

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO. <u>1</u>	DATED <u>1/26/2026</u>	ADDENDUM NO. _____	DATED _____
ADDENDUM NO. <u>2</u>	DATED <u>1/28/2026</u>	ADDENDUM NO. _____	DATED _____
ADDENDUM NO. _____	DATED _____	ADDENDUM NO. _____	DATED _____

Number	Description
1	Revised Table of Contents; Removed NTB No. 5750; Revised NTB No. 7538; Added NTB No. 7539; Revised Wage Rates; Amendment EBSx Download Required.
2	Revised Table of Contents; Revised Advertisement; Removed NTB No. 5750; Revised NTB No. 7538; Added NTB No. 7539; Revised Wage Rates; Amendment EBSx Download Required.

TOTAL ADDENDA: 2
(Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

Contractor

BY _____
Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

President Address

Secretary Address

Treasurer Address

The following is my (our) itemized proposal.
NHPP-0015-01(152)/ 109797301000
Lincoln County(ies)
Revised 01/26/2016

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION
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PROJECT: NHPP-0015-01(152)/109797301 - Lincoln

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(REVISIONS TO THE ABOVE WILL BE INDICATED ON THE SECOND SHEET
OF SECTION 905 AS ADDENDA)

01/22/2026 11:37 AM

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 901 - ADVERTISEMENT

Electronic bids will be received by the Mississippi Transportation Commission at 10:00 o'clock A.M., Tuesday, February 3, 2026, from the Bid Express Service and shortly thereafter publicly read in the Construction Division For:

Mill & Overlay approximately 8 miles of US 84 from Concrete Section at Monticello Street NE to the Lawrence County Line, known as Federal Aid Project No. NHPP-0015-01(152) / 109797301 in Lincoln County.

The attention of bidders is directed to the Contract Provisions governing selection and employment of labor. Minimum wage rates have been predetermined by the Secretary of Labor and are subject to Public Law 87-581, Work Hours Act of 1962, as set forth in the Contract Provisions.

The specifications are on file in the offices of the Mississippi Department of Transportation.

Contractors may request permission to bid online at <http://shop.mdot.ms.gov> at no cost. Upon approval, Contractors shall be eligible to submit a bid using Bid Express at <http://bidx.com>. Specimen proposals may be viewed and downloaded online at no cost at <http://mdot.ms.gov> or purchased online at <http://shop.mdot.ms.gov> at a cost of Ten Dollars (\$10.00) per proposal plus a small convenience fee. Cash or checks will not be accepted as payment.

Bid bond, signed or countersigned by a Mississippi Agent or Qualified Nonresident Agent, with Power of Attorney attached, a Cashier's check or Certified Check for five (5%) percent of bid, payable to STATE OF MISSISSIPPI, must accompany each proposal.

The attention of bidders is directed to the provisions of Subsection 102.07 pertaining to irregular proposals and rejection of bids.

BRAD WHITE
EXECUTIVE DIRECTOR

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 – NOTICE TO BIDDERS NO. 7538

CODE: (SP)

DATE: 01/28/2026

SUBJECT: Scope of Work

PROJECT: NHPP-0015-01(152) / 109797301 – Lincoln County

The contract documents do not include an official set of construction plans, but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, “Standard Drawings.”

The work to be accomplished using the pay items and corresponding specifications set forth in the contract is to mill & overlay the following section of US 84 in Lincoln County from the Concrete Section at Monticello Street NE to the Lawrence County Line.

Route	Length	Width	Milling	Surface Lift	Thickness
	(Mile)	(Feet)	(Inches)		(Inches)
US 84	7.7	28 & Var.	1.5	9.5-mm, HT	1.5

See sheet TS-1 for more details.

Work on the Project shall consist of the following:

1. The Contractor shall erect and maintain construction signing, provide all signs, set up nighttime lane closures (if needed) and traffic handling devices in accordance with the Traffic Control Plan. The cost for this work shall be included in the price bid for pay item 907-618-A: Maintenance of Traffic. All traffic control devices on this project shall comply with the latest version of the MUTCD. Fluorescent orange sheeting shall be used on all construction and traffic control signs except for those designated in the standards to be black legend and border on white background.
2. Prior to the overlay, the existing shoulders shall be clipped to provide proper drainage, and surplus material shall be spread along the edge of the shoulders, fore slopes, or other adjacent areas as directed by the Project Engineer, and will be an absorbed item.

Excess buildup of shoulder material at the following locations shall be removed from the project, cost for this work is to be included in the bid price for pay item 203-G002 Excess Excavation.

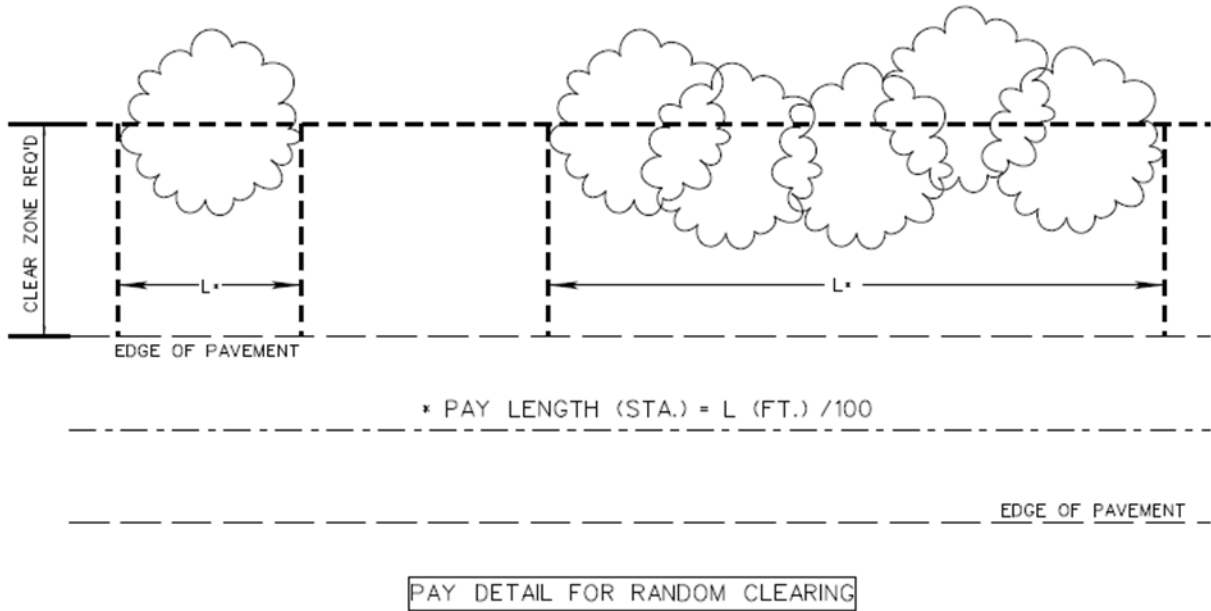
Station	Location	Length, LF	Volume, CY
167+16 – 172+23	EB Right Shoulder	507	117
203+68 – 207+04	EB Right Shoulder	336	78
211+20 – 214+27	EB Right Shoulder	307	71
		Total	266

3. All trees on MDOT right-of-way greater than 4 inches in diameter shall be cleared to a minimum distance of 70 feet from the edge of pavement or to the right-of-way line, whichever distance is less. Limbs shall be trimmed to a height of approximately 70’ above ground at the edge of the trimming limits. Trimmed material shall be mulched in place and remain onsite. Mulched material shall be spread such that no more than 4” in depth of material is left in any location. The contractor shall be mindful of overhead utilities in the area.

This work shall be paid for as 201-D001: Random Clearing, per station. Each side of the roadway (Left and Right) shall be measured separately. The median will be measured separately from the Left and Right sides of the roadway.

Estimated Locations

Station	Location LT-LT LN	Station	Location RT-RT LN	Station	Location Median
17+74 – 45+31	27.57	3+92 – 5+57	1.65	137+59 – 139+52	1.93
47+99 – 55+64	7.65	45+68 – 51+84	6.16	300+21 – 300+82	.61
74+20 – 77+68	3.48	88+12 – 91+43	3.31	304+41 – 304+66	.25
162+46 – 162+99	.53	102+68 – 104+89	2.21	388+92 – 389+65	.73
175+75 – 178+52	2.77	111+45 – 112+72	1.27		
197+48 – 198+55	1.07	117+56 – 118+32	.76		
217+25 – 217+85	.60	124+59 – 132+07	7.48		
250+32 – 251+77	1.45	136+20 – 153+78	17.58		
262+91 – 264+71	1.80	161+59 – 162+37	.78		
294+56 – 296+17	1.61	166+31 – 167+74	1.43		
306+43 – 310+56	4.13	177+38 – 179+55	2.17		
313+00 – 314+13	1.13	252+08 – 258+93	6.85		
316+60 – 324+84	8.24	285+70 – 302+12	16.42		
331+63 – 335+53	3.90	338+91 – 346+41	7.50		
340+72 – 341+36	.64	348+05 – 351+02	2.97		
349+32 – 351+46	2.14	375+95 – 388+23	12.28		
356+22 – 356+77	.55	390+32 – 391+53	1.21		
375+25 – 381+83	7.58	396+27 – 399+3	3.11		
383+13 – 384+55	1.42	401+12 – 403+18	2.06		
385+17 – 387+34	2.17				
389+14 – 399+884	10.70				
Total	91.13		97.20		3.52



4. The Contractor shall remove the existing asphalt pavement at various locations throughout the project as shown in the table below and/or as designated by the Engineer.

An additional 6” and variable of excess material shall be removed and backfilled with crushed stone over a layer of Geotextile Fabric, Type V, Non-woven. Where crushed stone is placed, the Contractor shall allow for drainage through the shoulder as directed by the Engineer. See TS-1 Detail “B” for details.

Station	Location	Length, LF	Width, FT	Area, SY	Volume, CY
129+71 – 133+18	RT of RT LN	347	14	540	90
129+71 – 133+18	LT of RT LN	347	14	540	90
			Total	1,080	180

This work shall be performed as a single operation.

A saw cutting pay item has been included in the quantities to be utilized in the removal of the asphalt pavement. If the item is not utilized, no payment will be made under this item.

5. The Contractor shall fine mill at the following locations: See TS-1.

<u>Location</u>	<u>Milling, SY</u>	<u>Remarks</u>
BOP - EOP	125,922	WB Lanes
BOP - EOP	125,351	EB Lanes
Local Roads	15,652	As directed – 13 Locations

Guardrail Pads	3,177	As directed – 6 Locations
Crossovers	23,523	As directed – 21 Locations
Paved Shoulders	1,164	0+00 – 1+65 WB Lanes 7+70 – 10+70 WB Lanes
Total	294,789	

6. The Contractor shall place the top lift of asphalt on the roadway left and right of the centerline from BOP to EOP as shown on TS-1. The finished cross-slope is to be 2% in tangent sections and match the existing super elevation rate in horizontal curves.

Asphalt surface shall be placed on all local roads, driveway aprons, crossovers, and guardrail pads. Asphalt for this work shall be paid as 9.5-mm, ST, Asphalt Pavement.

Driveway aprons shall be paved 10’ wide or as directed by the Engineer. All local roads shall be paved to the normal right-of-way line or as directed by the Engineer.

Asphalt quantities were estimated as follows:

Mainline	20,826 Tons
Local Roads	1,291 Tons
Driveway Pads	215 Tons
Crossovers	1,941 Tons
Guardrail Pads	262 Tons

Note: The Contractor shall be responsible for traffic control while MDOT personnel conduct density testing on the asphalt. The cost shall be included in the price bid for pay item: 907-618-A: Maintenance of Traffic.

7. The existing guardrail pads from the face of the rail extending outward shall be removed. The contractor shall then place crushed stone at these locations (6” depth). Excess excavation may be required. Existing guardrail items shall then be removed, and the following shall be installed, as per the standard drawings. A sawcut pay item has been included in the quantities for the removal of asphalt pavement. If the item is not utilized, no payment will be made under this item. See below for quantity estimations.

Bridge	Terminal End, EA	Bridge End Type I, EA	Delineator Yellow, EA	Delineator White, EA	W Beam, LF	Removal, LF
71.8A	2	2	7	7	175+175	471
71.8B	2	2	7	7	175+175	470
74.9A	2	2	7	7	175+175	470
74.9B	2	2	7	7	175+175	468

76.6A	2	2	7	7	175+175	470
76.6B	2	2	7	7	175+175	470
Totals	12	12	42	42	2,100	2,819

8. The Contractor shall place granular material on the shoulders to raise the existing shoulders to the new grade. The shoulder shall be bladed, shaped, and compacted to a minimum slope of 4%. Granular material will not be allowed to be placed directly on the top lift of asphalt, but must be placed directly on the gravel shoulder by means of a road widener machine approved by the Engineer. Light blading or mowing of the shoulders will be required prior to placement of the granular material. Please refer to Subsection 618.03.3 of the Standard Specifications and sheet SDTCP-16 for requirements related to pavement edge drop-offs.
9. The Contractor shall place rumble strips in the locations indicated in the typical sections on sheet TS-1 and RS-2 (Detail “A”).
10. The Contractor shall place all permanent pavement markings, including stripe and raised pavement markers, throughout the project as required by the Standard Drawings or as directed by the Engineer. Existing pavement markings located on bridges shall be removed as required prior to placing new markings. See sheet D7-1 for updated marker spacings for crossovers.
11. The Contractor shall be responsible for replacing crossover delineators throughout the project as required by the Standard Drawings or as directed by the Engineer.
12. The Contractor shall sweep clean and remove all debris on all bridges within the project limits as part of final cleanup.

An asphalt taper shall be placed at the temporary joints caused by the milling or overlay in order to provide for the safe movement of traffic. The taper shall be three feet (3’) in length per one inch of depth and will be an absorbed item.

Temporary striping shall be required after scrub seal, leveling and overlaying operations: Temporary striping shall be placed in the same location and layout as permanent stripe. All centerline, lane lines, edge lines, and no-passing stripes that have been removed during the day’s operations shall be replaced with temporary stripe before work is discontinued for the day or as soon thereafter as weather conditions permit, except that:

- Replacement of no-passing stripes may be delayed for a period not to exceed three (3) days for a two or three lane road.
- Temporary edge lines on projects requiring shoulders constructed of granular material may be delayed for a period not to exceed three (3) days.

All asphalt and concrete curbs along local roads from BOP to EOP shall be painted (two applications) with traffic paint and traffic beads as shown on sheet DCIS-1; color to correspond to adjacent pavement stripe; such costs shall be included in other items bid.

It shall be the responsibility of the Contractor to protect the roadway and all existing structures, such as bridges, culverts, signs, and curbs, from damage occurring as a result of the Contractor's operations. Damages to existing structures caused by the Contractor's operations shall be repaired or replaced at no cost to the Department.

Incidental work such as removing vegetation, shaping and compaction of shoulders, removing excess asphalt material, project clean-up, and other incidental work necessary to complete the project will not be measured for separate payment. Such costs shall be included in the price of other items bid.

It is the Contractor's responsibility to insure the drainage of surface water from milled areas. Where applicable, existing shoulder material is to remain in place to be incorporated into final sloping of the shoulders. Temporary wedges (paper joints) of full lane width asphalt shall be placed by the Contractor immediately after the fine milling process to allow the safe transition of traffic. These wedges shall be maintained in a satisfactory condition by the Contractor until the permanent asphalt is placed, cost to be absorbed.

