' with sight flare
407+00	408+00	100	1	NB Outside		60'
410+00	413+00	300	3	NB Outside		60'
415+00	418+00	300	3	NB Outside	Yes	60'
421+00	429+00	800	8	NB Outside	Yes	60'
433+00	446+00	1300	13	NB Outside	Yes	60'
446+00	459+00	1300	13	NB Outside		60' with sight flare
459+00	465+00	600	6	NB Outside	Yes	60'
465+00	469+00	400	4	NB Outside		60'
469+00	474+00	500	5	NB Outside	Yes	60'
477+00	479+00	200	2	NB Outside	Yes	60'
482+00	493+00	1100	11	NB Outside	Yes	60'
494+00	503+00	900	9	NB Outside	Yes	60'
506+00	511+00	500	5	NB Outside		60'
511+00	520+00	900	9	NB Outside	Yes	511 to 513 = 60' 513 to 517 = 70' 517 to 520 = 60'
520+00	528+00	800	8	NB Outside		60' with sight flare
537+00	544+00	700	7	NB Outside		60'
544+00	559+00	1500	15	NB Outside	Yes	544 to 545 = 60' 545 to 548 = 70' 548 to 553 = 60' 553 to 556 = 75' 556 to 559 = 60'
561+00	565+00	400	4	NB Outside	Yes	60'
565+00	568+71	371	4	NB Outside		60'
569+71	576+00	629	7	NB Outside		60'
576+00	580+00	400	4	NB Outside	Yes	60'
580+00	593+00	1300	13	NB Outside		60'
593+00	598+00	500	5	NB Outside	Yes	60'
604+00	633+00	2900	29	NB Outside	Yes	604 to 605+50 = 60' 605+50 to 615 = 70' 615 to 635 = 60'
633+00	635+00	200	2	NB Outside		60'
635+00	639+00	400	4	NB Outside		60'
642+00	675+00	3300	33	NB Outside	Yes	642 to 671 = 80' 671 to 675 = 60'
675+00	682+00	700	7	NB Outside		60'
682+00	702+15	2015	21	NB Outside		682 to 691+29 = 60' 691+29 to 702+15 = 85'

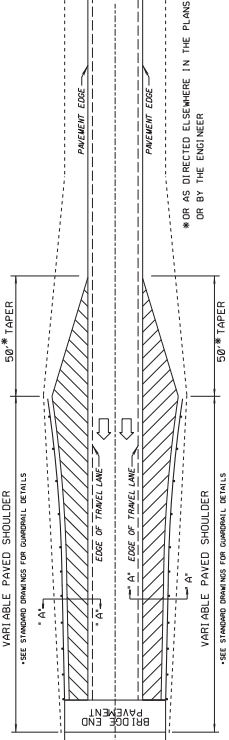
Random Clearing SB						
Starting Station	Ending Station	Length (ft)	No. of Stations	Location	> 3:1 Slope	ROW width
75+00	76+96	196	2	SB Outside		60'
77+10	78+00	90	1	SB Outside		60'
80+00	84+00	400	4	SB Outside		80 to 81 = 60' 81 to 84 = 75'
85+00	95+00	1000	10	SB Outside	Yes	85 to 86+87 = 75' 86+87 to 95 = 60'
117+00	120+00	300	3	SB Outside	Yes	117 to 119 = 75' 119 to 120 = 60'
121+00	130+00	900	9	SB Outside	Yes	60'
134+00	138+00	400	4	SB Outside	Yes	60'
139+00	142+00	300	3	SB Outside		60'
146+00	158+00	1200	12	SB Outside	Yes	146 to 155 = 80' 155-158 = 60'
158+00	165+00	700	7	SB Outside		60'
174+00	176+00	200	2	SB Outside		85'
176+00	179+00	300	3	SB Outside	Yes	85'
179+00	186+00	700	7	SB Outside		85'
186+00	190+00	400	4	SB Outside	Yes	186 to 188 = 85' 188 to 190 = 60'
190+00	198+00	800	8	SB Outside		60'
198+00	202+00	400	4	SB Outside	Yes	198 to 200 = 60' 200 to 202 = 75'
202+00	209+00	700	7	SB Outside		75'
209+00	211+00	200	2	SB Outside	Yes	75'
230+00	236+00	600	6	SB Outside	Yes	230 to 232 = 75' 232 to 236 = 60'
238+00	240+00	200	2	SB Outside	Yes	60'
243+00	254+00	1100	11	SB Outside	Yes	60'
275+00	297+00	2200	22	SB Outside	Yes	275 to 279 = 60' 279 to 293 = 100' 293 to 297 = 60'
315+00	317+00	200	2	SB Outside		60'
319+00	324+00	500	5	SB Outside		60'
326+00	334+00	800	8	SB Outside	Yes	60'
340+00	347+00	700	7	SB Outside	Yes	60'
347+00	349+00	200	2	SB Outside		60' with sight flare
353+00	354+00	100	1	SB Outside	Yes	60'
379+00	383+00	400	4	SB Outside	Yes	70'
384+00	408+00	2400	24	SB Outside	Yes	384 to 390+80 = 70' 390+80 to 408 = 60'
410+00	413+00	300	3	SB Outside		60'
427+00	429+00	200	2	SB Outside		60'
432+00	434+00	200	2	SB Outside		60'
434+00	447+00	1300	13	SB Outside	Yes	434 to 435 = 60' 435 to 438 = 70' 438 to 441 = 60' 441 to 444 = 90' 444 to 447 = 60'
447+00	463+00	1600	16	SB Outside		447 to 461 = 60' 461 to 463 = 70'
465+00	469+00	400	4	SB Outside		60'
469+00	474+00	500	5	SB Outside	Yes	60'
474+00	478+00	400	4	SB Outside		60' with sight flare
481+00	486+00	500	5	SB Outside		60' with sight flare
486+00	488+00	200	2	SB Outside	Yes	486 to 487 = 60' 487 to 488 = 80'
488+00	492+00	400	4	SB Outside		488 to 491 = 80' 491 to 492 = 60'
492+00	494+00	200	2	SB Outside	Yes	60'
496+00	500+00	400	4	SB Outside		60'
500+00	509+00	900	9	SB Outside		60' with sight flare
509+00	518+00	900	9	SB Outside	Yes	60'

518+00	523+00	500	5	SB Outside		60' with sight flare
523+00	556+00	3300	33	SB Outside	Yes	60' with sight flare
556+00	562+00	600	6	SB Outside		60'
562+00	568+71	671	7	SB Outside	Yes	60'
569+71	573+00	329	4	SB Outside	Yes	60'
573+00	577+00	400	4	SB Outside		60' with sight flare
579+00	593+00	1400	14	SB Outside	Yes	60'
597+00	599+00	200	2	SB Outside	Yes	60'
605+00	614+00	900	9	SB Outside	Yes	605 to 609 = 60' 609 to 614 = 75'
619+00	621+00	200	2	SB Outside		60'
621+00	628+00	700	7	SB Outside	Yes	60'
628+00	636+00	800	8	SB Outside		60'
641+00	648+00	700	7	SB Outside	Yes	641 to 646 = 60' 646 to 648 = 90'
648+00	654+00	600	6	SB Outside		70' with sight flare
654+00	662+00	800	8	SB Outside	Yes	654 to 656 = 80' 656 to 662 = 70'
662+00	664+00	200	2	SB Outside		70'
666+00	670+00	400	4	SB Outside	Yes	666 to 668 = 70' 668 to 670 = 60'
670+00	677+00	700	7	SB Outside		60'
680+00	686+00	600	6	SB Outside		60'
686+00	689+00	300	3	SB Outside	Yes	60'
689+00	692+00	300	3	SB Outside		80'
692+00	695+00	300	3	SB Outside	Yes	80'
695+00	699+00	400	4	SB Outside		695 to 696 = 80' 696 to 699 = 70'
699+00	701+00	200	2	SB Outside	Yes	70'

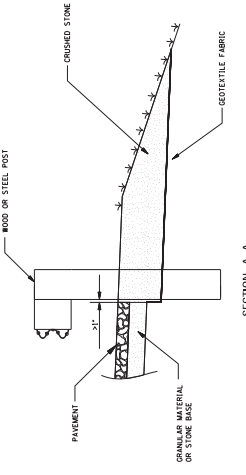
Total	Outside Lane	> 3:1 Slope	Difference	
NB	536	295	241	Stations
SB	426	270	156	Stations
Overall	962	565	397	Stations



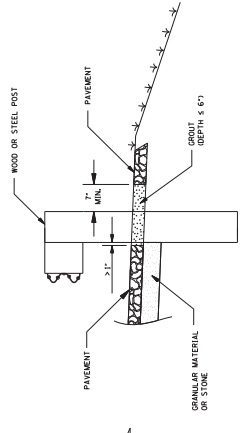
TYPICAL DETAIL OF PAVED SHOULDERS AT BRIDGE ENDS
 (2 - WAY TRAFFIC)
 SEE SECTION 'A' - 'A'



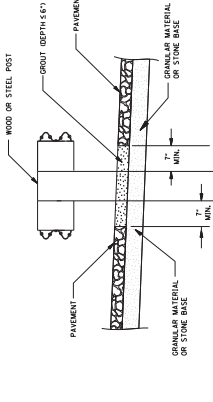
TYPICAL DETAIL OF PAVED SHOULDERS AT BRIDGE ENDS
 (1 - WAY TRAFFIC)
 SEE SECTION 'A' - 'A'



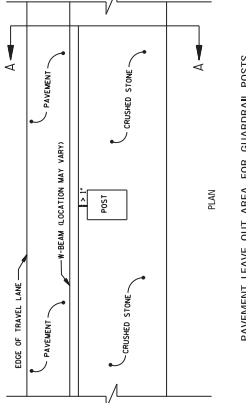
SECTION A-A



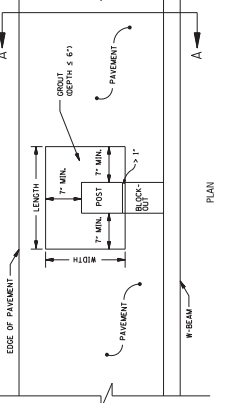
SECTION A-A



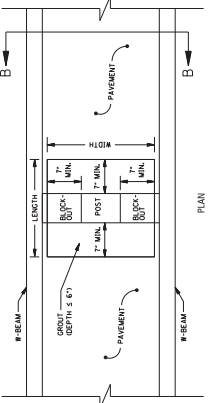
SECTION B-B



PAVEMENT LEAVE-OUT AREA FOR GUARDRAIL POSTS
 SINGLE-FACED GUARDRAIL (CRUSHED STONE OPTION)



PAVEMENT LEAVE-OUT AREA FOR GUARDRAIL POSTS
 SINGLE-FACED GUARDRAIL (GROUT OPTION)



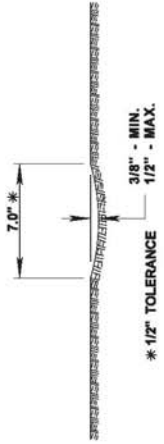
PAVEMENT LEAVE-OUT AREA FOR GUARDRAIL POSTS
 DOUBLE-FACED GUARDRAIL

POST	MIN. PAV'T LEAVE-OUT AREA	
	SINGLE-FACED	DOUBLE-FACED
LENGTH	28	28
WIDTH	15	15
WOOD	24	24
WOOD	17	17
WOOD	24	24
STEEL	18	18

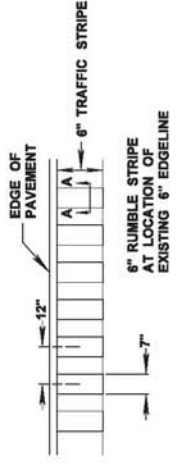
- GENERAL NOTES FOR CRUSHED STONE OPTION
- GUARDRAIL POSTS SHALL NOT BE COMPLETELY SURROUNDED BY PAVEMENT. THE AREA BEHIND AND LATERAL OF THE POST SHALL BE FILLED WITH A MINIMUM 28 DAY COMPRESSIVE STRENGTH OF 200 PSI.
 - STANDARD EMBEMENT DEPTH SHALL BE MEASURED FROM THE TOP OF THE PROJECTED PAVEMENT SURFACE.
 - IMMEDIATELY PRIOR TO PLACING GEOTEXTILE FABRIC AND CRUSHED STONE ON THE GUARDRAIL PAD, THE ENTIRE AREA SHALL BE TREATED UNIFORMLY WITH A SOIL STERILANT. THE SOIL STERILANT SHALL BE APPLIED TO THE ENTIRE AREA OF THE GUARDRAIL PAD. THE STERILANT SHALL BE APPROVED BY THE MANUFACTURER'S LABEL FOR PERENNIAL WEED CONTROL. ANY GRANULAR OR FILLER SOIL STERILANT WHICH IS NOT MADE FOR TREATING THE GUARDRAIL PAD AREA WITH SOIL STERILANT.
- GENERAL NOTES FOR GROUT OPTION
- GUARDRAIL POSTS SHALL NOT BE COMPLETELY SURROUNDED BY PAVEMENT. THE AREA BEHIND AND LATERAL OF THE POST SHALL BE FILLED WITH A MINIMUM 28 DAY COMPRESSIVE STRENGTH OF 200 PSI.
 - GROUT SHALL BE INSTALLED AT A DEPTH EQUAL TO THE SURROUNDING PAVEMENT IS GREATER THAN 6". IF DIFFERENCE SHALL BE FILLED IN WITH SHOULDER GRANULAR MATERIAL.
 - PAVEMENT LEAVE-OUT AREAS ARE REQUIRED FOR STEEL AND WOOD POSTS.
 - PAVEMENT LEAVE-OUT AREAS ARE REQUIRED FOR STEEL AND WOOD POSTS.
 - STANDARD EMBEMENT DEPTH SHALL BE MEASURED FROM THE TOP OF THE PROJECTED PAVEMENT SURFACE.

NOTE: THE ALTERNATIVES SHOWN HEREIN APPLY TO ALL GUARDRAIL CONSTRUCTION WHERE MON STRIPS ARE ADOPTED.

STATE	PROJECT NO.
MISS.	HSIP-0846-02(03)

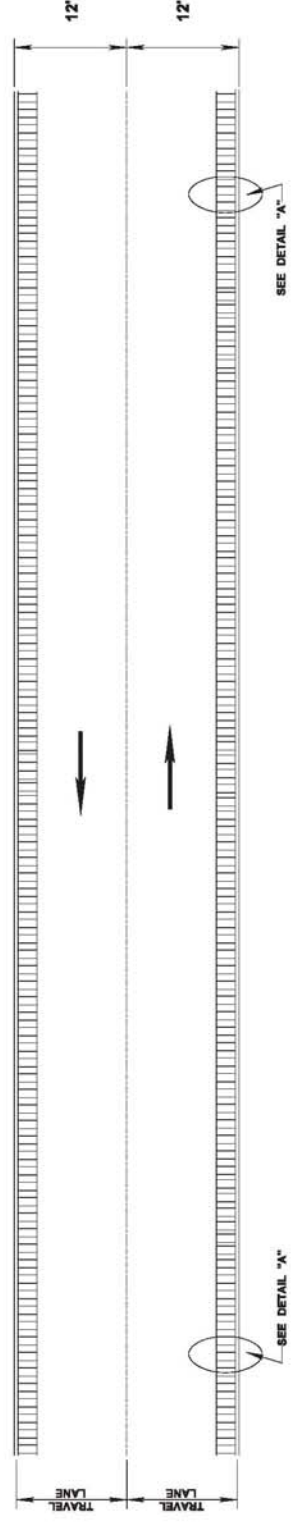


SECTION "A-A"



DETAIL "A"

- GENERAL NOTES**
- GROUND-IN RUMBLE STRIPES SHALL BE APPLIED ON LEFT AND RIGHT EDGELINES.
 - GROUND-IN RUMBLE STRIPES SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTIONS IN ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
 - COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS
 - GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO MAINLINE ONLY.



PLAN
NOT TO SCALE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
SPECIAL DESIGN: RUMBLE STRIPES (GROUND-IN) 2 LANE, 24 FT.	
PROJECT NUMBER	HS-2L
COUNTY	ALCORN
FILENAME	CUMDIB_STIP08_2.LOT06.DWG
DESIGN TEAM	ATKINS
CHECKED	DATE 01/06/14
SCALE	18

NB Lane HWY 9

Location (Sta. #)	Lane Location	Sign Code	Sign Description	Type 3 Object Markers	Size SQ IN	0.08"	0.1"	0.125"	Square Tube (LF)	Removal of Sign	Removal of Sign Including Post	Notes
51+75	NB Outside	M1-5	9(white,black)		576	4.00			15		1	Same Post
51+75	NB Outside	M3-1P	North(white,black)		288	2.00			15	1		Same Post
55+00	NB Outside	W3-1	Stop Ahead(yellow,black)		1296			9.00	15	1		
56+50	NB Outside	R2-1	Speed Limit 55(white,black)		720		5.00		15		1	
97+65	NB Outside	W2-2	Left County Rd Ahead(yellow,black)		1296			9.00	15		1	
103+00	NB Outside	W1-7	Left&Right Arrow(yellow,black)		1152			8.00	15		1	Missing
142+50	NB Outside	W2-2	Right County Rd Ahead(yellow,black)		1296			9.00	15		1	Missing
147+50	NB Outside	W1-7	Left&Right Arrow(yellow,black)		1152			8.00	15		1	Missing
147+50	NB Outside	R1-1	Stop(red,white)		1072.5			7.45	15		1	No Stop Ahead Needed
156+75	NB Outside	W2-2	Right County Rd Ahead(yellow,black)		1296			9.00	15		1	Missing
161+25	NB Outside	R1-1	Stop(red,white)		1072.5			7.45	15		1	No Stop Ahead Needed
161+25	NB Outside	W1-7	Left&Right Arrow(yellow,black)		1152			8.00	15		1	Missing
168+00	NB Outside	W2-1	Cross Arrow(yellow,black)		1296			9.00	15		1	Missing
174+50	NB Outside	R1-1	Stop(red,white)		1072.5			7.45	15		1	
174+50	NB Outside	W3-1	Stop Ahead(yellow,black)		1296			9.00	15		1	
191+50	NB Outside	W2-2	Right County Rd Ahead(yellow,black)		1296			9.00	15		1	
197+00	NB Outside	R1-1	Stop(red,white)		1072.5			7.45	15		1	
197+00	NB Outside	W3-1	Stop Ahead(yellow,black)		1296			9.00	15		1	
209+15	NB Outside	W2-2	Right County Rd Ahead(yellow,black)		1296			9.00	15		1	
214+50	NB Outside	R1-1	Stop(red,white)		1072.5			7.45	15		1	
214+50	NB Outside	W3-1	Stop Ahead(yellow,black)		1296			9.00	15		1	
284+85	NB Outside	OM-3	Object Marker(yellow,black)	1	0				0			Box Culvert
284+85	NB Outside	OM-3	Object Marker(yellow,black)	1	0				0			Box Culvert
306+50	NB Outside	W2-2	Left County Rd Ahead(yellow,black)		1296			9.00	15		1	
312+75	NB Outside	W1-7	Left&Right Arrow(yellow,black)		1152			8.00	15		1	
315+75	NB Outside	M1-5	9(white,black)		576	4.00			15		1	Same Post
315+75	NB Outside	M3-1P	North(white,black)		288	2.00			15		1	Same Post
340+00	NB Outside	W2-1	Cross Arrow(yellow,black)		1296			9.00	15		1	
344+50	NB Outside	M2-1P	Junction(white,black)		315	2.19			15		1	Same Post
344+50	NB Outside	M1-6	331(white,black)		576	4.00			15		1	Same Post
349+60	NB Outside	M3-1P	North(white,black)		288	2.00			15		1	
349+60	NB Outside	M1-6	331(white,black)		576	4.00			15		1	
349+60	NB Outside	N/A	Left Arrow(blue,white)		0				0		1	
349+60	NB Outside	M3-1P	North(white,black)		288	2.00			15		1	
349+60	NB Outside	M1-5	9(white,black)		576	4.00			15		1	
349+60	NB Outside	M6-3	Up Arrow(white,black)		315	2.19			15		1	
350+00	NB Outside	R1-1	Stop(red,white)		1072.5			7.45	15		1	
350+00	NB Outside	W3-1	Stop Ahead(yellow,black)		1296			9.00	15		1	
385+75	NB Outside	W8-13	Bridge Ics Before Road(yellow,black)		1296			9.00	15		1	
391+00	NB Outside	W12-7aP	Weight Limit(white,black)		1260			8.75	15		1	Same Post
391+00	NB Outside	W12-7aP	Emergency Vehicles(white,black)		900		6.25		15		1	Same Post
391+10	NB Outside	OM-3	Object Marker(yellow,black)	1	0				0			
392+29	NB Outside	OM-3	Object Marker(yellow,black)	1	0				0			
402+50	NB Outside	W2-2	Right County Rd Ahead(yellow,black)		1296			9.00	15		1	
409+50	NB Outside	R1-1	Stop(red,white)		1072.5			7.45	15		1	
409+50	NB Outside	W3-1	Stop Ahead(yellow,black)		1296			9.00	15		1	
446+00	NB Outside	W2-2	Right County Rd Ahead(yellow,black)		1296			9.00	15		1	
451+00	NB Outside	R1-1	Stop(red,white)		1072.5			7.45	15		1	
451+00	NB Outside	W3-1	Stop Ahead(yellow,black)		1296			9.00	15		1	
474+50	NB Outside	W2-1	Cross Arrow(yellow,black)		1296			9.00	15		1	

480+30	NB Outside	R1-1	Stop (red,white)		1072.5				7.45	15	1	
481+50	NB Outside	R1-1	Stop (red,white)		1072.5				7.45	15	1	
481+50	NB Outside	W3-1	Stop Ahead (yellow,black)		1296				9.00	15	11	
500+00	NB Outside	W2-1	Cross Arrow (yellow,black)		1296				9.00	15	1	
506+00	NB Outside	R1-1	Stop (red,white)		1072.5				7.45	15	1	
513+00	NB Outside	OM-3	Object Marker (yellow,black)	1	0					0		
513+00	NB Outside	OM-3	Object Marker (yellow,black)	1	0					0		
520+50	NB Outside	W2-1	Cross Arrow (yellow,black)		1296				9.00	15	1	Temporary Log Trucks Sign on post
524+00	NB Outside	OM-3	Object Marker (yellow,black)	1	0					0		Sign&Post Missing
524+00	NB Outside	OM-3	Object Marker (yellow,black)	1	0					0		Sign&Post Missing
552+00	NB Outside	W2-2	Right County Rd Ahead (yellow,black)		1296				9.00	15	1	
557+50	NB Outside	R1-1	Stop (red,white)		1072.5				7.45	15	1	
557+50	NB Outside	W3-1	Stop Ahead (yellow,black)		1296				9.00	15	1	
568+00	NB Outside	W2-2	Left County Rd Ahead (yellow,black)		1296				9.00	15	1	
575+50	NB Outside	W1-7	Left&Right Arrow (yellow,black)		1152				8.00	15	1	Sign&Post Missing
598+00	NB Outside	OM-3	Object Marker (yellow,black)	1	0					0		
598+00	NB Outside	OM-3	Object Marker (yellow,black)	1	0					0		
607+00	NB Outside	OM-3	Object Marker (yellow,black)	1	0					0		
607+00	NB Outside	OM-3	Object Marker (yellow,black)	1	0					0		
608+00	NB Outside	N/A	Curve Left (yellow,black)		0					0	1	
626+00	NB Outside	W2-2	Right County Rd Ahead (yellow,black)		1296				9.00	15	1	
631+00	NB Outside	R1-1	Stop (red,white)		1072.5				7.45	15	1	
647+00	NB Outside	W2-2	Left County Rd Ahead (yellow,black)		1296				9.00	15	1	
675+00	NB Outside	W1-7	Left&Right Arrow (yellow,black)		1152				8.00	15	1	

SB Lane HWY 9

Location (Sta. #)	Lane Location	Sign Code	Sign Description	Type 3 Object Markers	Size SQ IN	0.08"	0.1"	0.125"	Square Tube (LF)	Removal of Sign	Removal of Sign Including Post	Notes
55+00	SB Outside	W3-1	Stop Ahead (yellow,black)		1296			9.00	15	1		
61+00	SB Outside	W2-1P	Junction (white,black)		315	2.19			15		1	Same Post
61+00	SB Outside	M1-5	9 (white,black)		576	4.00			15	1		Same Post
103+00	SB Outside	W1-7	Left&Right Arrow (yellow,black)		1152			8.00	15		1	
142+00	SB Outside	W2-2	Left County Rd Ahead (yellow,black)		1296			9.00	15		1	Missing
147+50	SB Outside	W1-7	Left&Right Arrow (yellow,black)		1152			8.00	15		1	Missing
154+00	SB Outside	W2-2	Left County Rd Ahead (yellow,black)		1296			9.00	15		1	Missing
161+25	SB Outside	W1-7	Left&Right Arrow (yellow,black)		1152			8.00	15		1	Missing
167+00	SB Outside	W2-2	Left County Rd Ahead (yellow,black)		1296			9.00	15		1	Missing
174+50	SB Outside	R1-1	Stop (red,white)		1072.5			7.45	15		1	
174+50	SB Outside	W3-1	Stop Ahead (yellow,black)		1296			9.00	15		1	
180+50	SB Outside	W2-1	Cross Arrow (yellow,black)		1296			9.00	15		1	
197+00	SB Outside	W1-7	Left&Right Arrow (yellow,black)		1152			8.00	15		1	
202+00	SB Outside	W2-2	Left County Rd Ahead (yellow,black)		1296			9.00	15		1	
214+00	SB Outside	W1-7	Left&Right Arrow (yellow,black)		1152			8.00	15		1	
221+00	SB Outside	W2-2	Left County Rd Ahead (yellow,black)		1296			9.00	15		1	
279+00	SB Outside	W1-5	Curvy Rd Ahead (yellow,black)		900		6.25		15		1	
284+85	SB Outside	OM-3	Object Marker (yellow,black)	1	0				0			
284+85	SB Outside	OM-3	Object Marker (yellow,black)	1	0				0			

309+00	SB Outside	M3-3P	South(white,black)			288	2.00			15	1	1	Same Post	
309+00	SB Outside	M1-5	9(white,black)			576	4.00			15	1	1	Same Post	
312+75	SB Outside	R1-1	Stop(red,white)			1072.5			7.45	15		1		
312+75	SB Outside	W3-1	Stop Ahead(yellow,black)			1296			9.00	15		1		
318+50	SB Outside	W2-2	Right County Rd Ahead(yellow,black)			1296			9.00	15		1		
349+00	SB Outside	M3-3P	South(white,black)			288	2.00			15		1	Same Post	
349+00	SB Outside	M1-5	9(white,black)			576	4.00			15	1	1	Same Post	
350+00	SB Outside	W1-7	Left&Right Arrow(yellow,black)			1152			8.00	15		1		
352+00	SB Outside	M3-3P	South(white,black)			288	2.00			15		1	Same Post	
352+00	SB Outside	M1-5	9(white,black)			576	4.00			15	1	1	Same Post	
352+00	SB Outside	M6-3	Up Arrow(white,black)			315	2.19			15		1	Same Post	
352+00	SB Outside	M3-1P	North(white,black)			288	2.00			15		1	Same Post	
352+00	SB Outside	M1-6	331(white,black)			576	4.00			15		1	Same Post	
359+50	SB Outside	W2-1P	Junction(white,black)			315	2.19			15		1	Same Post	
359+50	SB Outside	M1-6	331(white,black)			576	4.00			15	1	1	Same Post	
361+72	SB Outside	W2-1	Cross Arrow(yellow,black)			1296			9.00	15		1		
391+10	SB Outside	OM-3	Object Marker(yellow,black)	1		0				0				
392+29	SB Outside	OM-3	Object Marker(yellow,black)	1		0				0				
392+45	SB Outside	W12-6	Weight Limit(white,black)			1260			8.75	15		1	Same Post	
392+45	SB Outside	W12-7aP	Emergency Vehicles(white,black)			900		6.25		15	1		Same Post	
400+00	SB Outside	W8-13	Bridge Ices Before Road(yellow,black)			1296			9.00	15		1		
409+50	SB Outside	W1-7	Left&Right Arrow(yellow,black)			1152			8.00	15		1		
456+00	SB Outside	W2-2	Left County Rd Ahead(yellow,black)			1296			9.00	15		1		
481+50	SB Outside	R1-1	Stop(red,white)			1072.5			7.45	15		1		
481+50	SB Outside	W3-1	Stop Ahead(yellow,black)			1296			9.00	15		1		
491+00	SB Outside	W2-1	Cross Arrow(yellow,black)			1296			9.00	15		1		
506+00	SB Outside	R1-1	Stop(red,white)			1072.5			7.45	15		1		
506+00	SB Outside	W3-1	Stop Ahead(yellow,black)			1296			9.00	15		1		
513+00	SB Outside	OM-3	Object Marker(yellow,black)	1		0				0				
513+00	SB Outside	OM-3	Object Marker(yellow,black)	1		0				0				
524+00	SB Outside	OM-3	Object Marker(yellow,black)	1		0				0			Sign&Post Missing	
524+00	SB Outside	OM-3	Object Marker(yellow,black)	1		0				0			Sign&Post Missing	
524+00	SB Outside	W2-1	Cross Arrow(yellow,black)			1296			9.00	15		1		
557+50	SB Outside	W1-7	Left&Right Arrow(yellow,black)			1152			8.00	15		1		
563+50	SB Outside	W2-2	Left County Rd Ahead(yellow,black)			1296			9.00	15		1		
575+50	SB Outside	R1-1	Stop(red,white)			1072.5			7.45	15		1		
575+50	SB Outside	W3-1	Stop Ahead(yellow,black)			1296			9.00	15		1		
584+50	SB Outside	W2-2	Right County Rd Ahead(yellow,black)			1296			9.00	15		1		
598+00	SB Outside	OM-3	Object Marker(yellow,black)	1		0				0				
598+00	SB Outside	OM-3	Object Marker(yellow,black)	1		0				0				
607+00	SB Outside	OM-3	Object Marker(yellow,black)	1		0				0				
607+00	SB Outside	OM-3	Object Marker(yellow,black)	1		0				0				
631+00	SB Outside	W1-7	Left&Right Arrow(yellow,black)			1152			8.00	15		1		
635+50	SB Outside	W2-2	Left County Rd Ahead(yellow,black)			1296			9.00	15		1		
641+00	SB Outside	R2-1	Speed Limit 55(white,black)			720		5.00		15		1		
653+50	SB Outside	R1-1	Stop(red,white)			1072.5			7.45	15		1		
653+50	SB Outside	W3-1	Stop Ahead(yellow,black)			1296			9.00	15		1		
658+50	SB Outside	W2-2	Right County Rd Ahead(yellow,black)			1296			9.00	15		1		
666+00	SB Outside	W1-5	Curvy Rd Ahead(yellow,black)			900		6.25		15		1		
675+00	SB Outside	R1-1	Stop(red,white)			1072.5			7.45	15		1		
683+00	SB Outside	W2-2	Right County Rd Ahead(yellow,black)			1296			9.00	15		1		
TOTALS										70.937	35	1770	19	107

<u>116+99</u>		<u>Slope</u>			
		Plan		Existing	
<u>Crown</u>	<u>Station</u>	<u>LT. of CL.</u>	<u>RT. Of CL.</u>	<u>LT. of CL.</u>	<u>RT. Of CL.</u>
NC	115+77	-2.0%	-2.0%		
	116+02	-2.0%	-1.0%		
TS	116+28	-2.0%	0.0%		
	116+52	-2.0%	1.0%		
RC	116+77	-2.0%	2.0%		
	116+85	-2.3%	2.3%		
	116+92	-2.6%	2.6%		
PC	116+99	-2.9%	2.9%		
	117+14	-3.5%	3.5%		
FS	117+30	-4.1%	4.1%	-4.3	0.2
	119+00	-4.1%	4.1%	-3.8	-0.3
	121+00	-4.1%	4.1%	-4	-0.7
	123+00	-4.1%	4.1%		
	125+00	-4.1%	4.1%	-4.4	1.1
	127+00	-4.1%	4.1%	-4	0.6
	129+00	-4.1%	4.1%	-4.7	0.7
	131+00	-4.1%	4.1%	-5	0.4
	133+00	-4.1%	4.1%	-4.3	0
	135+00	-4.1%	4.1%	-3.3	-0.8
FS	136+72	-4.1%	4.1%		
	136+88	-3.5%	3.5%		
PT	137+03	-2.9%	2.9%		
	137+10	-2.6%	2.6%		
	137+17	-2.3%	2.3%		
RC	137+25	-2.0%	2.0%		
	137+50	-2.0%	1.0%		
TS	137+74	-2.0%	0.0%		
	138+00	-2.0%	-1.0%		
NC	138+25	-2.0%	-2.0%		

<u>166+50</u>		<u>Slope</u>			
		Plan		Existing	
<u>Crown</u>	<u>Station</u>	<u>LT. of CL.</u>	<u>RT. Of CL.</u>	<u>LT. of CL.</u>	<u>RT. Of CL.</u>
NC	165+56	-2.0%	-2.0%		
	165+82	-2.0%	-1.0%		
TS	166+07	-2.0%	0.0%		
	166+32	-2.0%	1.0%		
RC	166+56	-2.0%	2.0%		
	166+54	-1.9%	1.9%		
	166+52	-1.8%	1.8%		
PC	166+50	-1.8%	1.8%		
	166+59	-2.1%	2.1%		
FS	166+68	-2.5%	2.5%		
	168+00	-2.5%	2.5%	-3.9	-1
	170+00	-2.5%	2.5%	-2.3	-1.6
	172+00	-2.5%	2.5%	-2.2	1
	174+00	-2.5%	2.5%	-2.1	-0.3
	176+00	-2.5%	2.5%	-3.4	0.8
	178+00	-2.5%	2.5%	-5	0.7
	180+00	-2.5%	2.5%	-3.5	1.2
	182+00	-2.5%	2.5%	-3.8	0.3
	184+00	-2.5%	2.5%	-2.2	0.8
	186+00	-2.5%	2.5%	-3.8	0
	188+00	-2.5%	2.5%	-3.3	-1.2
	190+00	-2.5%	2.5%		2.1
	192+00	-2.5%	2.5%		2.5
	194+00	-2.5%	2.5%		2.5
	196+00	-2.5%	2.5%		1.5
	198+00	-2.5%	2.5%		1
	200+00	-2.5%	2.5%		-0.7
	202+00	-2.5%	2.5%		0.5
FS	204+20	-2.5%	2.5%		
	204+29	-2.1%	2.1%		
PT	204+38	-1.8%	1.8%		
	204+36	-1.8%	1.8%		
	204+34	-1.9%	1.9%		
RC	204+32	-2.0%	2.0%		
	204+56	-2.0%	1.0%		
TS	204+81	-2.0%	0.0%		
	205+06	-2.0%	-1.0%		
NC	205+32	-2.0%	-2.0%		

<u>425+54</u>		<u>Slope</u>			
		Plan		Existing	
<u>Crown</u>	<u>Station</u>	<u>LT. of CL.</u>	<u>RT. Of CL.</u>	<u>LT. of CL.</u>	<u>RT. Of CL.</u>
NC	424+21	-2.0%	-2.0%		
	424+47	-1.0%	-2.0%		
TS	424+72	0.0%	-2.0%		
	424+97	1.0%	-2.0%		
RC	425+22	2.0%	-2.0%		
	425+33	2.4%	-2.4%		
	425+43	2.9%	-2.9%		
PC	425+54	3.3%	-3.3%		
	425+72	4.0%	-4.0%		
FS	425+89	4.7%	-4.7%	3.4	-3.2
	427+00	4.7%	-4.7%	2.9%	-3.8
	429+00	4.7%	-4.7%		
	431+00	4.7%	-4.7%	4.2	-2.8
	433+00	4.7%	-4.7%	2.8	-2.9
	435+00	4.7%	-4.7%	1.6	-1.6
	437+00	4.7%	-4.7%	1.6	
	439+00	4.7%	-4.7%	1.2	
	441+00	4.7%	-4.7%	3.5	-3.7
FS	443+65	4.7%	-4.7%	1	-4
	443+82	4.0%	-4.0%		
PT	444+00	3.3%	-3.3%		
	444+11	2.9%	-2.9%		
	444+21	2.4%	-2.4%		
RC	444+32	2.0%	-2.0%		
	444+57	1.0%	-2.0%		
TS	444+82	0.0%	-2.0%		
	445+07	-1.0%	-2.0%		
NC	445+33	-2.0%	-2.0%		

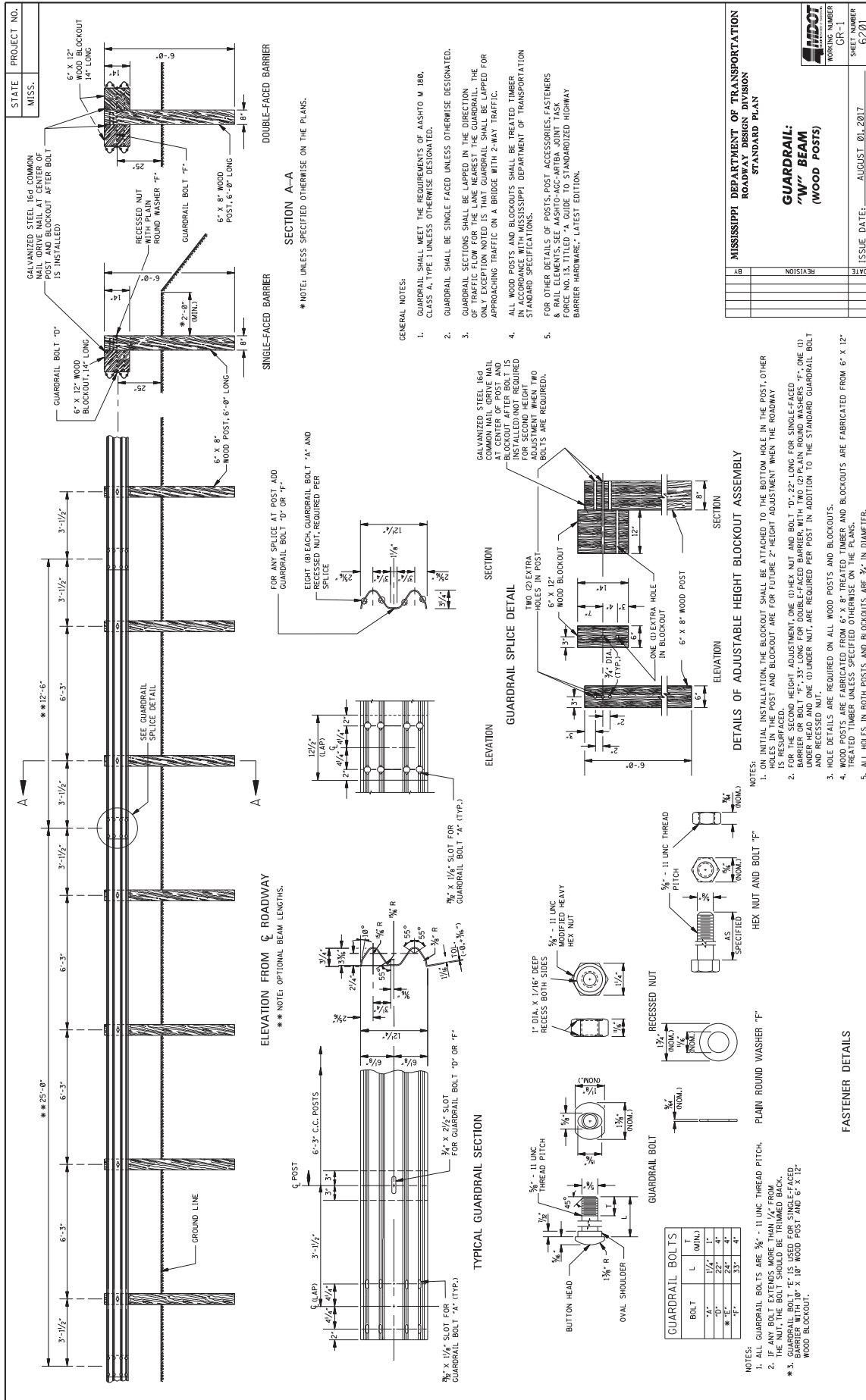
<u>569+71</u>		<u>Slope</u>			
		Plan		Existing	
<u>Crown</u>	<u>Station</u>	<u>LT. of CL.</u>	<u>RT. Of CL.</u>	<u>LT. of CL.</u>	<u>RT. Of CL.</u>
NC	568+38	-2.0%	-2.0%		
	568+64	-1.0%	-2.0%		
TS	568+89	0.0%	-2.0%		
	569+14	1.0%	-2.0%		
RC	569+39	2.0%	-2.0%		
	569+50	2.4%	-2.4%		
	569+60	2.9%	-2.9%		
PC	569+71	3.3%	-3.3%		
	569+89	4.0%	-4.0%		
FS	570+06	4.7%	-4.7%	0.8	-4.1
	572+00	4.7%	-4.7%	0.6	-4.8
	574+00	4.7%	-4.7%	0.2	-4.2
	576+00	4.7%	-4.7%	0.9	-5.3
	578+00	4.7%	-4.7%	1.5	-4.7
FS	580+50	4.7%	-4.7%		
	580+67	4.0%	-4.0%		
PT	580+85	3.3%	-3.3%		
	580+96	2.9%	-2.9%		
	581+06	2.4%	-2.4%		
RC	581+17	2.0%	-2.0%		
	581+42	1.0%	-2.0%		
TS	581+67	0.0%	-2.0%		
	581+92	-1.0%	-2.0%		
NC	582+18	-2.0%	-2.0%		

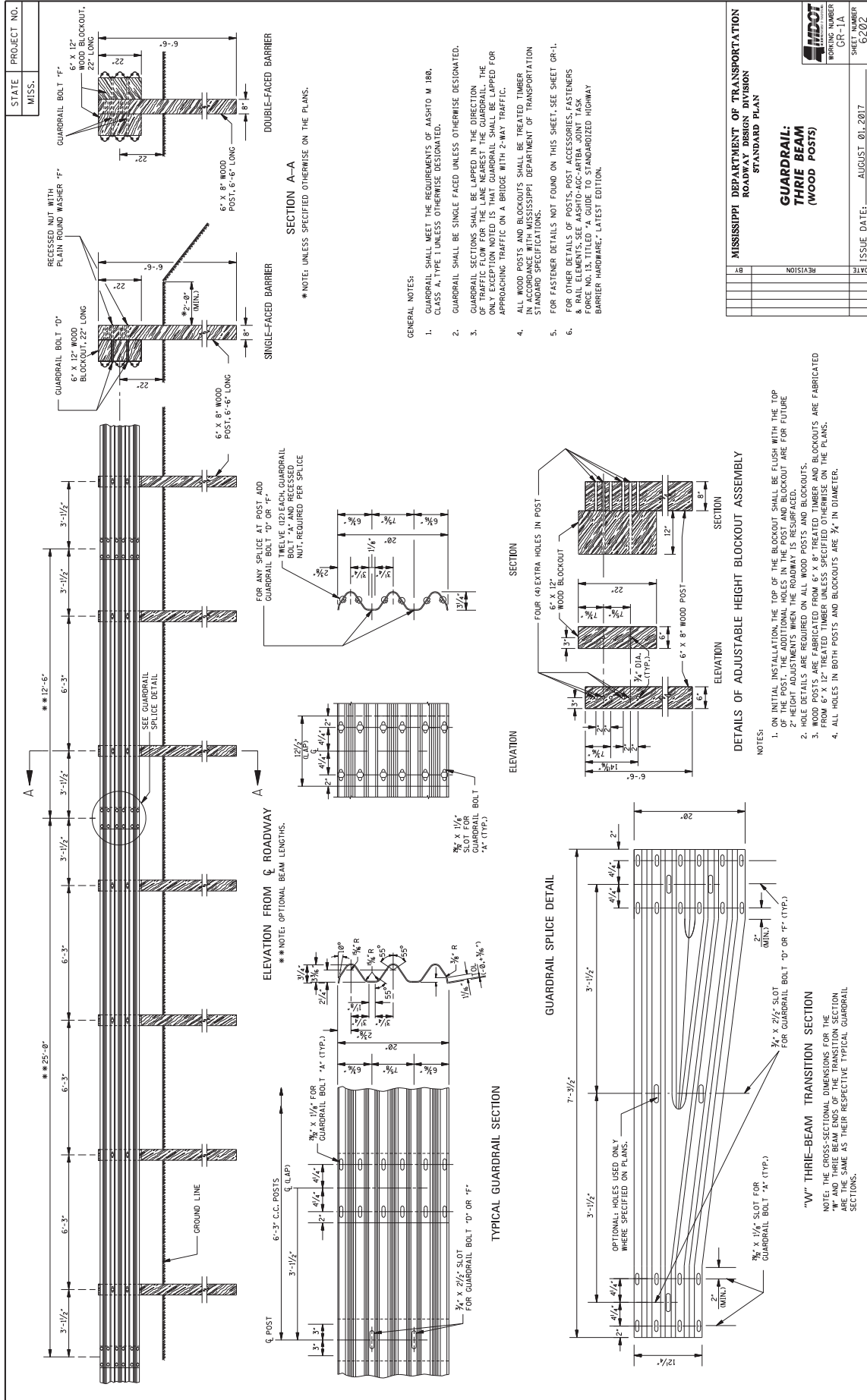
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

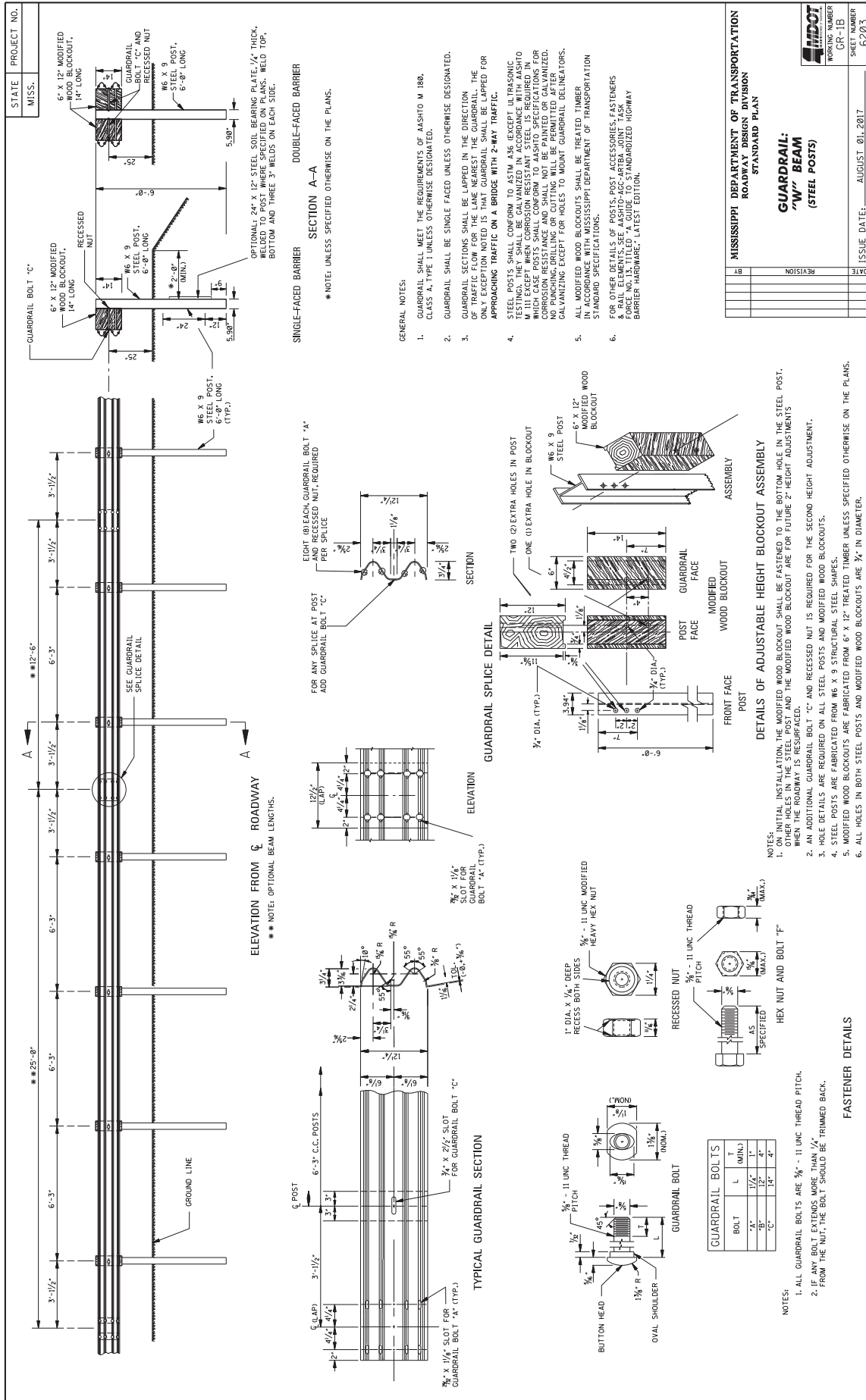
| **SUPPLEMENT TO NOTICE TO BIDDERS NO. 7624**

| **DATE:** **01/23/2026**

After the last drawing on page 33, add the following.







STATE PROJECT NO.	MISS.
MISS.	

TYPE "1" "W" BEAM END SECTION

NOTE: THE TYPE "1" BEAM END SECTION IS THE STANDARD FLARED "W" BEAM END SECTION AND IS USED WITH THE TYPE "A", AND TYPE "E" BRIDGE END SECTIONS. THE CROSS-SECTIONAL DIMENSIONS OF THIS PART ARE IDENTICAL TO THOSE OF THE STANDARD "W" BEAM GUARDRAIL.

TYPE "2" "W" BEAM END SECTION

NOTE: THE TYPE "2" BEAM END SECTION IS A MODIFICATION OF THE STANDARD ROUNDED "W" BEAM END SECTION AND IS USED EXCLUSIVELY WITH THE TYPE "C" BRIDGE END SECTION. THE CROSS-SECTIONAL DIMENSIONS OF THIS PART ARE IDENTICAL TO THOSE OF THE STANDARD "W" BEAM GUARDRAIL.

TYPE "3" "W" BEAM END SECTION

NOTE: THE TYPE "3" BEAM END SECTION IS THE STANDARD FLARED "W" BEAM END SECTION AND IS USED WITH THE TYPE "A", AND TYPE "E" BRIDGE END SECTIONS. THE CROSS-SECTIONAL DIMENSIONS OF THIS PART ARE IDENTICAL TO THOSE OF THE STANDARD "W" BEAM GUARDRAIL.

TYPE "4" "W" BEAM END SECTION

NOTE: THE TYPE "4" BEAM END SECTION IS THE STANDARD FLARED "W" BEAM END SECTION AND IS USED WITH THE TYPE "A", AND TYPE "E" BRIDGE END SECTIONS. THE CROSS-SECTIONAL DIMENSIONS OF THIS PART ARE IDENTICAL TO THOSE OF THE STANDARD "W" BEAM GUARDRAIL.

PLAN OF BRIDGE END SECTION

PAY LIMITS FOR BRIDGE END SECTION (APPLICABLE TO TYPE "A", "B", "C")

ELEVATION A-A

SECTION A-A

*NOTE: UNLESS OTHERWISE SPECIFIED ON THE PLANS.

FASTENER DETAILS

HIGH-STRENGTH STRUCTURAL HEX NUT AND BOLT "D"

RECTANGULAR GUARDRAIL PLATE WASHER

HARDENED ROUND WASHER "E"

FASTENER "F"

GENERAL NOTES:

- ALL NOTES AND DETAILS PERTAINING TO THIS SHEET NOT SPECIFICALLY MODIFIED ON THIS SHEET SHALL BE FOUND ON EITHER SHEET GR-1 (WOOD POSTS) OR SHEET GR-2 (STEEL POSTS).
- WOOD POSTS ARE SHOWN ON THIS SHEET. WHEN STEEL POSTS ARE INSTALLED, A SOIL BEARING PLATE IS PLACED ON THE TOP OF THE POST. THE RECTANGULAR GUARDRAIL PLATE WASHER IS PLACED UNDER THE STANDARD POST BOLT HEAD FOR POSTS 1 THRU 6. SEE DETAILS FOR STEEL POST INSTALLATIONS ON SHEET GR-2.
- FOR INFORMATION PERTAINING TO THE INSTALLATION OF THE TYPE "E" BRIDGE END SECTION, SEE SHEET GR-2A.
- THE HEIGHT OF RAIL AT THE BRIDGE END IS 29 1/2" AND WILL BE TRANSITIONED TO 25" AT POST 7.
- ALL GUARDRAIL ELEMENTS SHALL BE LAPPED IN THE DIRECTION OF TRAFFIC.
- POSTS WHICH ARE AT THE CENTERLINE BETWEEN THE ANCHOR BOLT, TYPE "E", SHALL BE CENTERED OVER THE CENTERLINE OF THE BRIDGE END WALL EXCEPT IN SKEWED BRIDGE ENDS WHERE THE BOLT NEAREST THE JOINT WILL BE A MINIMUM OF 3" FROM THE JOINT.

BRIDGE END SECTIONS AT BRIDGE END

TYPE "A" BRIDGE END SECTION

TYPE "C" BRIDGE END SECTION

GUARDRAIL CONNECTION AT BRIDGE END

SECTION B-B

SECTION C-C

ALTERNATIVE DETAIL OF PLATE "Z"

STANDARD DETAIL OF PLATE "B"

SECTION

GUARDRAIL CONNECTION DETAILS

NOTE: UNLESS OTHERWISE SPECIFIED, THE BLOCKOUT ASSEMBLY, FASTENER AND PLATE DETAILS SHOWN ABOVE ARE ALSO APPLICABLE TO THE TYPE "E" BRIDGE END SECTION. SEE GENERAL NOTE 3.

MISS.	REVISION
BR	DATE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

ROADWAY DESIGN DIVISION

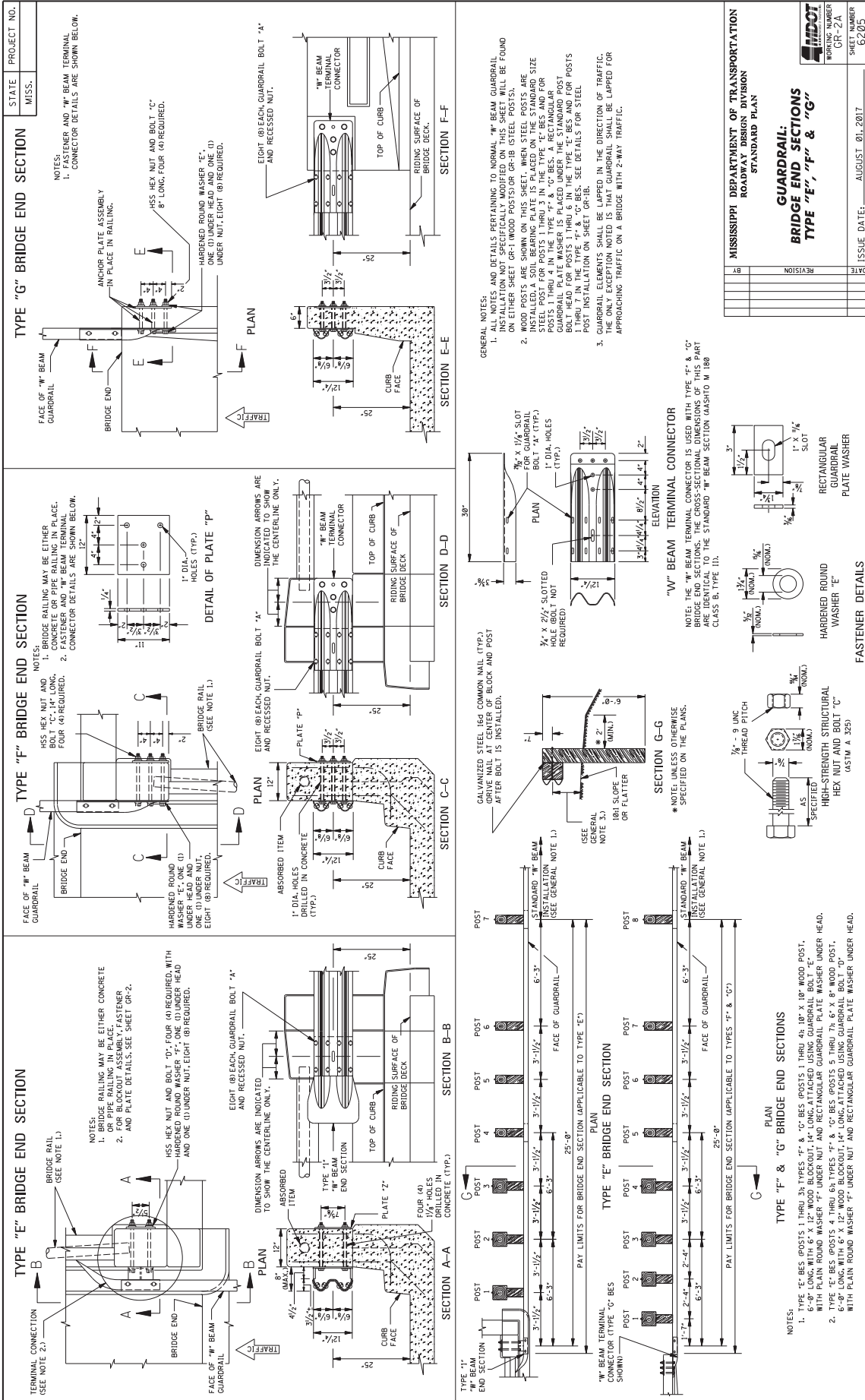
STANDARD PLAN

GUARDRAIL: BRIDGE END SECTIONS TYPE "A" & "C"

ISSUE DATE: AUGUST 01, 2017

SHEET NUMBER: GR-2

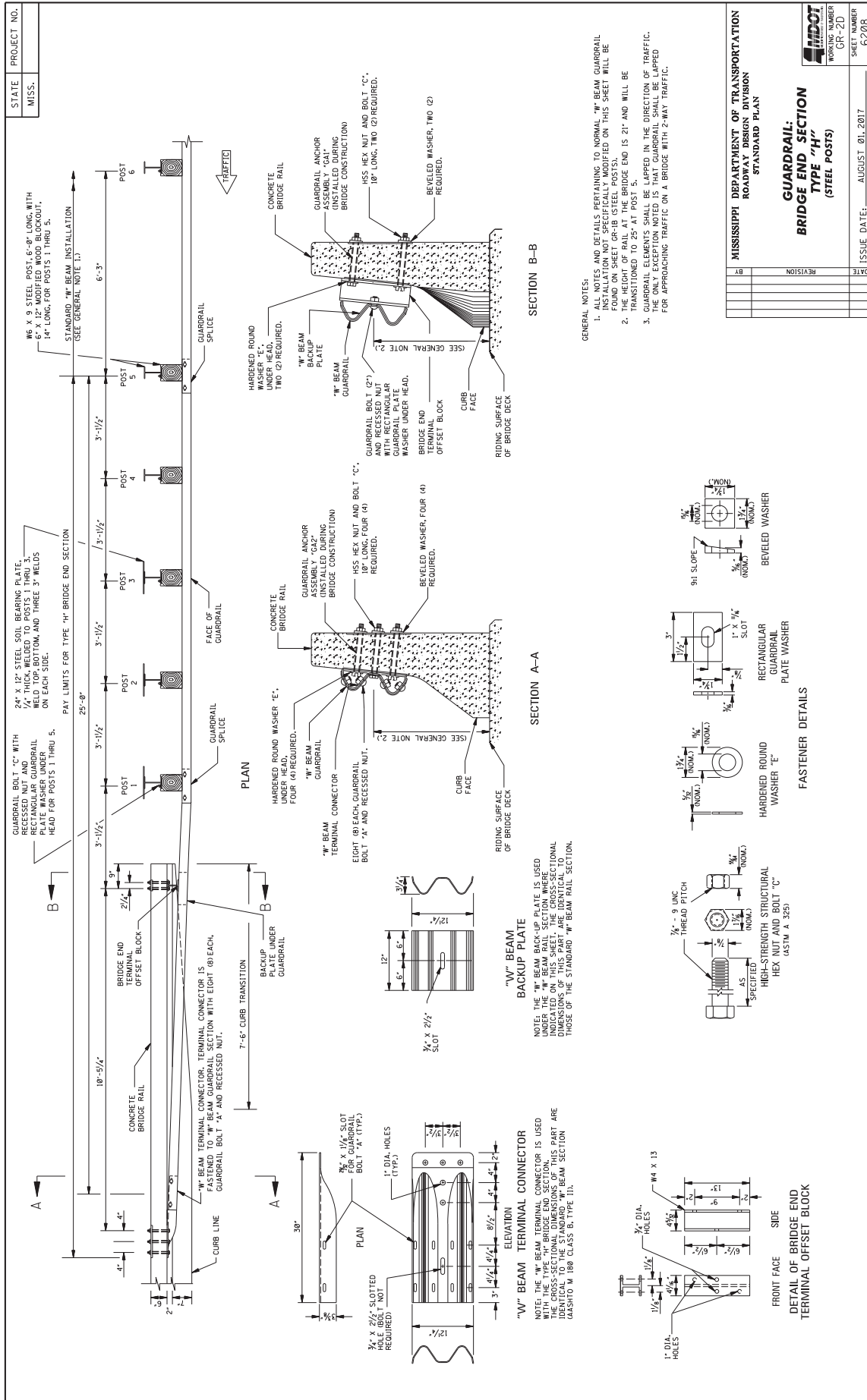
WORKING NUMBER: 6204

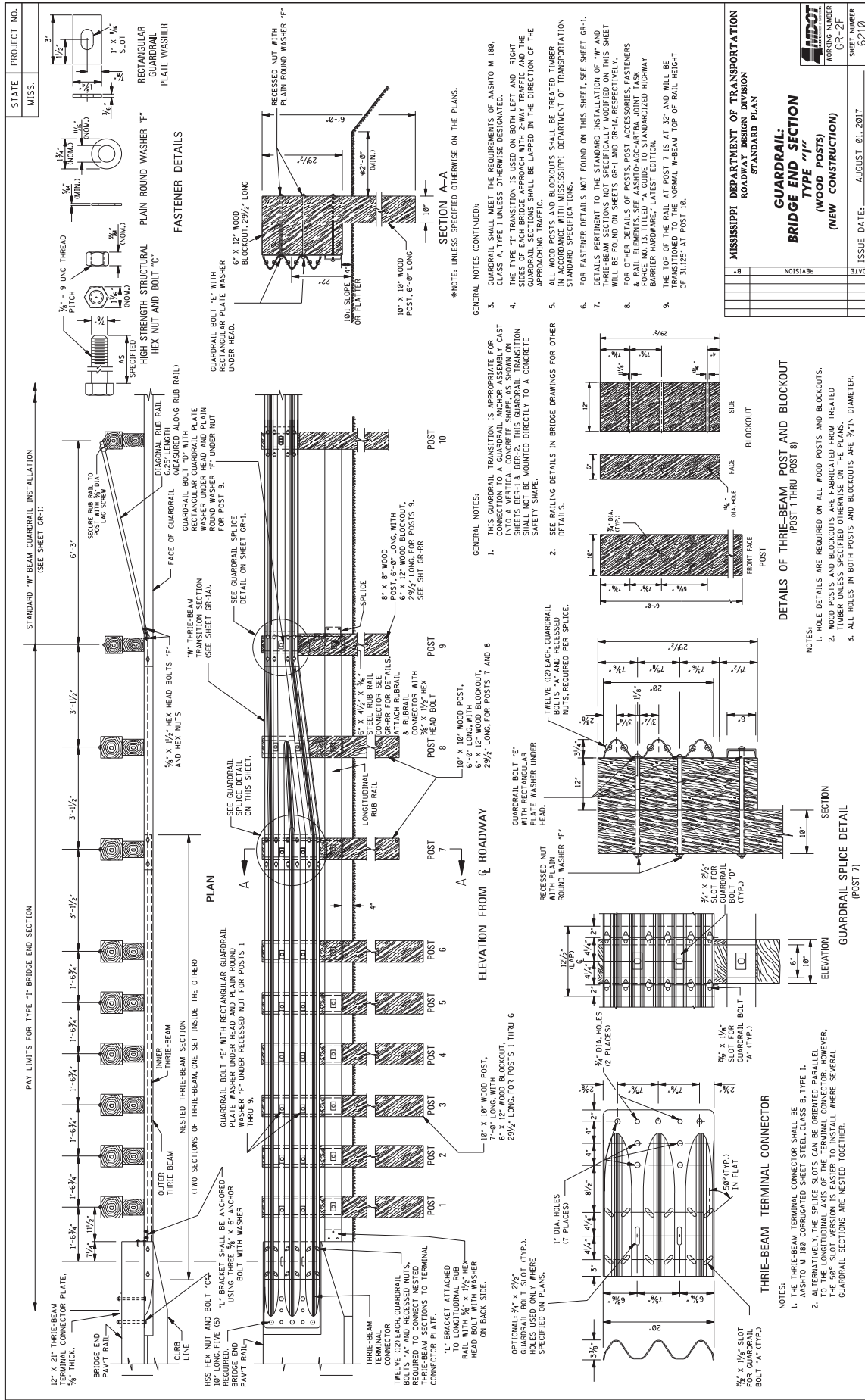


MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

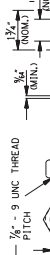
**GUARDRAIL:
BRIDGE END SECTIONS
TYPE "E", "F" & "G"**

ISSUE DATE: AUGUST 01, 2017
SHEET NUMBER: GN-2A
SHEET NUMBER: 02/05

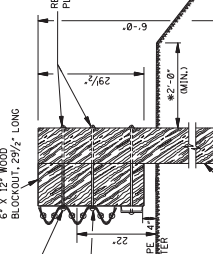




STATE PROJECT NO.
MISS.



FASTENER DETAILS



SECTION A-A

* NOTE: UNLESS SPECIFIED OTHERWISE ON THE PLANS.

- GENERAL NOTES (CONTINUED):
- 3. GUARDRAIL SHALL MEET THE REQUIREMENTS OF ASHTO M 180, CLASS A, TYPE 1 UNLESS OTHERWISE DESIGNATED.
- 4. WOOD POSTS SHALL BE FABRICATED FROM TREATED LUMBER TO THE SPECIFICATIONS OF ASHTO M 246. TRANSITION SECTIONS SHALL BE LAPPIED IN THE DIRECTION OF THE APPROACHING TRAFFIC.
- 5. ALL WOOD POSTS AND BLOCKOUTS SHALL BE TREATED TIMBER TO THE SPECIFICATIONS OF MISSISSIPPI DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS.
- 6. FOR FASTENER DETAILS NOT FOUND ON THIS SHEET, SEE SHEET GR-1.
- 7. THE-BEAM SECTIONS NOT SPECIALLY MODIFIED ON THIS SHEET WILL BE FOUND ON SHEETS GR-1 AND GR-14, RESPECTIVELY.
- 8. FOR OTHER DETAILS OF POSTS, POST ACCESSORIES, FASTENERS & RAIL ELEMENTS SEE ASHTO-HQ-ARTBA JOINT TASK FORCE REPORT, 'GUARDRAIL DESIGN FOR ENHANCED HIGHWAY BARRIER HARDWARE', LATEST EDITION.
- 9. THE TOP OF THE RAIL AT POST 7 IS AT 32" AND WILL BE TRANSITIONED TO THE NORMAL W-BEAM TOP OF RAIL HEIGHT OF 31 1/2" AT POST 10.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
REVISION	DATE

GUARDRAIL: BRIDGE END SECTION TYPE '1'
(NEW CONSTRUCTION)

ISSUE DATE: AUGUST 01, 2017

WORKING NUMBER: GR-2F
SHEET NUMBER: 6210

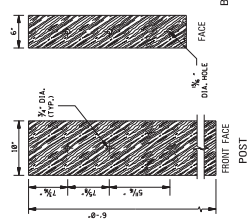
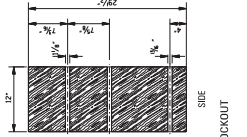
GENERAL NOTES:

- 1. THIS GUARDRAIL TRANSITION IS APPROPRIATE FOR CAST INTO A VERTICAL CONCRETE SHAPE AS SHOWN ON SHEETS BER-1 & BER-2. THIS GUARDRAIL TRANSITION SHALL NOT BE MOUNTED DIRECTLY TO A CONCRETE SAFETY SHAPE.
- 2. SEE RAILING DETAILS IN BRIDGE DRAWINGS FOR OTHER DETAILS.

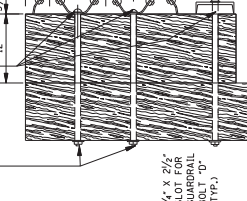
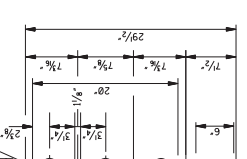
GENERAL NOTES:

- 1. HOLE DETAILS ARE REQUIRED ON ALL WOOD POSTS AND BLOCKOUTS.
- 2. WOOD POSTS AND BLOCKOUTS ARE FABRICATED FROM TREATED TIMBER UNLESS SPECIFIED OTHERWISE ON THE PLANS.
- 3. ALL HOLES IN BOTH POSTS AND BLOCKOUTS ARE 7/8 IN DIAMETER.

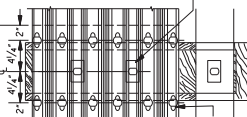
DETAILS OF THREE-BEAM POST AND BLOCKOUT (POST 1 THRU POST 8)



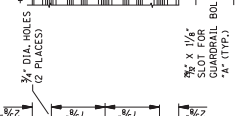
DETAILS OF GUARDRAIL SPLICE DETAIL (POST 7)



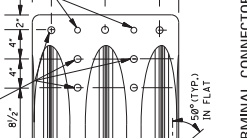
THREE-BEAM TERMINAL CONNECTOR



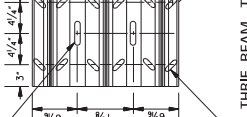
ELEVATION



SECTION

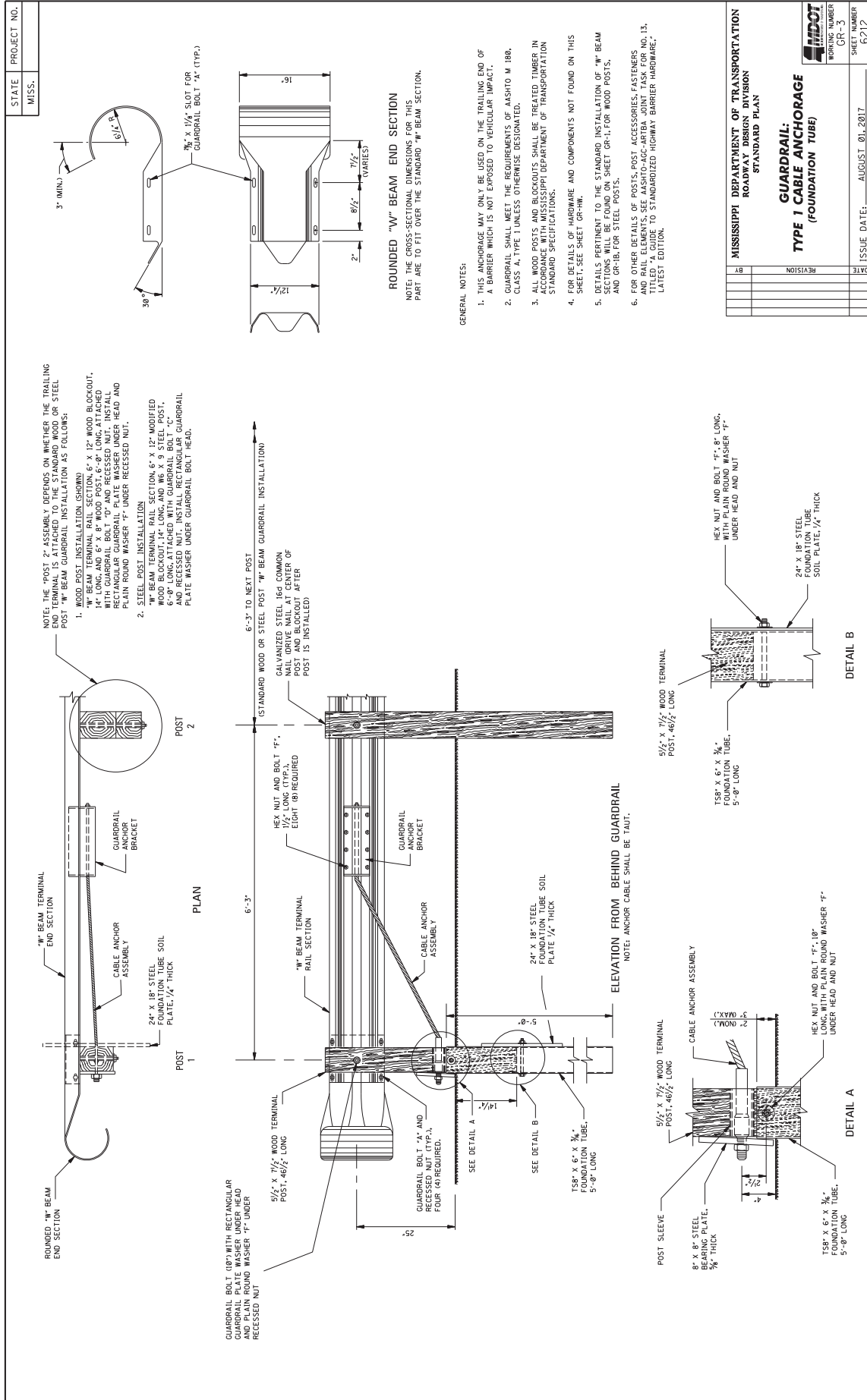


GUARDRAIL SPLICE DETAIL

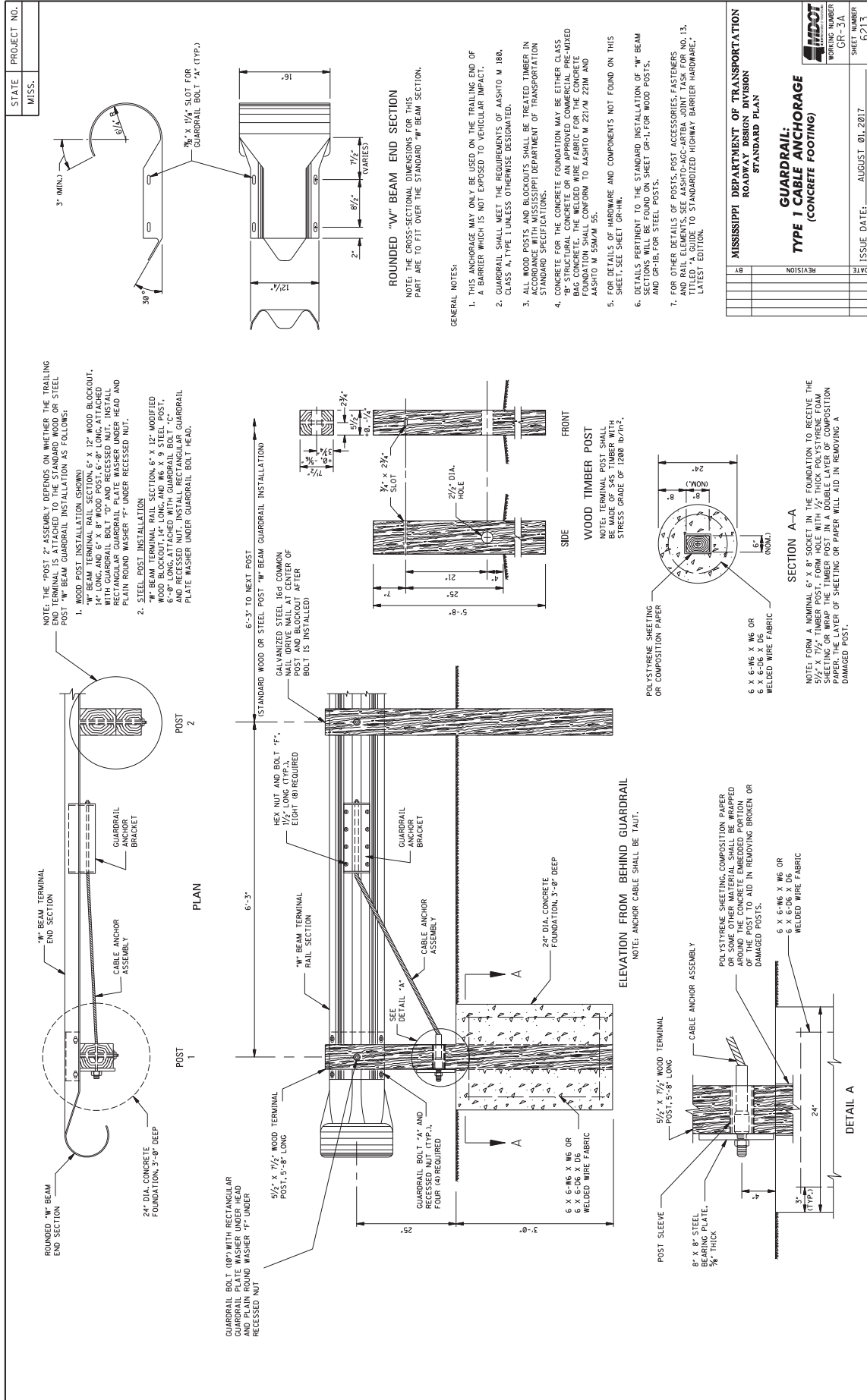


NOTES:

- 1. THE THREE-BEAM TERMINAL CONNECTOR SHALL BE ASHTO M 180 CORRUGATED SHEET STEEL, CLASS B, TYPE 1.
- 2. TO THE LONGITUDINAL AXIS OF THE TERMINAL CONNECTOR, HOWEVER, THE 50° SLOT VERSION IS EASIER TO INSTALL WHERE SEVERAL GUARDRAIL SECTIONS ARE NESTED TOGETHER.



MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
GUARDRAIL: TYPE 1 CABLE ANCHORAGE (FOUNDATION TUBE)	
ISSUE DATE:	AUGUST 01, 2017
SHEET NUMBER	6212
WORKING NUMBER	GR-3



NOTE: THE "POST 2" ASSEMBLY DEPENDS ON WHETHER THE TRAILING END TERMINAL IS ATTACHED TO THE STANDARD WOOD OR STEEL POST "W" BEAM GUARDRAIL INSTALLATION AS FOLLOWS:

1. WOOD POST INSTALLATION (SHOWN)
"W" BEAM TERMINAL RAIL SECTION, 6" x 12" WOOD BLOCKOUT, 6" x 6" x 6'-0" LONG ATTACHED TO GUARDRAIL BOLT "C" WITH GUARDRAIL BOLT "D" AND RECESSED NUT. INSTALL RECTANGULAR GUARDRAIL PLATE WASHER UNDER HEAD AND PLAIN ROUND WASHER "F" UNDER RECESSED NUT.
2. STEEL POST INSTALLATION
"W" BEAM TERMINAL RAIL SECTION, 6" x 12" MODIFIED WOOD BLOCKOUT ATTACHED TO GUARDRAIL BOLT "C" WITH GUARDRAIL BOLT "D" AND RECESSED NUT. INSTALL RECTANGULAR GUARDRAIL PLATE WASHER UNDER GUARDRAIL BOLT HEAD.

ROUNDED "W" BEAM END SECTION
NOTE: THE CROSS-SECTIONAL DIMENSIONS FOR THIS PART ARE TO FIT OVER THE STANDARD "W" BEAM SECTION.

GENERAL NOTES:

1. THIS ANCHORAGE MAY ONLY BE USED ON THE TRAILING END OF A BARRIER WHICH IS NOT EXPOSED TO VEHICULAR IMPACT.
2. GUARDRAIL SHALL MEET THE REQUIREMENTS OF AASHTO M 188, CLASS A, TYPE 1 UNLESS OTHERWISE DESIGNATED.
3. ALL WOOD POSTS AND BLOCKOUTS SHALL BE TREATED TIMBER IN ACCORDANCE WITH MISSISSIPPI DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS.
4. CONCRETE FOR THE CONCRETE FOUNDATION MAY BE EITHER CLASS "B" STRUCTURAL CONCRETE OR AN APPROVED COMMERCIAL PRE-MIXED BAG CONCRETE. THE WELDED WIRE FABRIC FOR THE CONCRETE FOUNDATION SHALL CONFORM TO AASHTO M 221/M 221M AND AASHTO M 58/M 58.
5. FOR DETAILS OF HARDWARE AND COMPONENTS NOT FOUND ON THIS SHEET, SEE SHEET GR-404.
6. DETAILS PERTINENT TO THE STANDARD INSTALLATION OF "W" BEAM GUARDRAIL SHALL BE SHOWN ON SHEET GR-1 FOR WOOD POSTS, AND GR-18 FOR STEEL POSTS.
7. FOR OTHER DETAILS OF POSTS, POST ACCESSORIES, FASTENERS AND HARDWARE, SEE SHEET GR-10, GR-11, GR-12, GR-13, GR-14, GR-15, GR-16, GR-17, GR-18, GR-19, GR-20, GR-21, GR-22, GR-23, GR-24, GR-25, GR-26, GR-27, GR-28, GR-29, GR-30, GR-31, GR-32, GR-33, GR-34, GR-35, GR-36, GR-37, GR-38, GR-39, GR-40, GR-41, GR-42, GR-43, GR-44, GR-45, GR-46, GR-47, GR-48, GR-49, GR-50, GR-51, GR-52, GR-53, GR-54, GR-55, GR-56, GR-57, GR-58, GR-59, GR-60, GR-61, GR-62, GR-63, GR-64, GR-65, GR-66, GR-67, GR-68, GR-69, GR-70, GR-71, GR-72, GR-73, GR-74, GR-75, GR-76, GR-77, GR-78, GR-79, GR-80, GR-81, GR-82, GR-83, GR-84, GR-85, GR-86, GR-87, GR-88, GR-89, GR-90, GR-91, GR-92, GR-93, GR-94, GR-95, GR-96, GR-97, GR-98, GR-99, GR-100.