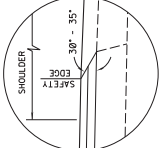


DESIGNED BY: DISTRICT 7
 CHECKED BY:
 DATE:

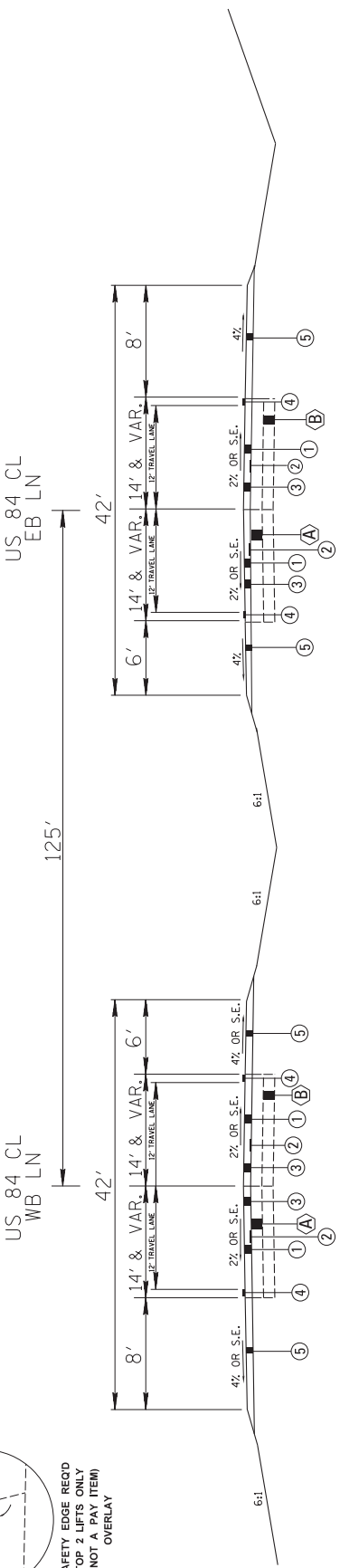
FMS CON: 109797/301000
 PROJECT NO.: NHP-0015-01(752)
 COUNTY: LINCOLN

Notice to Bidders No. 7538 - Cont

W.C. T. 201
 SHEET NO. 1



SAFETY EDGE REQ'D
 TOP 2 LIFTS ONLY
 (NOT A PAY ITEM)
 OVERLAY



TYPICAL SECTION
 NOT TO SCALE

B.O.P. STA. 0+00 - E.O.P. STA. 405+84

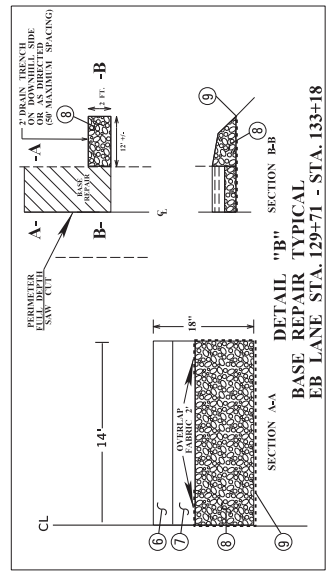
B.O.P. STA. 0+00 - E.O.P. STA. 405+84

PROPOSED

- ① 1.50" OF 9.5MM HT ASPHALT REQ'D
- ② TACK COAT REQ'D
- ③ 1.50" & VAR. FINE MILLING REQ'D
- ④ RUMBLE STRIPE REQ'D (SEE SHEET RS-2, DETAIL "A")
- ⑤ 1.50" & VAR. OF CL. 6, GR. D GRANULAR MATERIAL REQ'D
- ⑥ 3" (2 LIFTS @ 1.5") ASPHALT, HT (12.5MM MIXTURE) WHERE REQ'D
- ⑦ 9" (3 LIFTS @ 3") ASPHALT, HT (19MM MIXTURE) WHERE REQ'D
- ⑧ 6" CRUSHED STONE BASE WHERE REQ'D
- ⑨ TYPE V NONWOVEN GEOTEXTILE FABRIC WHERE REQ'D

EXISTING

- Ⓐ BITUMINOUS PAVEMENT IN PLACE
- Ⓑ GRANULAR SUBGRADE IN PLACE



DETAIL "B"
 BASE REPAIR TYPICAL
 EB LANE STA. 129+71 - STA. 133+18



DESIGNED BY: DISTRICT 7
 CHECKED BY:
 DATE:

FMS CON: 109797/301000
 PROJECT NO.: NHP-0015-152
 COUNTY: LINCOLN

Notice to Bidders No. 7588 - Construction of Traffic Control Signs

WY NO. TC 20-1
 SHEET NO. 4

SIGNS REQUIRED (CONT'D)			
SIGN NO.	SIZE	UNIT AREA (SQ. FT.)	REMARKS
W8-7	48" X 48"	16.00	LOOSE GRAVEL
W8-9	48" X 48"	16.00	LOW SHOULDER
W8-11	36" X 36"	9.00	UNEVEN LANES
W8-12	48" X 48"	16.00	NO CENTER STRIPE
W10-1	36" DIA.	7.07	
W10-1	24" X 24"	4.00	
W13-1	36" X 48" X 48"	5.56	NO PASSING ZONE
W14-3	48" X 48" X 64"	3.00	XXX FEET
W16-2	48" X 48"	16.00	BRIDGE MAY ICE IN COLD WEATHER
W19-2	48" X 48"	16.00	ADVANCE ROAD WORK
W20-1	36" X 36"	9.00	ADVANCE DETOUR
W20-2	48" X 48"	16.00	
W20-3	48" X 48"	16.00	ROAD CLOSED
W20-4	48" X 48"	16.00	ADVANCE
W20-4b	48" X 48"	16.00	ADVANCE
W20-5L	48" X 48"	16.00	ADVANCE
W20-5R	48" X 48"	16.00	ADVANCE
W20-7a	48" X 48"	16.00	RT. LN. CLOSED
W21-1	36" X 36"	9.00	WORKERS
W21-1a	36" X 36"	9.00	WORKERS
W21-2	36" X 36"	9.00	FRESH PAVEMENT
W21-3	48" X 48"	16.00	ADVANCE ROAD WORK
W21-5	48" X 48"	9.00	ROAD CLOSURE
W21-6	36" X 36"	16.00	SURVEY CREW
W24-1L	48" X 48"	16.00	
W24-1R	48" X 48"	16.00	
W24-1aL	48" X 48"	16.00	
W24-1aR	48" X 48"	16.00	
W24-1bL	48" X 48"	16.00	
W24-1bR	48" X 48"	16.00	
VP-1L	12" X 36"	3.00	
VP-1R	12" X 36"	3.00	
OM-3L	12" X 36"	3.00	
OM-3R	12" X 36"	3.00	
TOTAL SIGN AREA LESS THAN 10 SQ. FT.			32 SQ. FT.
TOTAL SIGN AREA GREATER THAN 10 SQ. FT.			440 SQ. FT.

SIGNS REQUIRED (CONT'D)			
SIGN NO.	SIZE	UNIT AREA (SQ. FT.)	REMARKS
W1-1L	48" X 48"	16.00	
W1-1R	48" X 48"	16.00	
W1-2L	48" X 48"	16.00	
W1-2R	48" X 48"	16.00	
W1-3L	48" X 48"	16.00	
W1-3R	48" X 48"	16.00	
W1-4a	48" X 48"	16.00	
W1-4b	48" X 48"	16.00	
W1-5L	48" X 48"	16.00	
W1-5R	48" X 48"	16.00	
W1-6L	48" X 24"	9.00	
W1-6R	60" X 30"	12.50	
W1-7L	48" X 24"	9.00	
W1-7R	60" X 30"	12.50	
W1-8L	18" X 24"	3.00	
W1-8R	36" X 48"	12.00	
W1-9L	48" X 48"	16.00	
W1-9R	48" X 48"	16.00	
W2-6	36" X 36"	9.00	
W3-1a	48" X 48"	16.00	
W3-2a	48" X 48"	16.00	
W3-3	48" X 48"	16.00	
W3-5	48" X 48"	16.00	
W4-1L	48" X 48"	16.00	
W4-1R	48" X 48"	16.00	
W4-2L	48" X 48"	16.00	
W4-2R	48" X 48"	16.00	
W4-3L	48" X 48"	16.00	
W4-3R	48" X 48"	16.00	
W4-6	48" X 48"	16.00	
W5-1a	48" X 48"	16.00	
W6-1	48" X 48"	16.00	
W6-2	48" X 48"	16.00	
W6-3	48" X 48"	16.00	
W8-1	48" X 48"	16.00	
W8-4	48" X 48"	16.00	
W8-6	48" X 48"	16.00	
TOTAL SIGN AREA LESS THAN 10 SQ. FT.			
TOTAL SIGN AREA GREATER THAN 10 SQ. FT.			440 SQ. FT.

SIGNS REQUIRED (CONT'D)			
SIGN NO.	SIZE	UNIT AREA (SQ. FT.)	REMARKS
R1-1	36" X 36"	7.46	STOP
R1-1	48" X 48"	13.25	
R1-2	36" X 36"	3.90	YIELD
R1-2	48" X 48"	6.93	
R1-2	60" X 60"	10.83	3-WAY 4-WAY, ETC.
R1-3	18" X 9"	1.13	
R1-3	24" X 12"	2.00	SPEED LIMIT
R2-1	24" X 30"	5.00	
R2-1	36" X 48"	12.00	
R2-1	48" X 60"	20.00	
R3-1	36" X 36"	9.00	
R3-1	48" X 48"	16.00	
R3-2	36" X 36"	9.00	
R3-2	48" X 48"	16.00	
R3-4	36" X 36"	9.00	
R3-4	48" X 48"	16.00	
R3-5L	30" X 36"	7.50	ONLY
R3-5R	30" X 36"	7.50	
R3-6L	30" X 36"	7.50	ONLY
R3-6R	30" X 36"	7.50	
R3-7L	30" X 30"	6.25	LEFT LANE MUST TURN LEFT
R3-7R	30" X 30"	6.25	
R4-1	24" X 30"	5.00	DO NOT PASS
R4-1	48" X 60"	20.00	
R4-2	24" X 30"	5.00	PASS WITH CARE
R4-2	48" X 60"	20.00	
R4-7	48" X 60"	20.00	
R4-8	48" X 60"	20.00	
R5-1	48" X 48"	16.00	DO NOT ENTER
R5-1a	42" X 30"	8.75	
R6-1L	36" X 12"	3.00	WRONG WAY
R6-1R	36" X 12"	3.00	
R6-2L	24" X 30"	5.00	ONE WAY
R6-2R	24" X 30"	5.00	
R11-2	48" X 30"	10.00	ROAD CLOSED
R11-3a	60" X 30"	12.50	
R11-3b	60" X 30"	12.50	ROAD CLOSED
R11-4	60" X 30"	12.50	
R12-1	36" X 48"	12.00	ROAD CLOSED
R16-3	36" X 48"	12.00	
R16-3	48" X 60"	20.00	ROAD CLOSED
R16-3	48" X 60"	20.00	

SIGNS REQUIRED			
SIGN NO.	SIZE	UNIT AREA (SQ. FT.)	REMARKS
G20-1	60" X 24"	10.00	ROAD WORK NEXT XX MILES
G20-2	48" X 24"	8.00	
G20-4	36" X 18"	4.50	PILOT CAR FOLLOW RHE
M1-1	24" X 24"	4.00	
M1-1	30" X 24"	5.00	1 OR 2 DIGIT
M1-4	24" X 24"	4.00	
M1-4	30" X 24"	5.00	3 DIGIT
M1-5	30" X 24"	5.00	
M3-1	24" X 12"	2.00	1 OR 2 DIGIT
M3-1	30" X 15"	3.13	
M3-2	24" X 12"	2.00	3 DIGIT
M3-2	30" X 15"	3.13	
M3-3	24" X 12"	2.00	1 OR 2 DIGIT
M3-3	30" X 15"	3.13	
M3-4	24" X 12"	2.00	3 DIGIT
M3-4	30" X 15"	3.13	
M4-8	24" X 12"	2.00	NORTH 1 OR 2 DIGIT
M4-8	30" X 15"	3.13	
M4-9	48" X 36"	12.00	DIGIT RATE MARKER
M4-9	48" X 36"	12.00	
M4-9B	48" X 36"	12.00	DIGIT RATE MARKER
M4-9B	48" X 36"	12.00	
M4-10L	48" X 18"	6.00	DIGIT RATE MARKER
M4-10R	48" X 18"	6.00	
M4-5	24" X 12"	2.00	DETOUR
M5-1R	21" X 15"	2.19	
M5-2L	21" X 15"	2.19	DETOUR
M5-2R	21" X 15"	2.19	
M6-1R	21" X 15"	2.19	DETOUR
M6-2R	21" X 15"	2.19	
M6-3	21" X 15"	2.19	DETOUR
M6-3	21" X 15"	2.19	

NOTES

- STANDARD
 - SPECIAL (USE WHERE WARRANTED)
 - INTERSTATE ROUTE MARKER
 - UNITED STATES ROUTE MARKER
 - STATE ROUTE MARKER
 - COLORS OF CARDINAL DIRECTION MARKERS AND DIRECTIONAL ROUTE MARKERS.
 - BLACK STRIPES ON YELLOW BACKGROUND
 - INTERSTATE USE ONLY
 - TOP OF SIGN - BLACK LETTERING ON ORANGE BACKGROUND.
 - BOTTOM OF SIGN - BLACK LETTERING ON WHITE BACKGROUND.
- THE BACKGROUND OF ALL WARNING SIGNS ("W" SERIES) EXCEPT W10-1 SHALL BE ORANGE. THE W10-1 BACKGROUND SHALL BE YELLOW IN ALL CASES.



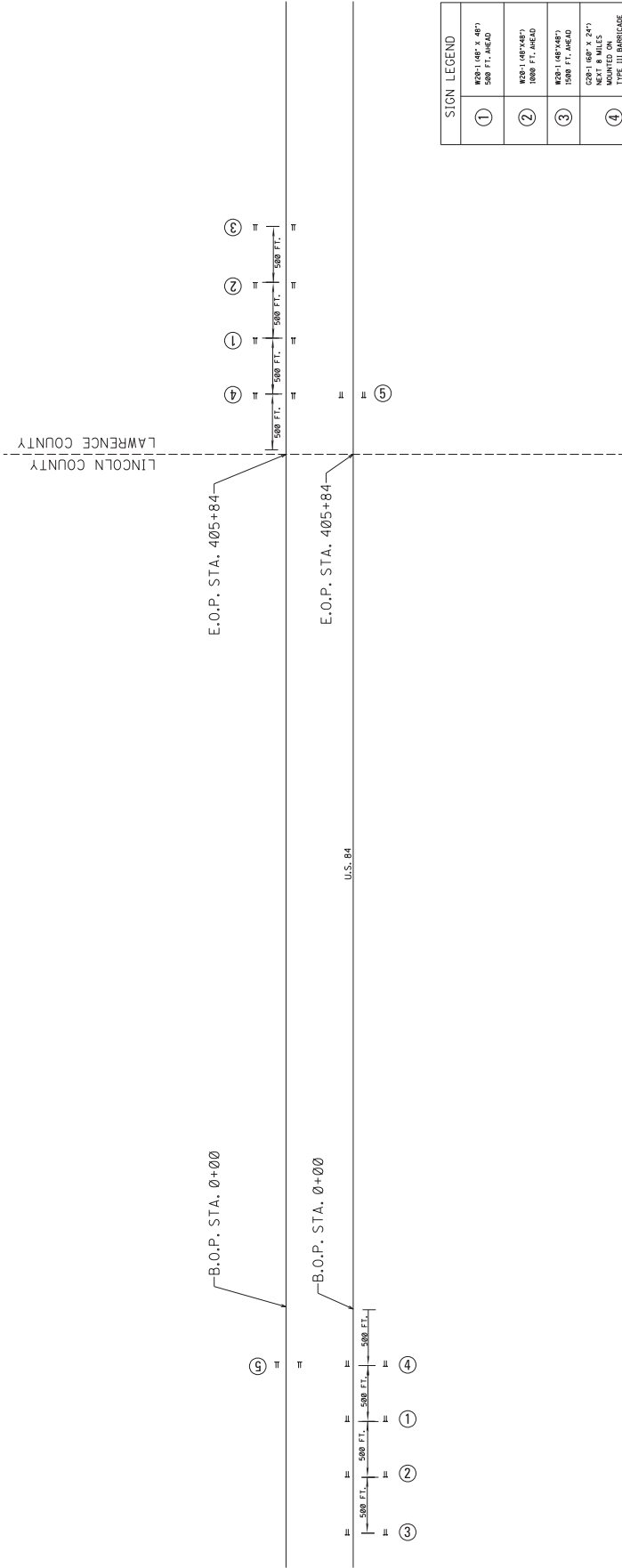
DESIGNED BY: DISTRICT 7
 DETAILED BY:
 CHECKED BY:
 DATE:

FMS CON: 109797/301000
 PROJECT NO.: NHP-0015-01(12)
 COUNTY: LINCOLN

Notice to Bidders No. 7538, Cont.