SECTION A-A

NOTE: FORM A NOMINAL 6" x 8" SOCKET IN THE FOUNDATION TO RECEIVE THE 5/2" x 7/2" WOOD TERMINAL. FORM HOLE WITH 1/2" THICK POLYSTYRENE FOAM INSULATION. POLYSTYRENE SHEETING OR COMPOSITION PAPER, THE LAYER OF SHEETING OR PAPER WILL AID IN REMOVING A DAMAGED POST.

ELEVATION FROM BEHIND GUARDRAIL
NOTE: ANCHOR CABLE SHALL BE TAUT.

POLYSTYRENE SHEETING, COMPOSITION PAPER OR COMPOSITION PAPER TAPPED AROUND THE CONCRETE EMERGED PORTION OF THE POST TO AID IN REMOVING BROKEN OR DAMAGED POSTS.

6 x 6-86 x D6 OR
6 x 6-86 x D6
WELDED WIRE FABRIC

DETAIL A

POST SLEEVE
8" x 8" STEEL BEARING PLATE, 3/4" THICK
5/2" x 7/2" WOOD TERMINAL POST, 15'-0" LONG
CABLE ANCHOR ASSEMBLY
POLYSTYRENE SHEETING, COMPOSITION PAPER OR COMPOSITION PAPER TAPPED AROUND THE CONCRETE EMERGED PORTION OF THE POST TO AID IN REMOVING BROKEN OR DAMAGED POSTS.
6 x 6-86 x D6 OR
6 x 6-86 x D6
WELDED WIRE FABRIC

GUARDRAIL BOLT (10") WITH RECTANGULAR GUARDRAIL PLATE WASHER UNDER HEAD AND PLAIN ROUND WASHER "F" UNDER RECESSED NUT

24" DIA. CONCRETE FOUNDATION, 3'-0" DEEP

5/2" x 7/2" WOOD TERMINAL POST, 15'-8" LONG

HEX NUT AND BOLT "F", 1/2" LONG (TYP.), EIGHT (8) REQUIRED

GUARDRAIL ANCHOR BRACKET

GUARDRAIL ANCHOR BRACKET

GUARDRAIL ANCHOR BRACKET

GUARDRAIL ANCHOR BRACKET

GUARDRAIL ANCHOR BRACKET

GUARDRAIL ANCHOR BRACKET

GUARDRAIL ANCHOR BRACKET

GUARDRAIL ANCHOR BRACKET

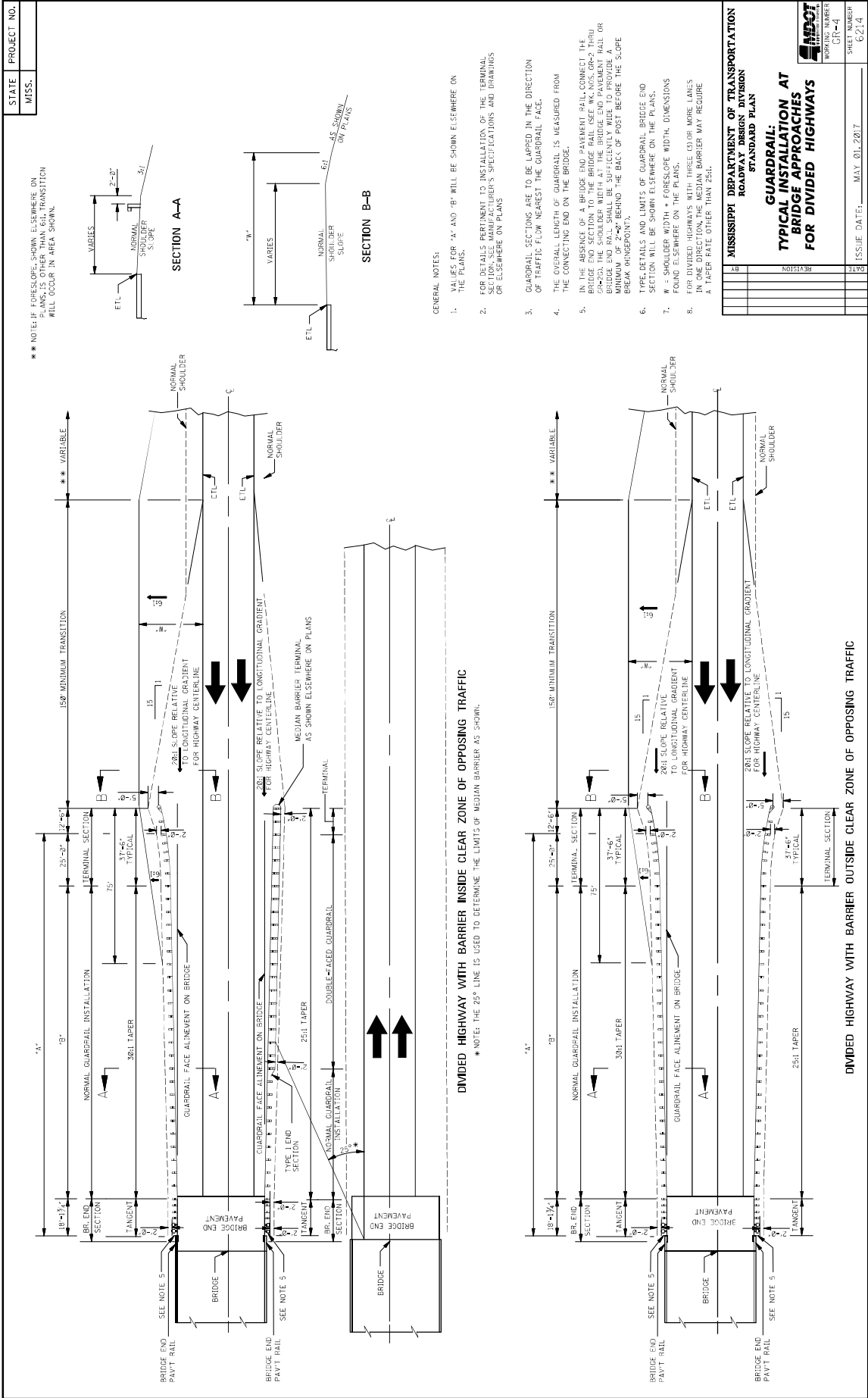
GUARDRAIL ANCHOR BRACKET

GUARDRAIL ANCHOR BRACKET

GUARDRAIL ANCHOR BRACKET

GUARDRAIL ANCHOR BRACKET

GUARDRAIL ANCHOR BRACKET



STATE PROJECT NO.
MISS.

** NOTE: IF FORESLOPE, SHOWN ELSEWHERE ON PLANS, IS OTHER THAN 6:1, TRANSITION WILL OCCUR IN AREA SHOWN.

SECTION A-A

SECTION B-B

GENERAL NOTES:

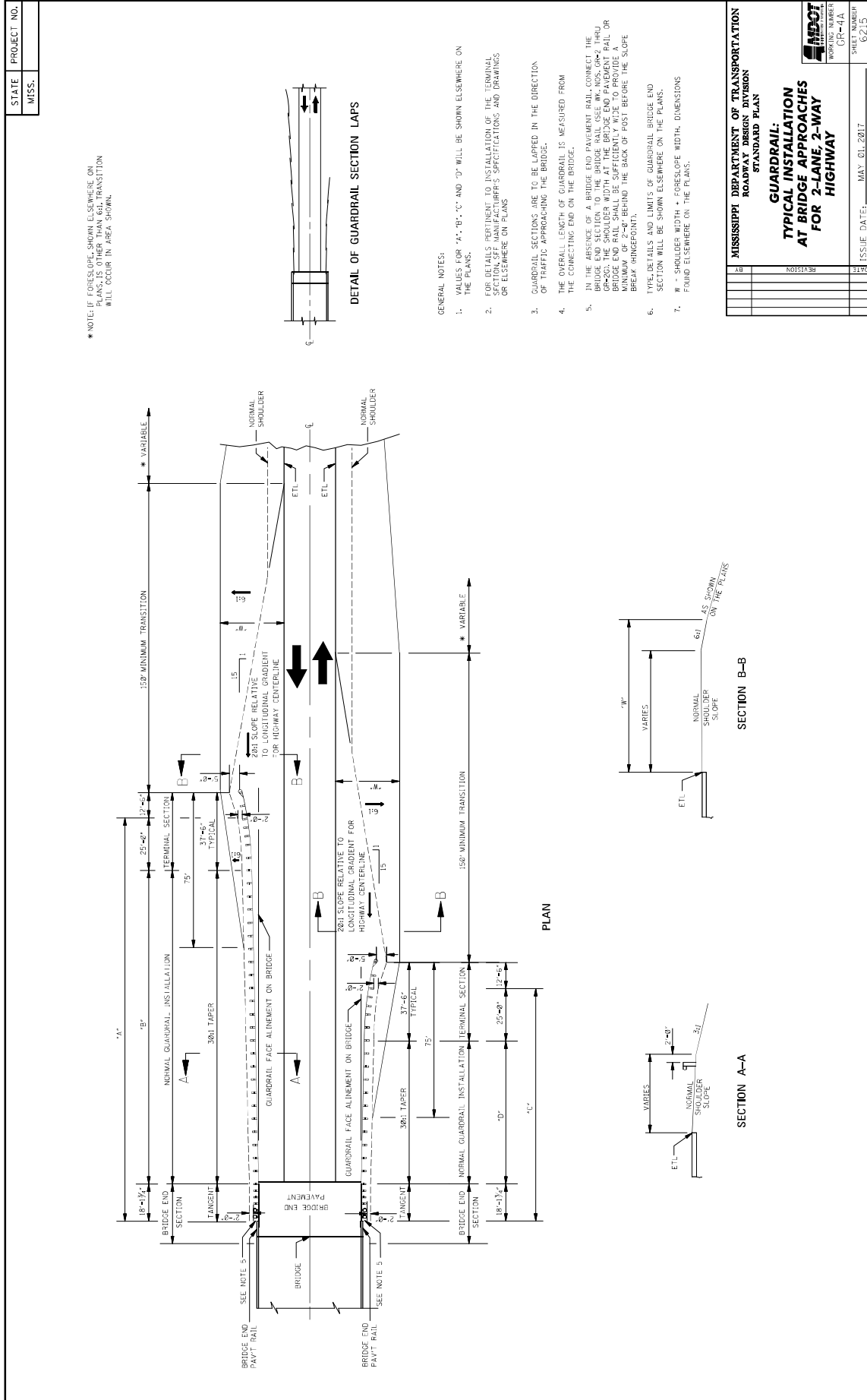
- VALUES FOR 'X' AND 'B' WILL BE SHOWN ELSEWHERE ON THE PLANS.
- FOR DETAILS PERTINENT TO INSTALLATION OF THE TERMINAL SECTION, SEE MANUFACTURER'S SPECIFICATIONS AND DRAWINGS OR ELSEWHERE ON PLANS.
- GUARDRAIL SECTIONS ARE TO BE LAPPED IN THE DIRECTION OF TRAFFIC FLOW NEAREST THE GUARDRAIL FACE.
- THE OVERALL LENGTH OF GUARDRAIL IS MEASURED FROM THE CONNECTING END ON THE BRIDGE.
- IN THE ABSENCE OF A BRIDGE END PAVEMENT RAIL, CONNECT THE BRIDGE END SECTION TO THE BRIDGE RAIL (SEE W-6, W-6R, W-2, THRU W-6R-2). THE SHOULDER WIDTH AT THE BRIDGE END PAVEMENT RAIL OR BRIDGE END SHALL BE SUFFICIENTLY WIDE TO PROVIDE A BREAK HINGEPONT BEHIND THE BACK OF POST BEFORE THE SCOPE.
- TYPE DETAILS AND LIMITS OF GUARDRAIL, BRIDGE END SECTION WILL BE SHOWN ELSEWHERE ON THE PLANS.
- W-6 SHOULDER WITH A FORESLOPE WIDTH, DIMENSIONS FOUND ELSEWHERE ON THE PLANS.
- FOR DIVIDED HIGHWAYS WITH THREE OR MORE LANES IN ONE DIRECTION, THE MEDIAN BARRIER MAY REQUIRE A TAPER RATE OTHER THAN 25:1.

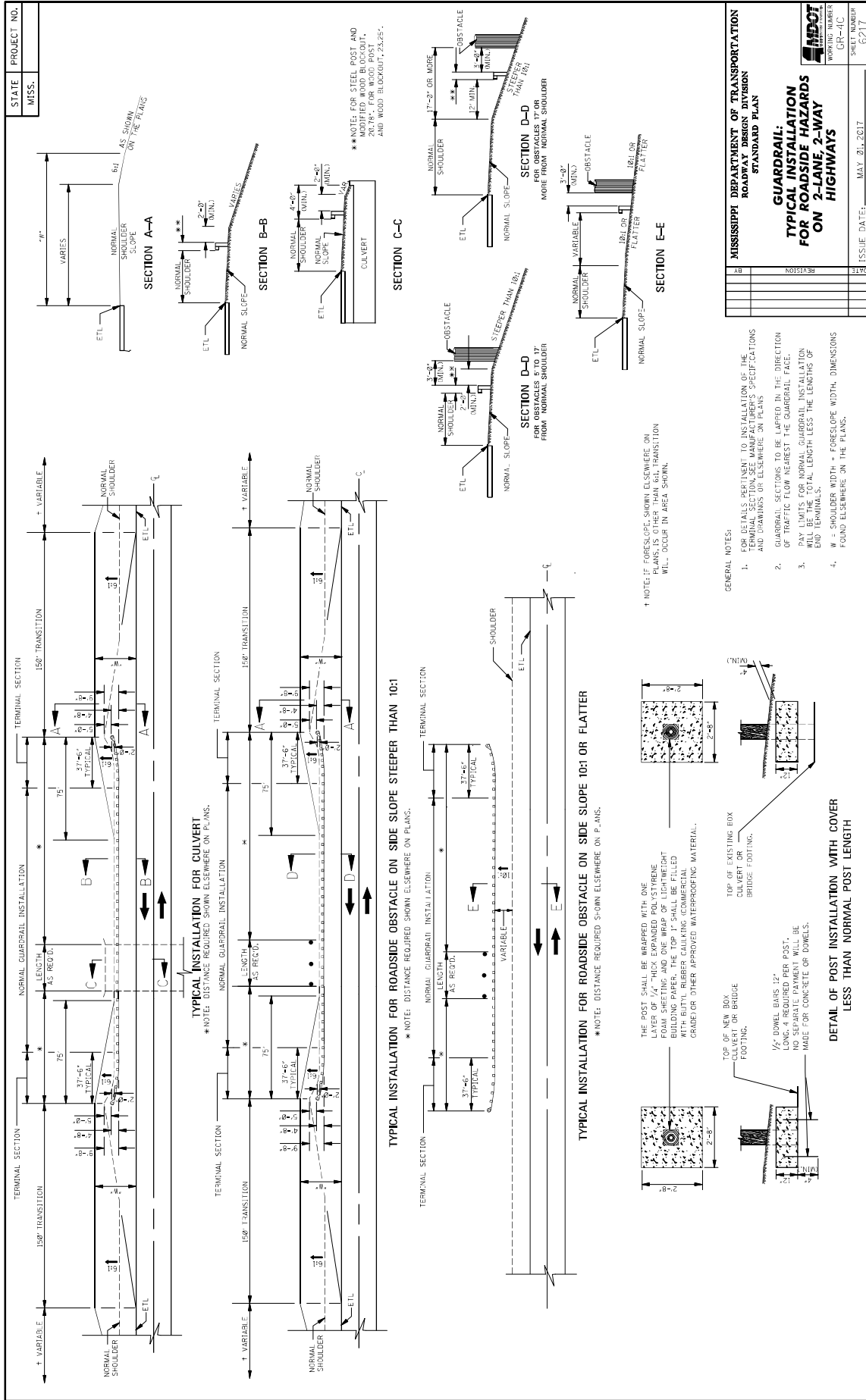
MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
GUARDRAIL: TYPICAL INSTALLATION AT BRIDGE APPROACHES FOR DIVIDED HIGHWAYS	
DATE	ISSUE DATE: MAY 01, 2017
REVISION	WORKING NUMBER CR-4
	SHEET NUMBER 6214

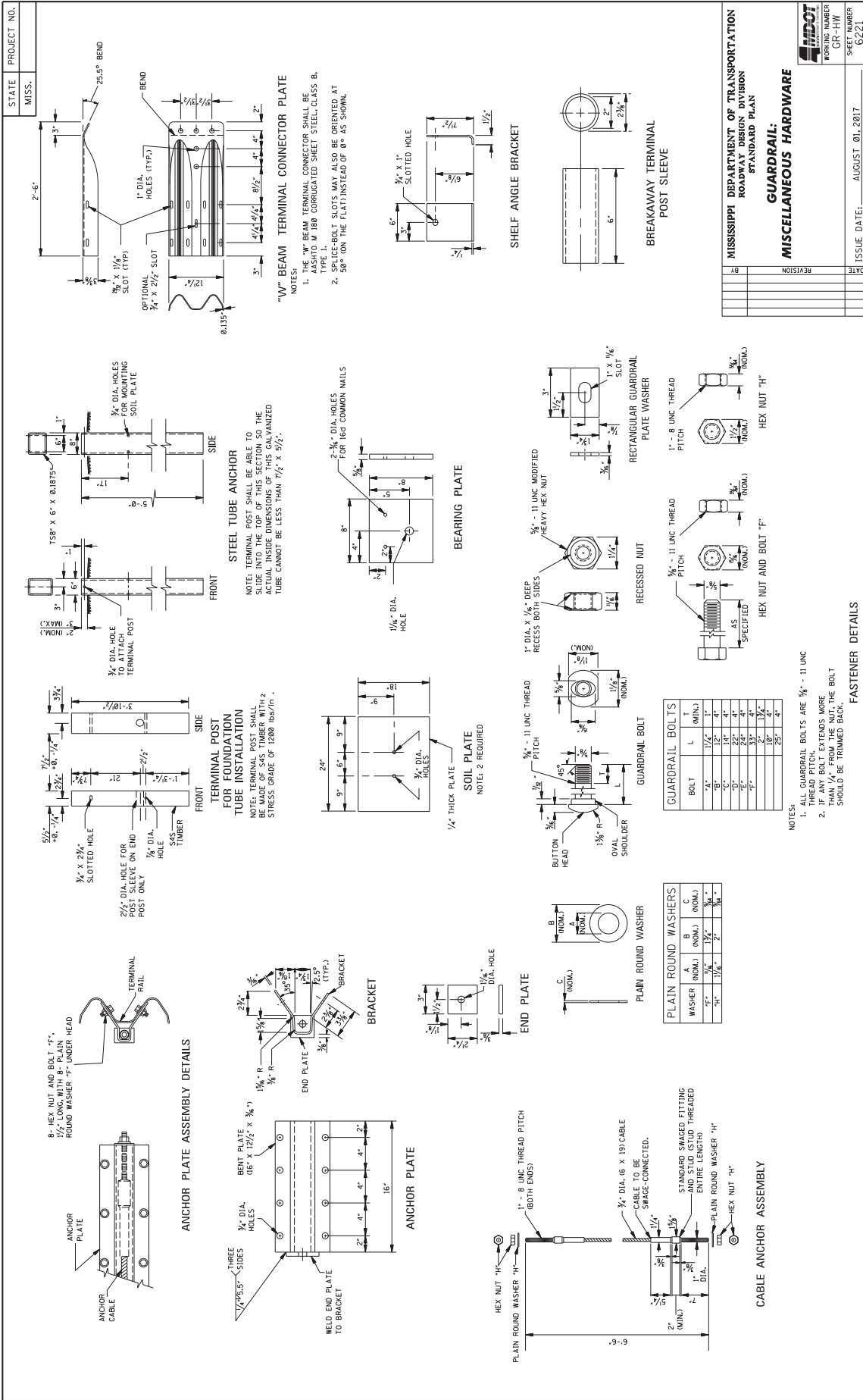
DIVIDED HIGHWAY WITH BARRIER INSIDE CLEAR ZONE OF OPPOSING TRAFFIC

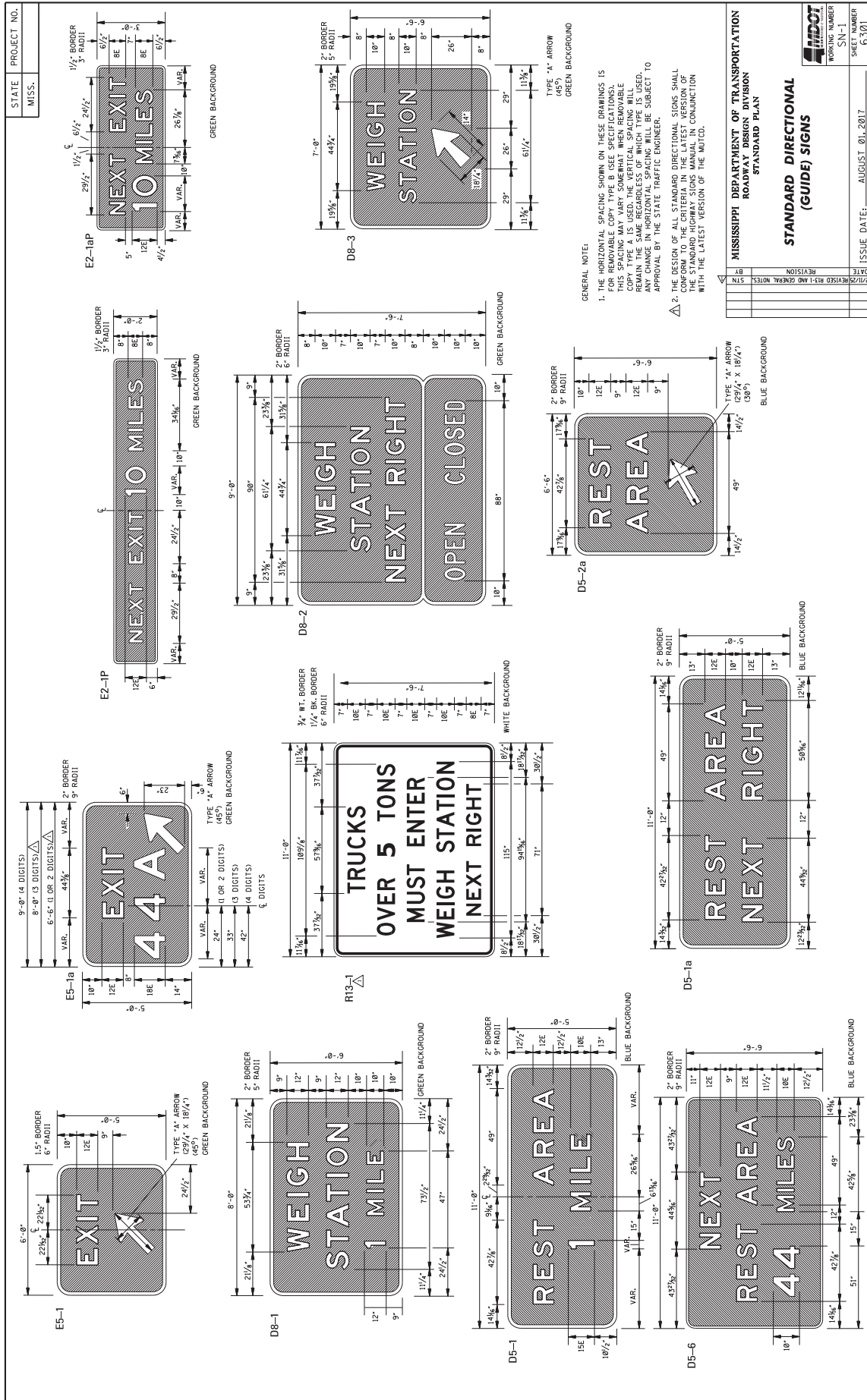
* NOTE: THE 25° LINE IS USED TO DETERMINE THE LIMITS OF MEDIAN BARRIER AS SHOWN.

DIVIDED HIGHWAY WITH BARRIER OUTSIDE CLEAR ZONE OF OPPOSING TRAFFIC

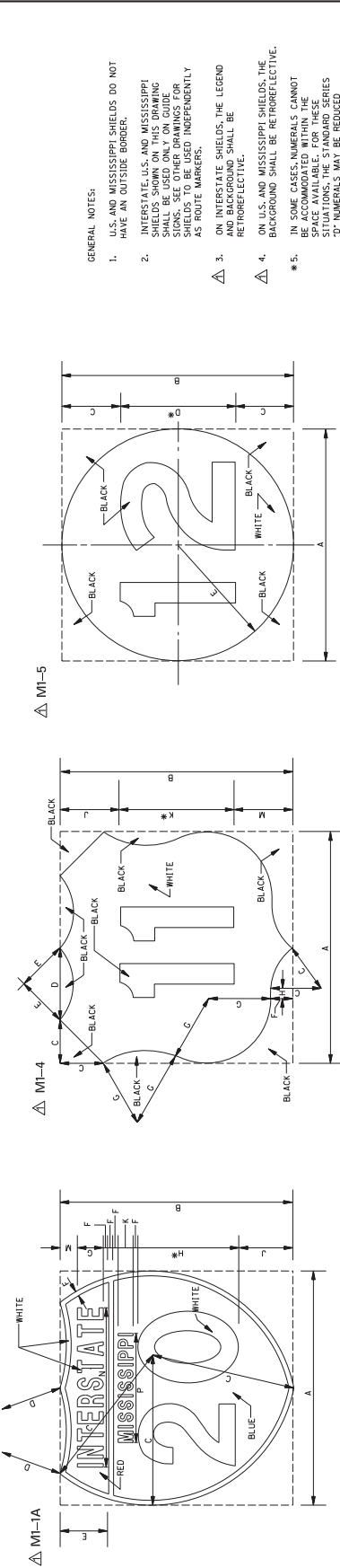








STATE	PROJECT NO.
MISS.	



12" NUMERALS		18" NUMERALS		24" NUMERALS	
2 DIGITS	3 DIGITS	2 DIGITS	3 DIGITS	2 DIGITS	3 DIGITS
A	24"	30"	36"	45"	60"
B	24"	24"	36"	36"	48"
C	15"	17"	25 1/2"	29 1/2"	34"
D	5"	5"	7 1/2"	7 1/2"	10"
E	5"	5"	7 1/2"	7 1/2"	10"
F	1/2"	1/2"	3/4"	3/4"	1"
G	2 1/2"	4"	4"	4"	5"
H	100	100	150	150	200
I	100	100	150	150	200
J	1/20	1/20	2/30	2/30	3/30
K	2"	2"	2 3/4"	2 3/4"	4"
L	15 3/4"	21 3/4"	24 1/2"	34 3/4"	43 3/4"
M	10 3/4"	15 3/4"	16 3/4"	21 1/2"	21 1/2"

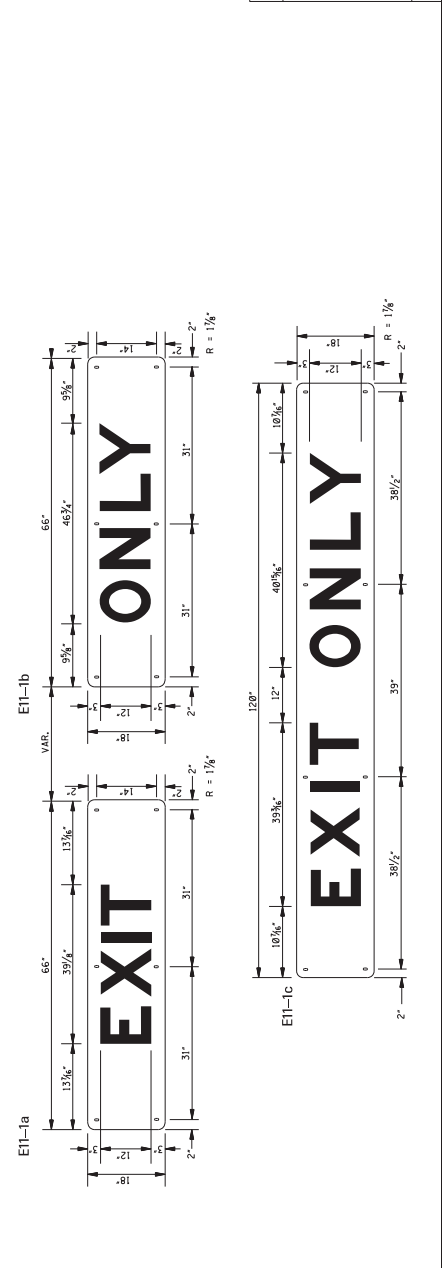
12" NUMERALS		18" NUMERALS		24" NUMERALS	
2 DIGITS	3 DIGITS	2 DIGITS	3 DIGITS	2 DIGITS	3 DIGITS
A	24"	30"	36"	45"	60"
B	24"	30"	36"	45"	60"
C	5"	5"	7 1/2"	7 1/2"	10"
D	7"	10"	10 1/2"	13 1/2"	14"
E	5"	5"	7 1/2"	7 1/2"	10"
F	2"	2"	3"	3"	4"
G	1"	4"	1 1/2"	5 1/2"	2"
H	1"	4"	1 1/2"	5 1/2"	2"
I	9 1/2"	9 1/2"	8 1/2"	8 1/2"	11"
J	12"	12"	18"	18"	24"
K	6 1/2"	6 1/2"	9 3/4"	9 3/4"	13"

DIMENSIONS (INCHES)					
A	B	C	D	E	F
12-DIGIT(S)	24"	36"	48"	18"	18"
12-DIGIT(S)	36"	36"	36"	18"	18"
12-DIGIT(S)	48"	48"	48"	12"	24"
3-DIGIT(S)	30"	24"	6"	12"	VAR.
3-DIGIT(S)	45"	36"	9"	18"	VAR.
3-DIGIT(S)	60"	48"	12"	24"	VAR.

- GENERAL NOTES:
- U.S. AND MISSISSIPPI SHIELDS DO NOT HAVE AN OUTSIDE BORDER.
 - INTERSTATE, U.S. AND MISSISSIPPI SHIELDS SHALL BE USED ONLY ON GUIDE SIGNS. SEE OTHER DRAWINGS FOR SHIELDS TO BE USED INDEPENDENTLY AS ROUTE MARKERS.
 - ON INTERSTATE SHIELDS, THE LEGEND BACKGROUND SHALL BE RETROREFLECTIVE.
 - ON U.S. AND MISSISSIPPI SHIELDS, THE BACKGROUND SHALL BE RETROREFLECTIVE.
 - IN SOME CASES, NUMERALS CANNOT BE ACCOMMODATED WITHIN THE SHIELD. IN THESE SITUATIONS, THE STANDARD SERIES "D" NUMERALS MAY BE REDUCED TO SERIES "C" NUMERALS OR AS SMALLER HEIGHT COMMONLY AVAILABLE.
 - THE DESIGN OF ALL ROUTE SHIELDS SHALL CONFORM TO THE CRITERIA IN THE LATEST VERSION OF THE MUTCD. THE DESIGN OF ALL "EXIT ONLY" PLACQUES SHALL CONFORM TO THE CRITERIA IN THE LATEST VERSION OF THE MUTCD.
 - FOR OVERHEAD SIGN INSTALLATIONS USE M-1.

GENERAL NOTES:

- THESE ARE OVERHEAD SIGNS FABRICATED ON 8 MIL THICK SIGN SHEET METAL AND SHALL BE PAINTED TO REFLECTORIAL PAINT AS DESIGNATED ON PLANS. THESE SIGNS WILL NOT BE PAID FOR AS SEPARATE SIGNS BUT SHALL BE CONSIDERED AS PART OF THE MAJOR SIGNS TO WHICH THEY ARE AFFIXED.
- LETTER SIZE: 12" SERIES "D"
COLOR: LEGEND - BLACK; BACKGROUND - HI-INTENSITY YELLOW
RETROREFLECTIVE TYPE AS SPECIFIED IN THE PLANS
- THE DESIGN OF ALL "EXIT ONLY" PLACQUES SHALL CONFORM TO THE CRITERIA IN THE LATEST VERSION OF THE MUTCD. THE DESIGN OF ALL "EXIT ONLY" PLACQUES SHALL CONFORM WITH THE LATEST VERSION OF THE MUTCD.



DATE	REVISION	BY
DATE	REVISION	BY

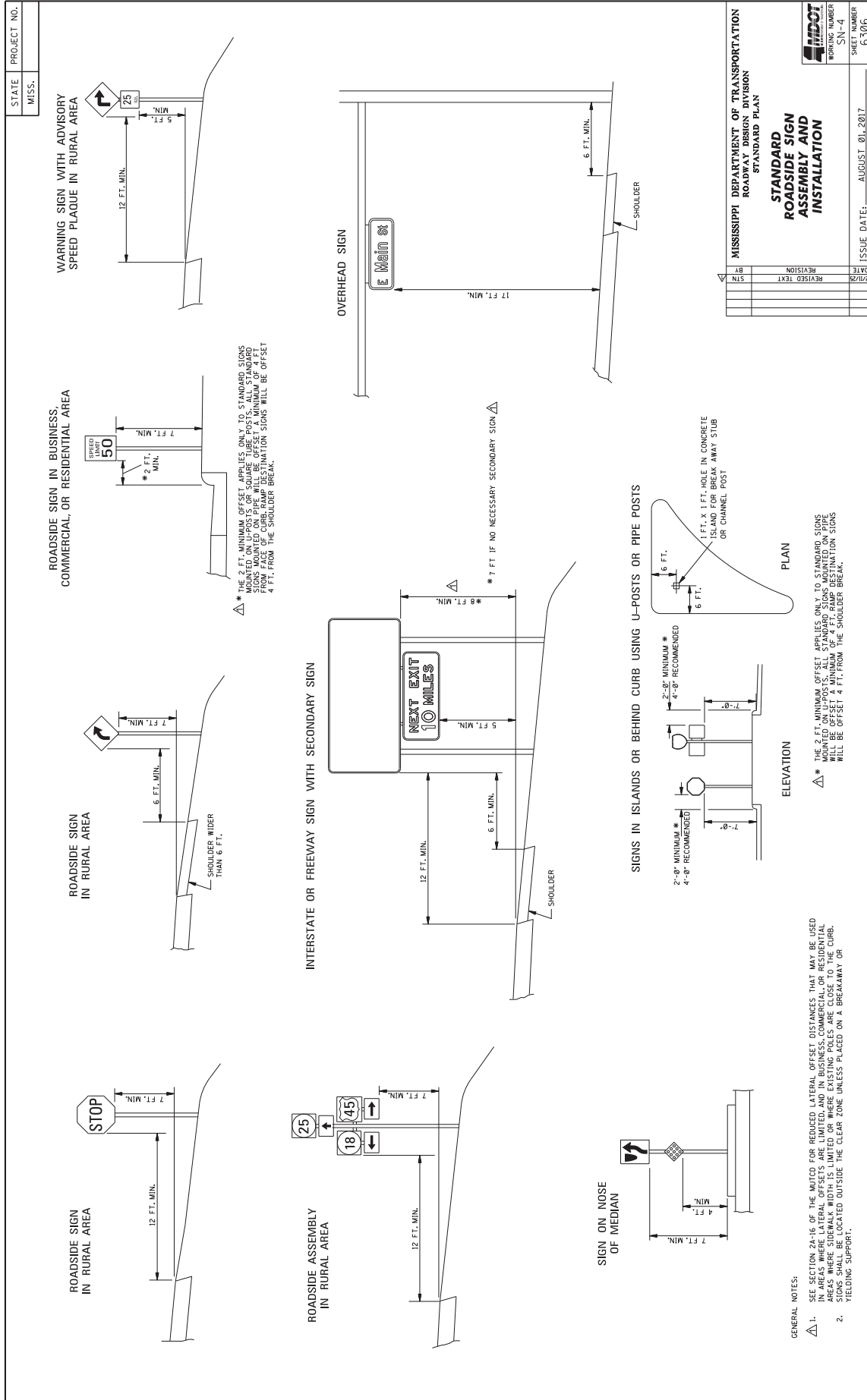
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**ROUTE SHIELDS
AND
"EXIT ONLY" PANELS**

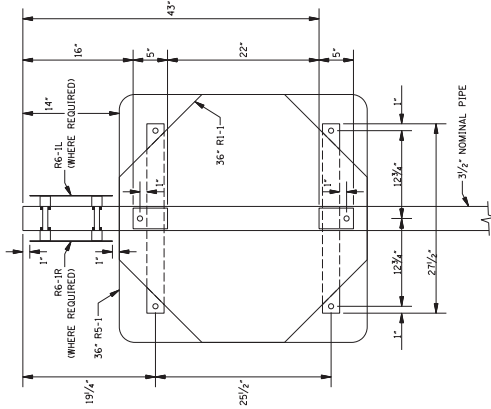
ISSUE DATE: AUGUST 01, 2017

WORKING NUMBER: SN-2

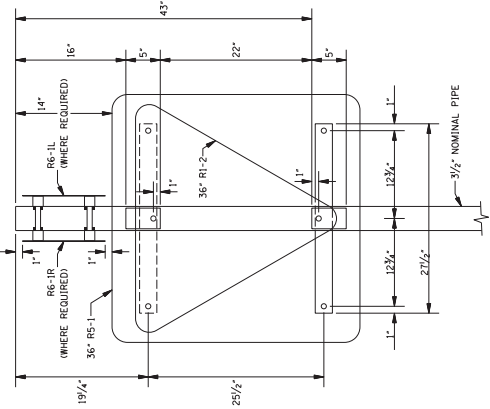
SHEET NUMBER: G302



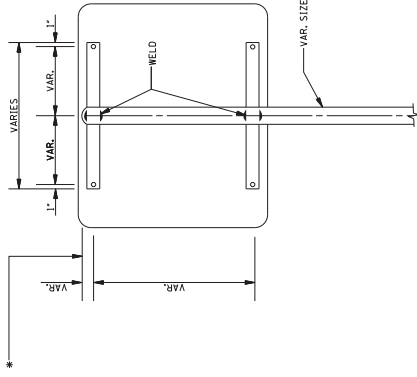
STATE	PROJECT NO.
MISS.	



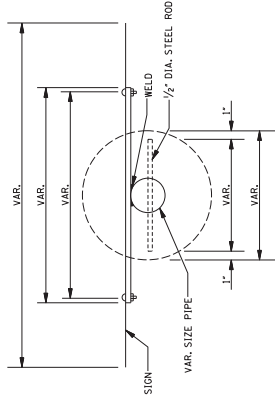
TYPICAL BACK-TO-BACK SIGN MOUNT
SHOWING RS-1 WITH R1-1



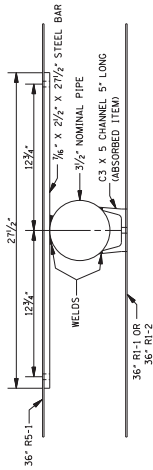
TYPICAL BACK-TO-BACK SIGN MOUNT
SHOWING RS-1 WITH R1-2



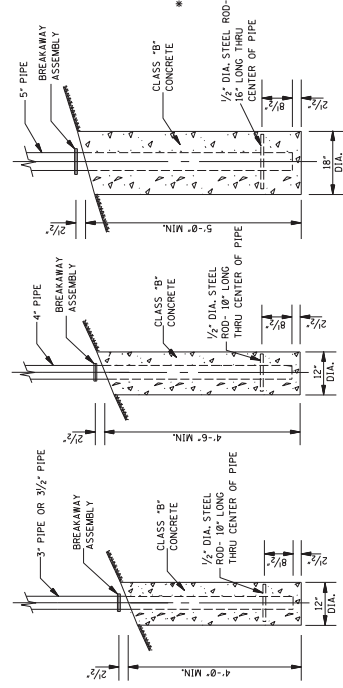
TYPICAL ASSEMBLY ON PIPE



TYPICAL PLAN VIEW



PLAN VIEW OF DOUBLE MOUNTING OF SIGNS

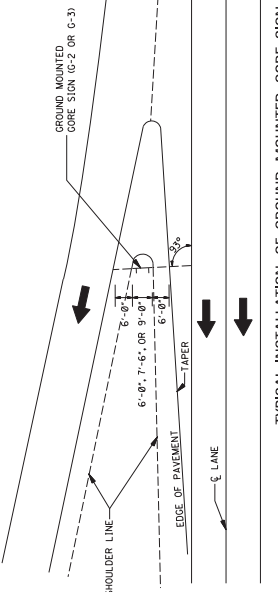
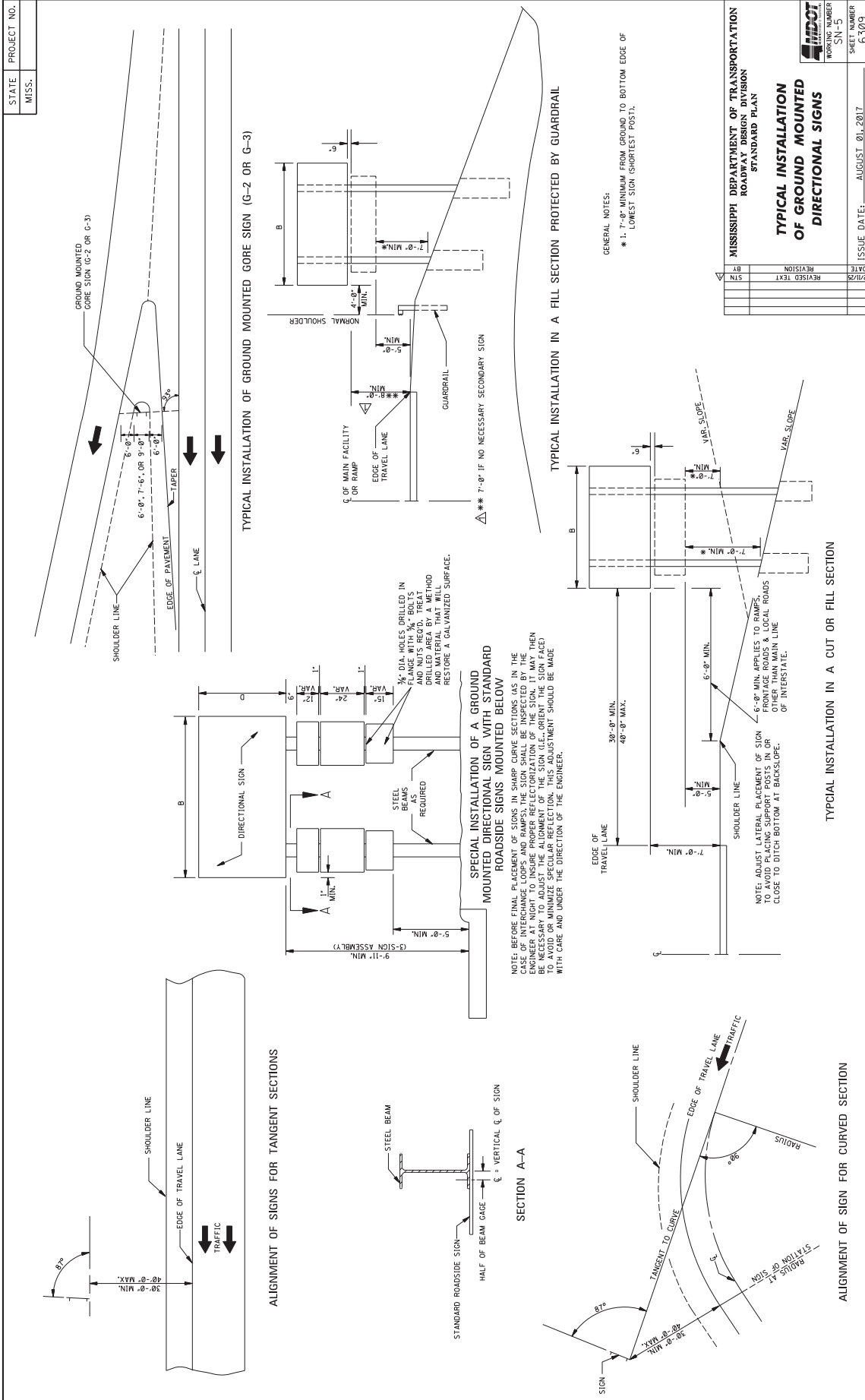


FOOTING DETAILS

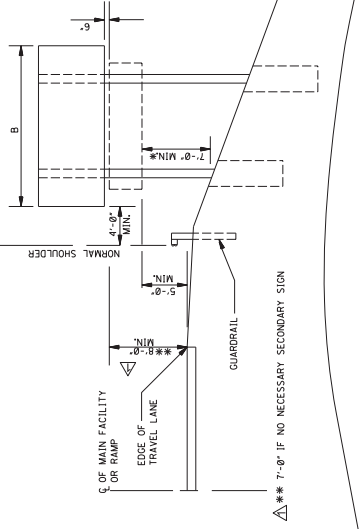
GENERAL NOTES:

- UNLESS OTHERWISE SPECIFIED, HORIZONTAL BRACES ARE 7/8" X 2 1/2" X VARIABLE LENGTH FLAT STEEL BARS. BARS ARE WELDED TO PIPE AS SHOWN. WHEN FABRICATION IS COMPLETE, ALL PARTS SHALL BE GALVANIZED AS PER SECTION 606 OF THE STANDARD SPECIFICATION.
- HOLES IN FLAT BARS ARE 3/8" DIAMETER.
- SIGNS ARE FASTENED TO THE FLAT BARS AND U-SECTION POSTS WITH M8 BOLTS WITH FLAT WASHERS AND LOCK NUTS.
- WHERE REQUIRED, SIGNS RS-1, RS-1R, RS-1R SHALL BE MOUNTED ON PIPES WITH CLAMPS OR BUCKLE BRACKETS NOT A PAY ITEM.
- ALL WELDS SHALL BE 3/8" FILLET.
- TOP OF POST

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
STANDARD SIGN ASSEMBLY AND INSTALLATION	
WORKING NUMBER SN-4B	SHEET NUMBER G-506
ISSUE DATE: AUGUST 01, 2017	

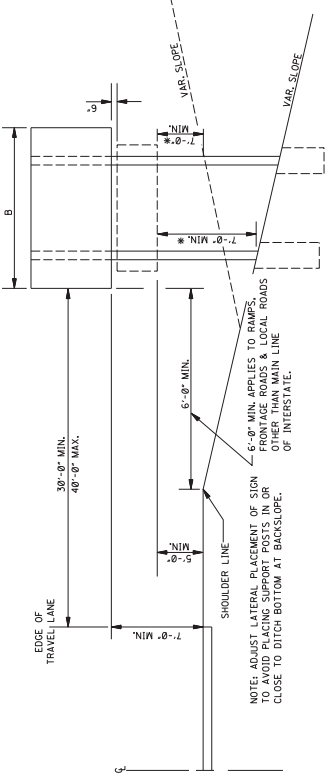


TYPICAL INSTALLATION OF GROUND MOUNTED GORE SIGN (G-2 OR G-3)



TYPICAL INSTALLATION OF A GROUND MOUNTED DIRECTIONAL SIGN WITH STANDARD ROADSIDE SIGNS MOUNTED BELOW

NOTE: BEFORE FINAL PLACEMENT OF SIGNS IN SHARP CURVE SECTIONS (AS IN THE CASE OF INTERSTATE RAMP ENDS) THE ENGINEER SHOULD CONSULT WITH THE ENGINEER AT NIGHT TO INSURE PROPER RECOGNITION OF THE SIGN (I.E., ORIENT THE SIGN FACE) TO AVOID OR MINIMIZE SPECULAR REFLECTION. THIS ADJUSTMENT SHOULD BE MADE WITH CARE AND UNDER THE DIRECTION OF THE ENGINEER.



TYPICAL INSTALLATION IN A CUT OR FILL SECTION

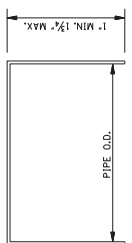
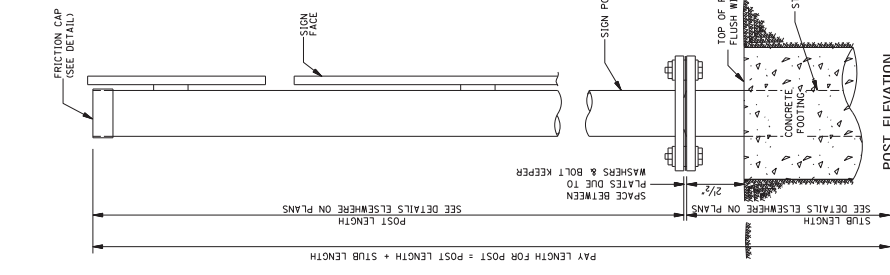
GENERAL NOTES:
 * 1. 7'-0" MINIMUM FROM GROUND TO BOTTOM EDGE OF LOWEST SIGN (SHORTEST POST).

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
ROADWAY DESIGN DIVISION	
STANDARD PLAN	
TYPICAL INSTALLATION OF GROUND MOUNTED DIRECTIONAL SIGNS	
DATE	ISSUE DATE: AUGUST 01, 2017
BY	SHEET NUMBER 6309
REVISED TEXT	WORKING NUMBER SN-5
REVISION	

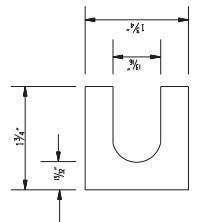
STATE	PROJECT NO.
MISS.	

GENERAL NOTES (SEE WK. NO. SN-6 FOR ADDITIONAL GENERAL NOTES)

1. THE TOP FLANGE OF THE FRICITION CAP, SLIP BASE, SHIM PLATE, THE SAFT EXTERIOR FINISHES OF THE BOTTOM PLATE, THE LIFTING CONE SHALL BE WELDED TO THE BOTTOM PLATE ONLY. A HOLE EQUAL TO THE INSIDE DIAMETER OF THE SIGN POST SHALL BE CUT THROUGH THE CENTER OF THE POST WITH THE HOLE EDGE BEVELED AS DETAIL. TOP & BOTTOM PLATES SHALL BE SYMMETRICAL FOR THE PURPOSE OF ASSEMBLY IN ANY POSITION.
2. BASE CONNECTION ASSEMBLY AS FOLLOWS:
 - A. ASSEMBLE POST TO STUB WITH 3 BOLTS AND WITH 3 FLAT WASHERS PER BOLT.
 - B. SHIM AS REQUIRED TO PLUMB POST.
 - C. TIGHTEN ALL BOLTS THE MAXIMUM POSSIBLE WITH A 12" TO 18" TORQUE. USE BRASS KEPTER PLATE, SHIMS AND TO CLEAN THREADS.
 - D. LOOSEN EACH BOLT IN TURN & RETIGHTEN IN A SYSTEMATIC ORDER TO PRESCRIBED TORQUE. (SEE BASE CONNECTION DETAIL)
 - E. BURR THREADS AT JUNCTION WITH NUT WITH A CENTER PUNCH TO PREVENT NUT FROM LOOSENING.
3. FRICTION CAPS TO BE MANUFACTURED FROM HOT ROLLED OR MILD STEEL. CAPS SHALL BE 20 GAUGE SHEET METAL. RIM EDGES SHALL BE REASONABLY STRAIGHT AND SMOOTH. CAPS SHALL BE SIZED AND FORMED IN SUCH A MANNER AS TO PROVIDE A UNIFORM CONTACT SURFACE. THE DEPTH SHALL BE SUFFICIENT TO GIVE POSITIVE PROTECTION AGAINST ENTRANCE OF RAINWATER. THEY SHALL BE FREE OF SHARP CREASES OR INDENTATIONS AND SHALL BE FINISHED TO THE SAME SHAPE AND APPEARANCE AS APPROVED BY THE ENGINEER.
4. STUB PROJECTION SHOULD BE MEASURED FROM A 5'-0" CHORD AS PER AASHTO LRFD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS, LATEST EDITION. SEE WK. NO. SN-6 FOR DIAGRAM.
5. AS AN ALTERNATIVE THE POST LENGTH OF THE SIGN POST CAN BE MADE-UP USING A NOMINAL LENGTH OF GALVANIZED PIPE. A FRICITION CAP SHALL BE WELDED TO THE END OF THE PIPE. APPROVED EQUAL WITH ALL NECESSARY HARDWARE REQUIRED TO SECURE THEM TO THE SIGN POST. A GALVANIZED KEPTER PLATE AND A GALVANIZED FRICTION CAP SHALL BE WELDED TO THE SIGN POST. ONE SET SCREW SHALL SECURE THE POST TO THE CASTING AND ONE SET SCREW TO PREVENT ROTATION. THE MANUFACTURER SHALL PROVIDE SHOP DRAWINGS OF THE COMPLETE ASSEMBLY FOR MDOT APPROVAL.

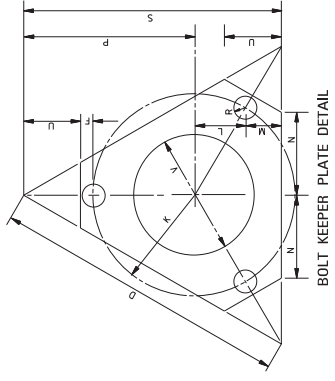


FRICTION CAP DETAIL
NOTE: SEE NOTE 3

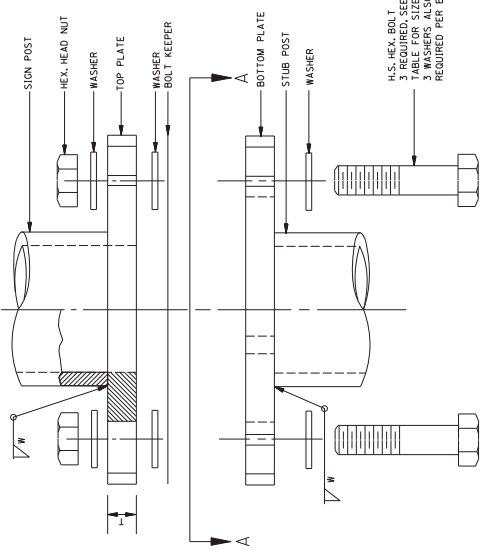


SHIM DETAIL

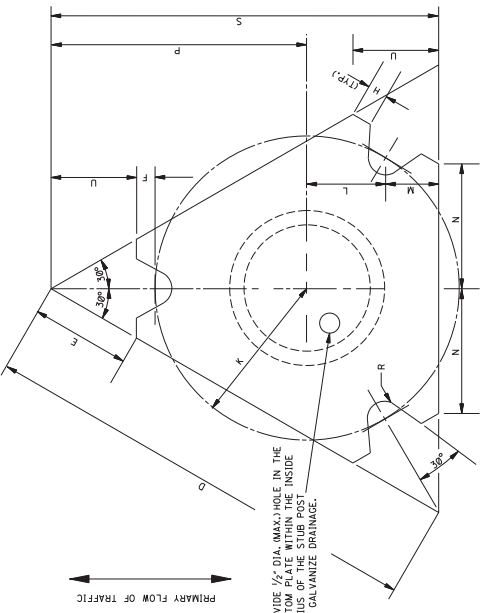
NOTE: DIMENSIONS 2" & 0.015" - THICK AND 2" & 0.015" - THICK SHIMS PER POST SHIMS SHALL BE FABRICATED FROM BRASS SHIM STOCK OR STRIP CONFORMING TO ASTM B 36.



BOLT KEEPER PLATE DETAIL



MULTI-DIRECTIONAL SIGN POST & STUB POST



SECTION A-A

NOTE: SEE DATA TABLE FOR DIMENSIONS

DIMENSIONS		BASE CONNECTION DATA TABLE																	
PIPE SIZES	BOLT TORQUE	T	W	C	D	E	F	G	H	J	K	L	M	N	P	S	U	V	R
3"	3/4" X 2 1/2"	3/4"	3/4"	1"	10 3/4"	2 3/4"	1/2"	1 3/4"	3/2"	1 3/4"	3/2"	1 1/4"	1 1/2"	2 3/8"	6"	9"	2"	4 1/4"	1 1/2"
3 1/2"	3/4" X 2 1/2"	3/4"	3/4"	1"	10 3/4"	2 3/4"	1/2"	1 3/4"	3/2"	1 3/4"	3/2"	1 1/4"	1 1/2"	2 3/8"	6"	9"	2"	4 1/4"	1 1/2"
5"	3/4" X 2 1/2"	3/4"	3/4"	1"	10 3/4"	2 3/4"	1/2"	1 3/4"	3/2"	1 3/4"	3/2"	1 1/4"	1 1/2"	2 3/8"	6"	9"	2"	4 1/4"	1 1/2"

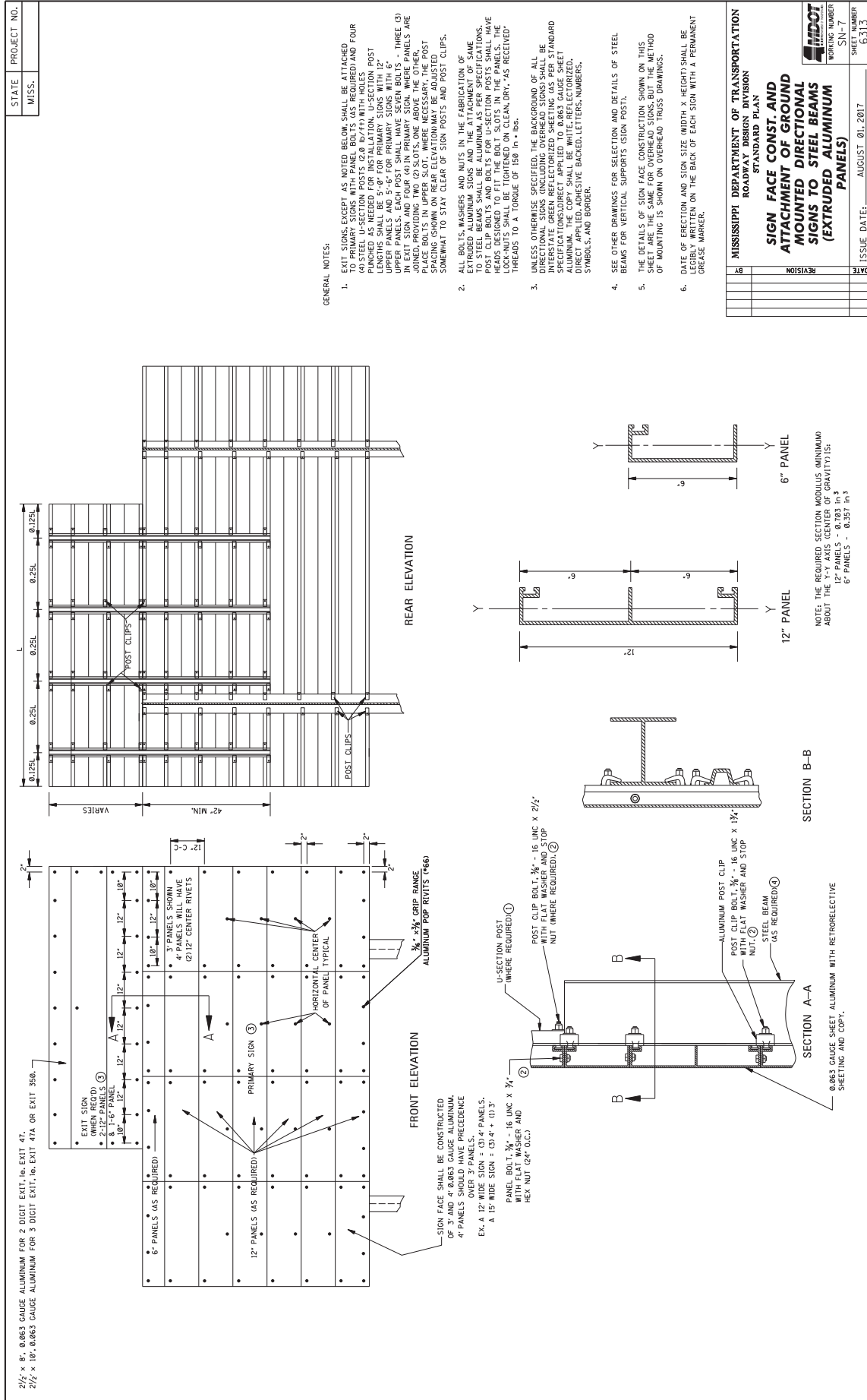
* NOTE: APPROXIMATE DIMENSIONS

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**BREAKAWAY
SIGN SUPPORTS**

ISSUE DATE: AUGUST 01, 2017

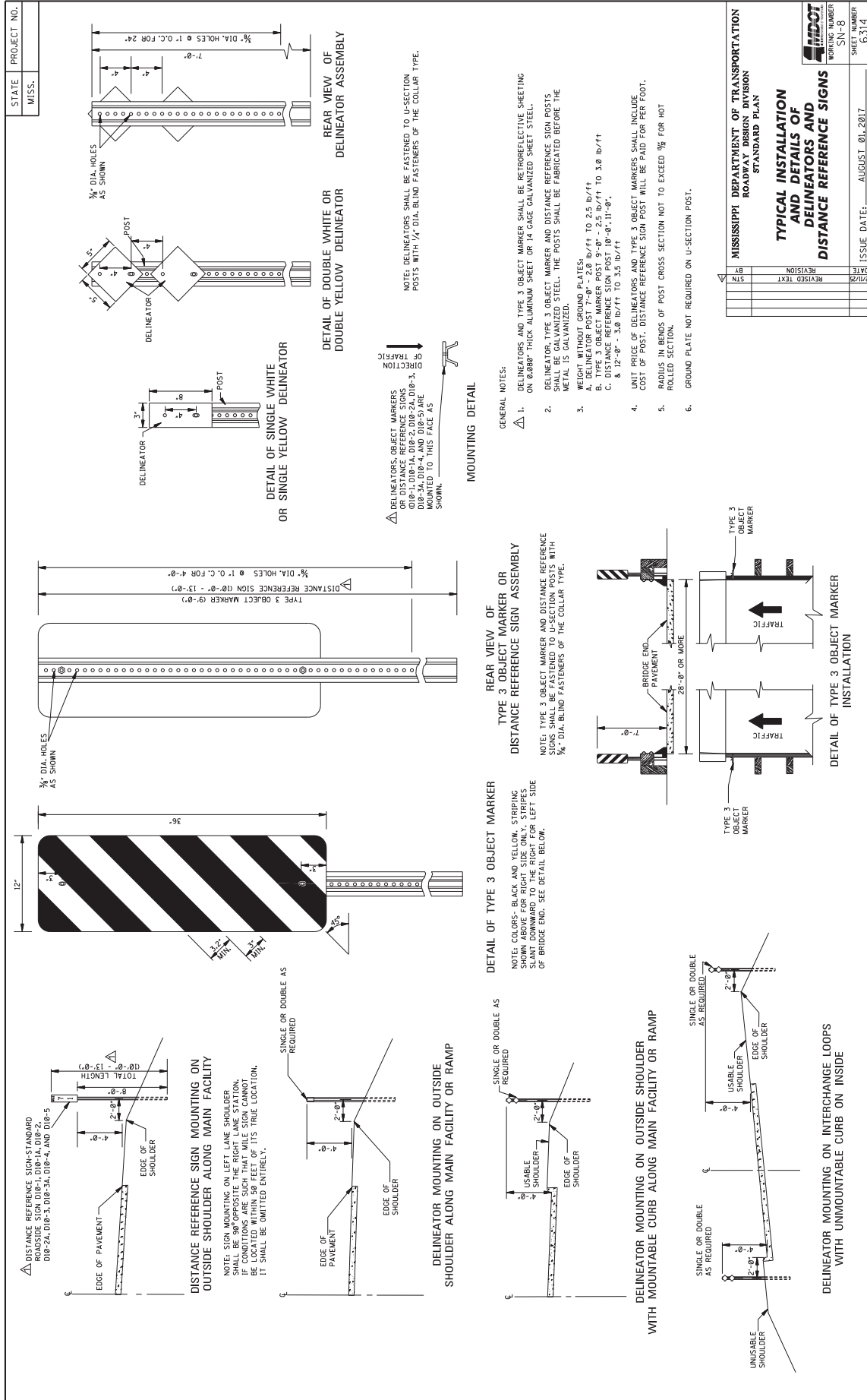
WORKING NUMBER: SN-CG
SHEET NUMBER: 6312



GENERAL NOTES:

- EXIT SIGNS EXCEPT AS NOTED BELOW SHALL BE ATTACHED TO PRIMARY SIGNS WITH POSTS. REQUIRED AND FOUR (4) STEEL U-SECTION POSTS (2 @ 10" O.C.) WITH HOLES PUNCHED AS NEEDED FOR INSTALLATION. U-SECTION POSTS SHALL BE 2" HIGH AND 1/2" WIDE. POSTS SHALL BE 1/2" DIA. UPPER PANELS AND 5/8" DIA. FOR PRIMARY SIGNS WITH 6" UPPER PANELS. EACH POST SHALL HAVE SEVEN BOLTS - THREE (3) IN EXIT SIGN AND FOUR (4) IN PRIMARY SIGN. WHERE PANELS ARE OVERLAPPING, THE POSTS SHALL BE SPACED TO CLEAR THE OVERLAP. SPACING (SHOWN ON REAR ELEVATION) MAY BE ADJUSTED SOMEWHAT TO STAY CLEAR OF SIGN POSTS AND POST CLIPS.
- ALL BOLTS, WASHERS AND NUTS IN THE FABRICATION OF EXTRUDED ALUMINUM SIGNS AND THE ATTACHMENT OF SAME TO STEEL BEAMS SHALL BE ALUMINUM, AS PER SPECIFICATIONS. UNLESS OTHERWISE SPECIFIED, THE BACKGROUND OF ALL DIRECTIONAL SIGNS (INCLUDING OVERHEAD SIGNS) SHALL BE INTERSTATE GREEN REFLECTORIZED SHEETING (AS PER STANDARD SPECIFICATIONS). UNLESS OTHERWISE SPECIFIED, THE BACKGROUND OF ALL OTHER SIGNS SHALL BE WHITE REFLECTORIZED ALUMINUM. THE COPY SHALL BE WHITE, REFLECTORIZED, DIRECT APPLIED, ADHESIVE BACKED, LETTERS, NUMBERS, SYMBOLS, AND BORDER.
- UNLESS OTHERWISE SPECIFIED, THE BACKGROUND OF ALL DIRECTIONAL SIGNS (INCLUDING OVERHEAD SIGNS) SHALL BE INTERSTATE GREEN REFLECTORIZED SHEETING (AS PER STANDARD SPECIFICATIONS). UNLESS OTHERWISE SPECIFIED, THE BACKGROUND OF ALL OTHER SIGNS SHALL BE WHITE REFLECTORIZED ALUMINUM. THE COPY SHALL BE WHITE, REFLECTORIZED, DIRECT APPLIED, ADHESIVE BACKED, LETTERS, NUMBERS, SYMBOLS, AND BORDER.
- SEE OTHER DRAWINGS FOR SELECTION AND DETAILS OF STEEL BEAMS FOR VERTICAL SUPPORTS (SIGN POSTS).
- THE DETAILS OF SIGN FACE CONSTRUCTION SHOWN ON THIS SHEET ARE THE SAME FOR OVERHEAD SIGNS, BUT THE METHOD OF MOUNTING IS SHOWN IN OVERHEAD TRUSS DRAWINGS.
- DATE OF REVISION AND SIGN SIZE (WIDTH X HEIGHT) SHALL BE LEGIBLY WRITTEN ON THE BACK OF EACH SIGN WITH A PERMANENT GREASE MARKER.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
SIGN FACE CONST. AND ATTACHMENT OF GROUND MOUNTED DIRECTIONAL SIGNS TO STEEL BEAMS (EXTRUDED ALUMINUM PANELS)	
WORKING NUMBER	SN-7
SHEET NUMBER	6315
ISSUE DATE:	AUGUST 01, 2017
DATE	REVISION

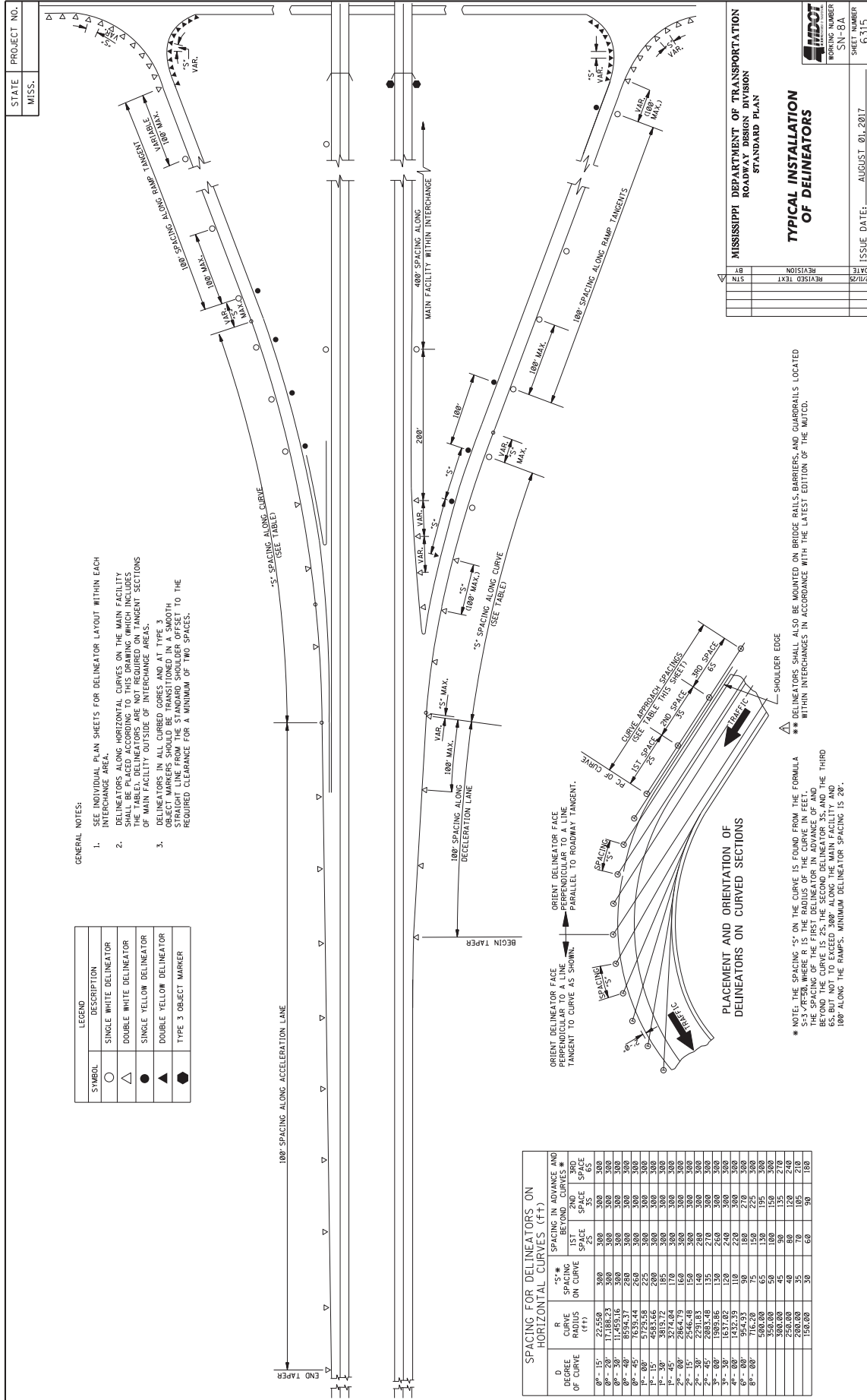


DELINEATORS OBJECT MARKERS D10-1, D10-1A, D10-2, D10-2A, D10-3, D10-3A, D10-4, AND D10-5 ARE MOUNTED TO THIS FACE AS SHOWN.

GENERAL NOTES:

- DELINEATORS AND TYPE 3 OBJECT MARKER SHALL BE RETROREFLECTIVE SHEETING ON 0.080" THICK ALUMINUM SHEET OR 14 GAGE GALVANIZED SHEET STEEL.
- DELINEATOR TYPE 3 OBJECT MARKER AND DISTANCE REFERENCE SIGN POSTS SHALL BE GALVANIZED STEEL. THE POSTS SHALL BE FABRICATED BEFORE THE METAL IS GALVANIZED.
- WEIGHT WITHOUT GROUND PLATES:
 - A. DELINEATOR POST 7'-0" - 2.0 lb/ft TO 2.5 lb/ft
 - B. DELINEATOR MARKER AS SHOWN 2.5 lb/ft TO 3.0 lb/ft
 - C. DISTANCE REFERENCE SIGN POST 0'-9" - 11'-0" 8 - 12'-0" - 3.0 lb/ft TO 3.5 lb/ft
- UNIT PRICE OF DELINEATORS AND TYPE 3 OBJECT MARKERS SHALL INCLUDE COST OF POST. DISTANCE REFERENCE SIGN POST WILL BE PAID FOR PER FOOT. ROLLED SECTION.
- RADIUS IN BENDS OF POST CROSS SECTION NOT TO EXCEED 3/8" FOR HOT
- GROUND PLATE NOT REQUIRED ON U-SECTION POST.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
TYPICAL INSTALLATION AND DETAILS OF DELINEATORS AND DISTANCE REFERENCE SIGNS	
DATE: _____	ISSUE DATE: AUGUST 01, 2017
BY: _____	SHEET NUMBER 6314
REVISION: _____	WORKING NUMBER SN-8
REVISED TEXT	REVISED



STATE PROJECT NO.
MISS.

- GENERAL NOTES:
- SEE INDIVIDUAL PLAN SHEETS FOR DELINEATOR LAYOUT WITHIN EACH INTERCHANGE AREA.
 - DELINEATORS ALONG HORIZONTAL CURVES ON THE MAIN FACILITY SHALL BE PLACED ACCORDING TO THIS DRAWING, WHICH INCLUDES THE SPACING AND ORIENTATION OF DELINEATORS FOR INTERCHANGE AREAS.
 - DELINEATORS IN ALL CURVED CORES AND AT TYPE 3 OBJECT MARKERS SHOULD BE TRANSLATED IN A SMOOTH STRAIGHT LINE FROM THE STANDARD SHOULDER OFFSET TO THE REQUIRED CLEARANCE FOR A MINIMUM OF TWO SPACES.

LEGEND	
SYMBOL	DESCRIPTION
○	SINGLE WHITE DELINEATOR
△	DOUBLE WHITE DELINEATOR
●	SINGLE YELLOW DELINEATOR
▲	DOUBLE YELLOW DELINEATOR
◆	TYPE 3 OBJECT MARKER

D DEGREE OF CURVE	R CURVE RADIUS (FT)	*S* SPACING IN ADVANCE AND BEYOND CURVES *		
		SPACE 25'	SPACE 35'	SPACE 65'
0° - 15°	22,500.00	300	300	300
0° - 15°	11,250.00	300	300	300
0° - 15°	5,625.00	300	300	300
0° - 15°	2,812.50	300	300	300
0° - 15°	1,406.25	300	300	300
0° - 15°	703.125	300	300	300
0° - 15°	351.562	300	300	300
0° - 15°	175.781	300	300	300
0° - 15°	87.890	300	300	300
0° - 15°	43.945	300	300	300
0° - 15°	21.972	300	300	300
0° - 15°	10.986	300	300	300
0° - 15°	5.493	300	300	300
0° - 15°	2.746	300	300	300
0° - 15°	1.373	300	300	300
0° - 15°	0.686	300	300	300
0° - 15°	0.343	300	300	300
0° - 15°	0.171	300	300	300
0° - 15°	0.086	300	300	300
0° - 15°	0.043	300	300	300
0° - 15°	0.021	300	300	300
0° - 15°	0.011	300	300	300
0° - 15°	0.005	300	300	300
0° - 15°	0.002	300	300	300
0° - 15°	0.001	300	300	300

PLACEMENT AND ORIENTATION OF DELINEATORS ON CURVED SECTIONS

* NOTE: THE SPACING "S" ON THE CURVE IS FOUND FROM THE FORMULA $S = \sqrt{R \cdot \Delta}$ WHERE R IS THE RADIUS OF THE CURVE AND Δ IS THE SPACING OF THE FIRST DELINEATOR IN ADVANCE OF AND BEYOND THE CURVE IS 25, THE SECOND DELINEATOR IS 35, AND THE THIRD IS 65, BUT NOT TO EXCEED 380' ALONG THE MAIN FACILITY AND 180' ALONG THE RAMP. MINIMUM DELINEATOR SPACING IS 20'.

△ DELINEATORS SHALL ALSO BE MOUNTED ON BRIDGE RAILS, BARRIERS, AND GUARDRAILS LOCATED WITHIN INTERCHANGES IN ACCORDANCE WITH THE LATEST EDITION OF THE MUTCD.

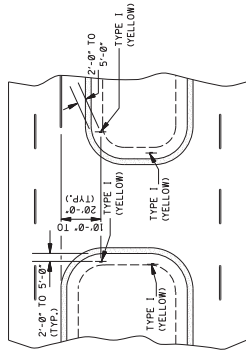
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

TYPICAL INSTALLATION OF DELINEATORS

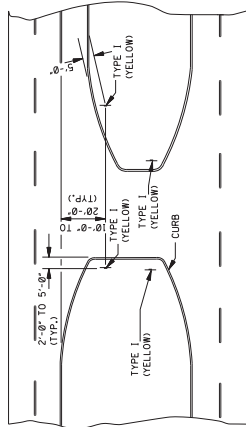
DATE: _____ REVISION: _____
BY: _____ REVISED TEXT: _____
SM: _____

ISSUE DATE: AUGUST 01, 2017
WORKING NUMBER: SN-8A
SHEET NUMBER: 6315

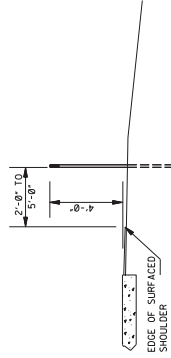
STATE	PROJECT NO.
MISS.	



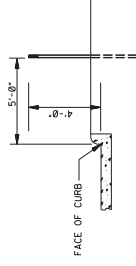
TYPICAL DELINEATION AT A CROSSOVER WITH USABLE SHOULDERS AND A MEDIAN WIDTH OVER 42'-0"



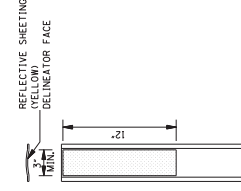
TYPICAL DELINEATION AT A CURBED CROSSOVER WITH A MEDIAN WIDTH OVER 42'-0"



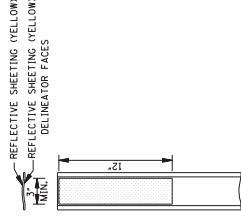
DELINEATOR MOUNTING ON CROSSOVER WITH USABLE SHOULDER



DELINEATOR MOUNTING ON CURBED CROSSOVER

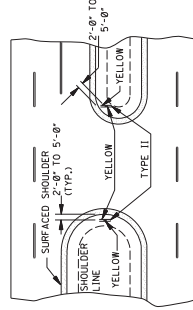


DETAIL OF TYPE I FLEXIBLE POST DELINEATOR

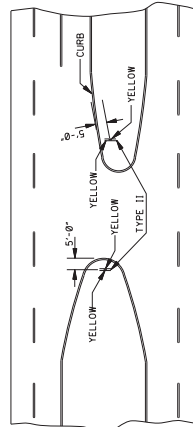


DETAIL OF TYPE II FLEXIBLE POST DELINEATOR

NOTE: FLEXIBLE POST DELINEATORS ALLOWED FOR USE MUST BE FROM MDOT'S APPROVED PRODUCTS LIST.



TYPICAL DELINEATION AT A CROSSOVER WITH USABLE SHOULDERS AND A MEDIAN WIDTH OF 42'-0" OR LESS



TYPICAL DELINEATION AT A CURBED CROSSOVER WITH A MEDIAN WIDTH OF 42'-0" OR LESS

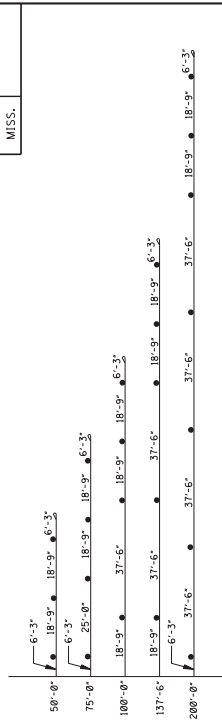
NOTE: PLACE DELINEATORS NO MORE THAN 2'-0" FROM EDGE OF TRAVEL LANES EDGES.