WYNO. DISTRICT
 SHEET NO. 9

NOTE: W20-1 (48"x48") SHALL BE REQ'D ON ALL LOCAL ROADS.



SIGN LEGEND	
①	W20-1 48" X 48" 500 FT. HEAD
②	W20-1 48" X 48" 1000 FT. HEAD
③	W20-1 48" X 48" 1500 FT. HEAD
④	W20-1 48" X 24" MOUNTED ON TYPE III BARRICADE (6 FT. HIGH, SINGLE FACED)
⑤	W20-1 48" X 24" MOUNTED ON TYPE III BARRICADE (SINGLE FACED)

*** NOT TO SCALE ***

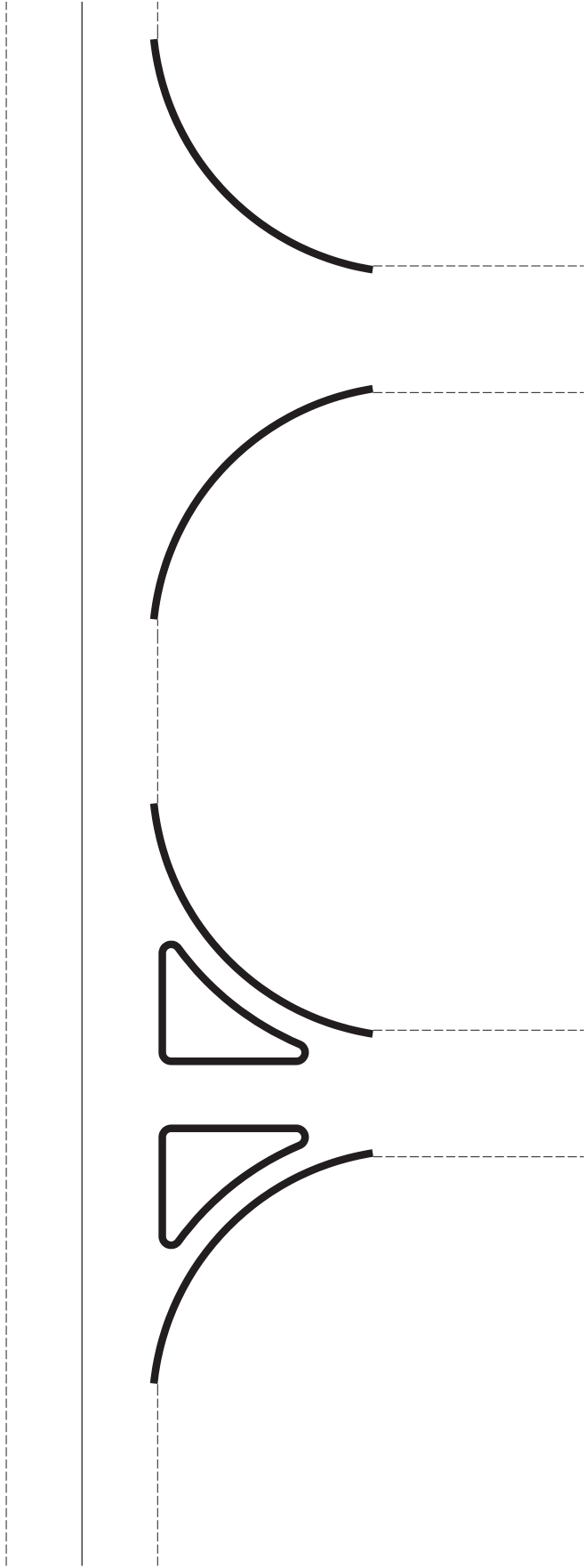
ALL TRAFFIC CONTROL ITEMS ON THIS SHEET SHALL BE INCLUDED IN BID FOR PAY ITEM 907-618-A001. MAINTENANCE OF TRAFFIC. FLUORESCENT ORANGE SHEETING SHALL BE USED ON ALL CONSTRUCTION AND TRAFFIC CONTROL SIGNS EXCEPT FOR THOSE DESIGNATED ON THE PLANS TO BE BLACK LEGEND AND BORDER ON WHITE BACKGROUND.



DESIGNED BY: DISTRICT 7	FMS CON: 109797/301000
DETAILED BY:	PROJECT NO.: NHP-0015-1(12)
CHECKED BY:	COUNTY: LINCOLN
DATE:	

DETAIL FOR CURB
 10 - Notice to Bidders No. 6958 - Cont.

WKS. NO. DCRS-1	SHEET NO. 6
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* * ALL ASPHALT AND CONCRETE CURBS ALONG RAMPS, LOCAL ROADS, ETC. FROM B.O.P. TO E.O.P. SHALL BE PAINTED (TWO APPLICATIONS) WITH TRAFFIC PAINT AND TRAFFIC BEADS; COLOR TO CORRESPOND TO ADJACENT PAVEMENT STRIPE; COST TO BE ABSORBED IN OTHER PAY ITEMS.



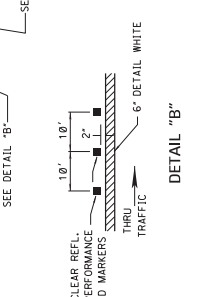
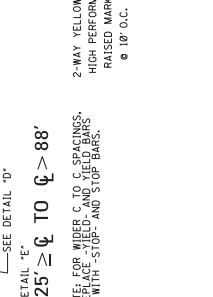
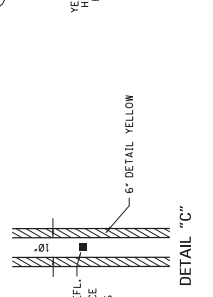
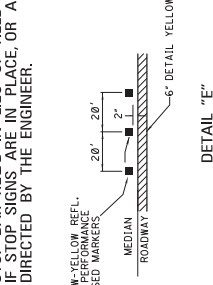
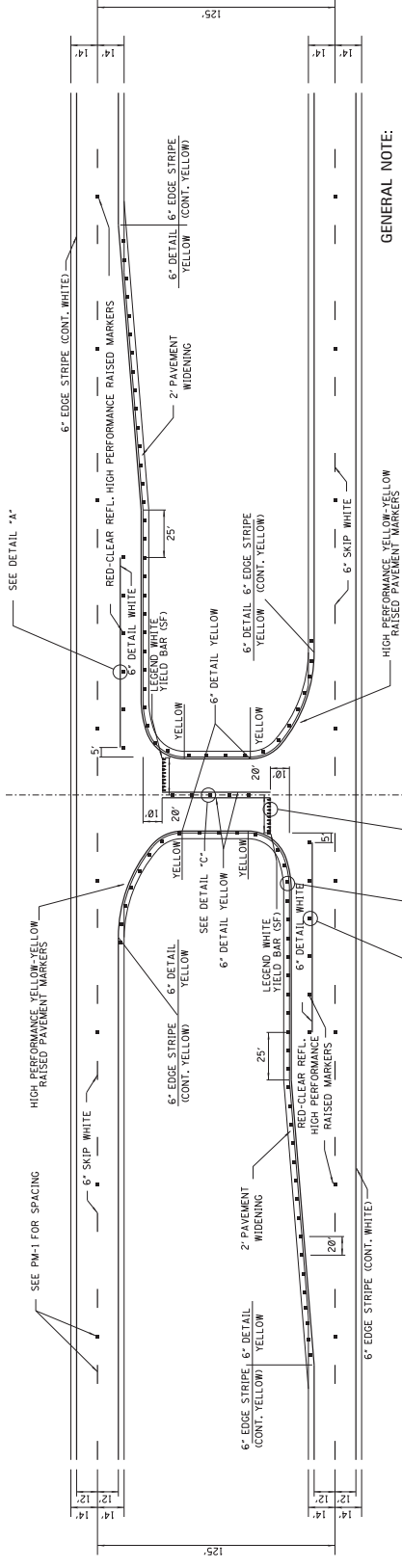
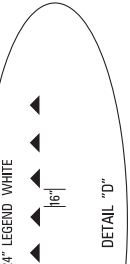
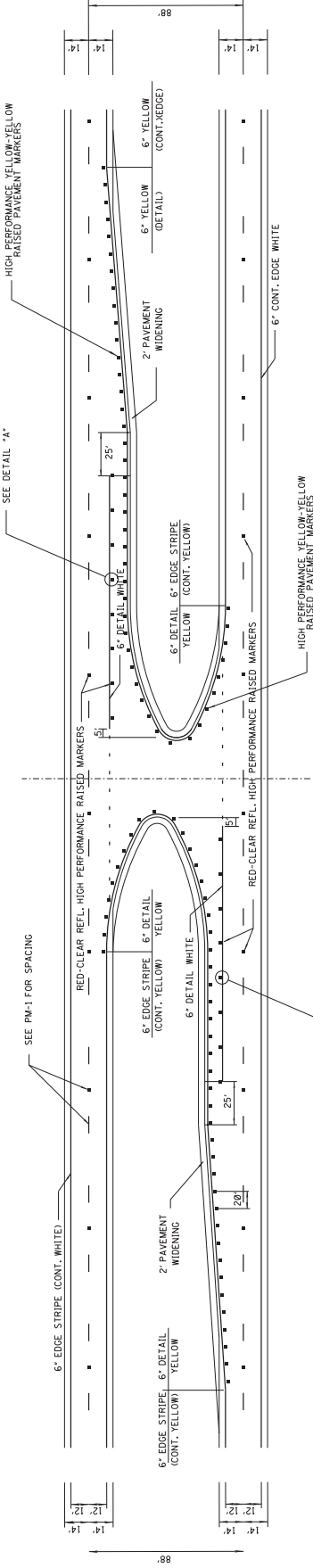
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

DESIGNED BY: DISTRICT 7
CHECKED BY:
DATE:

FMS CON: 109797/301000
PROJECT NO.: NHP-0015-2(12)
COUNTY: LINCOLN

DETAIL FOR MEDIAN CROSSOVERS
CROSSOVER RPM:
DATE: 12/11/2013

WY: 10
D.M.
SHEET NO. 8



GENERAL NOTE:
1 STOP BAR REQ'D IN LIEU OF YIELD BAR IF STOP SIGNS ARE IN PLACE, OR AS DIRECTED BY THE ENGINEER.

125' ≥ C TO C < 88'
NOTE: FOR WIDER C TO C SPACINGS, REPLACE YELLOW AND YIELD BARS WITH 8\"/>

NOTE: FOR WIDER C TO C SPACINGS, REFER TO OTHER SHEETS IN PLANS



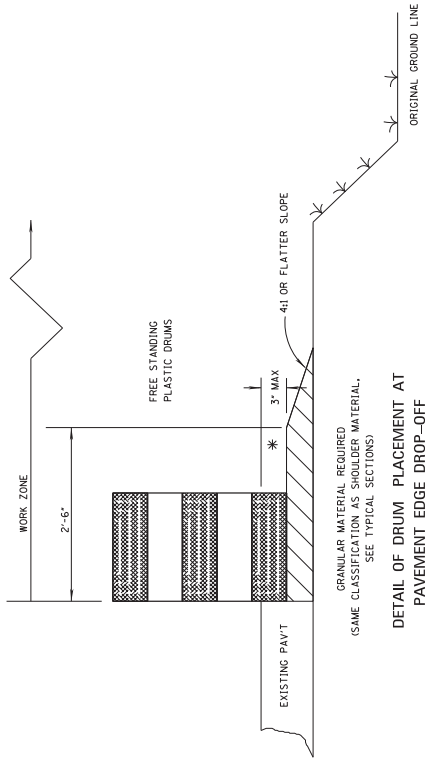
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

DESIGNED BY: DISTRICT 7
 DETAILED BY:
 CHECKED BY:
 DATE:

FMS CON: 109797/301000
 PROJECT NO.: NHP-0015-2(12)
 COUNTY: LINCOLN

TRAFFIC CONTROL DETAILS
 FROM PL-888 (REV. 11/15/12)
 Notice to Bidders

WY 2016
 SDT 09-16
 SHEET NO. 9



NOTES:

- * A. PAVEMENT EDGE DROP-OFF
 1. IF LESS THAN TWO AND ONE QUARTER (2.25) INCHES-NO PROTECTION REQUIRED; PLACE A SHOULDER WORK SIGN (W21-5) 500 FEET IN ADVANCE OF WORK ZONE SHOULDER AND A LOW SHOULDER SIGN (W8-9) AT THE BEGINNING AND THROUGHOUT THE WORK ZONE @ (1 MILE 10.C.).
 2. TWO AND ONE QUARTER TO THREE INCHES-PLACE DRUMS, VERTICAL PANELS OR BARRICADES EVERY 100 FEET ON TANGENT SECTIONS FOR SPEEDS OF 35 MPH OR GREATER. PLACE DRUMS, VERTICAL PANELS OR BARRICADES EVERY 100 FEET ON CURVED SECTIONS. DURING NIGHT HOURS FOR TANGENT SECTIONS WITH SPEEDS LESS THAN 50 MILES PER HOUR AND FOR CURVED DEVICES SHOULD BE PLACED EVERY 50 FEET. SPACING FOR TAPERS SHOULD BE IN ACCORDANCE WITH THE MULT.C.D. 1 / 3 L, WHERE L IS THE TAPER LENGTH IN FEET.
 3. GREATER THAN THREE (3) INCHES-POSITIVE SEPARATION OR WEDGE WITH 4:1 OR FLATTER SLOPE NEEDED. IF THERE IS EIGHT (8) FEET OR MORE DISTANCE BETWEEN THE EDGE OF TRAVEL LANE AND DROP-OFF, THEN DRUMS, PANELS OR BARRICADES MAY BE USED.
 4. FOR TEMPORARY CONDITIONS, DROP-OFFS GREATER THAN THREE (3) INCHES MAY BE PROTECTED WITH DRUMS, VERTICAL PANELS OR BARRICADES FOR SHORT DISTANCES DURING DAYLIGHT HOURS WHILE WORK IS BEING DONE IN THE DROP-OFF AREA.
 5. LESSER TREATMENTS THAN THOSE DESCRIBED ABOVE MAY BE CONSIDERED FOR LOW-VOLUME LOCAL STREETS.