GENERAL NOTES:

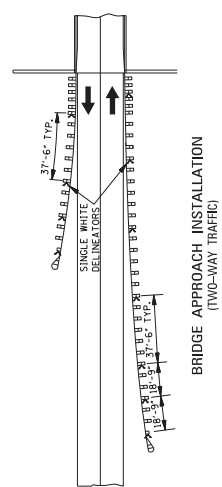
1. THE UNIT PRICE OF DELINEATORS INCLUDES COSTS OF DELINEATOR FACES, POST, HARDWARE AND INSTALLATION.
2. DELINEATOR FACE WILL BE RETROREFLECTIVE SHEETING TYPE AS SPECIFIED IN PLANS.
3. POST REQUIRING THE INSTALLATION OF A BASE SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATION.
4. THE COLOR OF DELINEATORS SHALL BE THE COLOR OF THE ADJACENT EDGE LINE PER MUTCD SECTIONS 3A.02, 3B.02, AND 3C.03.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
ROADWAY DESIGN DIVISION	
STANDARD PLAN	
TYPICAL CROSSOVER DELINEATION	
DATE	ISSUE DATE: AUGUST 01, 2017
BY	
CHKD	
REVISED TEXT	
REVISION	
BY	
DATE	
WORKING NUMBER SN-05	
SHEET NUMBER 6316	

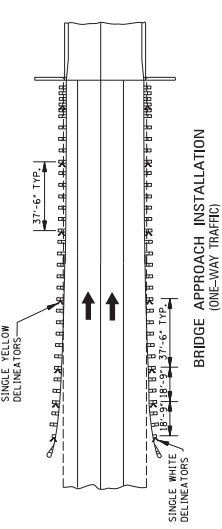
STATE PROJECT NO.	
MISS.	



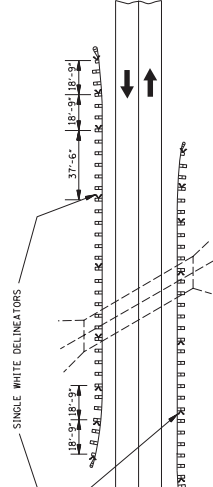
GRAPHIC SHOWING SPACINGS OF GUARDRAIL DELINEATORS AT SOME COMMONLY USED BRIDGE APPROACHES



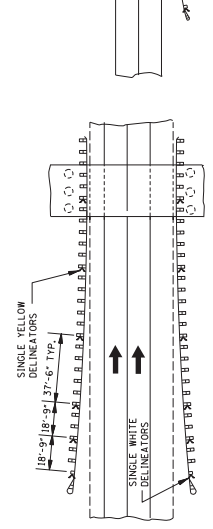
BRIDGE APPROACH INSTALLATION (TWO-WAY TRAFFIC)



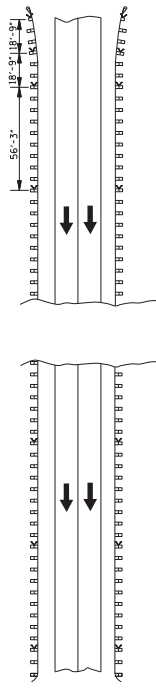
BRIDGE APPROACH INSTALLATION (ONE-WAY TRAFFIC)



ROADSIDE OBSTACLE INSTALLATION—LENGTH 250' OR LESS (TWO-WAY TRAFFIC)



ROADSIDE OBSTACLE INSTALLATION—LENGTH 250' OR LESS (ONE-WAY TRAFFIC)

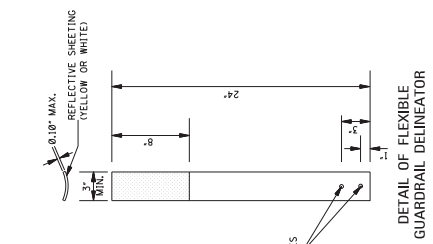


EMBANKMENT OR ROADSIDE OBSTACLE INSTALLATION—LENGTH GREATER THAN 250' (ONE-WAY TRAFFIC)

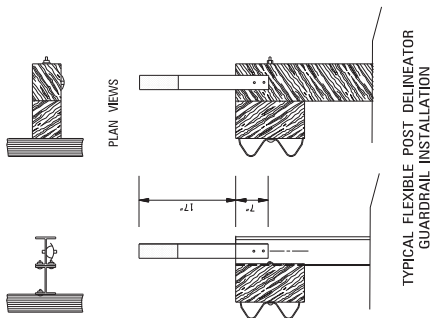
NOTE: ONE-WAY TRAFFIC SHOWING DELINEATOR SPACING FOR TWO-WAY TRAFFIC SIMILAR. DELINEATOR COLOR WILL BE THE SAME AS THE ADJACENT PAVEMENT EDGE MARKING. THE FIRST THREE (3) MARKERS WILL FACE TRAFFIC IN OFF LANE FOR TWO-WAY TRAFFIC AS SHOWN IN DRAWING FOR OBSTACLE INSTALLATION FOR TWO-WAY TRAFFIC.

GENERAL NOTES:

1. THE UNIT PRICE OF DELINEATOR INCLUDES COSTS OF DELINEATOR FACE(S) POST, HARDWARE AND INSTALLATION.
2. DELINEATORS SHALL CONSIST OF RETROREFLECTIVE DEVICES THAT ARE CAPABLE OF CLEARLY RETROREFLECTING LIGHT UNDER NORMAL ATMOSPHERIC CONDITIONS FROM A DISTANCE OF 1000 FEET WHEN ILLUMINATED BY THE HIGH BEAMS OF STANDARD AUTOMOBILE LIGHTS.
3. FOR THE SPACING OF DELINEATORS IN HORIZONTAL CURVES ON ROADWAYS OR BRIDGES, SEE TABLE 36-1, MUTCD 11th EDITION, FOR SPACING REQUIREMENTS.
4. DELINEATORS FOR GUARDRAIL SHALL BE MOUNTED ON FLEXIBLE POSTS AS FOLLOWS: THE DELINEATOR POSTS WILL BE FROM MOOT'S "APPROVED PRODUCTS LIST" AND WILL BE FASTENED TO GUARDRAIL POST IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATION.

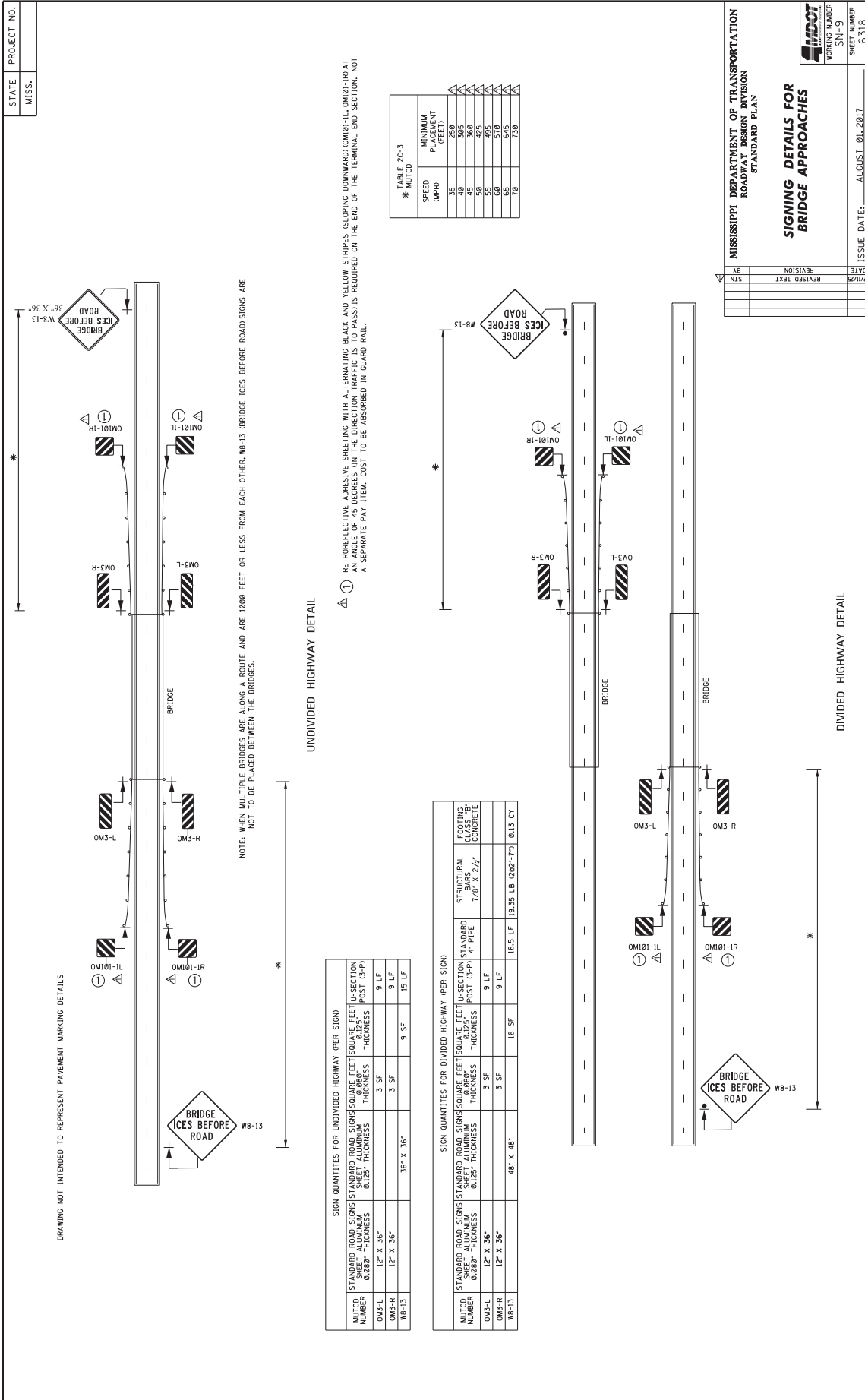


DETAIL OF FLEXIBLE GUARDRAIL DELINEATOR



TYPICAL FLEXIBLE POST DELINEATOR GUARDRAIL INSTALLATION

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
TYPICAL GUARDRAIL DELINEATION	
DATE	ISSUE DATE: AUGUST 01, 2017
BY	
REVISED TEXT	
REVISION	
WORKING NUMBER	SN-8C
SHEET NUMBER	6317



MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 – NOTICE TO BIDDERS NO. 7624

CODE: (SP)

DATE: 01/20/2026

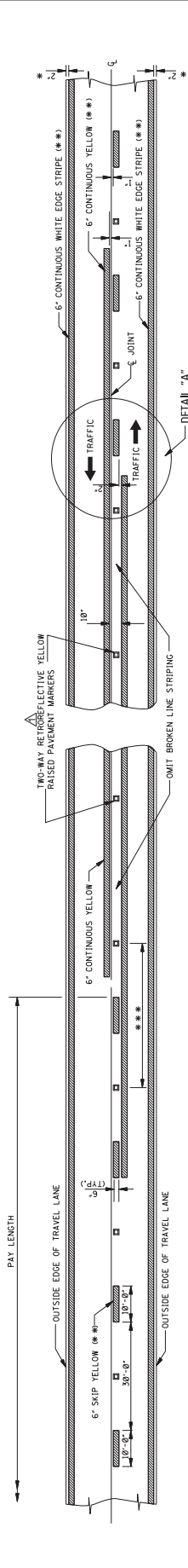
SUBJECT: Standard Drawings

Standard Drawings attached hereto shall govern appropriate items of required work.

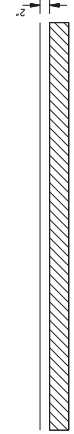
Larger copies of Standard Drawings may be purchased from:

MDOT Plans Print Shop
MDOT Shop Complex, Building C, Room 114
2567 North West Street
P.O. Box 1850
Jackson, MS 39215-1850
Telephone: (601) 359-7460
or FAX: (601) 359-7461
or e-mail: plans@mdot.state.ms.us

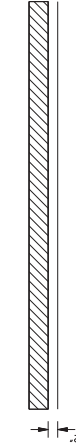
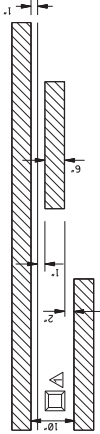
STATE	PROJECT NO.
MISS.	



TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)



NOTE: THE CRITERIA FOR NO-PASSING ZONES CAN BE FOUND IN THE MDOT ROADWAY DESIGN MANUAL, SECTION 11-1.0B1.



4-LANE WITH ONE-WAY TRAFFIC

- GENERAL NOTES:
- * 1. UNLESS SHOWN ELSEWHERE ON THE PLANS, FOR CURVE DATA AND GRADES REFER TO WK. SHEETS RS-1, RS-2, AND RS-3.
 - ** 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE AND RAISED PAVEMENT MARKERS.
 - *** 3. 6" SKIP YELLOW RETROREFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

TANGENT SECTIONS	URBAN AREA	RURAL AREA
HORIZONTAL CURVES	40'-0"	80'-0"
INTERCHANGE LIMITS	40'-0"	140'-0"

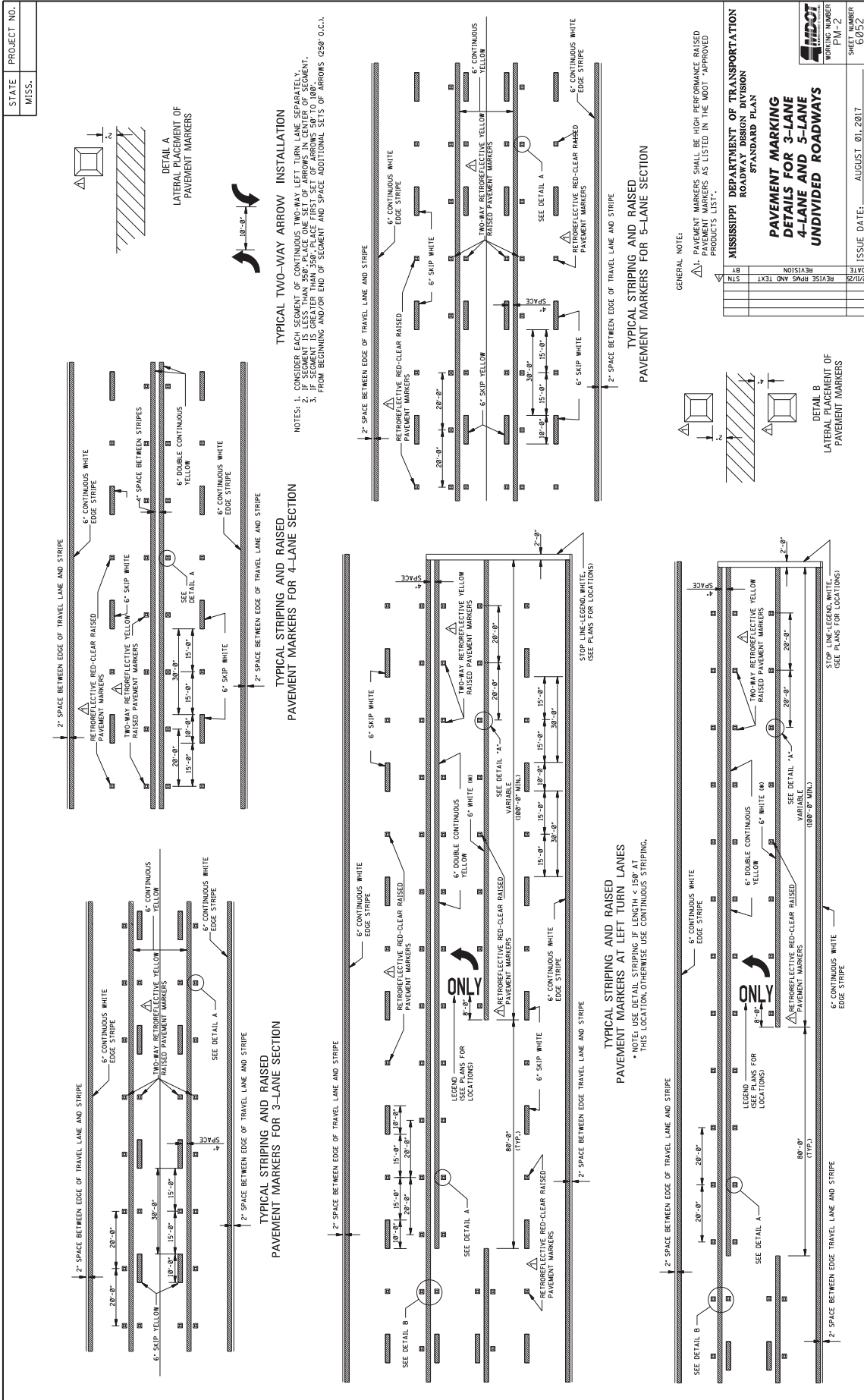
- * NOTE: ON THE MAIN FACILITY RETROREFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINES THROUGH ALL INTERCHANGE AREAS BEGINNING UPSTREAM FROM THE INTERCHANGE AND THROUGH THE RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
- △ 4. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RETROREFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MDOT "APPROVED PRODUCTS LIST."

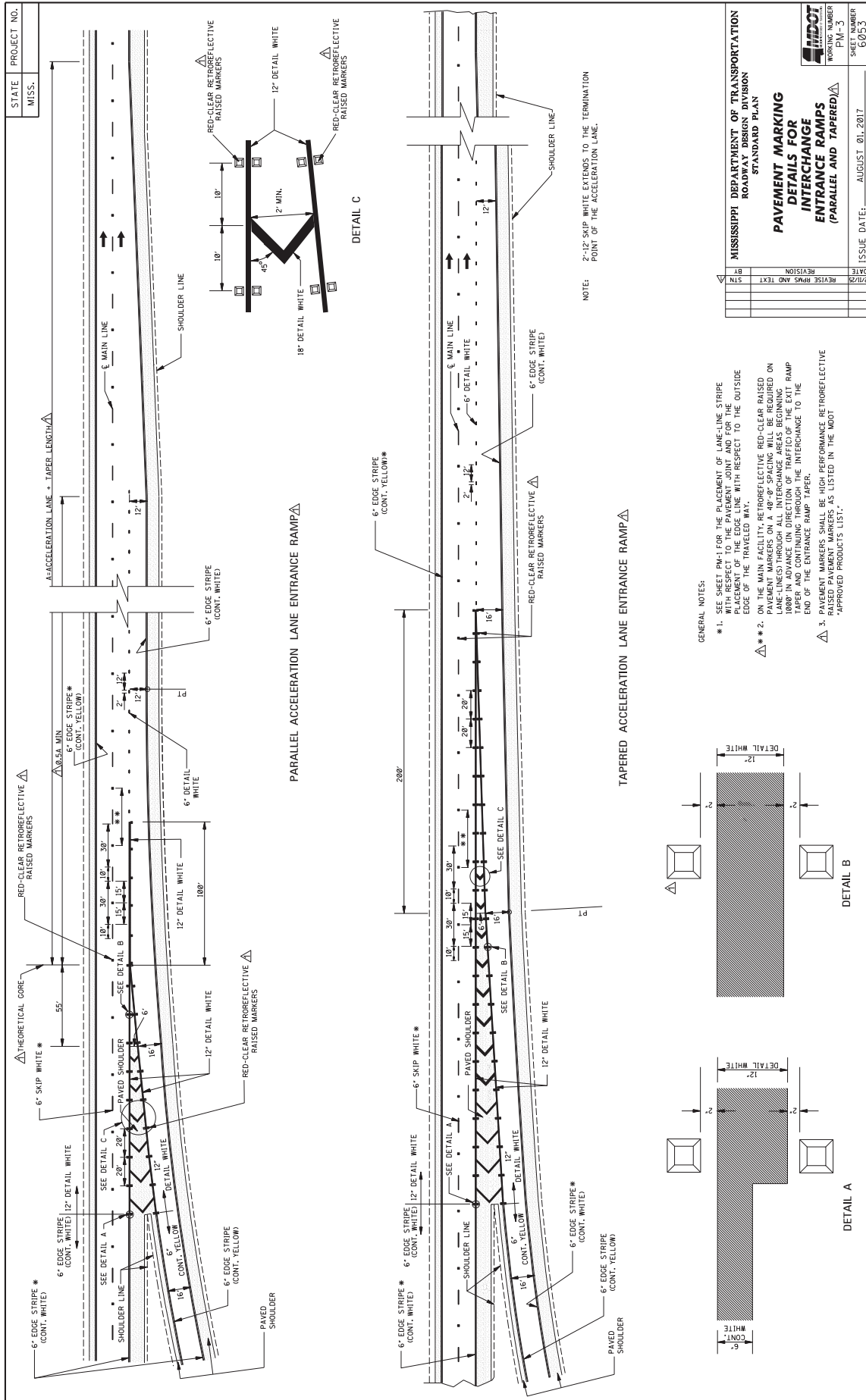
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**PAVEMENT MARKING
DETAILS FOR
2-LANE AND 4-LANE
DIVIDED ROADWAYS**

WORKING NUMBER: PM-1
SHEET NUMBER: 6031

ISSUE DATE: AUGUST 01, 2017





STATE PROJECT NO.
MISS.

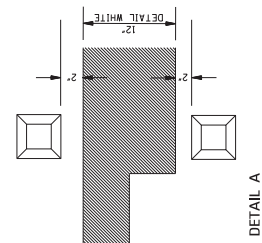
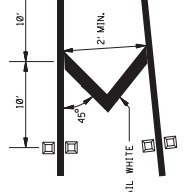
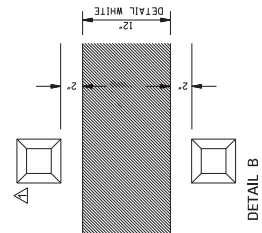
DATE	REVISION	BY

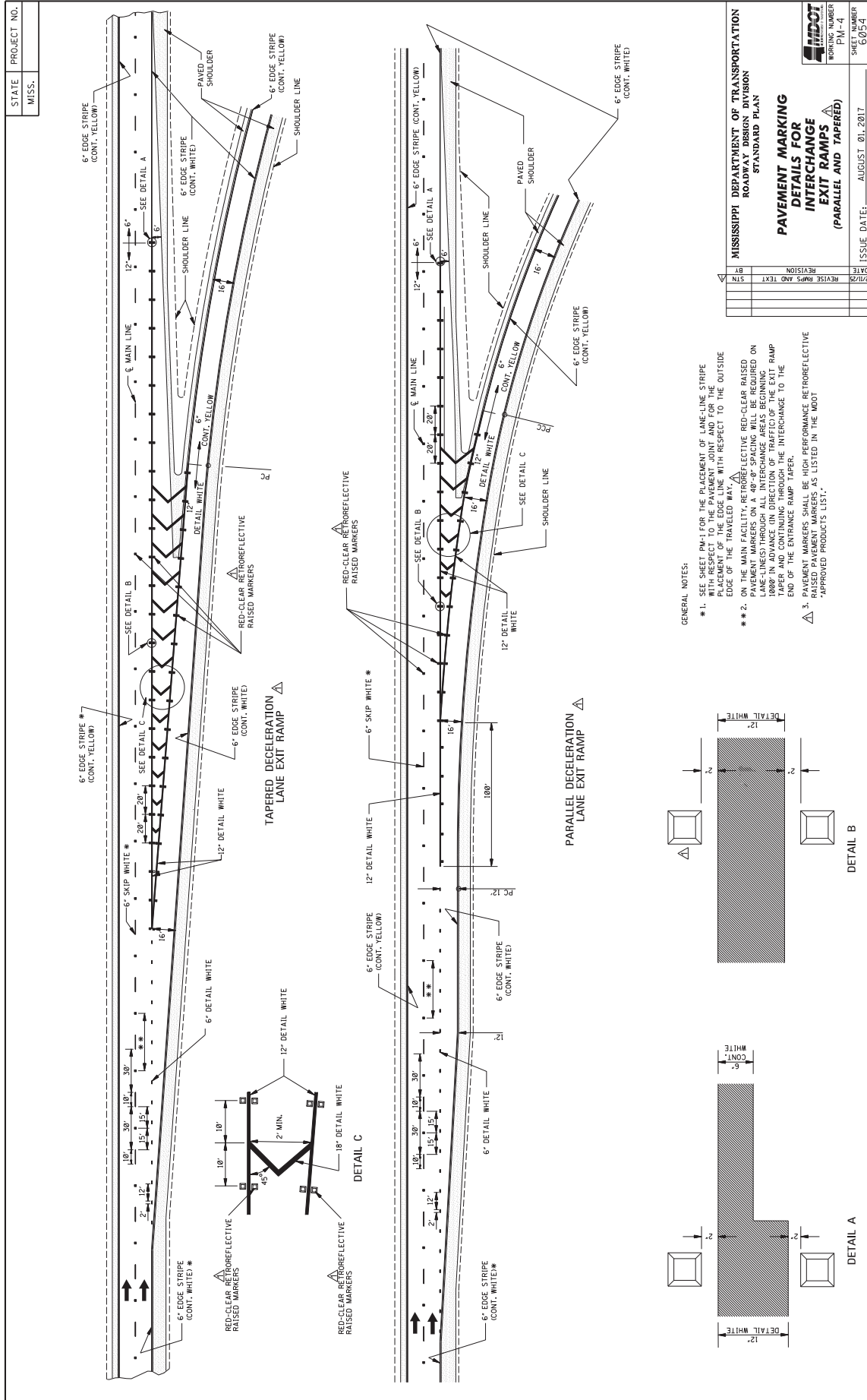
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN
**PAVEMENT MARKING
DETAILS FOR
INTERCHANGE
ENTRANCE RAMPS
(PARALLEL AND TAPERED)**

ISSUE DATE: AUGUST 01, 2017
SHEET NUMBER PW-3
WORKING NUMBER 0033

NOTE: 2'-0" SKIP WHITE EXTENDS TO THE TERMINATION POINT OF THE ACCELERATION LANE.

- GENERAL NOTES:
- # 1. SEE SHEET PW1 FOR THE PLACEMENT OF LANE-LINE STRIPE WITH RESPECT TO THE PAVEMENT JOINT AND FOR THE PLACEMENT OF THE EDGE LINE WITH RESPECT TO THE EDGE OF THE TRAVELED WAY.
 - # 2. ON THE MAIN FACILITY, RETROREFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON 1000' IN ADVANCE IN DIRECTION OF TRAFFIC OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
 - # 3. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RETROREFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MDT "APPROVED PRODUCTS LIST".





STATE	PROJECT NO.
MISS.	

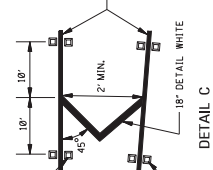
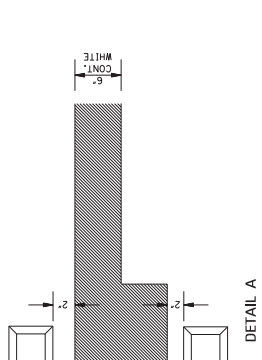
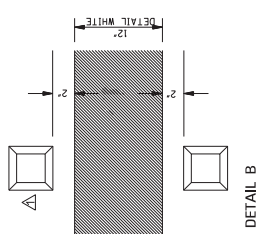
DATE	REVISION
BY	TEXT
BY	STN

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**PAVEMENT MARKING
DETAILS FOR
INTERCHANGE
EXIT RAMP
(PARALLEL AND TAPERED)**

ISSUE DATE: AUGUST 01, 2017
SHEET NUMBER: PM-4
WORKING NUMBER: G034

- GENERAL NOTES:
- * 1. SEE SHEET PM-1 FOR THE PLACEMENT OF LANE LINE STRIPE WITH RESPECT TO THE PAVEMENT JOINT AND FOR THE PLACEMENT OF THE EDGE LINE WITH RESPECT TO THE OUTSIDE EDGE OF THE FULLED WIDTH.
 - ** 2. FOR THE TAPERED EXIT RAMP, RED-CLEAR RAISED PAVEMENT MARKERS ON K-40-90 SPACING WILL BE REQUIRED ON LANE LINES THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE IN DIRECTION OF TRAFFIC OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
 - △ 3. PAVED PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RETROREFLECTIVE *APPROVED PRODUCTS LIST.



STATE MISS.	PROJECT NO.	
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GENERAL NOTES:

- TWO HORIZONTAL GAPS (CAUSED BY EXAMPLE) IN EACH WORD OF THE FULL WIDTH ARE PERMITTED IN EACH LETTER.
- NUMERALS, SYMBOLS, AND ARROWS SHALL BE INSTALLED IN ACCORDANCE WITH THE REQUIREMENTS OF THE "STANDARD HIGHWAY SIGNS" PUBLICATION AS REFERENCED IN SECTION 103.04 OF THE LATEST EDITION OF THE MUTCD.
- WORD, SYMBOL, AND ARROW MARKINGS SHALL BE WHITE, UNLESS OTHERWISE NOTED IN THE PLANS.
- PAY QUANTITIES FOR PAVEMENT MARKING LEGENDS ARE AS FOLLOWS:

LEGEND	AREA (FT ²)
STOP	24.6
RIGHT	28.6
LEFT	19.5
TURN	27.3
AHEAD	32.3
YIELD	26.8
EXIT	18.5
SIGNAL	32.5
SCHOOL	35.5

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

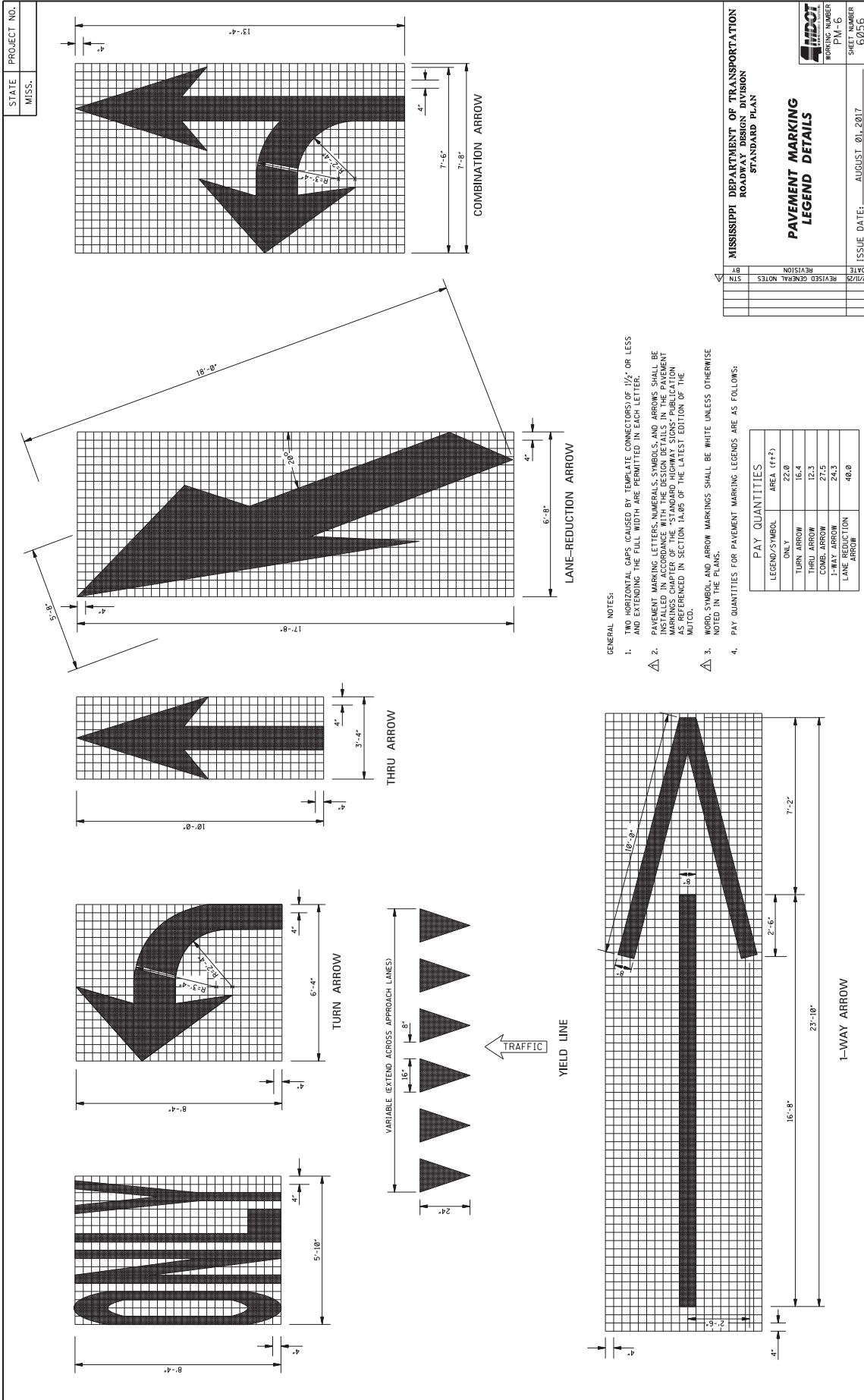
**PAVEMENT MARKING
LEGEND DETAILS**

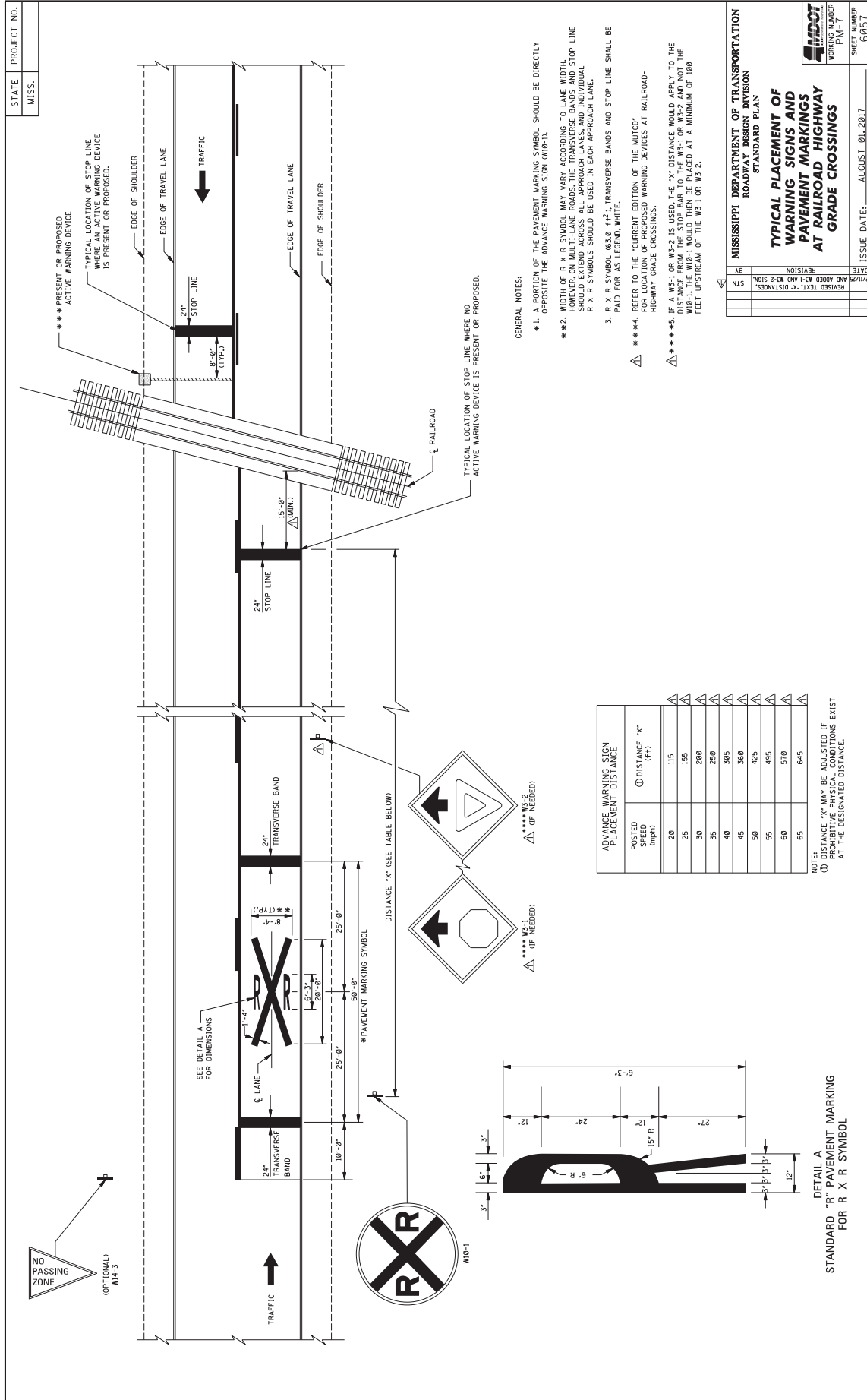
ISSUE DATE: AUGUST 01, 2017

WORKING NUMBER: PM-5

SHEET NUMBER: 6035

	DATE	REVISION	NOTES					
BY	DATE	BY	DATE	BY	DATE	BY	DATE	BY





STATE PROJECT NO.
MISS.

*** PRESENT OR PROPOSED ACTIVE WARNING DEVICE WHERE AN ACTIVE WARNING DEVICE IS PRESENT OR PROPOSED.

TYPICAL LOCATION OF STOP LINE WHERE AN ACTIVE WARNING DEVICE IS PRESENT OR PROPOSED.

EDGE OF SHOULDER

EDGE OF TRAVEL LANE

TRAFFIC

STOP LINE

EDGE OF TRAVEL LANE

EDGE OF SHOULDER

RAILROAD

24" STOP LINE

15'-0" (MIN)

24" STOP LINE

24" TRANSVERSE BAND

SEE DETAIL A FOR DIMENSIONS

ξ LINE

24" TRANSVERSE BAND

10'-0"

25'-0"

25'-0"

PAVEMENT MARKING SYMBOL

DISTANCE 'X' (SEE TABLE BELOW)

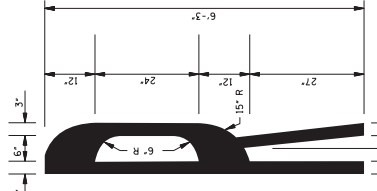
ξ RAILROAD

TYPICAL LOCATION OF STOP LINE WHERE NO ACTIVE WARNING DEVICE IS PRESENT OR PROPOSED.

GENERAL NOTES:
#1. A PORTION OF THE ADVANCE MARKING SYMBOL SHOULD BE DIRECTLY OPPOSITE THE ADVANCE WARNING SIGN (W10-1).
#2. WIDTH OF R X R SYMBOL MAY VARY ACCORDING TO LANE WIDTH. TOP LINE SHOULD EXTEND ACROSS ALL APPROACH LANES, AND INDIVIDUAL R X R SYMBOLS SHOULD BE USED IN EACH APPROACH LANE.
3. R X R SYMBOL (63.0, 4'2", TRANSVERSE BANDS AND STOP LINE SHALL BE PAID FOR AS LEGEND WHITE.
#4. REFER TO THE CURRENT EDITION OF THE MUTCD FOR LOCATION OF PROPOSED MARKING DEVICES AT RAILROAD-HIGHWAY GRADE CROSSINGS.
#5. IF A W10-1 OR W10-2 IS USED, THE 'X' DISTANCE WOULD APPLY TO THE DISTANCE FROM THE STOP BAR TO THE W10-1 OR W10-2 AND NOT THE W10-1. THE W10-1 WOULD THEN BE PLACED AT A MINIMUM OF 100 FEET UPSTREAM OF THE W10-1 OR W10-2.

POSTED SPEED (mph)	DISTANCE 'X' (ft)
20	115
25	155
30	200
35	250
40	305
45	360
50	425
55	495
60	570
65	645

NOTE:
① DISTANCE 'X' MAY BE ADJUSTED IF PROHIBITIVE PHYSICAL CONDITIONS EXIST AT THE DESIGNATED DISTANCE.

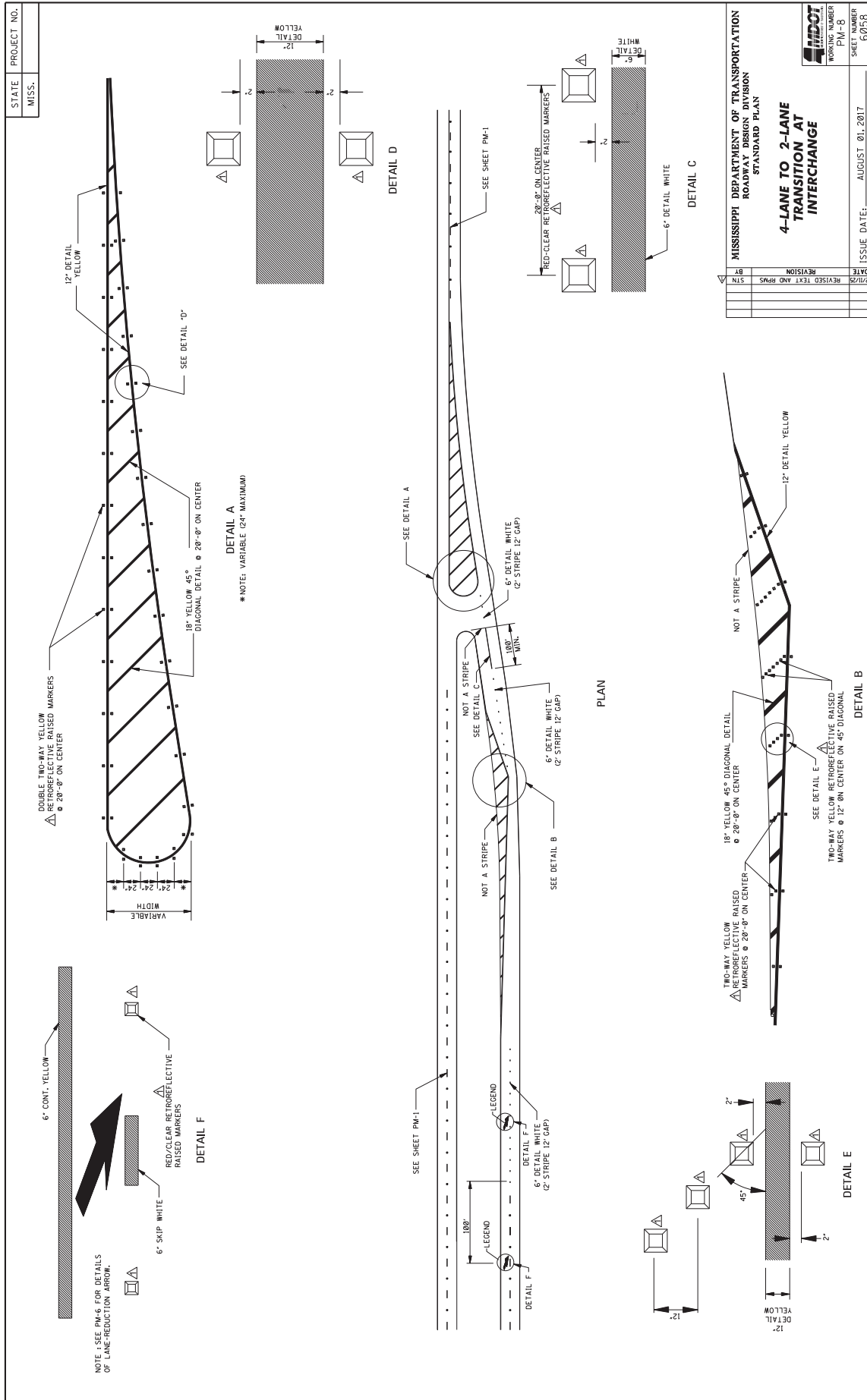


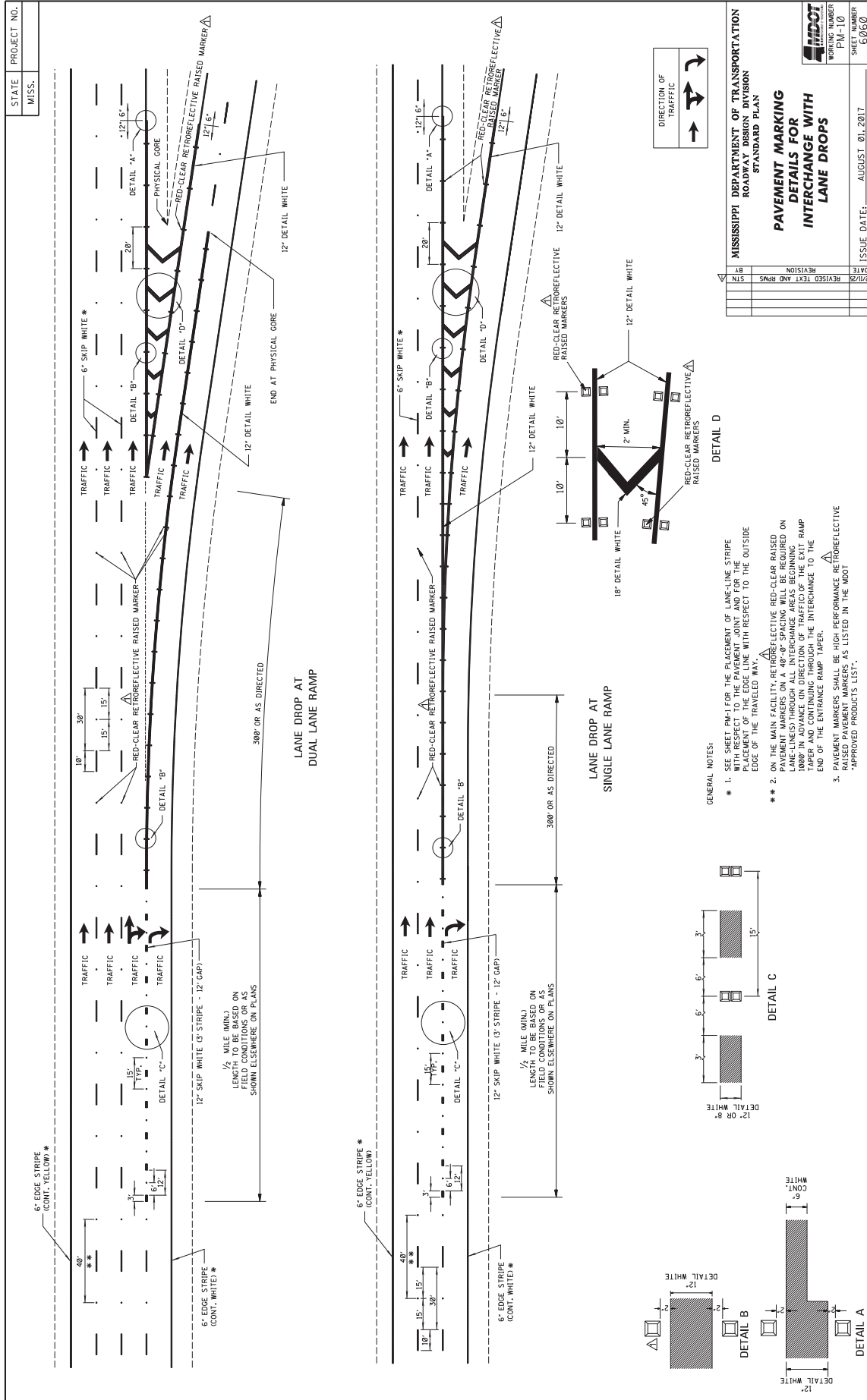
DETAIL A
STANDARD "R" PAVEMENT MARKING FOR R X R SYMBOL

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

TYPICAL PLACEMENT OF WARNING SIGNS AND PAVEMENT MARKINGS AT RAILROAD HIGHWAY GRADE CROSSINGS

ISSUE DATE: AUGUST 01, 2017
SHEET NUMBER 60571



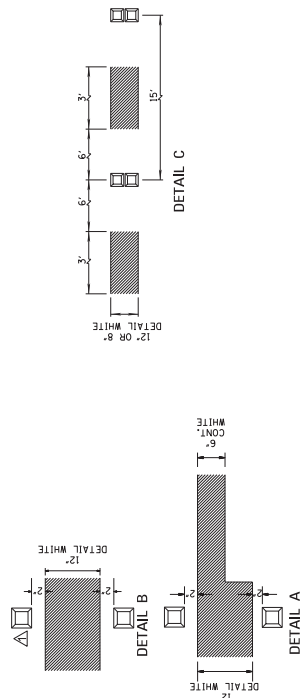


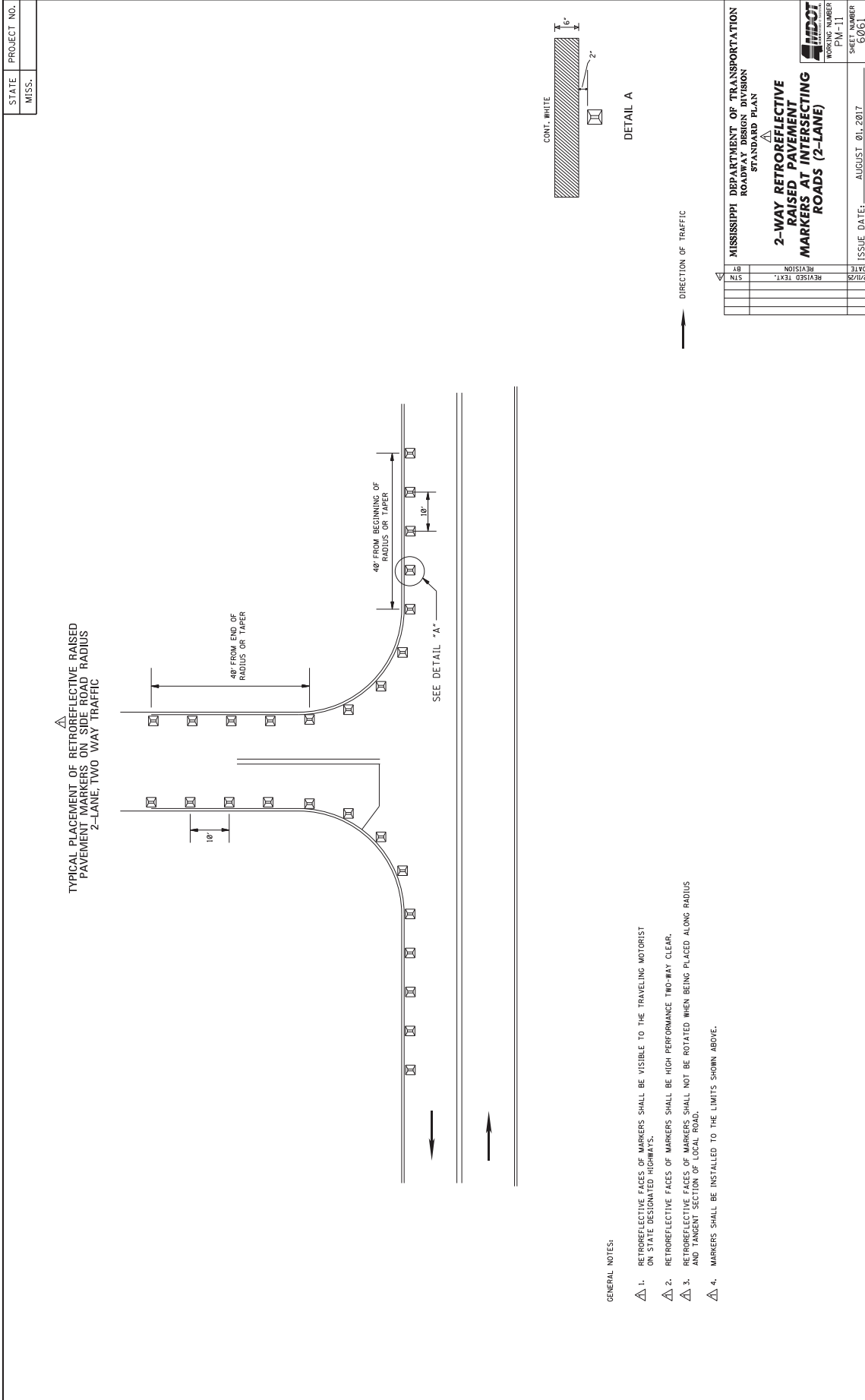
STATE	PROJECT NO.
MISS.	

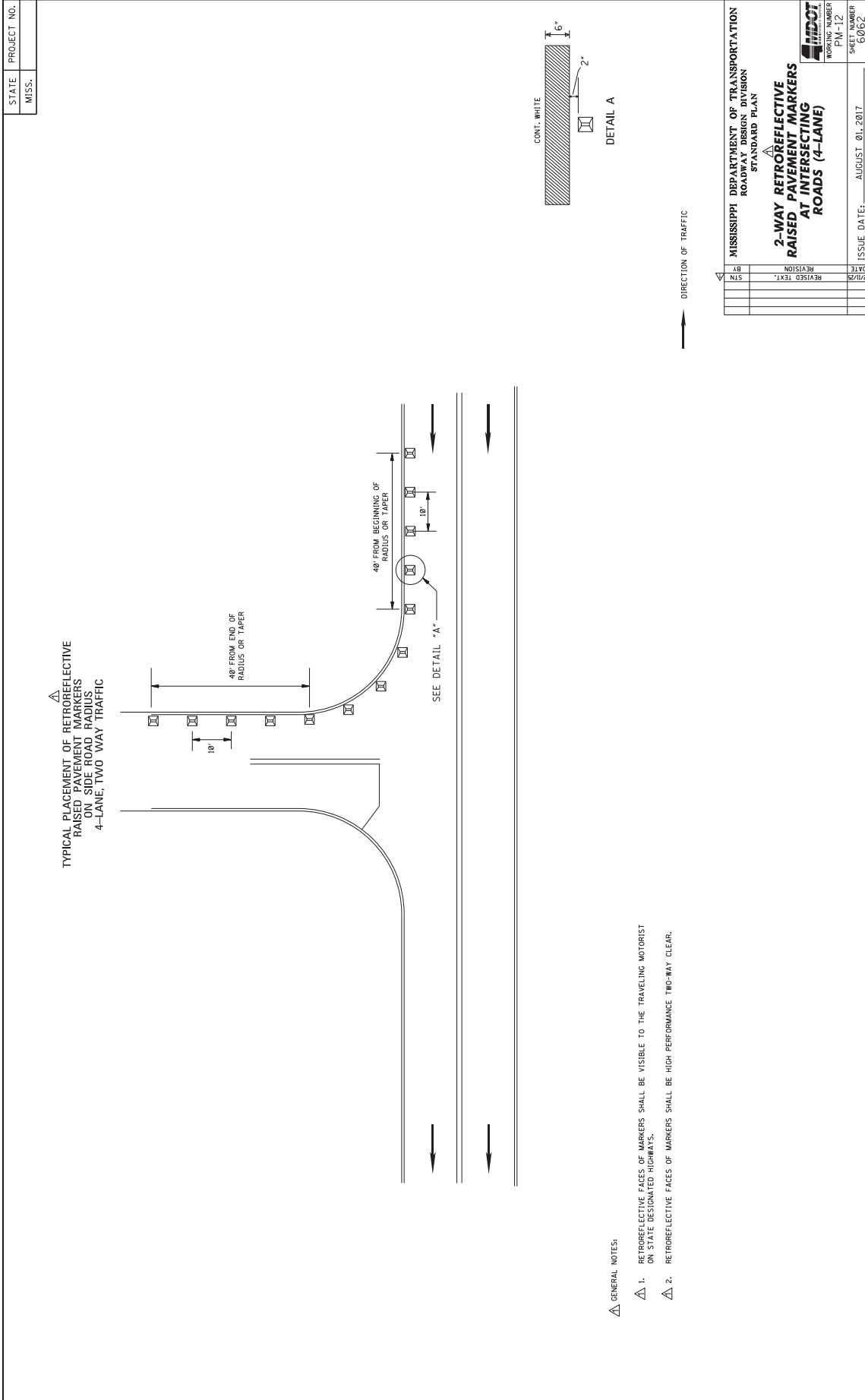
MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
PAVEMENT MARKING DETAILS FOR INTERCHANGE WITH LANE DROPS	
DATE	ISSUE DATE: AUGUST 01, 2017
BY	WORKING NUMBER
BY	PM-10
BY	SHET NUMBER
BY	0260

GENERAL NOTES:

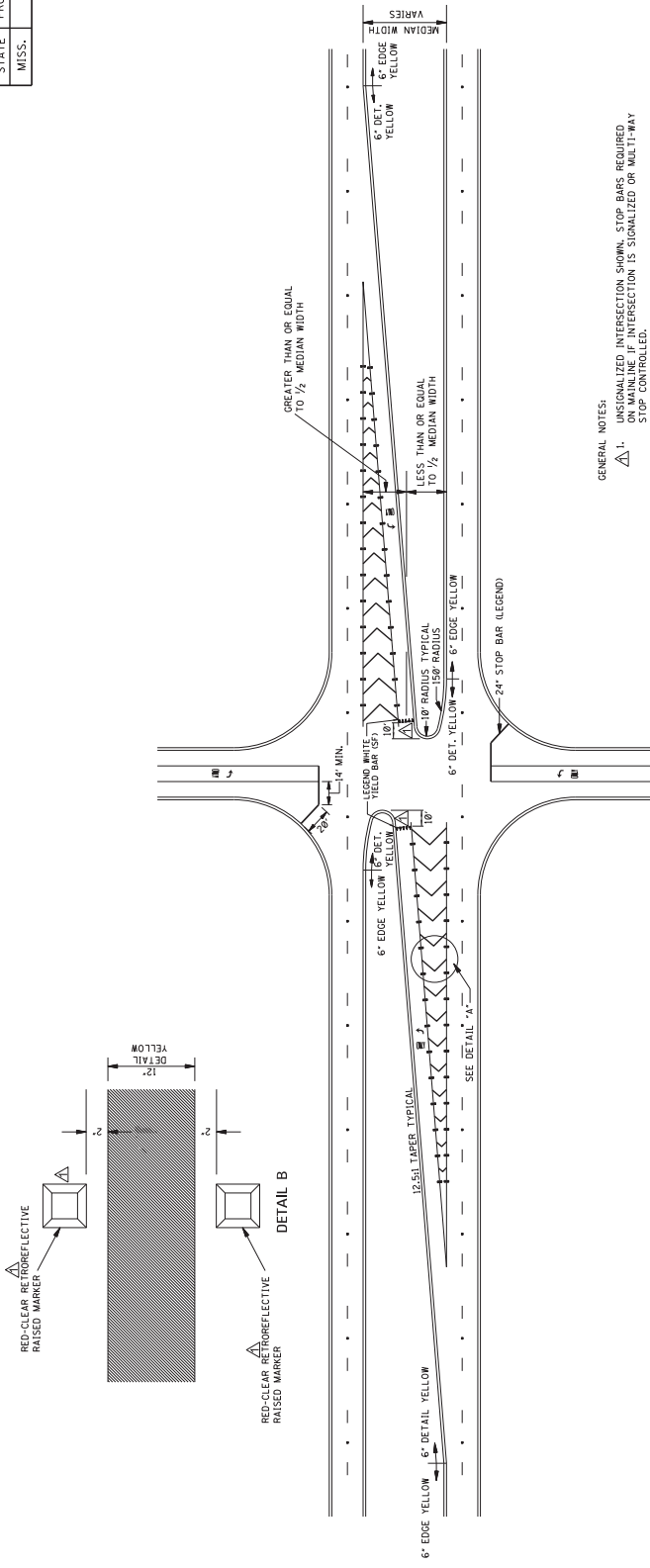
- * 1. SEE SHEET PM-1 FOR THE PLACEMENT OF LANE-LINE STRIPE WITH RESPECT TO THE PAVEMENT JOINT AND FOR THE PLACEMENT OF THE EDGE LINE WITH RESPECT TO THE OUTSIDE EDGE OF THE TRAVELED WAY.
- ** 2. ON THE MAIN FACILITY, RETROREFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON APPROXIMATELY 1000' IN ADVANCE IN DIRECTION OF TRAFFIC OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
- 3. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RETROREFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MDT "APPROVED PRODUCTS LIST".





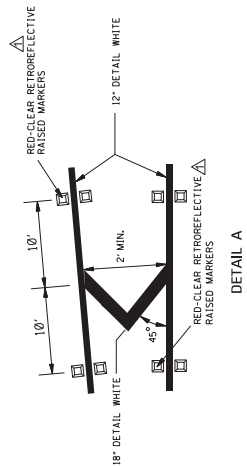
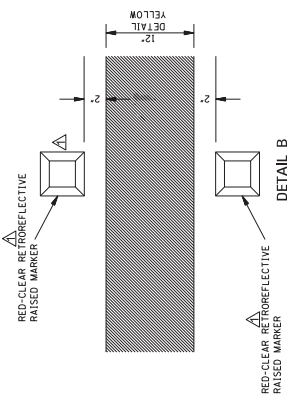


STATE	PROJECT NO.
MISS.	



GENERAL NOTES:

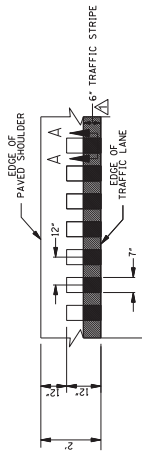
1. UNIGNALIZED INTERSECTION SHOWN. STOP BARS REQUIRED AT UNIGNALIZED INTERSECTION IS SIGNALIZED OR MULTIWAY STOP CONTROLLED.
2. SEE M.S. NOS. PM-1, PM-3 and PM-6 FOR OTHER DETAILS.
3. TAPER LENGTH VARIES WITH MEDIAN WIDTH.
4. TAPERED OFFSET LEFT TURN LANE SHOWN. PARALLEL OFFSET LEFT TURN LANE PAVEMENT MARKINGS ARE SIMILAR.
5. 3-LANE SIDE ROAD SHOWN. SEE PERMANENT PAVEMENT MARKING DETAIL SHEETS FOR SIDE ROAD PAVEMENT MARKING DETAILS.
6. SEE PM-12 FOR INTERSECTING ROAD RAISED PAVEMENT MARKER PLACEMENT.



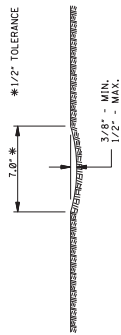
MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
OFFSET LEFT TURN LANES	
DATE	ISSUE DATE: AUGUST 01, 2017
BY	
REVISION	
REVISED TEXT AND RAMP	
ADDED TEXT AND RAMP	
SYN	
SHEET NUMBER	6063
WORKING NUMBER	PM-13

STATE	PROJECT NO.
MISS.	

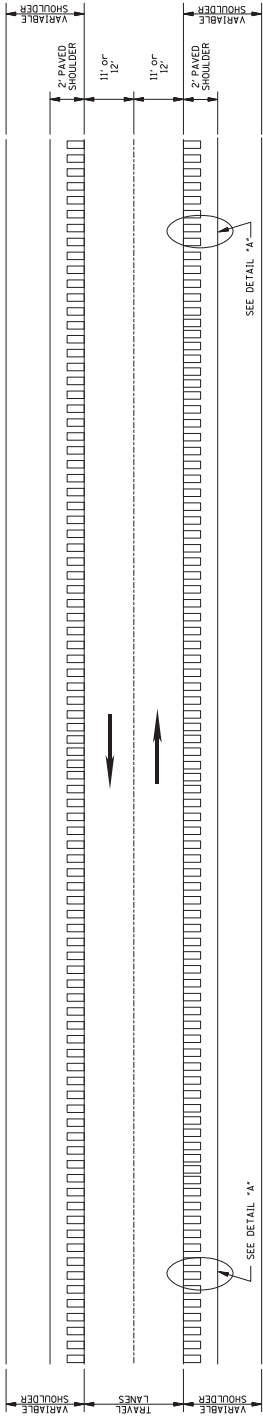
- GENERAL NOTES
- GROUND-IN RUMBLE STRIPES SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT.
 - GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO ALL PAVED SHOULDERS ON ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER.
 - COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS.
 - GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO:
 - MAINLINE
 - INTERSECTING ROADWAY IF OVERLAID OR RECONSTRUCTED BEYOND NORMAL MAINLINE R.O.M.
 - ANY ROADWAY WITH EXISTING RUMBLE STRIPES PRIOR TO CONSTRUCTION.
 - OMIT GROUND-IN RUMBLE STRIPES WHERE TRAFFIC LANE IS LESS THAN 11 FEET WIDE.



DETAIL "A"



SECTION "A-A"



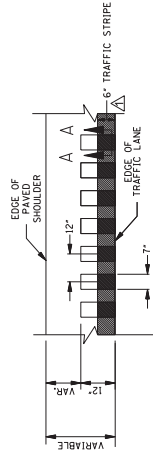
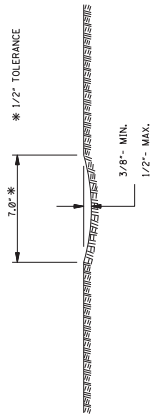
PLAN
NOT TO SCALE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
RUMBLE STRIPES 2-LANE HIGHWAYS (ASPHALT LANES, 2-FT ASPHALT SHOULDERS)	
DATE	ISSUE DATE: AUGUST 01, 2017
BY	WORKING NUMBER 6064
REVISION	SHEET NUMBER RS-1
DATE	PROJECT NUMBER 6064

STATE	PROJECT NO.
MISS.	

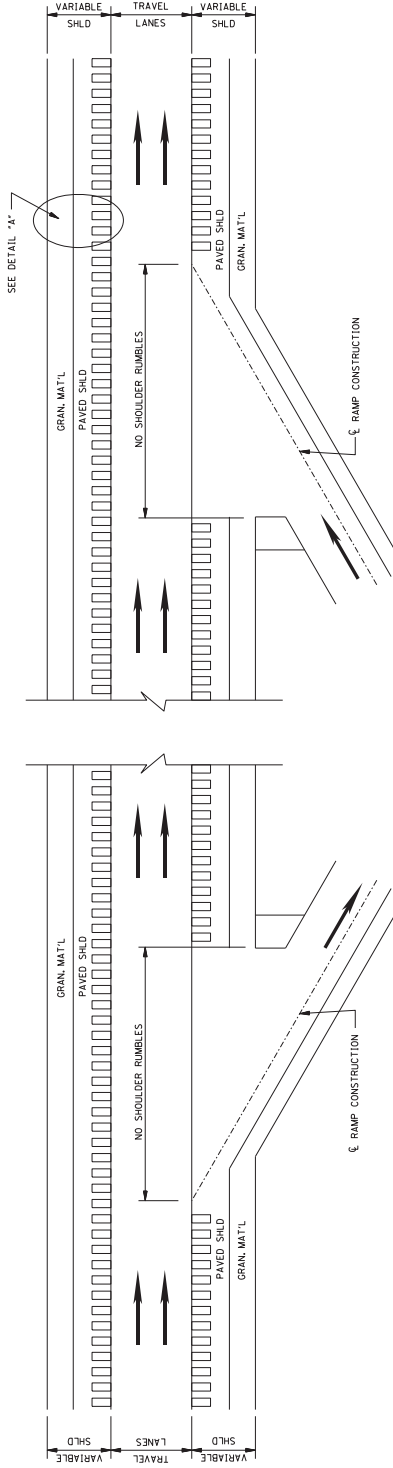
GENERAL NOTES

1. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT.
2. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO ALL PAVED SHOULDERS ON ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER.
3. COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS
4. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO:
 - A. MAINLINE
 - B. INTERSECTING ROADWAY IF OVERLAID OR RECONSTRUCTED BEYOND NORMAL MAINLINE R.O.M.
 - C. ANY ROADWAY WITH EXISTING RUMBLE STRIPES PRIOR TO CONSTRUCTION.



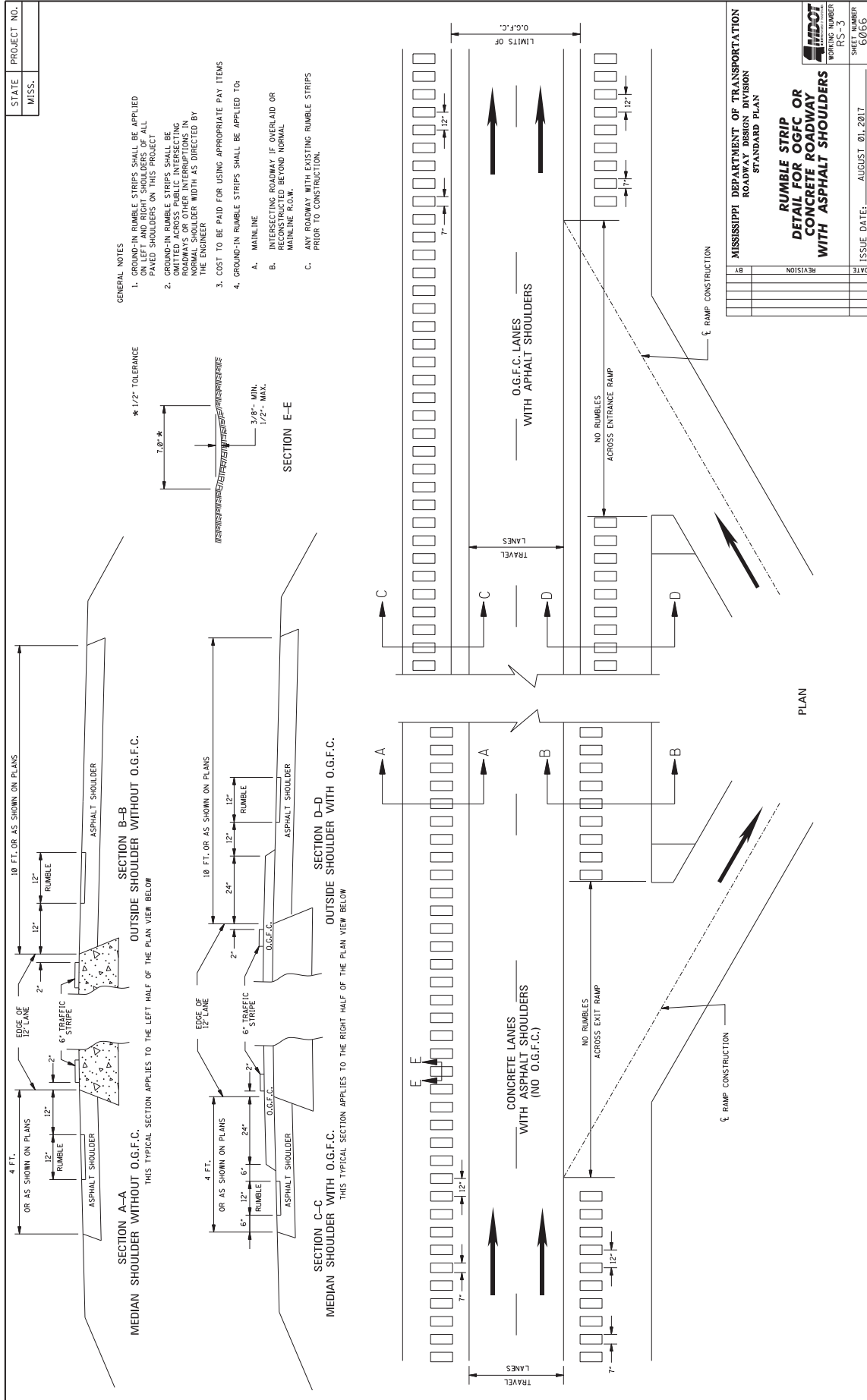
SECTION "A-A"

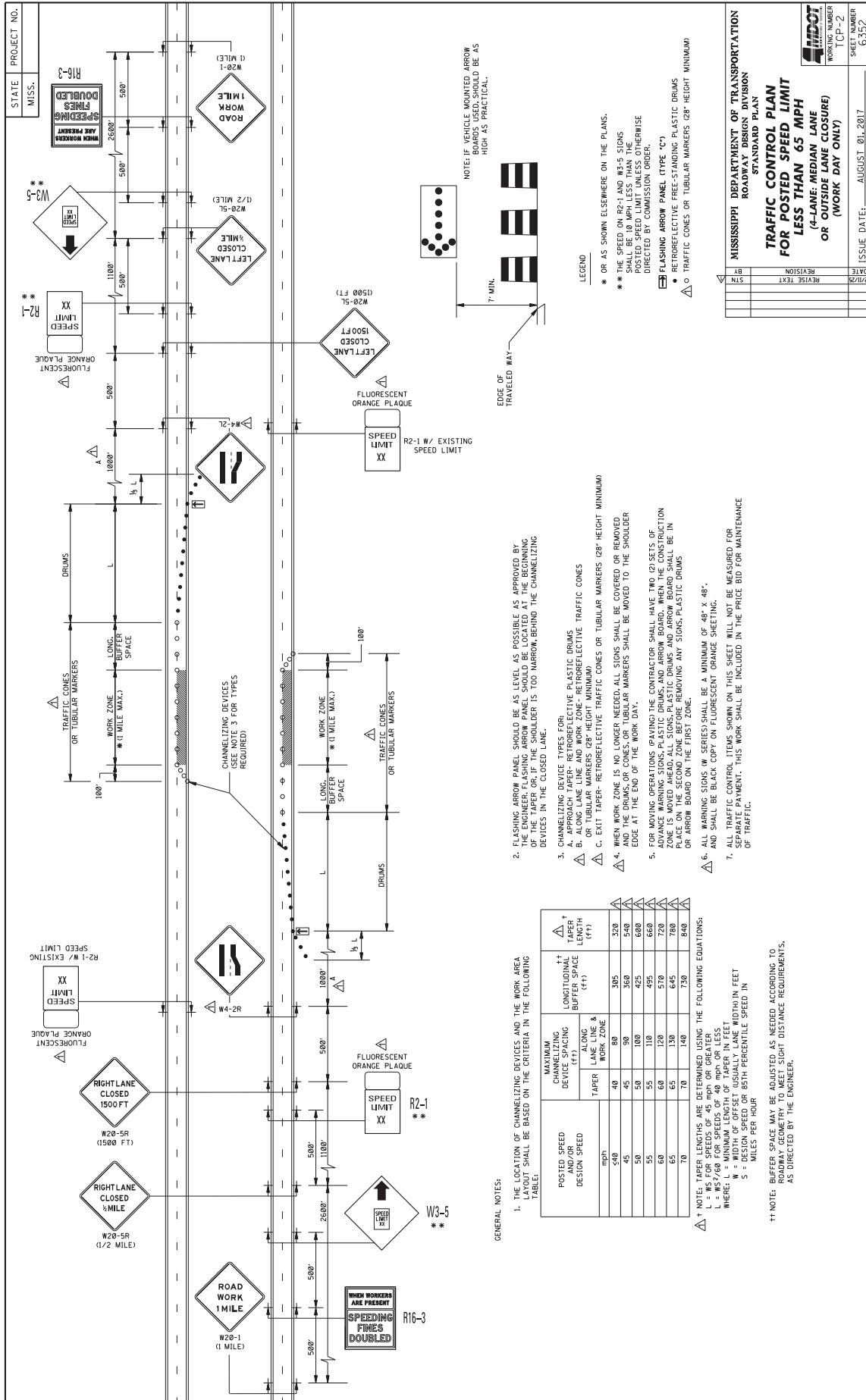
DETAIL "A"

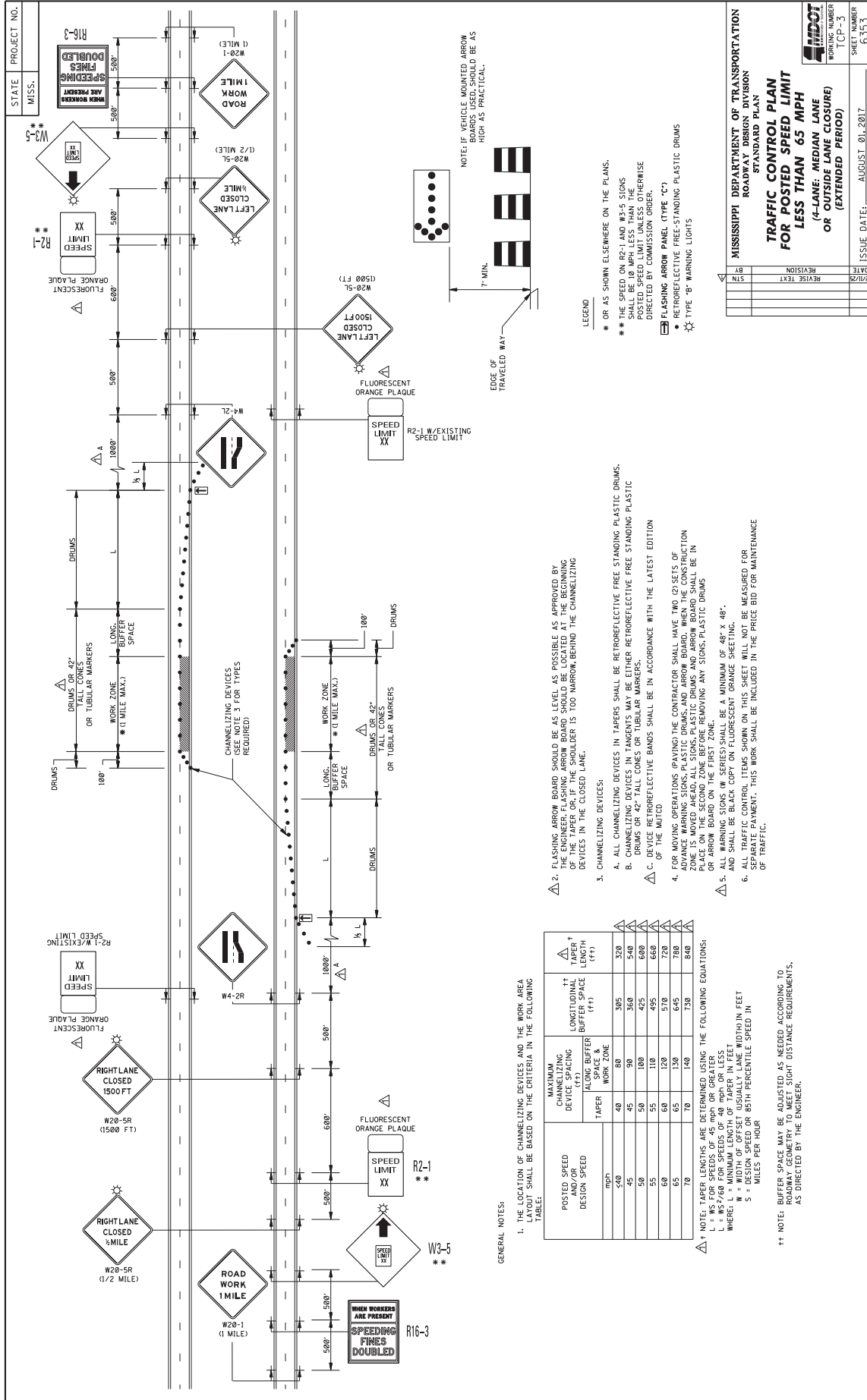


PLAN
NOT TO SCALE
DETAILS OF
RUMBLE STRIPS

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
RUMBLE STRIPES 4-LANE HIGHWAYS (ASPHALT LANES, 2-FT OR WIDER, ASPHALT SHOULDERS)	
WORKING NUMBER	RS-2
SHEET NUMBER	0005
ISSUE DATE:	AUGUST 01, 2017
DATE	
REVISION	
REVISED STRIPS DETAIL	
BY	







GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

POSTED SPEED AND/OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICES SPACING (ft)		LONGITUDINAL BUFFER SPACE (ft)	MINIMUM TAPER LENGTH (ft)	MINIMUM TAPER LENGTH (ft)
	TAPER	WORK ZONE			
40	40	80	305	320	320
45	45	90	360	540	540
50	50	100	425	600	600
55	55	110	495	660	660
60	60	120	570	720	720
65	65	130	645	780	780
70	70	140	730	840	840

NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

†† NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

NOTE: TAPER LENGTHS ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 L = WS FOR SPEEDS OF 45 MPH OR GREATER.
 L = WS/60 FOR SPEEDS OF 40 MPH OR LESS.
 WHERE: W = WIDTH OF OFFSET (USUALLY LANE WIDTH) IN FEET
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

2. FLASHING ARROW BOARD SHOULD BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW BOARD SHOULD BE LOCATED AT THE BEGINNING OF THE WORK AREA. THE BUFFER IS 100' WIDE AND BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.
3. CHANNELIZING DEVICES:
 - A. ALL CHANNELIZING DEVICES IN TAPERS SHALL BE RETROREFLECTIVE FREE STANDING PLASTIC DRUMS.
 - B. CHANNELIZING DEVICES IN TANGENTS MAY BE EITHER RETROREFLECTIVE FREE STANDING PLASTIC DRUMS OR 42" TALL CONES OR TUBULAR MARKERS.
 - C. DEVICE RETROREFLECTIVE BANDS SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE MUTCD.
4. FOR MOVING OPERATIONS (PAVING) THE CONTRACTOR SHALL HAVE TWO (2) SETS OF ADVANCE WARNING DEVICES (RETROREFLECTIVE PLASTIC DRUMS, AND TUBULAR MARKERS) IN PLACE ON THE SECOND ZONE BEFORE REMOVING ANY SIGNS/PLASTIC DRUMS OR ARROW BOARD ON THE FIRST ZONE.
5. ALL WARNING SIGNS (W SERIES) SHALL BE A MINIMUM OF 48" X 48" AND SHALL BE BLACK COPY ON FLUORESCENT ORANGE SHEETING.
6. SEPARATE PAYMENT: THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