B. DRUM SPACING

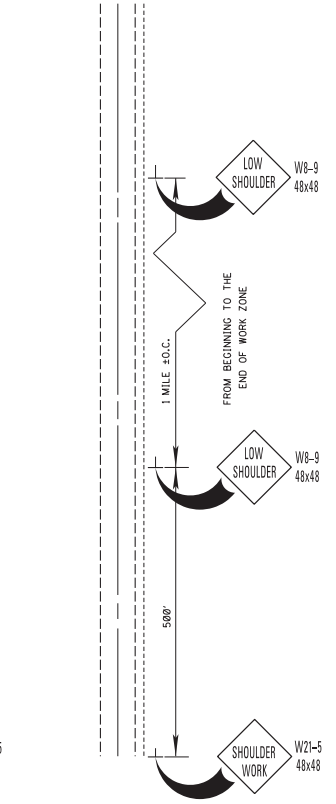
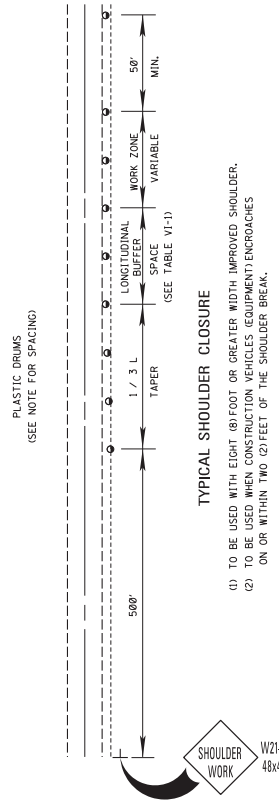
1. TANGENTS = 2 X S
2. TAPERS = L / 3
- WHERE L = S X W
- L = TAPER LENGTH IN FEET
- S = SPEED IN MPH (POSTED OR 85 PERCENTILE)
- W = WIDTH OF OFFSET IN FEET

C. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.

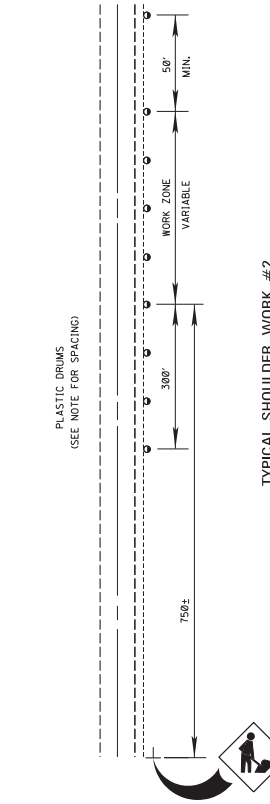
TABLE VI-1. GUIDELINES FOR LENGTH OF LONGITUDINAL BUFFER SPACE

* SPEED (MPH)	LENGTH (FEET)
25	55
30	65
35	85
40	120
45	170
50	220
55	280
60	340
65	415
85	485

* * * PASTED SPEED OFF-BREAK 85 PERCENTILE SPEEDS PRIOR TO WORK STARTING OR THE ANTICIPATED OPERATING SPEED IN MPH.



TYPICAL SHOULDER WORK #2



NOTE: WORK OUTSIDE TWO (2) FOOT AND WITHIN TEN (10) FEET OF THE SHOULDER BREAK MAY BE PROTECTED BY PLACING DRUMS ALONG THE SHOULDER EDGE, 300 FEET PRIOR TO AND 50 FEET BEYOND THE WORK AREA, OR SEE NOTE A-3 THIS SHEET.

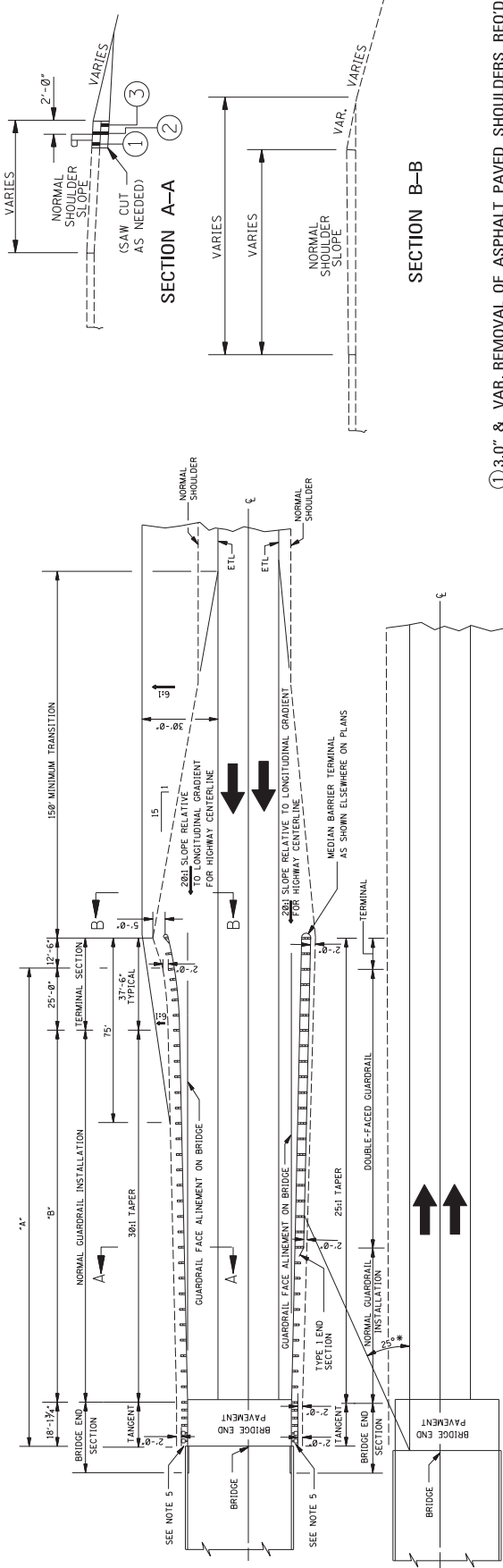


DESIGNED BY: DISTRICT 7
 CHECKED BY:
 DATE:

FMS CON: 109797/301000
 PROJECT NO.: NHP-0015-10(12)
 COUNTY: LINCOLN

GUARRAIL TYPICAL INSTALLATION
 DIVIDED HIGHWAYS

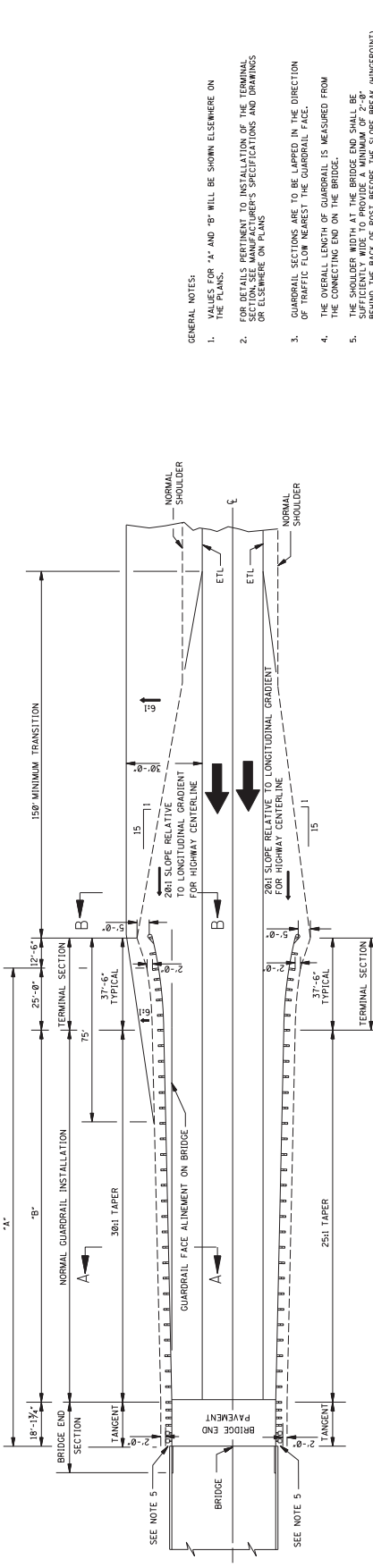
WK NO. GR-4100D
 SHEET NO. 10



SECTION A-A
 (SAW CUT AS NEEDED)
 NORMAL SHOULDER SLOPE
 VARIES
 2'-0\"/>

SECTION B-B
 NORMAL SHOULDER SLOPE
 VAR.
 VARIES

- ① 3.0' & VAR. REMOVAL OF ASPHALT PAVED SHOULDERS REQ'D.
- ② 6.0' CRUSHED LIMESTONE REQ'D.
- ③ 3.0' & VAR. EXCESS EXCAVATION REQ'D.



SECTION A-A
 (SAW CUT AS NEEDED)
 NORMAL SHOULDER SLOPE
 VARIES
 2'-0\"/>

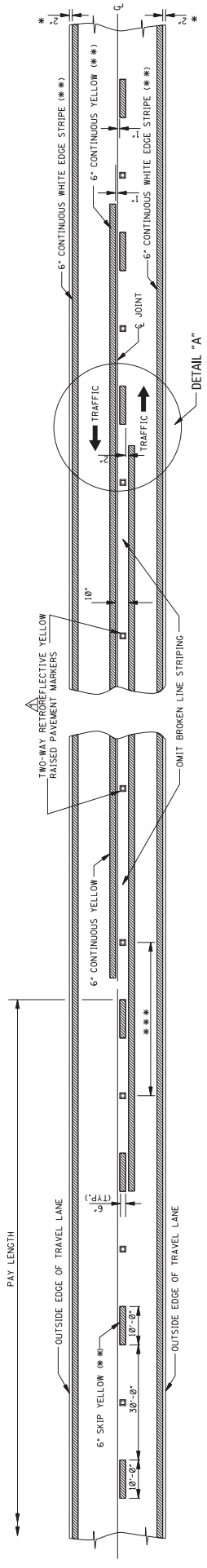
SECTION B-B
 NORMAL SHOULDER SLOPE
 VAR.
 VARIES

- ① 3.0' & VAR. REMOVAL OF ASPHALT PAVED SHOULDERS REQ'D.
- ② 6.0' CRUSHED LIMESTONE REQ'D.
- ③ 3.0' & VAR. EXCESS EXCAVATION REQ'D.

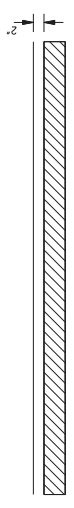
- GENERAL NOTES:
1. VALUES FOR "A" AND "B" WILL BE SHOWN ELSEWHERE ON THE PLANS.
 2. FOR DETAILS PERTAINING TO INSTALLATION OF THE TERMINAL SECTION, SEE MANUFACTURER'S SPECIFICATIONS AND DRAWINGS OR ELSEWHERE ON PLANS.
 3. GUARRAIL SECTIONS ARE TO BE LAPPED IN THE DIRECTION OF TRAFFIC FLOW NEAREST THE GUARRAIL FACE.
 4. THE OVERALL LENGTH OF GUARRAIL IS MEASURED FROM THE CONNECTING END ON THE BRIDGE.
 5. THE SHOULDER WIDTH AT THE BRIDGE END SHALL BE SUFFICIENTLY WIDE TO PROVIDE A MINIMUM OF 2'-0" BEHIND THE BACK OF POST BEFORE THE SLOPE BREAK (HINGEPOINT). TYPE, DETAILS AND LIMITS OF GUARRAIL BRIDGE END SECTION WILL BE SHOWN ELSEWHERE ON THE PLANS.

DIVIDED HIGHWAY WITH BARRIER INSIDE CLEAR ZONE OF OPPOSING TRAFFIC

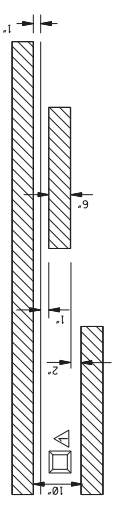
* NOTE: THE 25° LINE IS USED TO DETERMINE THE LIMITS OF MEDIAN BARRIER AS SHOWN.



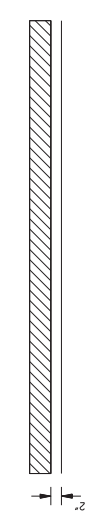
TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)



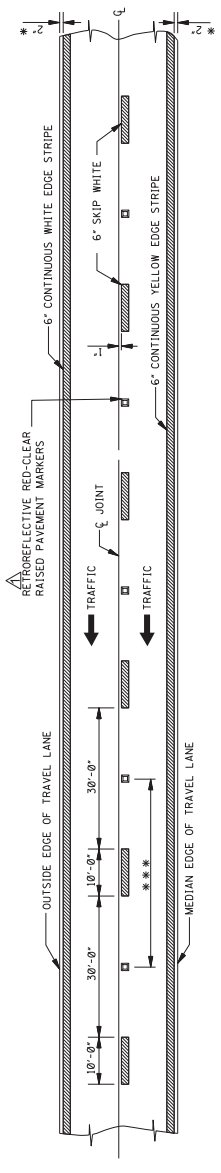
NOTE: THE CRITERIA FOR NO-PASSING ZONES CAN BE FOUND IN THE MOOT ROADWAY DESIGN MANUAL, SECTION 11-1.01.



DETAIL "A"



DETAIL "A"



4-LANE WITH ONE-WAY TRAFFIC

GENERAL NOTES:

- * 1. UNLESS SHOWN ELSEWHERE ON THE PLANS, FOR STRIPING ON RUMBLE STRIP SECTIONS REFER TO WK. SHEETS RS-1, RS-2, AND RS-3.
 - ** 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR PLASTIC AS INDICATED IN PAY ITEMS).
 - *** 3. SPACING OF RETROREFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:
- | TANGENT SECTIONS | RURAL AREA (40'-0" TO 80'-0") | URBAN AREA (80'-0" TO 400'-0") |
|--------------------|-------------------------------|--------------------------------|
| HORIZONTAL CURVES | 40'-0" | 40'-0" |
| INTERCHANGE LIMITS | 40'-0" | 1-40'-0" |
- 1. NOTE: ON THE MAIN FACILITY, RETROREFLECTIVE RED-CLEAR PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED THROUGHOUT THE INTERCHANGE AREA BEGINNING 100'-0" FROM ANY DIRECTION OF TRAFFIC THROUGH THE TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
 - 2. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RETROREFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MOOT "APPROVED PRODUCTS LIST."

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**PAVEMENT MARKING
DETAILS FOR
2-LANE AND 4-LANE
DIVIDED ROADWAYS**

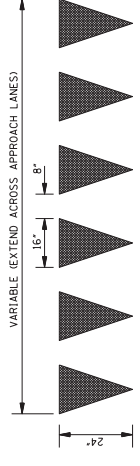
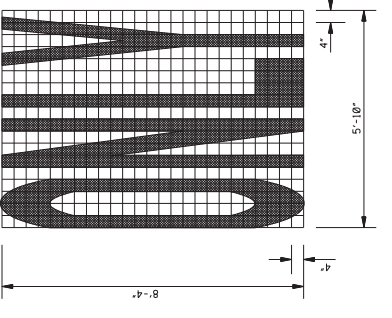
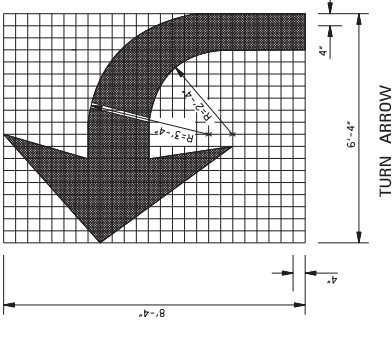
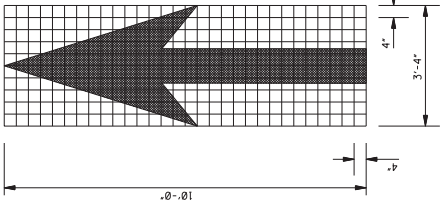
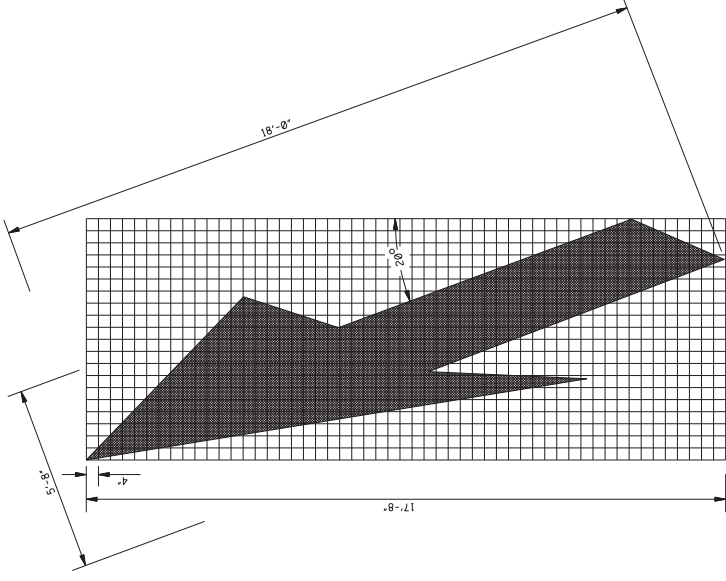
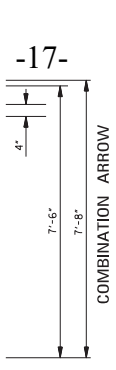
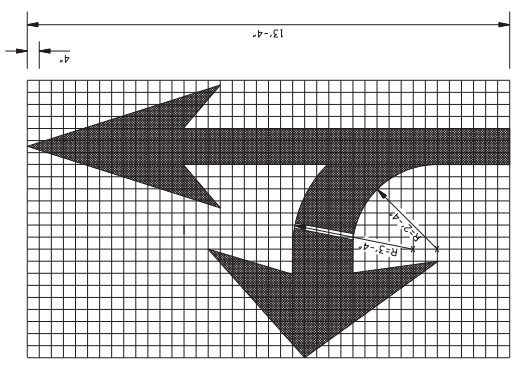
ISSUE DATE: AUGUST 01, 2017

PROJECT NUMBER: 6051

REVISION NUMBER: CM-1

DATE	REVISION

STATE PROJECT NO.
MISS.

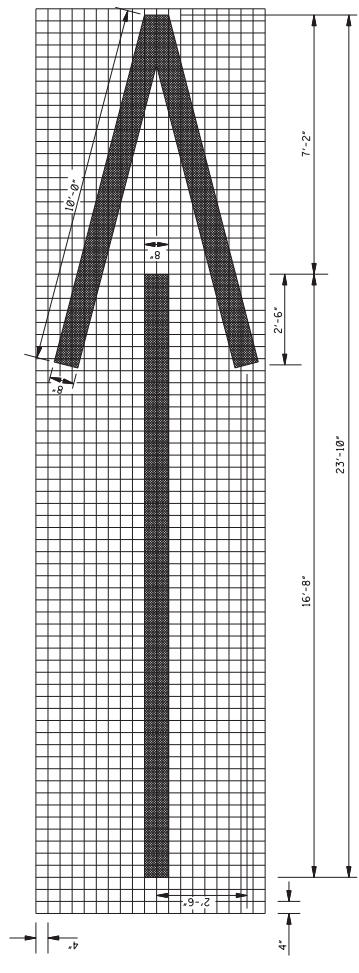


THRU ARROW

TURN ARROW

VARIABLE (EXTEND ACROSS APPROACH LANES)

YIELD LINE



1-WAY ARROW

-17-

COMBINATION ARROW

LANE-REDUCTION ARROW

Notice to Bidders No. 538

- GENERAL NOTES:
1. TWO HORIZONTAL GAPS (CAUSED BY TEMPLATE CONNECTORS) OF 1/2" OR LESS AND EXTENDING THE FULL WIDTH ARE PERMITTED IN EACH LETTER.
 2. PAVEMENT MARKING LETTERS, NUMERALS, SYMBOLS, AND ARROWS SHALL BE INSTALLED IN ACCORDANCE WITH THE DESIGN DETAILS IN THE PAVEMENT MARKINGS CHAPTER OF THE "STANDARD HIGHWAY SIGNS" PUBLICATION AS REFERENCED IN SECTION 1A.85 OF THE LATEST EDITION OF THE MUTCD.
 3. WORD, SYMBOL, AND ARROW MARKINGS SHALL BE WHITE UNLESS OTHERWISE NOTED IN THE PLANS.
 4. PAY QUANTITIES FOR PAVEMENT MARKING LEGENDS ARE AS FOLLOWS:

PAY QUANTITIES	
LEGEND/SYMBOL	AREA (ft ²)
ONLY 'I'	22.0
TURN ARROW	16.4
THRU ARROW	12.3
COMB. ARROW	27.5
1-WAY ARROW	24.3
LANE REDUCTION ARROW	40.0

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

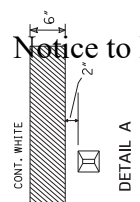
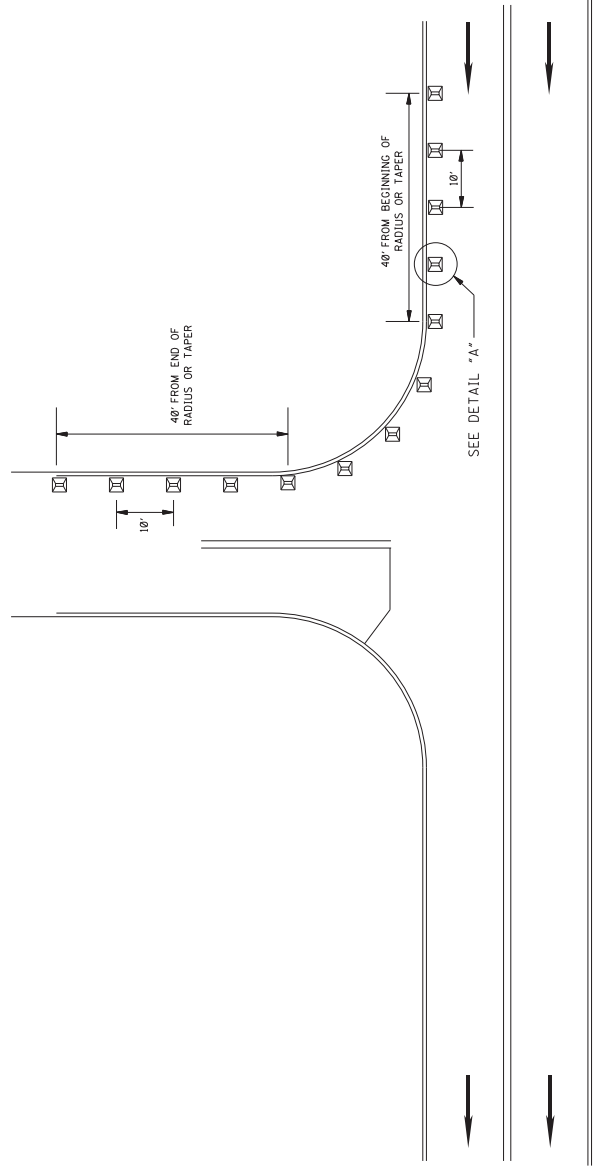
**PAVEMENT MARKING
LEGEND DETAILS**

ISSUE DATE: AUGUST 01, 2017

6056

DATE	BY	REVISION

△ TYPICAL PLACEMENT OF RETROREFLECTIVE RAISED PAVEMENT MARKERS ON SIDE ROAD RADIUS 4-LANE, TWO WAY TRAFFIC



△ GENERAL NOTES:

- △ 1. RETROREFLECTIVE FACES OF MARKERS SHALL BE VISIBLE TO THE TRAVELING MOTORIST ON STATE DESIGNATED HIGHWAYS.
- △ 2. RETROREFLECTIVE FACES OF MARKERS SHALL BE HIGH PERFORMANCE TWO-WAY CLEAR.

→ DIRECTION OF TRAFFIC

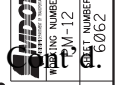
DATE	BY	REVISION

ISSUE DATE: AUGUST 01, 2017

PROJECT NUMBER: 6062

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

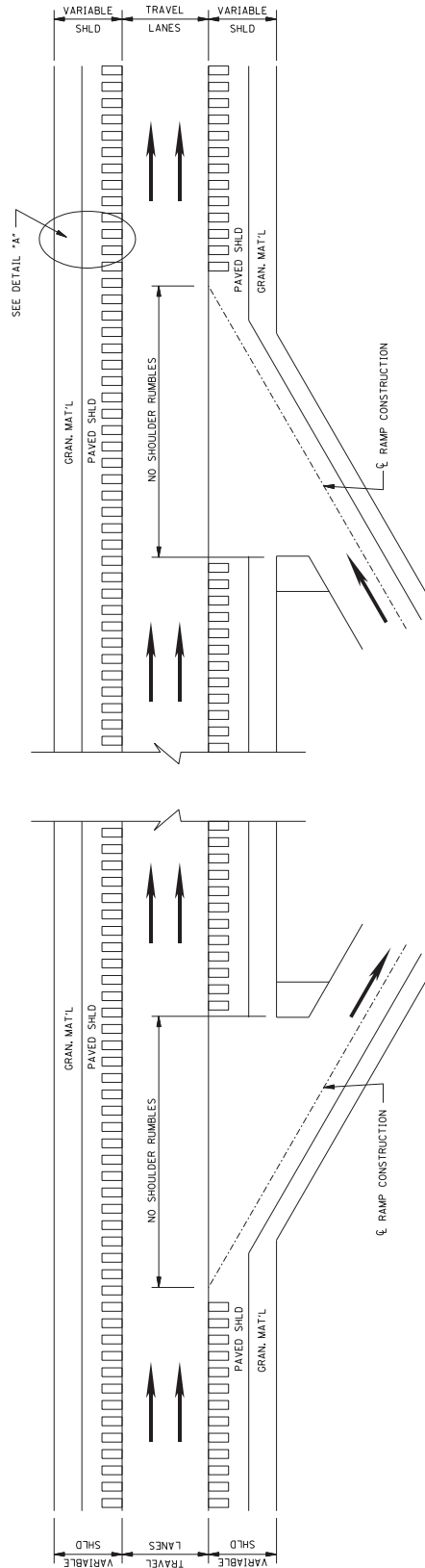
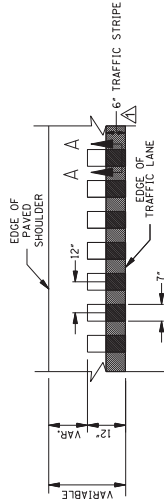
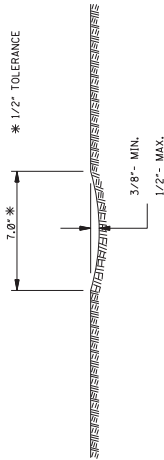
2-WAY RETROREFLECTIVE RAISED PAVEMENT MARKERS AT INTERSECTING ROADS (4-LANE)



PLAN NUMBER: 12

GENERAL NOTES

- GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO ALL PAVED SHOULDERS ON THIS PROJECT.
- GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO ALL PAVED SHOULDERS ON THIS PROJECT. ROADWAYS OR OTHER INTERSECTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER.
- COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS.
- GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO:
 - MAINLINE
 - INTERSECTING ROADWAY IF OVERLAID OR RECONSTRUCTED BEYOND NORMAL MAINLINE R.O.W.
 - ANY ROADWAY WITH EXISTING RUMBLE STRIPES PRIOR TO CONSTRUCTION.



DATE	BY	REVISION
		REVISED STRIPE DETAIL.

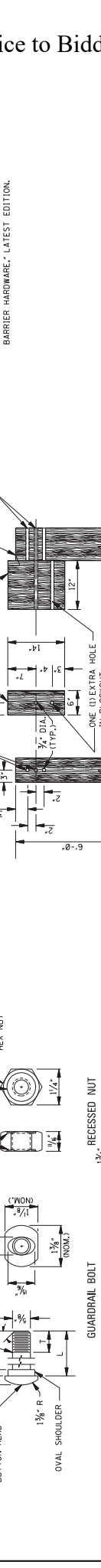
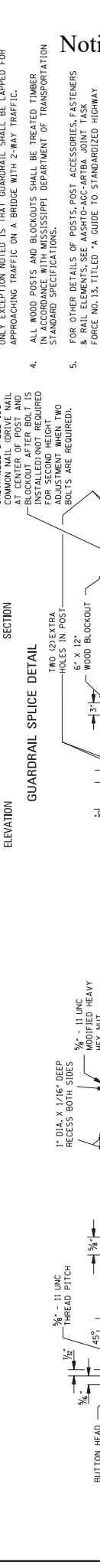
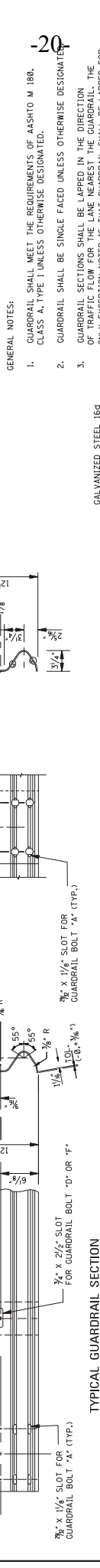
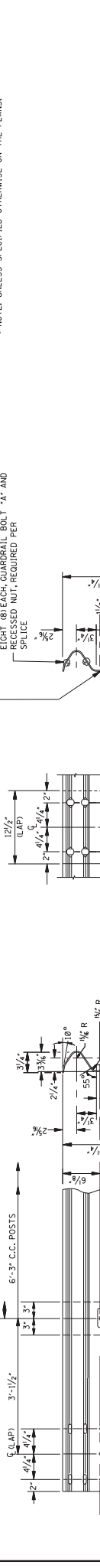
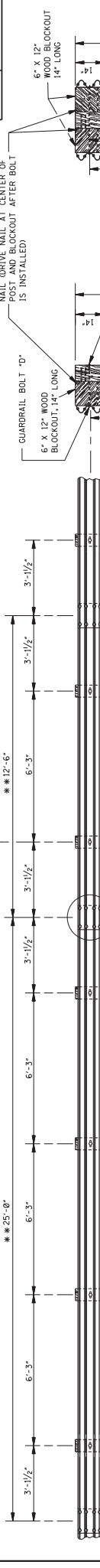
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**RUMBLE STRIPES
4-LANE HIGHWAYS
(ASPHALT LANES,
2-FT OR WIDER,
ASPHALT SHOULDERS)**

PLAN NUMBER
PS-2

ISSUE DATE: AUGUST 01, 2017

ISSUE NUMBER
6065



GUARDRAIL BOLTS	
BOLT	T
"A"	1"
"D"	2 1/2"
"E"	2 1/2"
"F"	3 1/2"

- NOTES:
1. ALL GUARDRAIL BOLTS ARE 3/4" - 11 UNC THREAD PITCH.
 2. IF ANY BOLT EXTENDS MORE THAN 1/4" FROM THE NUT, THE BOLT SHOULD BE TRIMMED BACK.
 3. GUARDRAIL BOLT "E" IS USED FOR SINGLE-FACED BARRIER AND BOLT "F" IS USED FOR DOUBLE-FACED BARRIER AND BOLT "D" IS USED FOR 16" WOOD POST AND 6" x 12" WOOD BLOCKOUT.

- DETAILS OF ADJUSTABLE HEIGHT BLOCKOUT ASSEMBLY
- NOTES:
1. ON INITIAL INSTALLATION, THE BLOCKOUT SHALL BE ATTACHED TO THE BOTTOM HOLE IN THE POST. OTHER HOLES IN THE POST AND BLOCKOUT ARE FOR FUTURE 2" HEIGHT ADJUSTMENT WHEN THE ROADWAY IS RESURFACED.
 2. FOR THE SECOND HEIGHT ADJUSTMENT, ONE (1) HEX NUT AND BOLT "D", 22" LONG FOR SINGLE-FACED BARRIER OR BOLT "E", 33" LONG FOR DOUBLE-FACED BARRIER, WITH TWO (2) PLAIN ROUND WASHERS "F", ONE (1) PLAIN ROUND WASHER "F", ONE (1) UNDER NUT, ARE REQUIRED PER POST IN ADDITION TO THE STANDARD GUARDRAIL BOLT AND RECESSED NUT.
 3. WOOD DETAILS ARE REQUIRED ON ALL WOOD POSTS AND BLOCKOUTS.
 4. WOOD POSTS ARE FABRICATED FROM 6" x 8" TREATED TIMBER AND BLOCKOUTS ARE FABRICATED FROM 6" x 12" TREATED TIMBER UNLESS SPECIFIED OTHERWISE ON THE PLANS.
 5. ALL HOLES IN BOTH POSTS AND BLOCKOUTS ARE 3/4" IN DIAMETER.