LEGEND

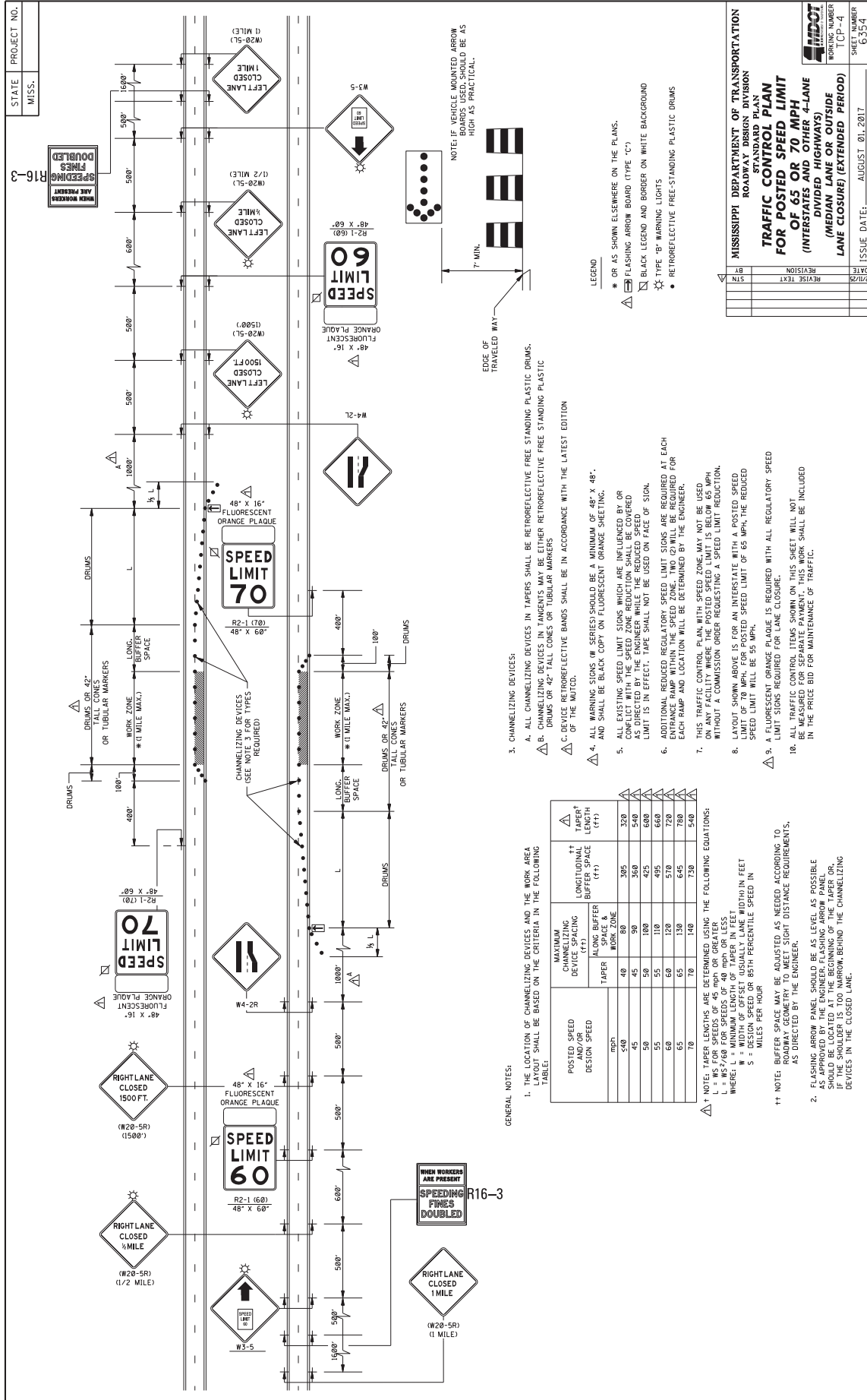
- * OR AS SHOWN ELSEWHERE ON THE PLANS.
- ** THE SPEED ON R2-1 AND R2-1S SIGNS SHALL BE 10 MPH LESS THAN THE POSTED SPEED LIMIT UNLESS OTHERWISE DIRECTED BY COMMISSION ORDER.
- ▢ FLASHING ARROW PANEL (TYPE 'C')
- RETROREFLECTIVE FREE-STANDING PLASTIC DRUMS
- ⊙ TYPE 'B' WARNING LIGHTS

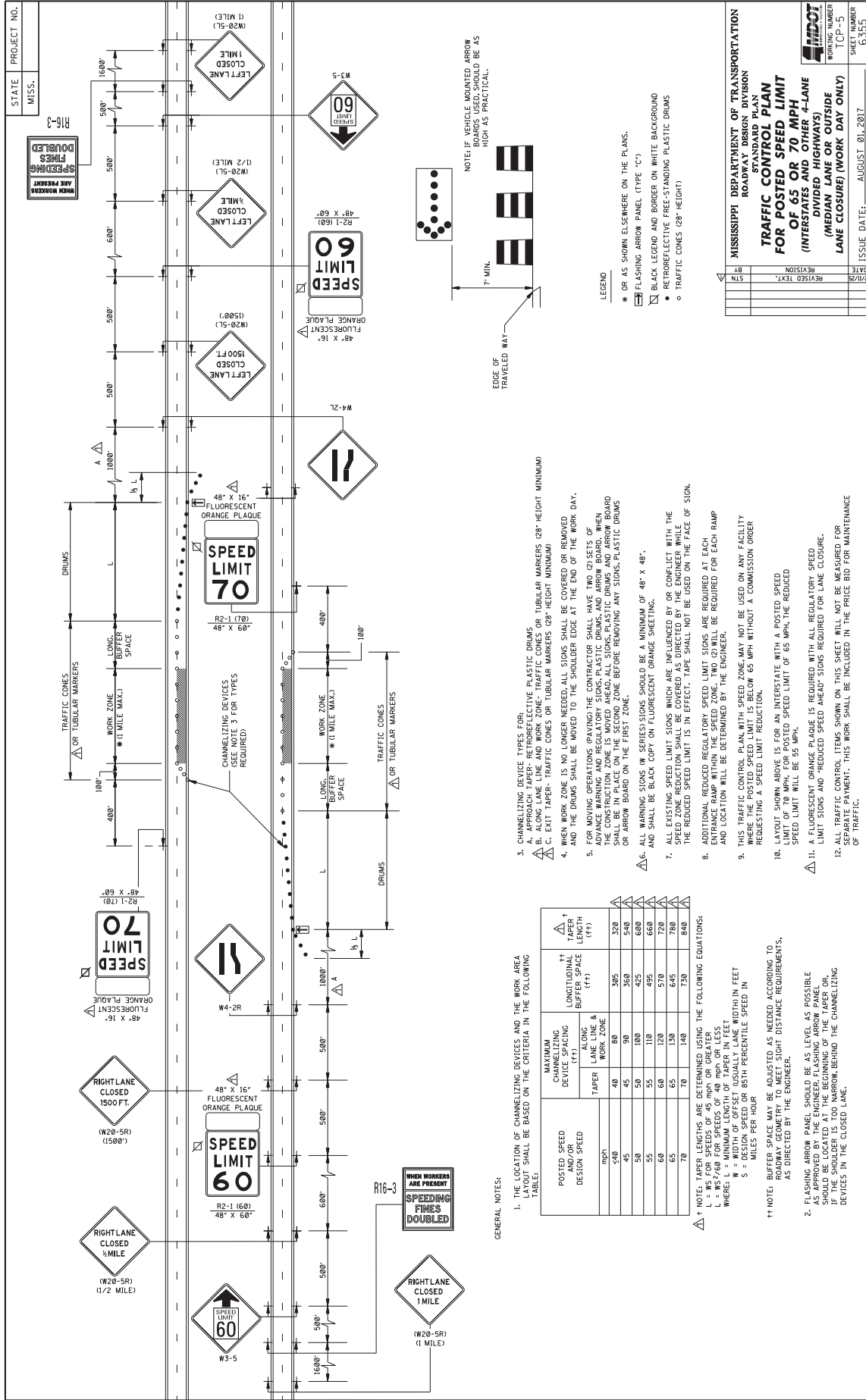
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 ROADWAY DESIGN DIVISION
 STANDARD PLAN

**TRAFFIC CONTROL PLAN
 FOR POSTED SPEED LIMIT
 LESS THAN 65 MPH
 (4-LANE, MEDIAN LANE
 OR OUTSIDE LANE CLOSURE)
 (EXTENDED PERIOD)**

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 ROADWAY DESIGN DIVISION
 STANDARD PLAN

DATE: _____
 REVISION: _____
 SHEET NUMBER: TCP-3
 SHEET NUMBER: 6353
 ISSUE DATE: AUGUST 01, 2017





GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

POSTED SPEED AND/OR DESIGN SPEED (mph)	MAXIMUM CHANNELIZING DEVICE SPACING (ft)		LONGITUDINAL BUFFER SPACE (ft)	TAPER LENGTH (ft)
	TAPER	LANE LINE & WORK ZONE		
40	40	80	305	320
45	45	90	360	540
50	50	100	425	680
55	55	110	495	660
60	60	120	570	720
65	65	130	645	780
70	70	140	730	840

† NOTE: TAPER LENGTHS ARE DETERMINED USING THE FOLLOWING EQUATIONS:

L = WS FOR SPEEDS OF 45 MPH OR GREATER

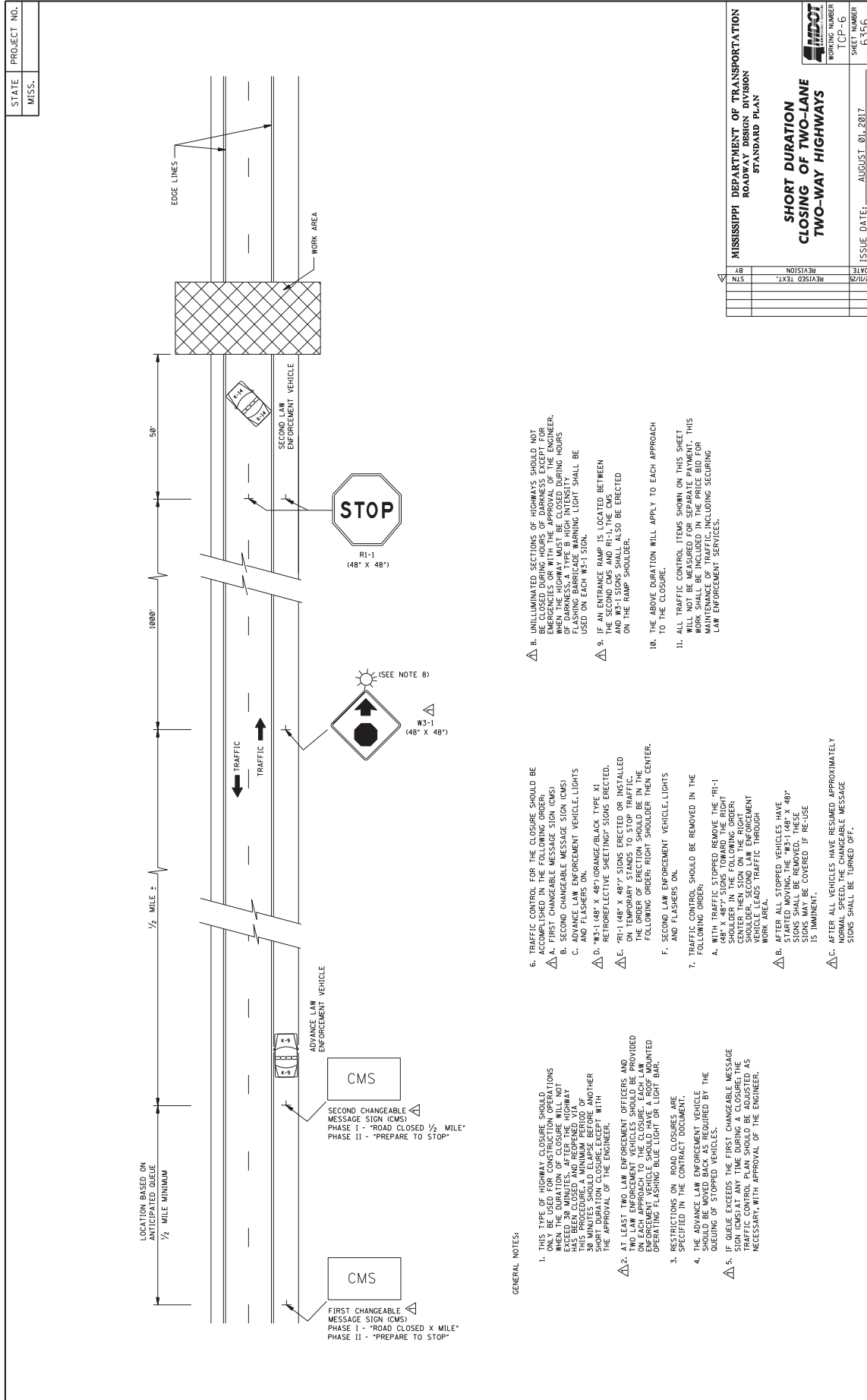
W = WIDTH OF OFFSET (USUALLY LANE WIDTH) IN FEET

S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

†† BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO LOCAL RIGHT OF WAY DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

2. FLASHING ARROW PANEL SHOULD BE AS LEVEL AS POSSIBLE AND LOCATED AT THE BEGINNING OF THE TAPER OR IF THE SHOULDER IS TOO NARROW BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.

3. CHANNELIZING DEVICE TYPES FOR:
 - A. APPROACH TAPER- RETROREFLECTIVE PLASTIC DRUMS
 - B. ALONG LANE LINE AND WORK ZONE- TRAFFIC CONES OR TUBULAR MARKERS (28" HEIGHT MINIMUM)
 - C. EXIT TAPER- TRAFFIC CONES OR TUBULAR MARKERS (28" HEIGHT MINIMUM)
4. WHEN WORK ZONE IS NO LONGER NEEDED, ALL SIGNS SHALL BE COVERED OR REMOVED AND THE DRUMS SHALL BE MOVED TO THE SHOULDER EDGE AT THE END OF THE WORK DAY.
5. FOR MOVING OPERATIONS (PAVING) THE CONTRACTOR SHALL HAVE TWO (2) SETS OF ADVANCE WARNING AND REGULATORY SIGNS- PLASTIC DRUMS, AND ARROW BOARD. WHEN WORKING IN THE WORK ZONE, THE ARROW BOARD SHALL BE IN PLACE ON THE SECOND ZONE BEFORE REMOVING ANY SIGNS, PLASTIC DRUMS OR ARROW BOARD ON THE FIRST ZONE.
6. ALL WARNING SIGNS (W SERIES) SIGNS SHOULD BE A MINIMUM OF 48" X 48" AND SHALL BE BLACK COPY ON FLUORESCENT ORANGE SHEETING.
7. ALL EXISTING SPEED LIMIT SIGNS WHICH ARE INFLUENCED BY OR CONFLICT WITH THE SPEED ZONE REDUCTION SHALL BE COVERED AS DIRECTED BY THE ENGINEER WHILE THE REDUCED SPEED LIMIT IS IN EFFECT. TAPE SHALL NOT BE USED ON THE FACE OF SIGN.
8. ADDITIONAL RAMP REGULATORY SPEED LIMIT SIGNS ARE REQUIRED FOR EACH RAMP ENTRANCE RAMP WITHIN THE SPEED ZONE. TWO (2) WILL BE REQUIRED FOR EACH RAMP AND LOCATION WILL BE DETERMINED BY THE ENGINEER.
9. THIS TRAFFIC CONTROL PLAN WITH SPEED ZONE MAY NOT BE USED ON ANY FACILITY WHERE THE POSTED SPEED LIMIT IS BELOW 65 MPH WITHOUT A COMMISSION ORDER REQUESTING A SPEED LIMIT REDUCTION.
10. LAYOUT SHOWN ABOVE IS FOR AN INTERSTATE WITH A POSTED SPEED LIMIT OF 70 MPH. FOR POSTED SPEED LIMIT OF 65 MPH, THE REDUCED SPEED LIMIT WILL BE 55 MPH.
11. A FLUORESCENT ORANGE PLAQUE IS REQUIRED WITH ALL REGULATORY SPEED LIMIT SIGNS AND "REDUCED AHEAD" SIGNS REQUIRED FOR LANE CLOSURE.
12. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.



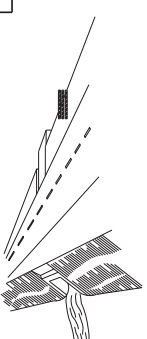
STATE	PROJECT NO.
MISS.	

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
SHORT DURATION CLOSING OF TWO-LANE TWO-WAY HIGHWAYS	
DATE	ISSUE DATE: AUGUST 01, 2017
REVISION	SHEET NUMBER TCP-6
REVISED TEXT	WORKING NUMBER 6356

GENERAL NOTES:

- THIS TYPE OF HIGHWAY CLOSURE SHOULD BE USED ONLY WHEN THE DURATION OF CLOSURE WILL NOT EXCEED 30 MINUTES. AFTER THE HIGHWAY CLOSURE, THE ADVANCE LAW ENFORCEMENT VEHICLE SHOULD ELAPSE BEFORE ANOTHER ADVANCE LAW ENFORCEMENT VEHICLE WITH THE APPROVAL OF THE ENGINEER.
- AT LEAST TWO LAW ENFORCEMENT OFFICERS AND ONE ADVANCE LAW ENFORCEMENT VEHICLE SHOULD BE PROVIDED ON EACH APPROACH TO THE CLOSURE. EACH LAW ENFORCEMENT VEHICLE SHOULD HAVE A PROTRUDING OPERATING FLASHING BLUE LIGHT ON LIGHT BAR.
- RESTRICTIONS ON ROAD CLOSURES ARE SPECIFIED IN THE CONTRACT DOCUMENT.
- THE ADVANCE LAW ENFORCEMENT VEHICLE SHOULD BE POSITIONED AS DETERMINED BY THE QUEUING OF STOPPED VEHICLES.
- IF QUEUE EXCEEDS THE FIRST CHANGEABLE MESSAGE SIGN (CMS) AT ANY TIME DURING A CLOSURE, THE TRAFFIC CONTROL PLAN SHOULD BE ADJUSTED AS NECESSARY, WITH APPROVAL OF THE ENGINEER.
- TRAFFIC CONTROL FOR THE CLOSURE SHOULD BE ACCOMPLISHED IN THE FOLLOWING ORDER:
 - FIRST CHANGEABLE MESSAGE SIGN (CMS)
 - SECOND CHANGEABLE MESSAGE SIGN (CMS)
 - ADVANCE LAW ENFORCEMENT VEHICLE, LIGHTS ON
 - "R1-1 (48" X 48") STOP SIGN, TYPE XI, RETROREFLECTIVE SHEETING" SIGNS ERRECTED.
 - "R1-1 (48" X 48") SIGNS ERRECTED OR INSTALLED ON TEMPORARY STANDS TO STOP TRAFFIC. THE ORDER OF ERECTION SHOULD BE IN THE FOLLOWING ORDER: RIGHT SHOULDER THEN CENTER, AND FLASHERS ON.
 - SECOND LAW ENFORCEMENT VEHICLE, LIGHTS ON.
- TRAFFIC CONTROL SHOULD BE REMOVED IN THE FOLLOWING ORDER:
 - WITH TRAFFIC STOPPED REMOVE THE "R1-1 (48" X 48") SIGNS TOWARD THE RIGHT SHOULDER THEN SIGN ON THE RIGHT SHOULDER. SECOND LAW ENFORCEMENT VEHICLE LEADS TRAFFIC THROUGH WORK AREA.
 - AFTER ALL STOPPED VEHICLES HAVE STARTED MOVING, THE "R1-1 (48" X 48") SIGNS SHALL BE COVERED. THESE SIGNS SHOULD BE COVERED IF RE-USE IS IMMINENT.
- AFTER ALL VEHICLES HAVE RESUMED APPROXIMATELY NORMAL SPEED, THE CHANGEABLE MESSAGE SIGNS SHALL BE TURNED OFF.
- UNILLUMINATED SECTIONS OF HIGHWAYS SHOULD NOT BE CLOSED DURING HOURS OF DARKNESS EXCEPT FOR EMERGENCY REPAIRS. THE APPROVAL OF THE ENGINEER, WHEN THE CLOSURE MUST BE CLOSING DURING HOURS OF DARKNESS, A TYPE B HIGH INTENSITY FLASHING BARRICADE WARNING LIGHT SHALL BE USED ON EACH W5-1 SIGN.
- IF AN ENTRANCE RAMP IS LOCATED BETWEEN THE WORK AREA AND THE RAMP SHOULDER, AND W5-1 SIGNS SHALL ALSO BE ERRECTED ON THE RAMP SHOULDER.
- THE ABOVE DURATION WILL APPLY TO EACH APPROACH TO THE CLOSURE.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE AND REPAIRS, INCLUDING SECURING LAW ENFORCEMENT SERVICES.

STATE	PROJECT NO.
MISS.	

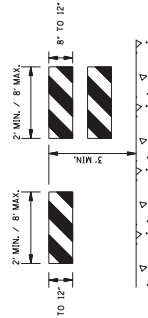


BARRICADE CLOSING A ROAD

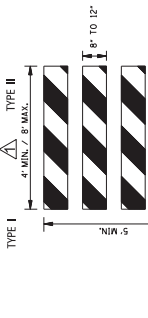
BARRICADE CHARACTERISTICS

	I	II	III
BARRICADE	8" MIN. / 12" MAX.	8" MIN. / 12" MAX.	8" MIN. / 12" MAX.
LENGTH OF RAIL **	24" MIN. / 96" MAX.	24" MIN. / 96" MAX.	48" MIN. / 96" MAX.
WIDTH OF STRIPE #	6"	6"	6"
HEIGHT	36" MIN.	36" MIN.	60" MIN.
NUMBER OF SELF-REFLECTORIZED RAIL FACES	2 (ONE EACH DIRECTION)	4 (TWO EACH DIRECTION)	3 IF FACING TRAFFIC IN ONE DIRECTION 6 IF FACING TRAFFIC IN TWO DIRECTIONS

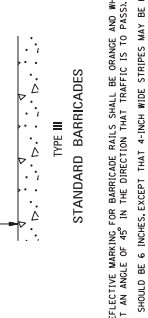
STANDARD BARRICADES



TYPE I



TYPE II



TYPE III

WING BARRICADES

1. WING BARRICADES ARE TYPE III BARRICADES ERECTED ON THE SHOULDER ON ONE OR BOTH SIDES OF THE PAVEMENT TO GIVE THE SITUATION TO TRAVELERS. THE BARRICADES SHOULD BE USED AS A WARNING FOR THE ADVANCE WARNING SIGNS OR FLASHERS.

2. WING BARRICADES SHOULD BE USED:

A. IN ADVANCE OF A CONSTRUCTION PROJECT EVEN WHEN NO PART OF THE ROADWAY IS ACTUALLY CLOSED.

B. IN ADVANCE OF ALL BRIDGE OR CULVERT REPAIRING OPERATIONS.

3. FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.

4. BARRICADES INTENDED FOR USE ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH SPEED ROADWAYS, SHALL HAVE A MINIMUM OF 270 SQ FT OF REFLECTIVE AREA FACING TRAFFIC.

5. DO NOT PLACE SANDBAGS OR OTHER DEVICES ON BARRICADE RAILS TO PROVIDE MASS SANDBAGS; HOWEVER, THEY MAY BE PLACED AS BALLAST TO THE LOWER PARTS OF THE FRAME.

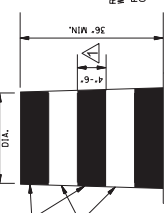
6. FOR ADDITIONAL INFORMATION OR DETAILS, SEE MUTCD, LATEST EDITION.

7. BARRICADES FOR USE AS CATEGORY II WORK ZONE DEVICES.

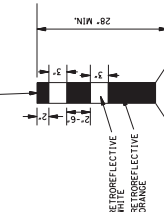
8. A LIST OF CRASHWORTHY BARRICADES AND OTHER CATEGORY II DEVICES CAN BE FOUND ON FHWA'S WEBSITE: <http://highways.dot.gov/safety/rnd/reduce-crash-severity>

9. WHERE ROAD USERS INCLUDE PEDESTRIANS, THE PROVISION OF SUPPLEMENTAL AUDIBLE INFORMATION OR DETECTABLE BARRIERS OR BARRICADES SHOULD BE PROVIDED FOR PEOPLE WITH VISION DISABILITIES.

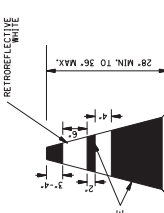
10. BARRICADE RAIL SUPPORTS SHOULD NOT PROJECT INTO PEDESTRIAN CIRCULATION ROUTES MORE THAN 4 INCHES FOR THE SUPPORTS LOCATED BETWEEN 27 INCHES TO 80 INCHES ABOVE THE EXISTING SURFACE.



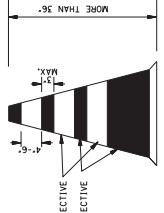
PLASTIC DRUM DETAIL
(18" - 36")



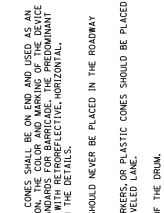
TUBULAR MARKER DETAIL



PLASTIC CONE DETAIL
(28" - 36")



PLASTIC DRUM DETAIL
(MORE THAN 36")



PLASTIC CONE DETAIL
(36" OR GREATER)

TYPE 3 OBJECT MARKER (OMS-R)

1. TYPE 3 OBJECT MARKERS SHALL BE USED AT ALL EXPOSED BRIDGE ABUTMENTS AND AT OTHER LOCATIONS AS DEEMED NECESSARY BY THE ENGINEER.

2. THE OMS-R IS SHOWN. THE OMS-L IS SIMILAR EXCEPT THE STRIPES ARE PLACED ON THE UPPER RIGHT SIDE AND SHALL BE PLACED ON THE LEFT SIDE OF THE OBJECT.

3. THE INNER EDGE OF THE MARKER SHALL BE IN LINE WITH THE INNER EDGE OF THE OBSTRUCTION.

4. THE STRIPING SHALL CONSIST OF ALTERNATING BLACK AND RETROREFLECTIVE YELLOW SLOPING DOWNWARD TOWARD THE SIDE ON WHICH TRAFFIC IS THE PASS.

RETROREFLECTIVE CHEVRON ALIGNMENT SIGN DETAIL (WT-8)

1. A CHEVRON SIGN CONSISTS OF A BLACK CHEVRON TYPE MARKING ON AN ORANGE BACKGROUND AND SHALL POINT IN THE DIRECTION OF TRAFFIC FLOW.

2. THE CHEVRON SIGN SHALL BE MOUNTED ON CRASHWORTHY SUPPORT.

3. CHEVRON SIGNS MAY BE USED TO SUPPLEMENT OTHER STANDARD DEVICES WHERE ONE OR MORE LANES ARE CLOSED FOR CONSTRUCTION OR MAINTENANCE. THEY SHOULD BE PLACED APPROXIMATELY 2'-0" BEHIND THE LANE TRANSITION STRIPE.

4. CHEVRON SIGNS SHALL BE INSTALLED AT A MINIMUM HEIGHT OF 4 FEET MEASURED VERTICALLY FROM THE BOTTOM OF THE SIGN TO THE ELEVATION OF THE NEAR EDGE OF THE TRAVELWAY.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

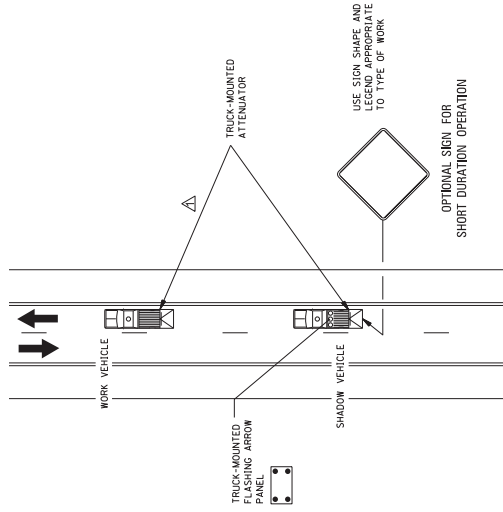
HIGHWAY SIGN AND BARRICADE DETAILS FOR CONSTRUCTION PROJECTS

DATE	REVISION	BY

WORKING NUMBER: TSP-6
SHEET NUMBER: 0530
ISSUE DATE: AUGUST 01, 2017

STATE	PROJECT NO.
MISS.	

MOBILE OPERATIONS ON TWO-LANE ROAD

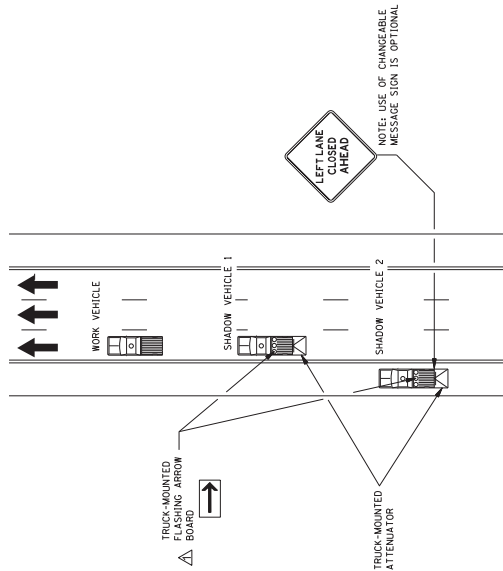


MOBILE OPERATIONS ON TWO-LANE ROAD

NOTES FOR TWO-LANE OPERATION:

1. WHERE PRACTICAL AND WHEN NEEDED, THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS. IF THIS CAN NOT BE DONE FREQUENTLY AS AN ALTERNATIVE, A "DO NOT PASS" SIGN MAY BE PLACED ON THE REAR OF THE VEHICLE BLOCKING THE LANE.
2. THE DISTANCE BETWEEN THE WORK AND SHADOW VEHICLES MAY VARY ACCORDING TO TERRAIN, PAINT DRYING TIME, AND OTHER FACTORS. SHADOW VEHICLES ARE USED TO WARN TRAFFIC OF THE OPERATION AHEAD. WHENEVER ADEQUATE SPACE IS AVAILABLE, SHADOW VEHICLES SHOULD BE POSITIONED AS NEAR AS POSSIBLE TO MAINTAIN THE MINIMUM DISTANCE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. THE SHADOW VEHICLE SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
3. ADDITIONAL SHADOW VEHICLES TO WARN AND REDUCE THE SPEED OF ONCOMING OR OPPOSING TRAFFIC MAY BE USED. POLICE PATROL CARS MAY BE USED FOR THIS PURPOSE.
4. A TRUCK-MOUNTED ATTENUATOR (TMA) SHOULD BE USED ON THE SHADOW VEHICLE AND MAY BE USED ON THE WORK VEHICLE.
5. THE WORK VEHICLE SHALL BE EQUIPPED WITH BEACONS, AND THE SHADOW VEHICLE SHALL BE EQUIPPED WITH BEACONS, LIGHTS, AND ROTATING BEACONS. LIGHTS MOUNTED ON THE REAR, ADJACENT TO THE SIGN, SHADOW AND WORK VEHICLES SHALL DISPLAY FLASHING OR ROTATING BEACONS BOTH FORWARD AND TO THE REAR.
6. VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBTSCURED BY EQUIPMENT ON SIGNPOSTS. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
7. ARROW BOARD TO BE USED IN CAUTION MODE.
8. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

MOBILE OPERATIONS ON MULTILANE ROAD

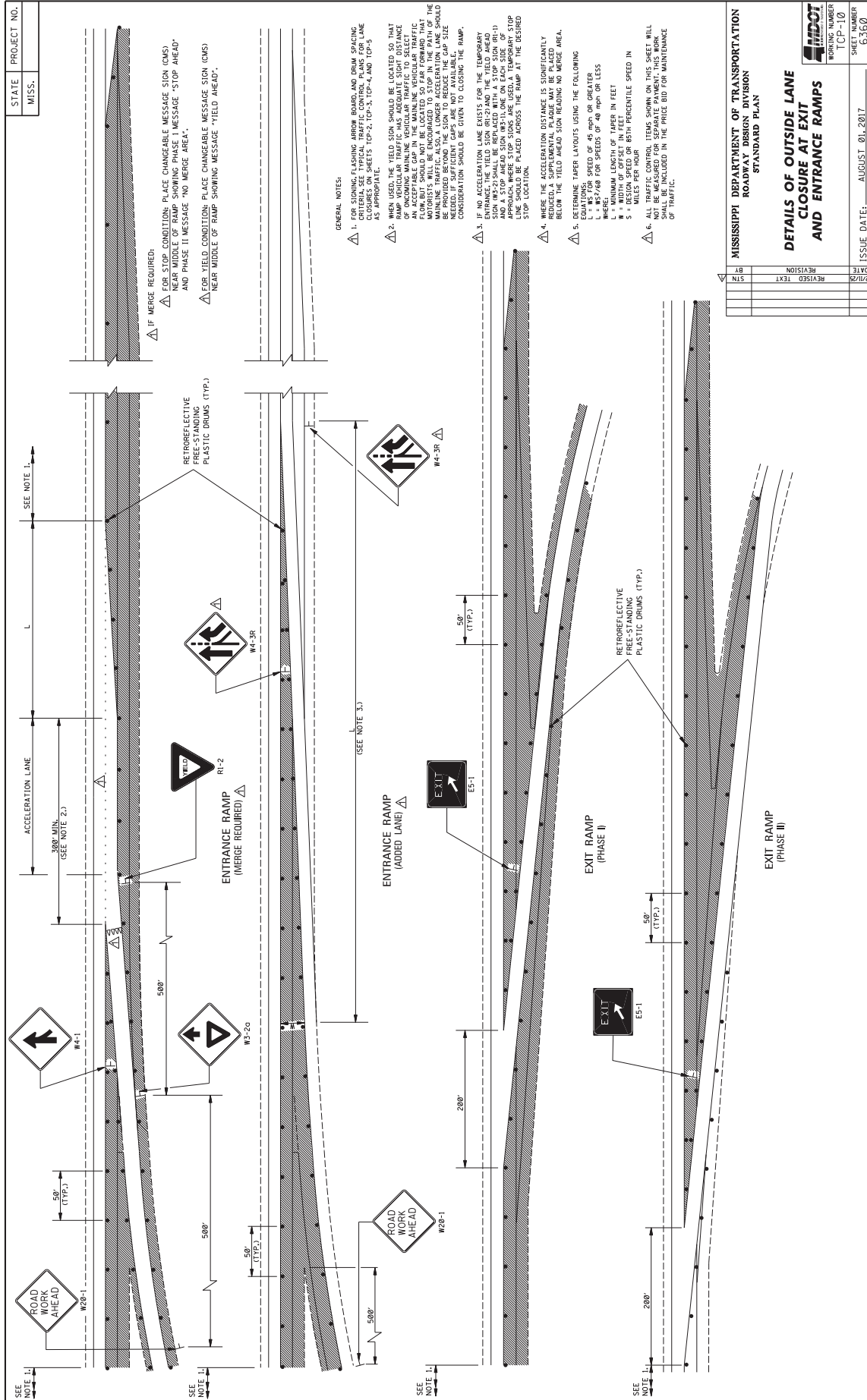


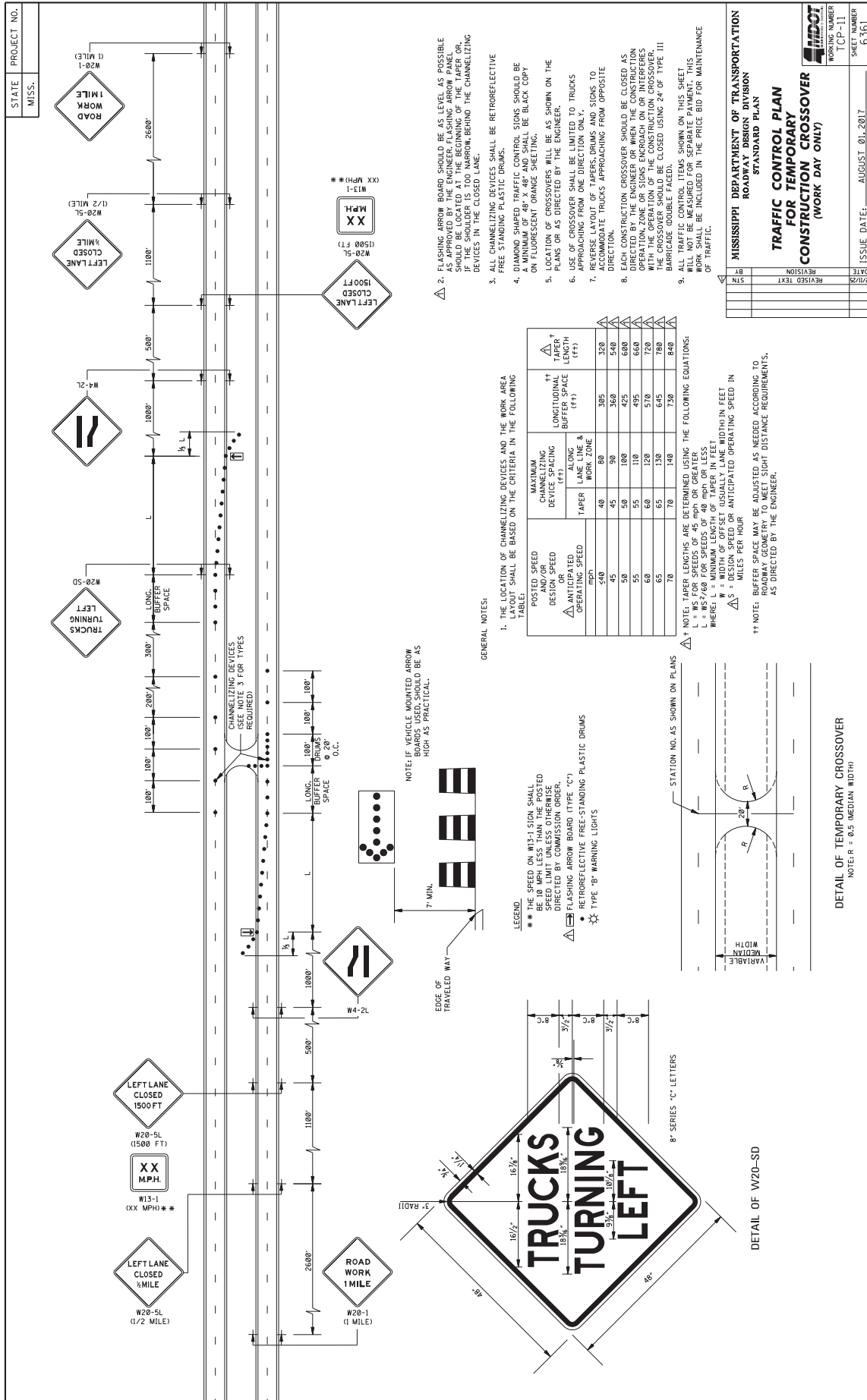
MOBILE OPERATIONS ON MULTILANE ROAD

NOTES FOR MULTILANE LANE OPERATION:

1. SHADOW AND WORK VEHICLES SHALL DISPLAY HIGH-INTENSITY FLASHING BEACONS. A TRUCK-MOUNTED ATTENUATOR (TMA) AND ARROW BOARD SHALL BE USED WHEN A FREEWAY LANE IS CLOSED. WHEN MORE THAN ONE FREEWAY LANE IS CLOSED, A SEPARATE ARROW BOARD SHALL BE USED FOR EACH CLOSED LANE.
2. SHADOW VEHICLE 2 SHOULD BE EQUIPPED WITH AN ARROW BOARD AND TRUCK-MOUNTED ATTENUATOR (TMA). APPROPRIATE LANE CLOSURE SIGN SHOULD BE PLACED ON SHADOW VEHICLE 2 SO AS NOT TO OBTSCURE THE ARROW BOARD.
3. SHADOW VEHICLE 1 SHOULD BE EQUIPPED WITH AN ARROW BOARD AND TRUCK-MOUNTED ATTENUATOR (TMA).
4. SHADOW VEHICLE 2 SHOULD TRAVEL AT A VARYING DISTANCE FROM THE WORK OPERATION SO AS TO PROVIDE ADEQUATE SIGHT DISTANCE FOR TRAFFIC APPROACHING FROM THE REAR.
5. THE SPACING BETWEEN THE WORK VEHICLES AND THE SHADOW VEHICLES, AND BETWEEN EACH SHADOW VEHICLE, SHOULD BE MINIMIZED TO DETER ROAD USERS FROM DRIVING IN BETWEEN.
6. ON HIGH-SPEED ROADWAYS, A THIRD SHADOW VEHICLE (NOT SHOWN) MAY BE USED WITH SHADOW VEHICLE 1 IN THE CLOSED LANE, SHADOW VEHICLE 2 STRADDLING THE EDGE LINE, AND SHADOW VEHICLE 3 ON THE SHOULDER.
7. ARROW BOARD SHALL BE AS A MINIMUM TYPE B, 60" x 36" IN ACCORDANCE WITH THE CRITERIA PRESENTED IN THE MUTCD.
8. WORK SHOULD NORMALLY BE DONE DURING OFF-PEAK HOURS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
ROADWAY DESIGN DIVISION	
STANDARD PLAN	
TRAFFIC CONTROL PLAN	
MOBILE OPERATIONS	
MULTILANE ROADS	
TWO-LANE ROADS	
DATE	ISSUE DATE: AUGUST 01, 2017
BY	
REVISED	
TEXT AND DETAILS	
BY	
REVISED	
WORKING NUMBER	TCP-9
SHEET NUMBER	6359