STATE PROJECT NO. MISS. 6" x 12" WOOD BLOCKOUT 14" LONG 6" x 12" WOOD BLOCKOUT 14" LONG RECESSED NUT WITH PLAIN ROUND WASHER "F" GUARDRAIL BOLT "F" 6" x 8" WOOD POST, 6'-0" LONG DOUBLE-FACED BARRIER SECTION A-A * NOTE: UNLESS SPECIFIED OTHERWISE ON THE PLANS. GENERAL NOTES: 1. GUARDRAIL SHALL MEET THE REQUIREMENTS OF AASHTO M 188, CLASS A, TYPE 1 UNLESS OTHERWISE DESIGNATED. 2. GUARDRAIL SHALL BE SINGLE FACED UNLESS OTHERWISE DESIGNATED. 3. GUARDRAIL SECTIONS SHALL BE LAPPED IN THE DIRECTION OF TRAFFIC FLOW FOR THE LANE NEAREST THE GUARDRAIL. THE ONLY EXCEPTION NOTED IS THAT GUARDRAIL SHALL BE LAPPED FOR APPROACHING TRAFFIC ON A BRIDGE WITH 2-WAY TRAFFIC. 4. ALL WOOD POSTS AND BLOCKOUTS SHALL BE TREATED TIMBER IN ACCORDANCE WITH MISSISSIPPI DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS. 5. FOR OTHER DETAILS OF POSTS, POST ACCESSORIES, FASTENERS & RAIL ELEMENTS, SEE AASHTO-ACC-ARTBA JOINT TASK FORCE NO. 13, TITLED "A GUIDE TO STANDARDIZED HIGHWAY BARRIER HARDWARE", LATEST EDITION.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

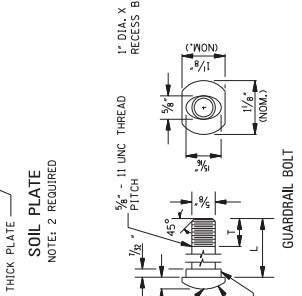
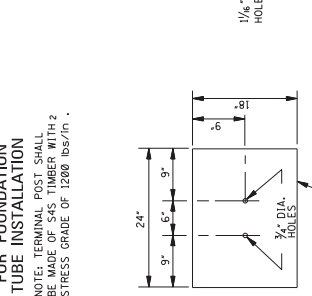
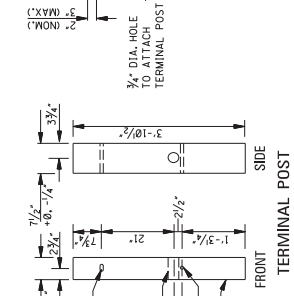
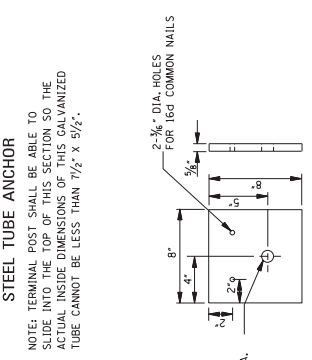
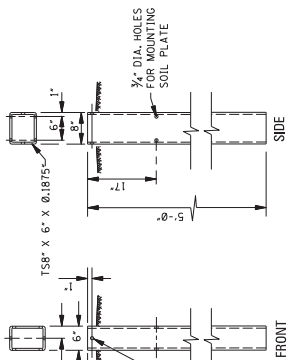
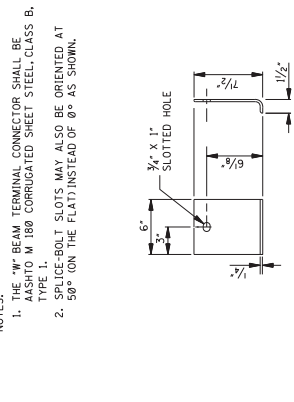
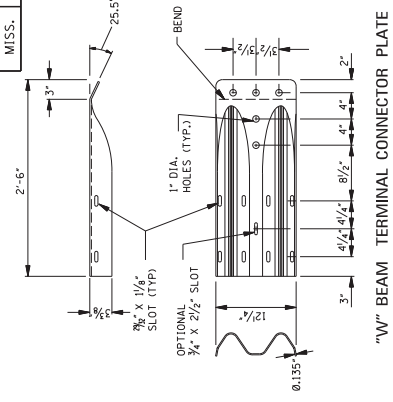
GUARDRAIL:
"W" BEAM
(WOOD POSTS)

ISSUE DATE: AUGUST 01, 2017

DATE	REVISION

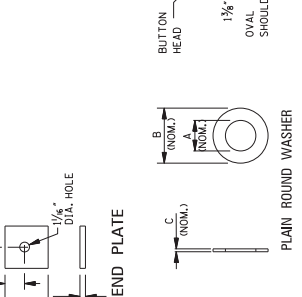
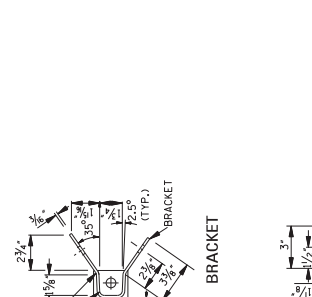
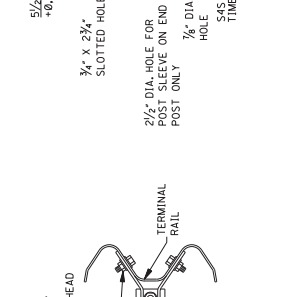


PLANNING NUMBER: CR-1
SHEET NUMBER: 6201



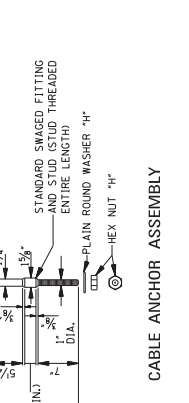
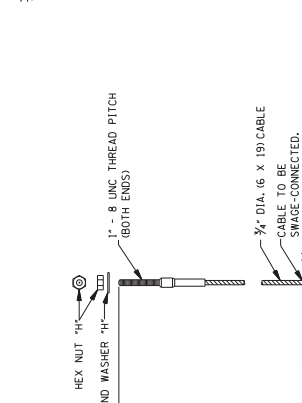
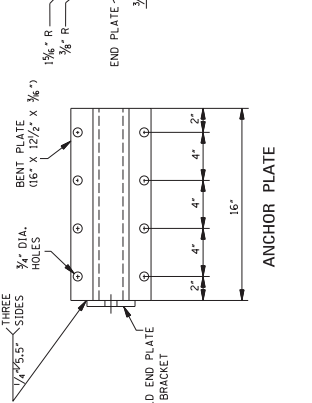
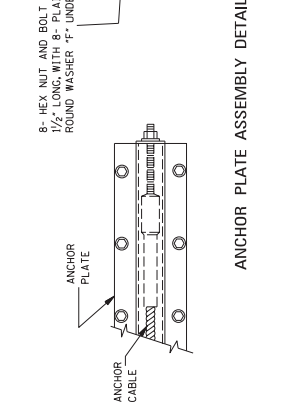
BOLT	L	T (MIN.)
"A"	17/2"	4"
"B"	12"	4"
"C"	14"	4"
"D"	12"	4"
"E"	33"	4"
"F"	2"	13/4"
	10"	4"
	23"	4"

NOTES:
1. ALL GUARDRAIL BOLTS ARE 3/8" - 11 UNC THREAD PITCH.
2. IF ANY BOLT EXTENDS MORE THAN 1/2" FROM THE BOLT SHOULD BE TRIMMED BACK.



WASHER (NOM.)	A (NOM.)	B (NOM.)	C (NOM.)
"F"	1 1/4"	1 3/4"	3/4"
"H"	1 1/4"	2"	3/4"

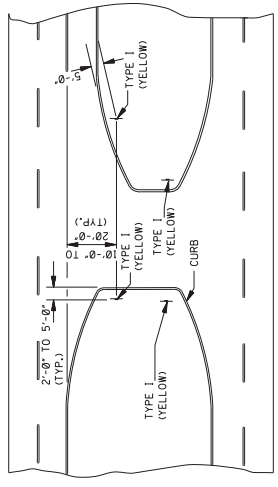
NOTES:
1. ALL GUARDRAIL BOLTS ARE 3/8" - 11 UNC THREAD PITCH.
2. IF ANY BOLT EXTENDS MORE THAN 1/2" FROM THE BOLT SHOULD BE TRIMMED BACK.



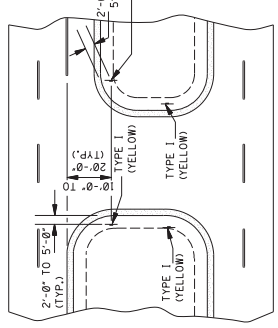
NOTES:
1. ALL GUARDRAIL BOLTS ARE 3/8" - 11 UNC THREAD PITCH.
2. IF ANY BOLT EXTENDS MORE THAN 1/2" FROM THE BOLT SHOULD BE TRIMMED BACK.

**TYPICAL CROSSOVER
DELINEATION**

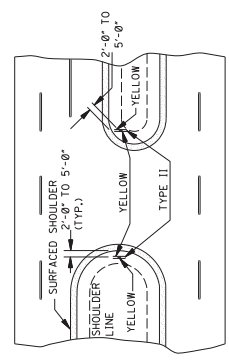
ISSUE DATE: AUGUST 01, 2017
SHEET NUMBER 6316



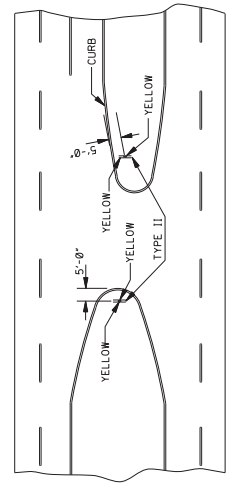
TYPICAL DELINEATION AT A CROSSOVER WITH USABLE SHOULDERS AND A MEDIAN WIDTH OVER 42'-0"



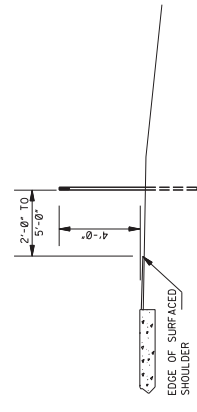
TYPICAL DELINEATION AT A CURBED CROSSOVER WITH A MEDIAN WIDTH OVER 42'-0"



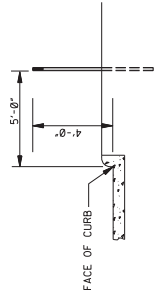
TYPICAL DELINEATION AT A CROSSOVER WITH USABLE SHOULDERS AND A MEDIAN WIDTH OF 42'-0" OR LESS



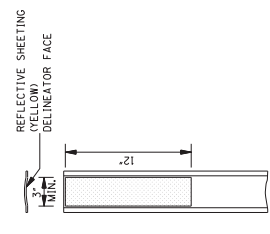
TYPICAL DELINEATION AT A CURBED CROSSOVER WITH A MEDIAN WIDTH OF 42'-0" OR LESS



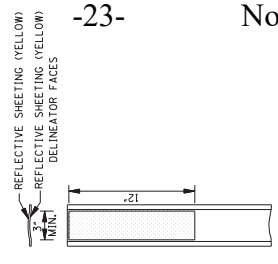
DELINEATOR MOUNTING ON CROSSOVER WITH USABLE SHOULDER



DELINEATOR MOUNTING ON CURBED CROSSOVER



DETAIL OF TYPE I FLEXIBLE POST DELINEATOR



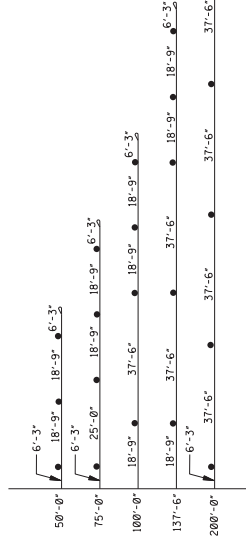
DETAIL OF TYPE II FLEXIBLE POST DELINEATOR

NOTE: FLEXIBLE POST DELINEATORS ALLOWED FOR USE MUST BE FROM MOOT'S APPROVED PRODUCTS LIST.

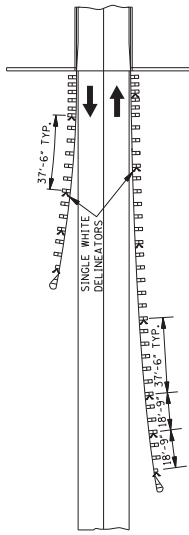
NOTE: PLACE DELINEATORS NO MORE THAN 28'-0" FROM EDGE OF TRAVEL LANES EDGES.

- GENERAL NOTES:
1. THE UNIT PRICE OF DELINEATOR INCLUDES COST(S) OF DELINEATOR FACE(S), POST, HARDWARE AND INSTALLATION.
 2. DELINEATOR FACE WILL BE RETROREFLECTIVE SHEETING TYPE AS SPECIFIED IN PLANS.
 3. POST REQUIRING THE INSTALLATION OF A BASE SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATION.
 4. THE COLOR OF DELINEATORS SHALL BE THE COLOR OF THE ADJACENT EDGE LINE PER MUTCO SECTIONS 3A.03, 3B.09, AND 3C.03

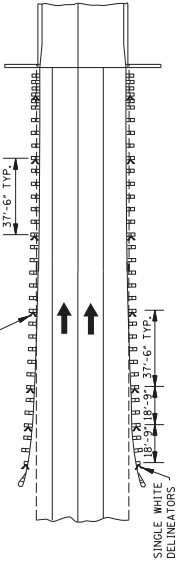
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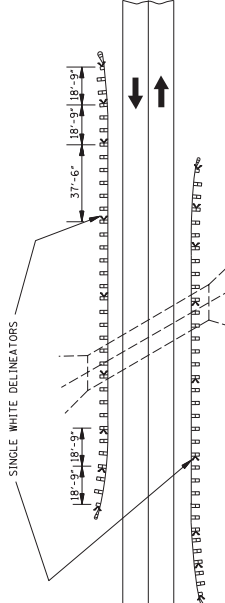
GRAPHIC SHOWING SPACINGS OF GUARDRAIL DELINEATORS AT SOME COMMONLY USED BRIDGE APPROACHES



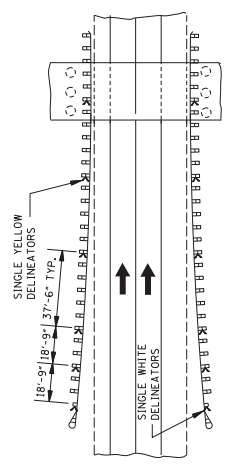
BRIDGE APPROACH INSTALLATION (TWO-WAY TRAFFIC)



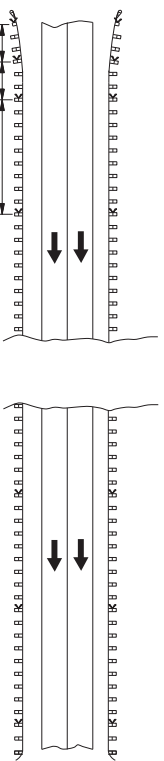
BRIDGE APPROACH INSTALLATION (ONE-WAY TRAFFIC)



ROADSIDE OBSTACLE INSTALLATION—LENGTH 250' OR LESS (TWO-WAY TRAFFIC)



ROADSIDE OBSTACLE INSTALLATION—LENGTH 250' OR LESS (ONE-WAY TRAFFIC)

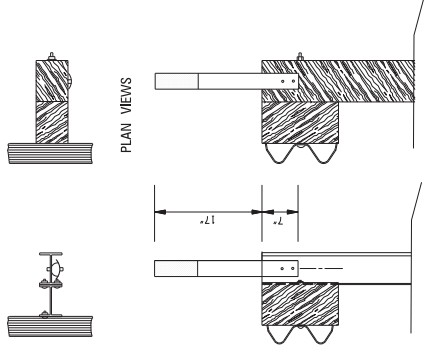


EMBANKMENT OR ROADSIDE OBSTACLE INSTALLATION—LENGTH GREATER THAN 250' (ONE-WAY TRAFFIC)

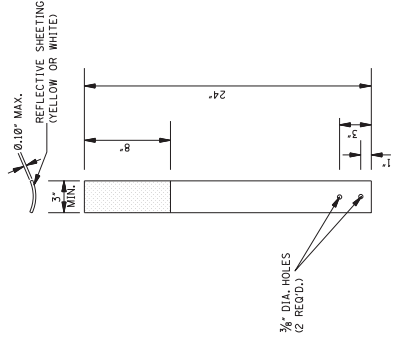
NOTE: ONE-WAY TRAFFIC SHOWN. DELINEATOR SPACING FOR TWO-WAY TRAFFIC SIMILAR. DELINEATOR COLOR WILL BE THE SAME AS THE ADJACENT PAVEMENT EDGE MARKING. THE FIRST THREE (3) MARKERS WILL FACE TRAFFIC IN OFF LANE FOR TWO-WAY TRAFFIC AS SHOWN IN DRAWING FOR OBSTACLE INSTALLATION FOR TWO-WAY TRAFFIC.

GENERAL NOTES:

1. THE UNIT PRICE OF DELINEATOR INCLUDES: COSTS OF DELINEATOR (FACE(S)), POST, HARDWARE AND INSTALLATION.
2. DELINEATORS SHALL CONSIST OF RETROREFLECTIVE DEVICES THAT ARE CAPABLE OF CLEARLY RETROREFLECTING LIGHT UNDER NORMAL ATMOSPHERIC CONDITIONS FROM A DISTANCE OF 1000 FEET WHEN ILLUMINATED BY THE HIGH BEAMS OF STANDARD AUTOMOBILE LIGHTS.
3. FOR THE SPACINGS OF DELINEATORS IN HORIZONTAL CURVES ON ROADWAYS OR BRIDGES, SEE TABLE 305-1, MUTCD 11TH EDITION, FOR SPACING REQUIREMENTS.
4. DELINEATORS FOR GUARDRAIL SHALL BE MOUNTED ON FLEXIBLE POSTS AS FOLLOWS: THE DELINEATOR POSTS WILL BE FROM MOOT'S "APPROVED PRODUCTS LIST" AND WILL BE FASTENED TO GUARDRAIL POST IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.



TYPICAL FLEXIBLE POST DELINEATOR GUARDRAIL INSTALLATION



DETAIL OF FLEXIBLE GUARDRAIL DELINEATOR

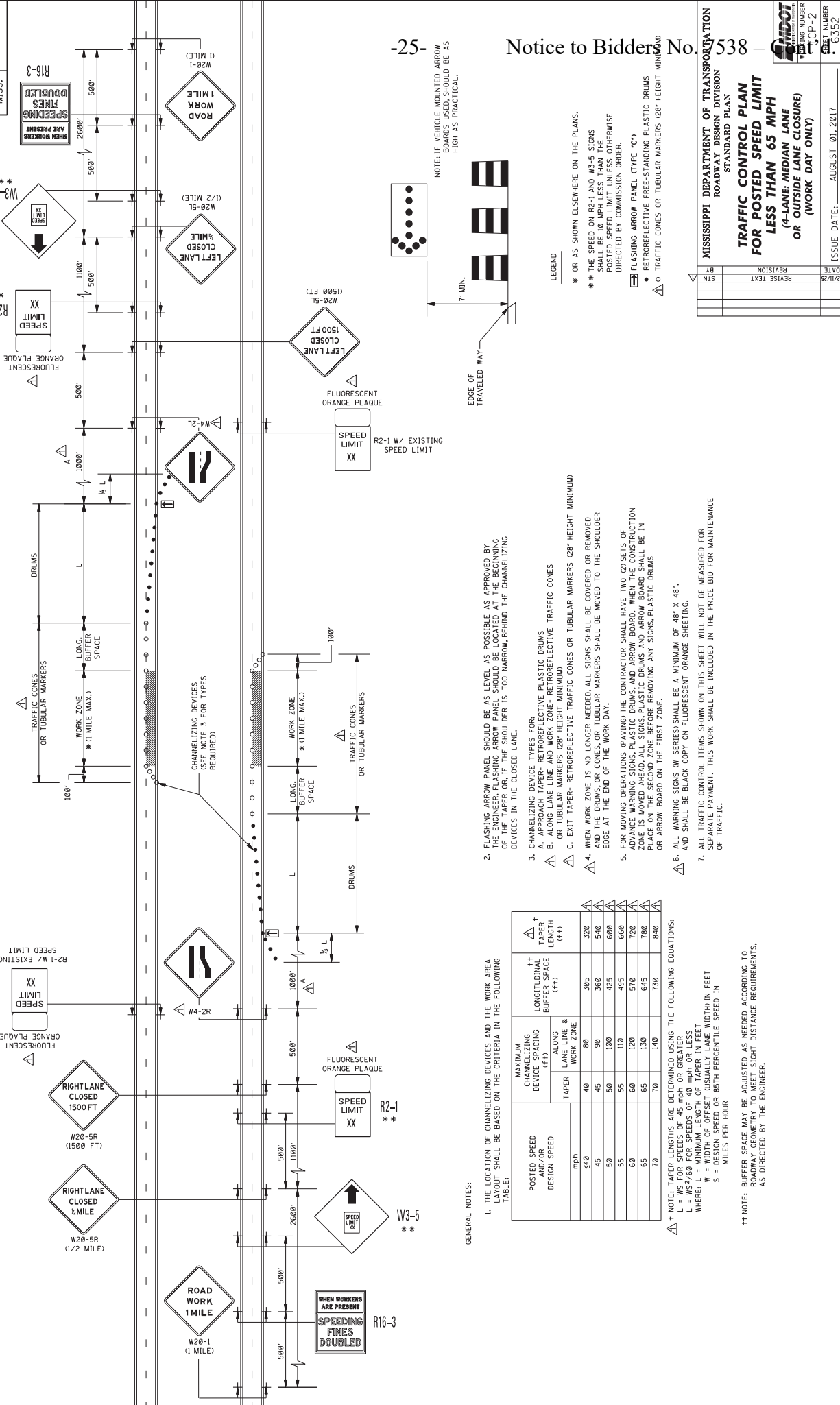
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MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

TYPICAL GUARDRAIL DELINEATION

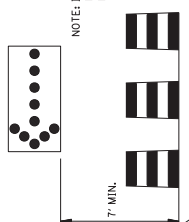
ISSUE DATE: AUGUST 01, 2017

PROJECT NUMBER: 6317



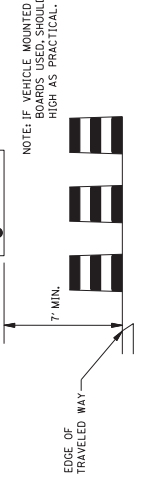
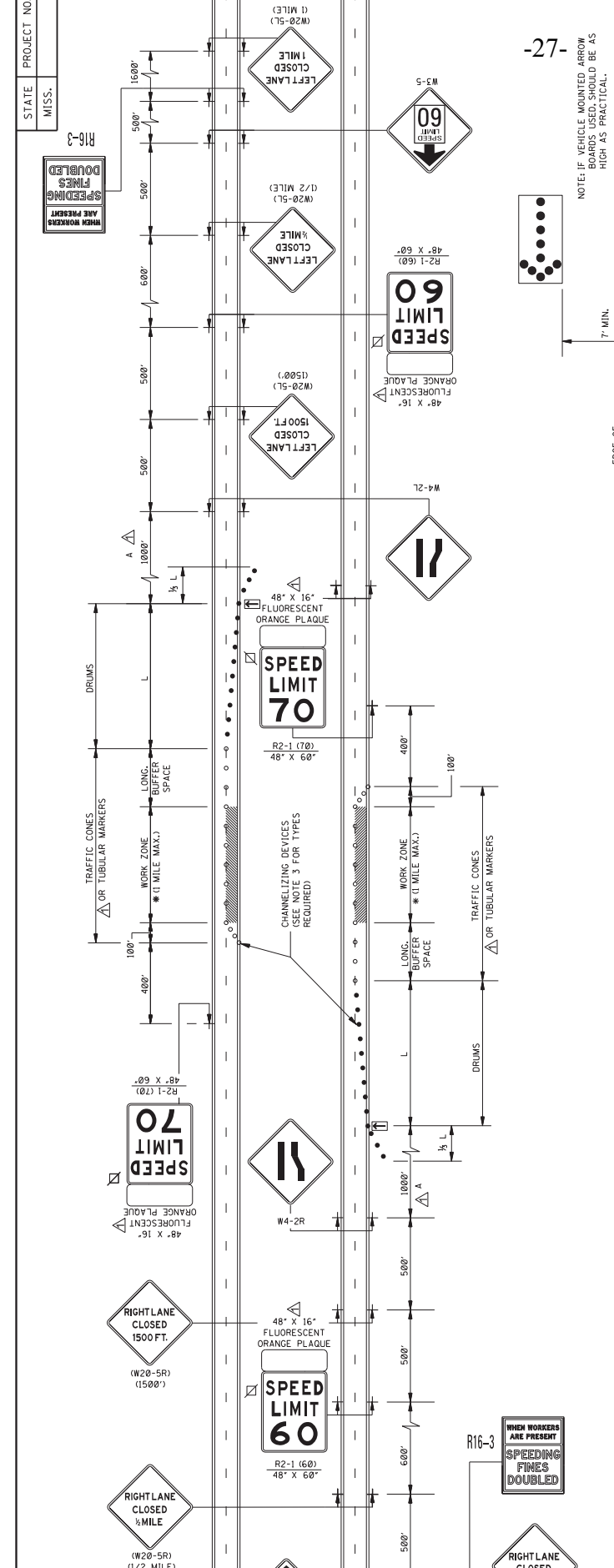
- GENERAL NOTES:
1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LIMIT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:
- | POSTED SPEED AND/OR DESIGN SPEED (mph) | MAXIMUM CHANNELIZING DEVICE SPACING (ft) | | LONGITUDINAL BUFFER SPACE (ft) | TAPER LENGTH (ft) |
|----------------------------------------|------------------------------------------|-------|--------------------------------|-------------------|
| | ALONG LANE LINE & WORK ZONE | TAPER | | |
| 40 | 80 | 395 | 320 | |
| 45 | 90 | 360 | 540 | |
| 50 | 100 | 425 | 600 | |
| 55 | 110 | 495 | 660 | |
| 60 | 120 | 570 | 720 | |
| 65 | 130 | 645 | 780 | |
| 70 | 140 | 730 | 840 | |
- † NOTE: TAPER LENGTHS ARE DETERMINED USING THE FOLLOWING EQUATIONS:
L = WS FOR SPEEDS OF 45 mph OR GREATER
L = WS²/600 FOR SPEEDS OF 40 mph OR LESS
WHERE: L = MINIMUM LENGTH OF TAPER IN FEET
W = WIDTH OF OFFSET USUALLY LANE WIDTH IN FEET
S = SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR
2. FLASHING ARROW PANEL SHOULD BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR, IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.
 3. CHANNELIZING DEVICE TYPES FOR:
 - A. ALONG LANE LINE AND WORK ZONE - RETROREFLECTIVE TRAFFIC CONES
 - B. OR TUBULAR MARKERS (28" HEIGHT MINIMUM)
 - C. EXIT TAPER - RETROREFLECTIVE TRAFFIC CONES OR TUBULAR MARKERS (28" HEIGHT MINIMUM)
 4. WHEN WORK ZONE IS NO LONGER NEEDED, ALL SIGNS SHALL BE COVERED OR REMOVED AND THE DRUMS, OR CONES, OR TUBULAR MARKERS SHALL BE MOVED TO THE SHOULDER EDGE AT THE END OF THE WORK DAY.
 5. FOR MOVING OPERATIONS (PAVING) THE CONTRACTOR SHALL HAVE TWO (2) SETS OF ADVANCE WARNING SIGNS, PLASTIC DRUMS, AND ARROW BOARD. WHEN THE CONSTRUCTION ZONE IS MOVED AHEAD, ALL SIGNS, PLASTIC DRUMS, AND ARROW BOARD SHALL BE IN PLACE AND MOVED AHEAD. REMOVE ANY SIGNS, PLASTIC DRUMS OR ARROW BOARD ON THE FIRST ZONE.
 6. ALL WARNING SIGNS (W SERIES) SHALL BE A MINIMUM OF 48" X 48". AND SHALL BE BLACK COPY ON FLUORESCENT ORANGE SHEETING.
 7. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

- LEGEND
- * OR AS SHOWN ELSEWHERE ON THE PLANS.
 - ** THE SPEED ON R2-1 AND W3-5 SIGNS SHALL BE 10 MPH LESS THAN THE POSTED SPEED LIMIT UNLESS OTHERWISE DIRECTED BY COMMISSION ORDER.
 - ◻ FLASHING ARROW PANEL (TYPE "C")
 - RETROREFLECTIVE FREE-STANDING PLASTIC DRUMS
 - ◁ RETROREFLECTIVE OR TUBULAR MARKERS (28" HEIGHT MINIMUM)



NOTE: IF VEHICLE MOUNTED ARROW BOARDS USED, SHOULD BE AS HIGH AS PRACTICAL.

DATE	REVISION	BY



NOTE: IF VEHICLE MOUNTED ARROW BOARDS USED, SHOULD BE AS HIGH AS PRACTICAL.