1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA TAPER SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:
2. FLASHING ARROW BOARD SHOULD BE AS FEW AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR, IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.
3. ALL CHANNELIZING DEVICES SHALL BE RETROREFLECTIVE FREE STANDING PLASTIC DRUMS.
4. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHOULD BE ON FLUORESCENT ORANGE SHEETING. BE BLACK COPY PLANS OR AS DIRECTED BY THE ENGINEER.
5. LOCATION OF CROSSOVERS WILL BE AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
6. USE OF CROSSOVERS SHALL BE LIMITED TO TRUCKS APPROACHING FROM ONE DIRECTION ONLY.
7. REVERSE LAYOUT OF TAPERS, DRUMS AND SIGNS TO ACCOMMODATE TRUCKS APPROACHING FROM OPPOSITE DIRECTION.
8. EACH CONSTRUCTION CROSSOVER SHOULD BE CLOSED AS EARLY AS POSSIBLE AND KEPT CLOSED UNTIL THE OPERATION ZONE OR SIGNS ENDOUR ON OR INTERFERES WITH THE OPERATION OF THE CONSTRUCTION CROSSOVER. THE CROSSOVER SHOULD BE CLOSED USING 24" OF TYPE III BARRIER.
9. ALL TRAFFIC CONTROL SIGNS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

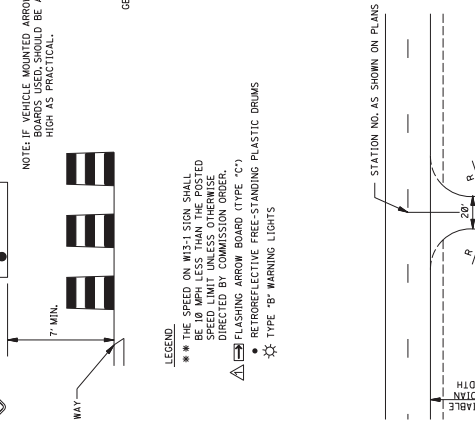
GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA TAPER SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

POSTED SPEED AND/OR DESIGN SPEED (MPH)	MAXIMUM CHANNELIZING DEVICE SPACING (FT)	LONGITUDINAL TAPER BUFFER SPACE (FT)	TAPER LENGTH (FT)
50	49	80	395
45	45	80	368
50	50	100	425
55	55	110	495
60	60	120	570
65	65	130	645
70	70	140	730
75	75	150	810

NOTE: TAPER LENGTHS ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 $L = WS^2/400$ FOR SPEEDS OF 40 MPH OR LESS
 WHERE: L = MINIMUM LENGTH OF TAPER IN FEET
 W = WIDTH OF OFFSET USUALLY LANE WIDTH IN FEET
 S = ANTICIPATED OPERATING SPEED IN MILES PER HOUR

NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

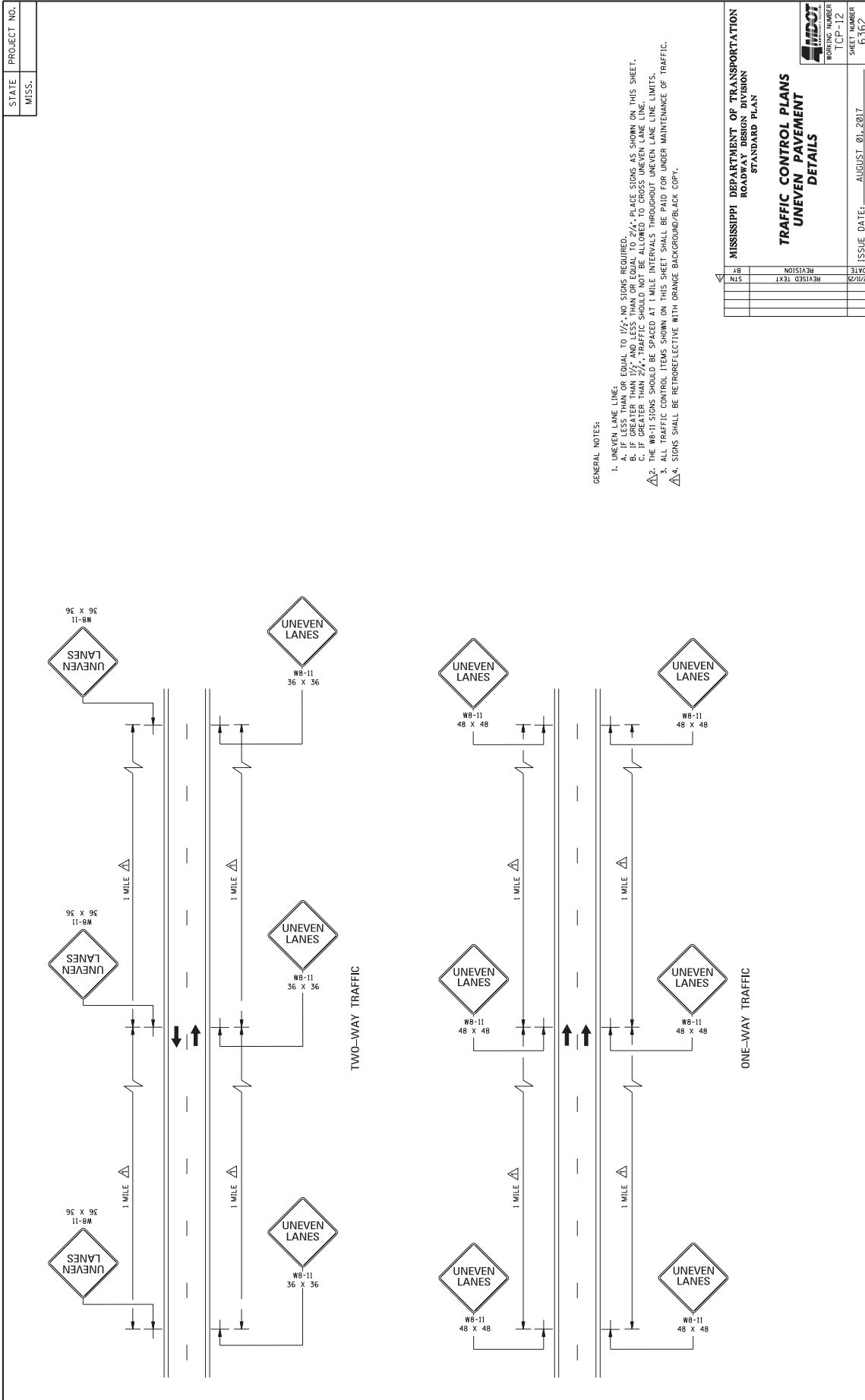


STATE PROJECT NO. MISS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 ROADWAY DESIGN DIVISION
 STANDARD PLAN
**TRAFFIC CONTROL PLAN
 FOR TEMPORARY
 CONSTRUCTION CROSSOVER
 (WORK DAY ONLY)**

WORKING NUMBER: TSP-11
 SHEET NUMBER: 6361

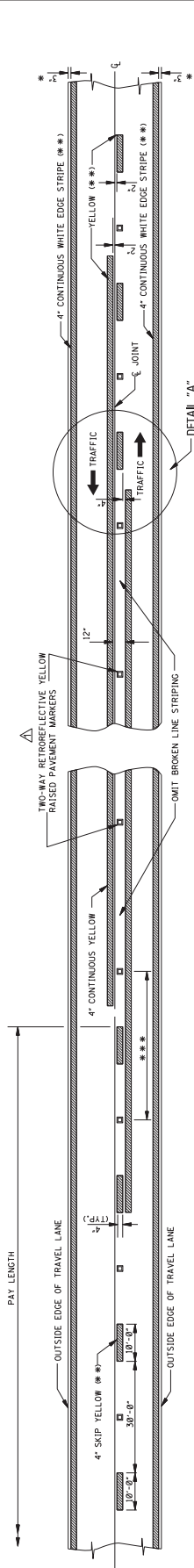
ISSUE DATE: AUGUST 01, 2017



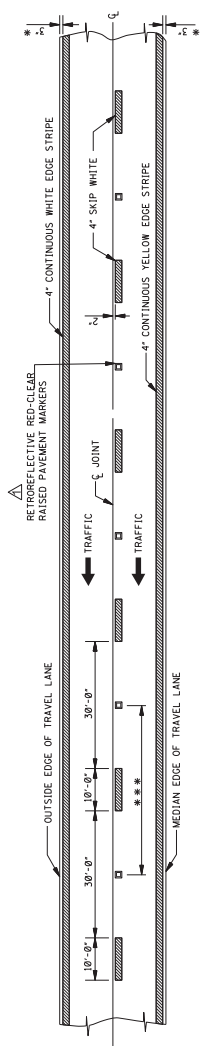
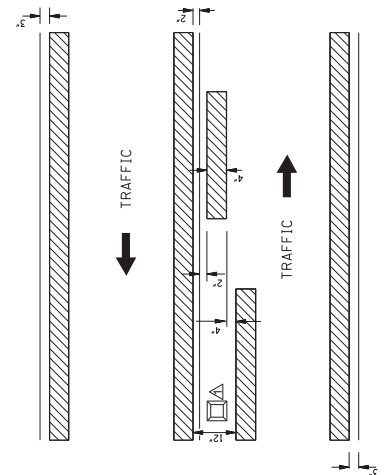
STATE	PROJECT NO.
MISS.	

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
TRAFFIC CONTROL PLANS UNEVEN PAVEMENT DETAILS	
DATE	ISSUE DATE: AUGUST 01, 2017
BY	
CHKD	
DATE	
REVISION	
REVISED TEXT	
STN.	
WORKING NUMBER	TCP-12
SHEET NUMBER	6362

STATE PROJECT NO.
MISS.



TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)



4-LANE WITH ONE-WAY TRAFFIC



GENERAL NOTES:

- * 1. 3" UNLESS SHOWN ELSEWHERE ON THE PLANS.
- ** 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR TAPE AS INDICATED IN PAY ITEMS).
- △ 3. RETROREFLECTIVE RAISED PAVEMENT MARKERS TO BE USED IF TEMPORARY MARKINGS ARE TO REMAIN IN PLACE OVER 3 MONTHS
- △ *** 4. SPACING OF RETROREFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

TANGENT SECTIONS	URBAN AREA (40'-0" - 47'-10")	RURAL AREA (47'-10" - 80'-0")
HORIZONTAL CURVES	40'-0"	80'-0"
INTERCHANGE EBITS	40'-0"	140'-0"

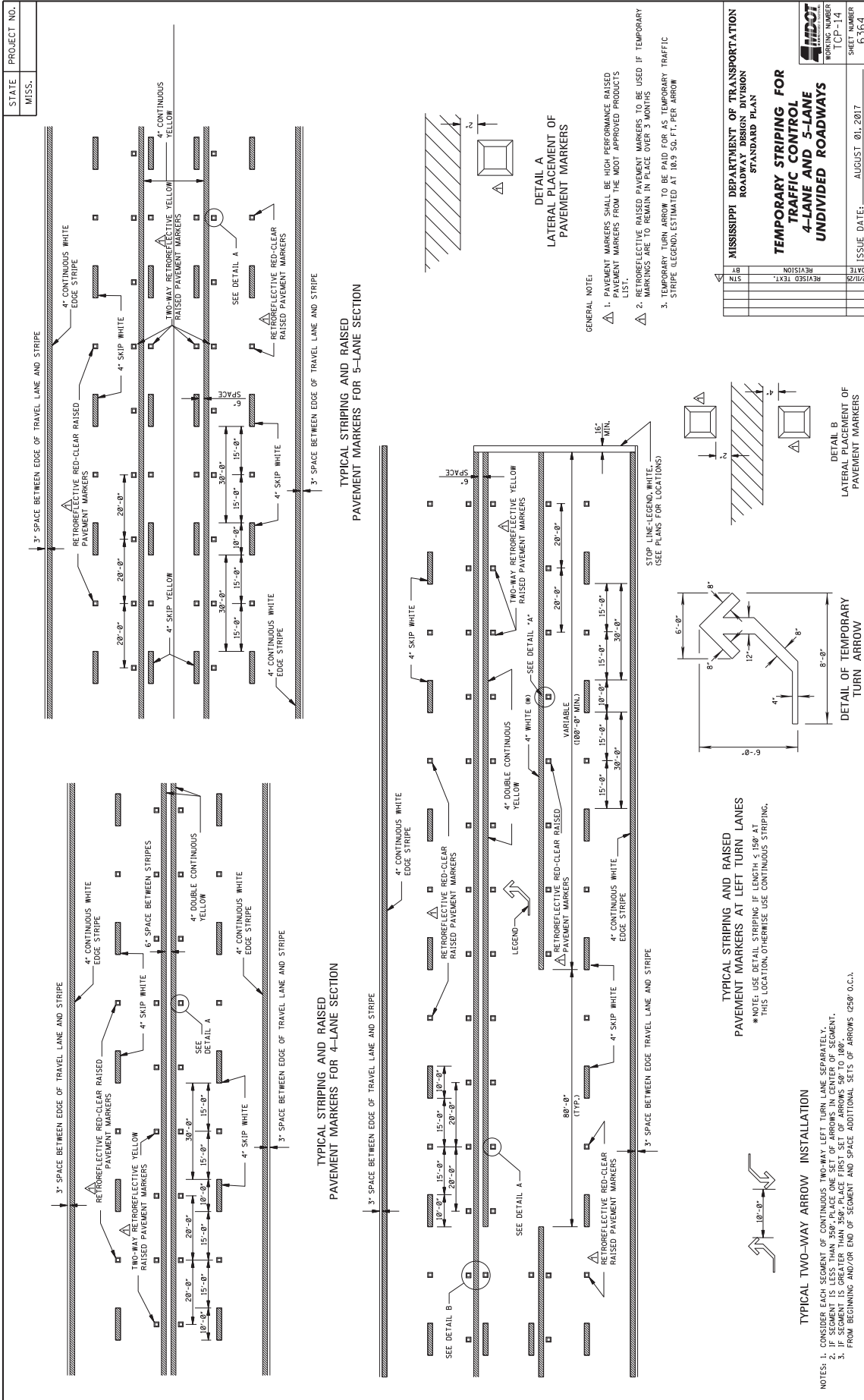
- △ 5. NOTE: ON THE MAIN FACILITY, RETROREFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS SHALL BE USED THROUGHOUT THE INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE IN DIRECTION OF TRAFFIC OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
- △ 6. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RETROREFLECTIVE MARKERS FROM THE AASHTO APPROVED PRODUCTS LIST.

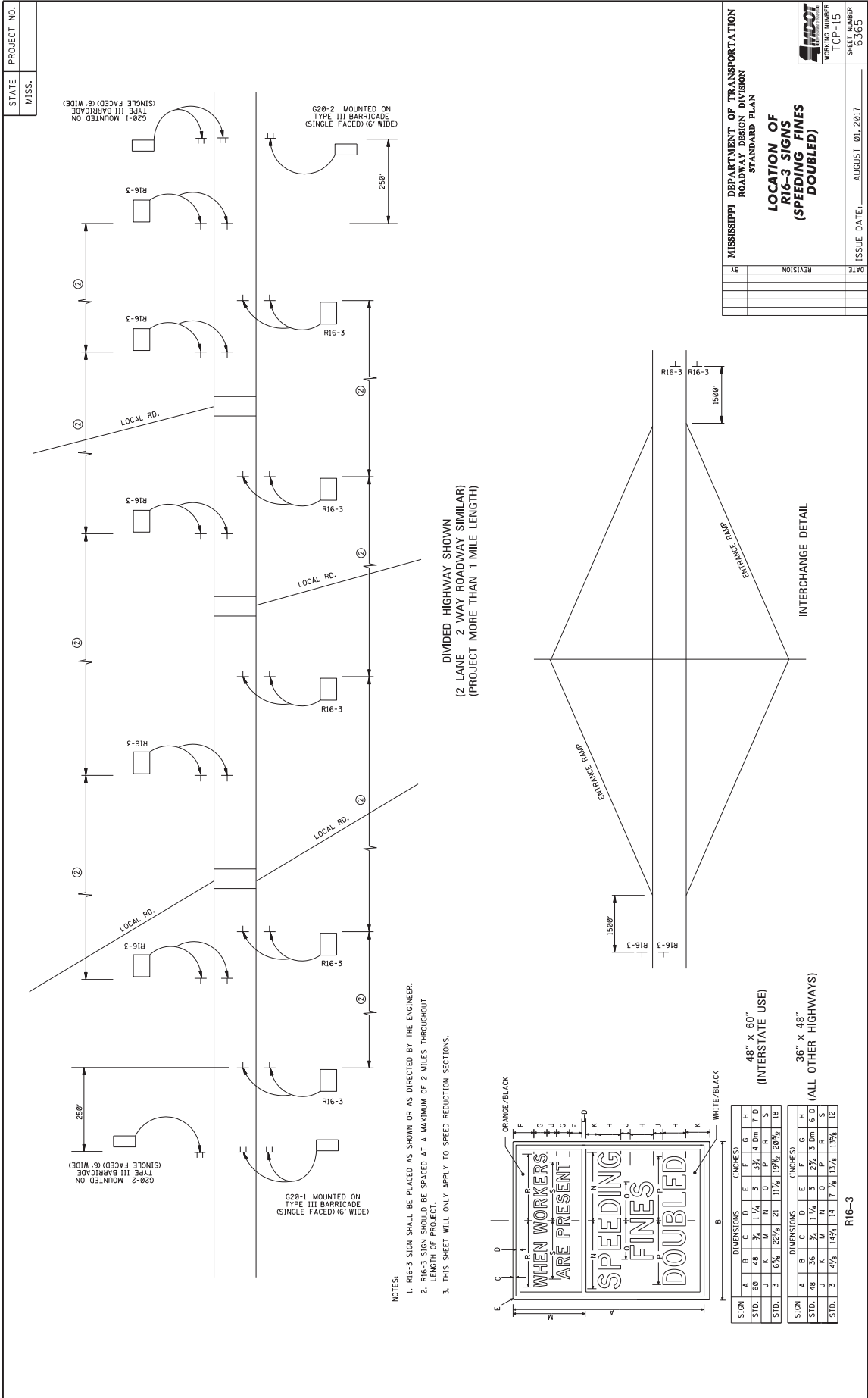
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**TEMPORARY STRIPING FOR
2-LANE AND 4-LANE
DIVIDED HIGHWAYS**

WORKING NUMBER
TCP-113

ISSUE DATE: AUGUST 01, 2017
SHEET NUMBER
6363

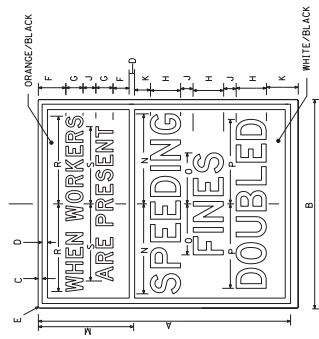




STATE PROJECT NO.
MISS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
LOCATION OF R16-3 SIGNS (SPEEDING FINES DOUBLED)	
WORKING NUMBER TCP-15	SHEET NUMBER 6363
ISSUE DATE: AUGUST 01, 2017	
DATE	REVISION

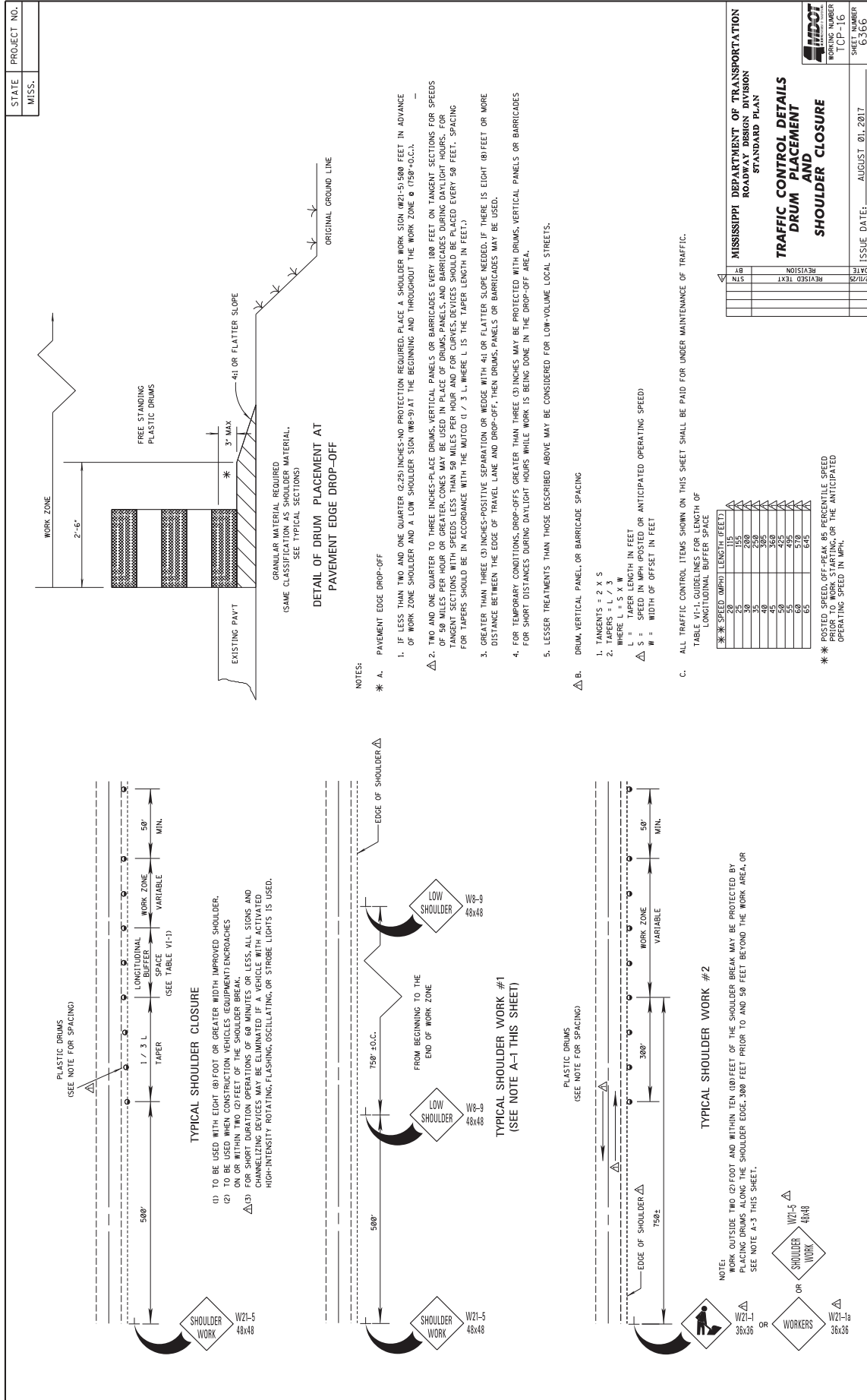
- NOTES:
- R16-3 SIGN SHALL BE PLACED AS SHOWN OR AS DIRECTED BY THE ENGINEER.
 - R16-3 SIGN SHOULD BE SPACED AT A MAXIMUM OF 2 MILES THROUGHOUT LENGTH OF PROJECT.
 - THIS SHEET WILL ONLY APPLY TO SPEED REDUCTION SECTIONS.



48" x 60"
(INTERSTATE USE)

36" x 48"
(ALL OTHER HIGHWAYS)

R16-3



"General Decision Number: MS20260083 01/02/2026

Superseded General Decision Number: MS20250083

State: Mississippi

Construction Type: Highway

County: Calhoun County in Mississippi.

HIGHWAY CONSTRUCTION PROJECTS

Modification Number Publication Date
0 01/02/2026

SUMS2010-006 08/04/2014

	Rates	Fringes
CARPENTER (Form Work Only).....	\$ 14.13	0.00
CARPENTER, Excludes Form Work....	\$ 13.49	0.00
CEMENT MASON/CONCRETE FINISHER...	\$ 13.70	0.00
ELECTRICIAN.....	\$ 21.80	7.93
HIGHWAY/PARKING LOT STRIPING: Truck Driver (Line Striping Truck).....	\$ 17.59	0.00
INSTALLER - GUARDRAIL.....	\$ 11.51	0.00
IRONWORKER, REINFORCING.....	\$ 14.82	0.00
LABORER: Common or General, Including Asphalt Raking, Shoveling, Spreading and Concrete Work.....	\$ 10.36	0.00
LABORER: Flagger.....	\$ 9.76	0.00
LABORER: Grade Checker.....	\$ 12.77	0.00
LABORER: Landscape.....	\$ 8.35	0.00
LABORER: Mason Tender - Cement/Concrete.....	\$ 11.08	0.00
LABORER: Pipelayer.....	\$ 10.76	0.00
LABORER: Laborer-Cones/ Barricades/Barrels - Setter/Mover/Sweeper.....	\$ 10.38	0.00
OPERATOR: Asphalt Spreader.....	\$ 16.03	0.00
OPERATOR: Backhoe/Excavator/Trackhoe.....	\$ 14.40	0.00
OPERATOR: Boring Machine.....	\$ 15.14	0.00
OPERATOR: Broom/Sweeper.....	\$ 10.94	0.00

OPERATOR: Bulldozer.....	\$ 15.00	0.00
OPERATOR: Concrete Saw.....	\$ 15.68	0.00
OPERATOR: Crane.....	\$ 18.32	0.00
OPERATOR: Distributor.....	\$ 12.59	0.00
OPERATOR: Drill.....	\$ 19.22	0.00
OPERATOR: Grader/Blade.....	\$ 14.57	0.00
OPERATOR: Loader.....	\$ 11.54	0.00
OPERATOR: Mechanic.....	\$ 15.13	0.00
OPERATOR: Milling Machine.....	\$ 15.12	0.00
OPERATOR: Oiler.....	\$ 12.33	0.00
OPERATOR: Paver (Asphalt, Aggregate, and Concrete).....	\$ 14.47	0.00
OPERATOR: Piledriver.....	\$ 15.13	0.00
OPERATOR: Roller (All Types)....	\$ 11.54	0.00
OPERATOR: Scraper.....	\$ 13.15	0.00
OPERATOR: Tractor.....	\$ 11.25	0.00
OPERATOR: Trencher.....	\$ 15.00	0.00
TRUCK DRIVER: Flatbed Truck.....	\$ 13.79	0.00
TRUCK DRIVER: Lowboy Truck.....	\$ 13.30	0.00
TRUCK DRIVER: Mechanic.....	\$ 14.23	0.00
TRUCK DRIVER: Off the Road Truck.....	\$ 12.29	0.00
TRUCK DRIVER: Water Truck.....	\$ 10.58	0.00
TRUCK DRIVER: Dump Truck (All Types).....	\$ 14.29	0.00
TRUCK DRIVER: Semi/Trailer Truck.....	\$ 15.81	0.00

WELDERS - Receive rate prescribed for craft performing operation to which welding is incidental.

=====
Note: Executive Order (EO) 13706, Establishing Paid Sick Leave for Federal Contractors applies to all contracts subject to the Davis-Bacon Act for which the contract is awarded (and any solicitation was issued) on or after January 1, 2017. If this contract is covered by the EO, the contractor must provide employees with 1 hour of paid sick leave for every 30 hours they work, up to 56 hours of paid sick leave each year. Employees must be permitted to use paid sick leave for their own illness, injury or other health-related needs, including

preventive care; to assist a family member (or person who is like family to the employee) who is ill, injured, or has other health-related needs, including preventive care; or for reasons resulting from, or to assist a family member (or person who is like family to the employee) who is a victim of, domestic violence, sexual assault, or stalking. Additional information on contractor requirements and worker protections under the EO is available at <https://www.dol.gov/agencies/whd/government-contracts>.

Note: Executive Order 13658 generally applies to contracts subject to the Davis-Bacon Act that were awarded on or between January 1, 2015 and January 29, 2022, and that have not been renewed or extended on or after January 30, 2022. Executive Order 13658 does not apply to contracts subject only to the Davis-Bacon Related Acts regardless of when they were awarded. If a contract is subject to Executive Order 13658, the contractor must pay all covered workers at least \$13.30 per hour (or the applicable wage rate listed on this wage determination, if it is higher) for all hours spent performing on the contract in 2025. The applicable Executive Order minimum wage rate will be adjusted annually. Additional information on contractor requirements and worker protections under Executive Order 13658 is available at www.dol.gov/whd/govcontracts.

Unlisted classifications needed for work not included within the scope of the classifications listed may be added after award only as provided in the labor standards contract clauses (29CFR 5.5 (a) (1) (iii)).

The body of each wage determination lists the classifications and wage rates that have been found to be prevailing for the type(s) of construction and geographic area covered by the wage determination. The classifications are listed in alphabetical order under rate identifiers indicating whether the particular rate is a union rate (current union negotiated rate), a survey rate, a weighted union average rate, a state adopted rate, or a supplemental classification rate.

Union Rate Identifiers

A four-letter identifier beginning with characters other than ""SU"", ""UAVG"", ?SA?, or ?SC? denotes that a union rate was prevailing for that classification in the survey. Example: PLUM0198-005 07/01/2024. PLUM is an identifier of the union whose collectively bargained rate prevailed in the survey for this classification, which in this example would be Plumbers. 0198 indicates the local union number or district council number where applicable, i.e., Plumbers Local 0198. The next number, 005 in the example, is an internal number used in processing the wage determination. The date, 07/01/2024 in the example, is the effective date of the most current negotiated rate.

Union prevailing wage rates are updated to reflect all changes over time that are reported to WHD in the rates in the collective bargaining agreement (CBA) governing the classification.

Union Average Rate Identifiers

The UAVG identifier indicates that no single rate prevailed for those classifications, but that 100% of the data reported for the classifications reflected union rates. EXAMPLE:

UAVG-OH-0010 01/01/2024. UAVG indicates that the rate is a weighted union average rate. OH indicates the State of Ohio. The next number, 0010 in the example, is an internal number used in producing the wage determination. The date, 01/01/2024 in the example, indicates the date the wage determination was updated to reflect the most current union average rate.

A UAVG rate will be updated once a year, usually in January, to reflect a weighted average of the current rates in the collective bargaining agreements on which the rate is based.

Survey Rate Identifiers

The ""SU"" identifier indicates that either a single non-union rate prevailed (as defined in 29 CFR 1.2) for this classification in the survey or that the rate was derived by computing a weighted average rate based on all the rates reported in the survey for that classification. As a weighted average rate includes all rates reported in the survey, it may include both union and non-union rates. Example: SUFL2022-007 6/27/2024. SU indicates the rate is a single non-union prevailing rate or a weighted average of survey data for that classification. FL indicates the State of Florida. 2022 is the year of the survey on which these classifications and rates are based. The next number, 007 in the example, is an internal number used in producing the wage determination. The date, 6/27/2024 in the example, indicates the survey completion date for the classifications and rates under that identifier.

?SU? wage rates typically remain in effect until a new survey is conducted. However, the Wage and Hour Division (WHD) has the discretion to update such rates under 29 CFR 1.6(c)(1).

State Adopted Rate Identifiers

The ""SA"" identifier indicates that the classifications and prevailing wage rates set by a state (or local) government were adopted under 29 C.F.R 1.3(g)-(h). Example: SAME2023-007 01/03/2024. SA reflects that the rates are state adopted. ME refers to the State of Maine. 2023 is the year during which the state completed the survey on which the listed classifications and rates are based. The next number, 007 in the example, is an internal number used in producing the wage determination. The date, 01/03/2024 in the example, reflects the date on which the classifications and rates under the ?SA? identifier took effect under state law in the state from which the rates were adopted.

WAGE DETERMINATION APPEALS PROCESS

1) Has there been an initial decision in the matter? This can be:

- a) a survey underlying a wage determination
- b) an existing published wage determination
- c) an initial WHD letter setting forth a position on a wage determination matter
- d) an initial conformance (additional classification and rate) determination

On survey related matters, initial contact, including requests for summaries of surveys, should be directed to the WHD Branch of Wage Surveys. Requests can be submitted via email to davisbaconinfo@dol.gov or by mail to:

Branch of Wage Surveys
Wage and Hour Division
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

Regarding any other wage determination matter such as conformance decisions, requests for initial decisions should be directed to the WHD Branch of Construction Wage Determinations. Requests can be submitted via email to BCWD-Office@dol.gov or by mail to:

Branch of Construction Wage Determinations
Wage and Hour Division
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

2) If an initial decision has been issued, then any interested party (those affected by the action) that disagrees with the decision can request review and reconsideration from the Wage and Hour Administrator (See 29 CFR Part 1.8 and 29 CFR Part 7). Requests for review and reconsideration can be submitted via email to dba.reconsideration@dol.gov or by mail to:

Wage and Hour Administrator
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

The request should be accompanied by a full statement of the interested party's position and any information (wage payment data, project description, area practice material, etc.) that the requestor considers relevant to the issue.

3) If the decision of the Administrator is not favorable, an interested party may appeal directly to the Administrative Review Board (formerly the Wage Appeals Board). Write to:

Administrative Review Board
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210.

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END OF GENERAL DECISION

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MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SUPPLEMENT TO SPECIAL PROVISION NO. 907-618-4

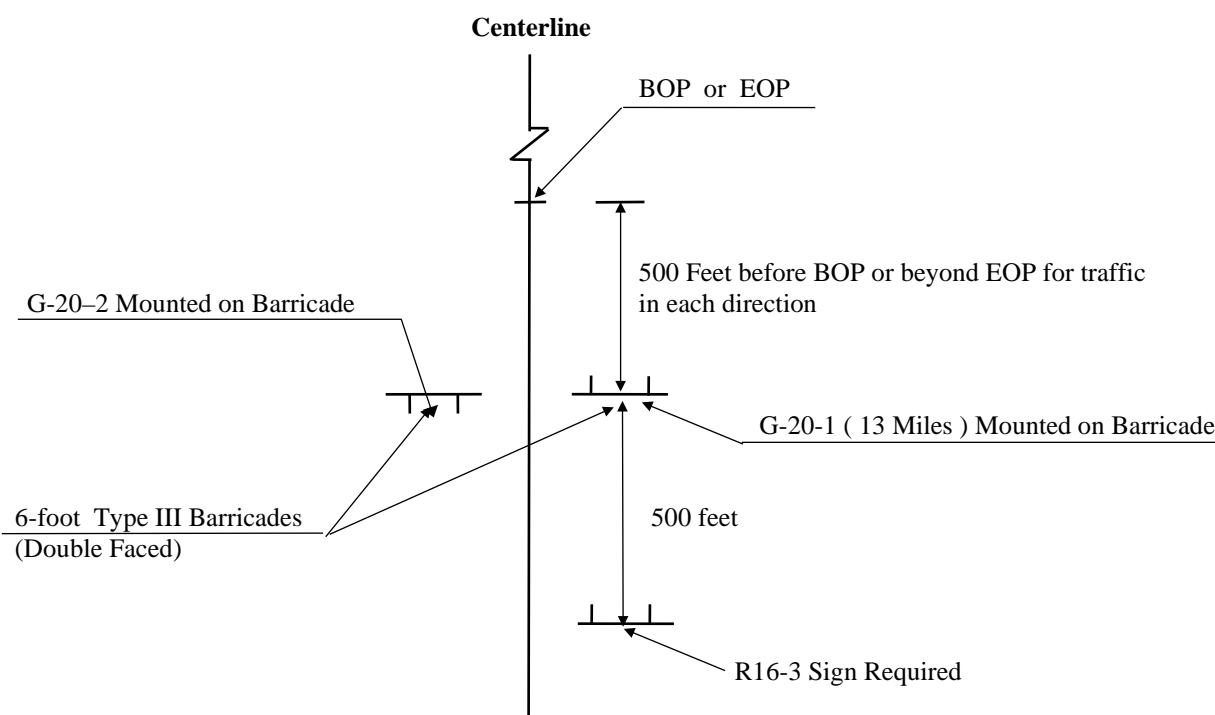
DATE: 01/20/2026

PROJECT: STP-0050-01(043) –109755/301000 – Calhoun County

Delete the paragraph in Subsection 907-618.01.2 on page 1, and substitute the following.

For compliance with the traffic control plan, the Contractor will be required to install and maintain traffic control devices at various locations throughout the project. Payment for these devices will be included in the price bid for pay item no. 907-618-A: Maintenance of Traffic per lump sum.

Additional traffic control devices will be required as follows.



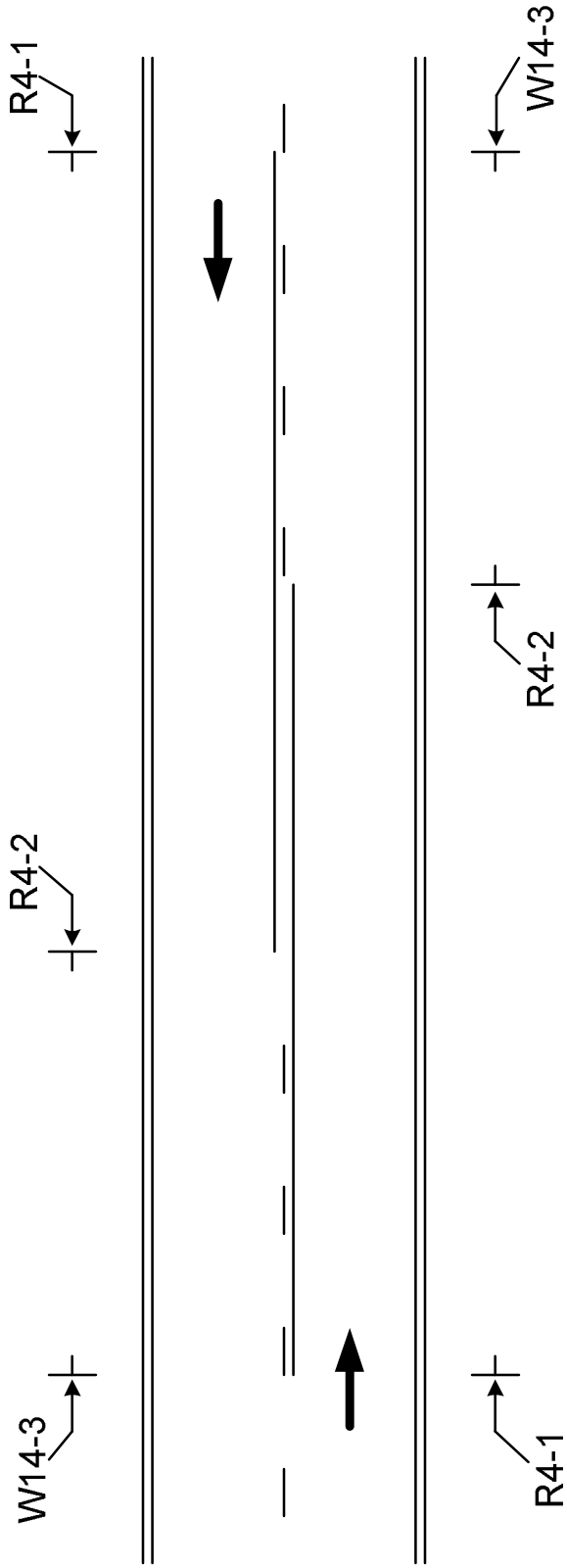
ADDITIONAL TRAFFIC CONTROL SIGNS REQUIRED:

- 34 - W20-1 "AHEAD" signs required. One (1) W20-1 "AHEAD" sign is required at each local road or street entering the project.
- 145 - R4-1 "DO NOT PASS" signs required.
- 17 - R4-2 "PASS WITH CARE" signs required.
- 18 - W14-3 "NO PASSING ZONE" signs required.
- 14 - R16-3 "SPEEDING FINES DOUBLED" signs required.

R4-1 "DO NOT PASS", R4-2 "PASS WITH CARE", and W14-3 "NO PASSING ZONE" signs are required in accordance with Subsection 618.03.3, this drawing, and as specified in the Manual on Uniform Traffic Control Devices.

R16-3 signs shall be spaced in accordance with sheet titled "Location of R16-3 Signs".

All construction signs and barricades shown on this page shall be included in the bid price for pay item 907-618-A, Maintenance of Traffic. Fluorescent orange sheeting shall be used on all construction and traffic control signs except for R16-3, R4-1 and R4-2 signs which shall be black legend and border on white background.



The W14-3, No Passing Zone sign, shall be placed on the left side of the road at the beginning of each no passing zone.

The R4-1, Do Not Pass signs, shall be placed on the right side of the road at the beginning of the no passing zone. Additional R4-1 signs shall be placed right and left in increments of 750 to 1000 feet throughout the length of the no passing zone.

The R4-2, Pass With Care sign, shall be placed on the right side of the road at the end of the no passing zone.

The R4-1, R4-2 and W14-3 signs are to be used when standard pavement markings are not in place. The signs may also be used to emphasize pavement markings.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-618-4

CODE: (SP)

DATE: 02/01/2018

SUBJECT: Additional Signing Requirements

Section 618, Maintenance of Traffic and Traffic Control Plan, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-618.01.2--Traffic Control Plan. At the end of Subsection 618.01.2 on page 441, add the following:

For compliance with the traffic control plan, the Contractor will be required to install and maintain traffic control devices at various locations throughout the project. Payment for these devices will be included in the price bid for pay item no. 618-A, Maintenance of Traffic per lump sum.