LEGEND

- * OR AS SHOWN ELSEWHERE ON THE PLANS.
- ◻ FLASHING ARROW PANEL (TYPE 'C')
- ◻ BLACK LEGEND AND BORDER ON WHITE BACKGROUND
- ◻ RETROREFLECTIVE FREE-STANDING PLASTIC DRUMS
- TRAFFIC CONES (28" HEIGHT)

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
TRAFFIC CONTROL PLAN
FOR POSTED SPEED LIMIT
OF 65 OR 70 MPH
(INTERSTATES AND OTHER 4-LANE
DIVIDED HIGHWAYS)
(MEDIAN LANE OR OUTSIDE
LANE CLOSURE) (WORK DAY ONLY)

DATE	BY	REVISION

ISSUE DATE: AUGUST 01, 2017

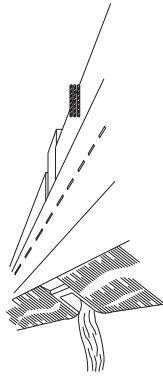
GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LIMIT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE.
2. FLASHING ARROW PANEL SHOULD BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR, IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.
3. APPROACH TAPER- RETROREFLECTIVE PLASTIC DRUMS OR TUBULAR MARKERS (28" HEIGHT MINIMUM)
4. ALONG LANE LINE AND WORK ZONE- TRAFFIC CONES OR TUBULAR MARKERS (28" HEIGHT MINIMUM)
5. EXIT TAPER- TRAFFIC CONES OR TUBULAR MARKERS (28" HEIGHT MINIMUM)
6. WHEN WORK ZONE IS NO LONGER NEEDED, ALL SIGNS SHALL BE COVERED OR REMOVED AND THE DRUMS SHALL BE MOVED TO THE SHOULDER EDGE AT THE END OF THE WORK DAY.
7. FOR MOVING OPERATIONS (PAVING) THE CONTRACTOR SHALL HAVE TWO (2) SETS OF ADVANCE WARNING AND REGULATORY SIGNS, PLASTIC DRUMS, AND ARROW BOARD. WHEN THE CONSTRUCTION ZONE IS MOVED AHEAD, ALL SIGNS, PLASTIC DRUMS, AND ARROW BOARD SHALL BE IN PLACE ON THE SECOND ZONE BEFORE REMOVING ANY SIGNS, PLASTIC DRUMS OR ARROW BOARD ON THE FIRST ZONE.
8. ALL WARNING SIGNS (W SERIES) SIGNS SHOULD BE A MINIMUM OF 48" X 48" AND SHALL BE BLACK COPY ON FLUORESCENT ORANGE SHEETING.
9. ALL EXISTING SPEED LIMIT SIGNS WHICH ARE INFLUENCED BY OR CONFLICT WITH THE SPEED ZONE REDUCTION SHALL BE COVERED AS DIRECTED BY THE ENGINEER WHILE THE REDUCED SPEED LIMIT IS IN EFFECT. TAPE SHALL NOT BE USED ON THE FACE OF SIGN.
10. ADDITIONAL REDUCED REGULATORY SPEED LIMIT SIGNS ARE REQUIRED AT EACH ENTRANCE RAMP WITHIN THE SPEED ZONE. TWO (2) WILL BE REQUIRED FOR EACH RAMP AND LOCATION WILL BE DETERMINED BY THE ENGINEER.
11. THIS TRAFFIC CONTROL PLAN WITH SPEED ZONE MAY NOT BE USED ON ANY FACILITY REQUESTING A SPEED LIMIT REDUCTION.
12. LAYOUT SHOWN ABOVE IS FOR AN INTERSTATE WITH A POSTED SPEED LIMIT OF 70 MPH. FOR POSTED SPEED LIMIT OF 65 MPH, THE REDUCED SPEED LIMIT WILL BE 55 MPH.
13. A FLUORESCENT ORANGE PLAQUE IS REQUIRED WITH ALL REGULATORY SPEED LIMIT SIGNS AND "REDUCED SPEED AHEAD" SIGNS REQUIRED FOR LANE CLOSURE.
14. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

POSTED SPEED AND/OR DESIGN SPEED (mph)	MAXIMUM CHANNELIZING DEVICE SPACING (ft)		LONGITUDINAL BUFFER SPACE (ft)	TAPER LENGTH (ft)
	ALONG LANE LINE & WORK ZONE	TAPER		
50	40	80	395	320
55	45	90	360	540
60	50	100	425	600
65	55	110	495	660
70	60	120	570	720
75	65	130	645	780
80	70	140	730	840

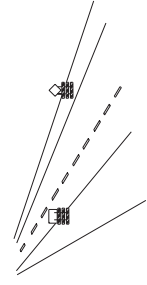
† NOTE: TAPER LENGTHS ARE DETERMINED USING THE FOLLOWING EQUATIONS:
L = WS FOR SPEEDS OF 45 mph OR GREATER
L = WS²/60 FOR SPEEDS OF 40 mph OR LESS
WHERE: L = MINIMUM LENGTH OF TAPER IN FEET
W = WIDTH OF OFFSET USUALLY LANE WIDTH IN FEET
S = WIDTH OF OFFSET OR 85TH PERCENTILE SPEED IN MILES PER HOUR

†† NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.



WING BARRICADES

- WING BARRICADES ARE TYPE III BARRICADES ERECTED ON THE SHOULDER OF A ROAD TO PROTECT THE SHOULDER FROM THE ADVANCEMENT OF A NARROWING OR RESTRICTED ROADWAY. WING BARRICADES MAY BE USED AS A MOUNTING FOR THE ADVANCE WARNING SIGNS OR FLASHERS.
- WING BARRICADES SHOULD BE USED:
 - IN ADVANCE OF A CONSTRUCTION PROJECT EVEN WHEN NO PART OF THE ROADWAY IS ACTUALLY CLOSED.
 - IN ADVANCE OF ALL BRIDGE OR CULVERT WIDENING OPERATIONS.

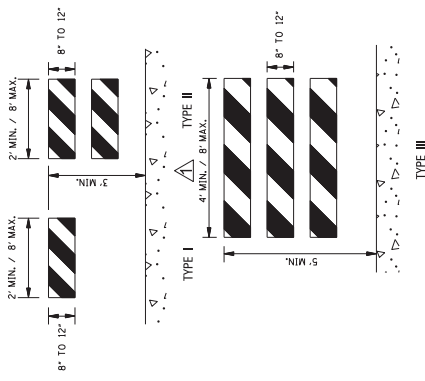


BARRICADE CLOSING A ROAD

BARRICADE CHARACTERISTICS

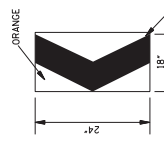
	I	II	III
WIDTH OF RAIL **	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.
LENGTH OF RAIL **	24" MIN. / 95" MAX.	24" MIN. / 95" MAX.	48" MIN. / 95" MAX.
WIDTH OF STRIPE *	6"	6"	6"
HEIGHT	36" MIN.	36" MIN.	60" MIN.
NUMBER OF RETROREFLECTORIZED RAIL FACES	2 (ONE EACH DIRECTION)	4 (TWO EACH DIRECTION)	3 IF FACING TRAFFIC IN TWO DIRECTIONS 6 IF FACING TRAFFIC IN TWO DIRECTIONS

- * 1. FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.
- ** 2. BARRICADES INTENDED FOR USE ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH SPEED FACILITIES SHALL HAVE A MINIMUM OF 210 IP OF REFLECTIVE AREA FACING TRAFFIC.



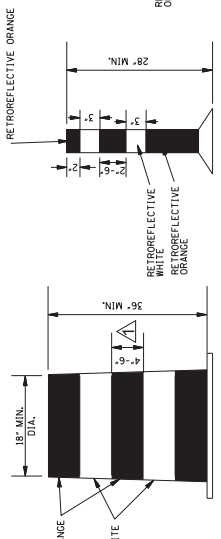
STANDARD BARRICADES

- THE RETROREFLECTIVE MARKING FOR BARRICADE RAILS SHALL BE ORANGE AND WHITE (SLOPING DOWNWARD AT AN ANGLE OF 45° IN THE DIRECTION THAT TRAFFIC IS TO PASS).
- RAIL STRIPE SHOULD BE 6 INCHES, EXCEPT THAT 4-INCH WIDE STRIPES MAY BE USED IF RAIL LENGTHS ARE LESS THAN 36 INCHES.
- DO NOT PLACE SANDBARS OR OTHER DEVICES ON BARRICADE RAILS TO PROVIDE MASS SLOWDOWN. SANDBARS MAY BE PLACED AS BALLAST TO THE LOWER PARTS OF THE FRAME OR THE STAYS TO BARRICADES.
- FOR ADDITIONAL INFORMATION OR DETAILS, SEE MUTCD, LATEST EDITION.
- BARRICADES ARE CLASSIFIED BY FHWA AS CATEGORY II WORK ZONE DEVICES. A LIST OF CRASHWORTHY BARRICADES AND OTHER CATEGORY II DEVICES CAN BE FOUND ON FHWA'S WEBSITE: <http://highways.dot.gov/safety/rwd/reduce-crash-severity>
- WHERE ROAD USERS INCLUDE PEDESTRIANS, THE PROVISION OF SUPPLEMENTAL AUDIBLE INFORMATION OR DETECTABLE BARRIERS OR BARRICADES SHOULD BE PROVIDED FOR PEOPLE WITH VISION DISABILITIES.
- BARRICADE RAIL SUPPORTS SHOULD NOT PROJECT INTO PEDESTRIAN CIRCULATION ROUTES MORE THAN 4 INCHES FOR THE SUPPORTS LOCATED BETWEEN 27 INCHES TO 88 INCHES ABOVE THE EXISTING SURFACE.

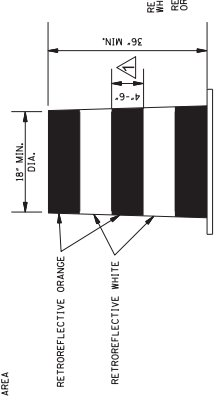


RETROREFLECTIVE CHEVRON ALIGNMENT SIGN DETAIL (W1-8)

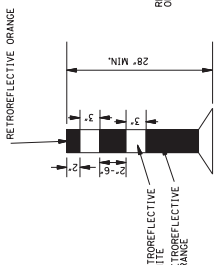
- CHEVRON SIGN CONSISTS OF A BLACK CHEVRON TYPE MARKING ON AN ORANGE BACKGROUND AND SHALL POINT IN THE DIRECTION OF TRAFFIC FLOW.
- THE CHEVRON SIGN SHALL BE MOUNTED ON CRASHWORTHY SUPPORT.
- CHEVRON SIGNS MAY BE USED TO SUPPLEMENT OTHER STANDARD DEVICES WHERE ONE OR MORE LANES ARE CLOSED FOR CONSTRUCTION OR MAINTENANCE. THEY SHOULD BE PLACED APPROXIMATELY 2'-0" BEHIND THE LANE TRANSITION STRIPE.
- CHEVRON SIGNS SHALL BE INSTALLED AT A MINIMUM HEIGHT OF 4 FEET MEASURED VERTICALLY FROM THE BOTTOM OF THE SIGN TO THE ELEVATION OF THE NEAR EDGE OF THE TRAVELWAY.



PLASTIC DRUM DETAIL (36" OR GREATER)



PLASTIC DRUM DETAIL (28" - 36")



PLASTIC CONE DETAIL (28" - 36")

- PLASTIC DRUMS, TUBULAR MARKERS, OR PLASTIC CONES SHALL BE ON BIG END OF CONE OR DRUM. EXPEDITED METHOD FOR TRAFFIC CHANNELIZATION, THE COLOR AND MARKING OF THE DEVICE USED SHALL BE CONSISTENT WITH MARKING STANDARDS FOR BARRICADE. THE PREDOMINANT COLOR ON THE DEVICE USED SHALL BE ORANGE WITH RETROREFLECTIVE, HORIZONTAL CIRCUMFERENTIAL WHITE STRIPES AS SHOWN ON THE DETAILS.
- DRUMS, TUBULAR MARKERS, OR PLASTIC CONES SHOULD NEVER BE PLACED IN THE ROADWAY WITHOUT WARNING SIGNS.
- WHERE PRACTICAL, PLASTIC DRUMS, TUBULAR MARKERS, OR PLASTIC CONES SHOULD BE PLACED NO CLOSER THAN 3'-0" FROM THE EDGE OF TRAVELED LANE.
- BALLAST SHALL NOT BE PLACED ON THE TOP OF THE DRUM.

Notice to Bidders No. 538

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

HIGHWAY SIGN AND BARRICADE DETAILS FOR CONSTRUCTION PROJECTS

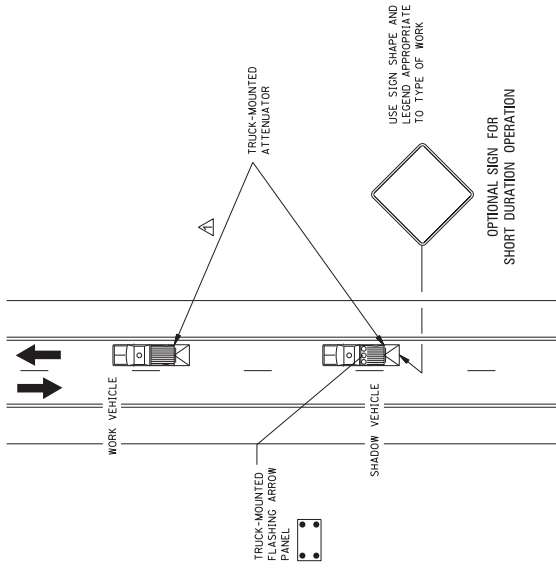
DATE	REVISION	BY

ISSUE DATE: AUGUST 01, 2017

PROJECT NUMBER: 6358

PLAN NUMBER: 100-8

MOBILE OPERATIONS ON TWO-LANE ROAD

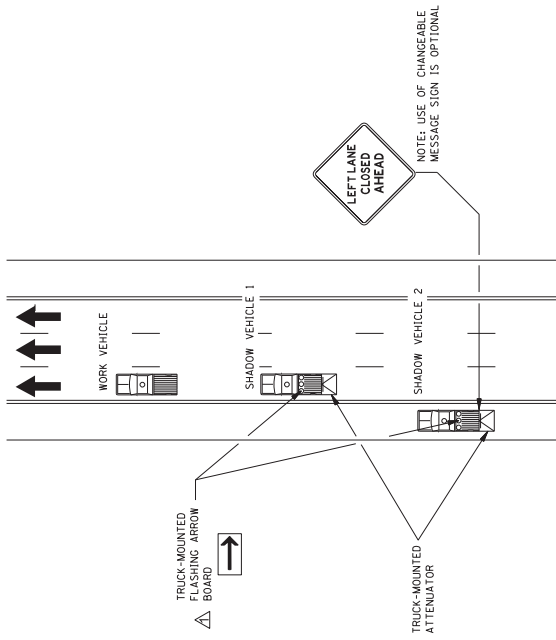


MOBILE OPERATIONS ON TWO-LANE ROAD

NOTES FOR TWO-LANE OPERATION:

- WHERE PRACTICAL AND WHEN NEEDED, THE WORK AND SHADOW VEHICLES SHOULD BE POSITIONED PERIODICALLY TO ALLOW TRAFFIC TO PASS. THIS CAN NOT BE DONE FREQUENTLY AS AN ALTERNATIVE, A "DO NOT PASS" SIGN MAY BE PLACED ON THE REAR OF THE VEHICLE BLOCKING THE LANE.
- THE DISTANCE BETWEEN THE WORK AND SHADOW VEHICLES MAY VARY ACCORDING TO THE TYPE OF ROADWAY. SHADOW VEHICLES SHOULD BE USED TO WARN TRAFFIC OF THE OPERATION AHEAD, WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR. THE SHADOW VEHICLE SHOULD MAINTAIN THE MINIMUM DISTANCE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. THE SHADOW VEHICLE SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- ADDITIONAL SHADOW VEHICLES TO WARN AND REDUCE THE SPEED OF ONCOMING OR OPPOSING TRAFFIC MAY BE USED. POLICE PATROL CARS MAY BE USED FOR THIS PURPOSE.
- A TRUCK-MOUNTED ATTENUATOR (TMA) SHOULD BE USED ON THE SHADOW VEHICLE AND MAY BE USED ON THE WORK VEHICLE.
- VEHICLES SHALL BE EQUIPPED WITH BEACONS, AND THE SHADOW LIGHTS MOUNTED ON THE REAR, ADJACENT TO THE SIGN. SHADOW AND WORK VEHICLES SHALL DISPLAY FLASHING OR ROTATING BEACONS BOTH FORWARD AND TO THE REAR.
- VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBTURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
- ARROW BOARD TO BE USED IN CAUTION MODE.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

MOBILE OPERATIONS ON MULTILANE ROAD



MOBILE OPERATIONS ON MULTILANE ROAD

NOTES FOR MULTILANE OPERATION:

- SHADOW AND WORK VEHICLES SHALL DISPLAY HIGH-INTENSITY FLASHING BEACONS. THE SHADOW VEHICLE SHALL BE EQUIPPED WITH BEACONS, AND THE SHADOW LIGHTS MOUNTED ON THE REAR, ADJACENT TO THE SIGN. SHADOW AND WORK VEHICLES SHALL DISPLAY FLASHING OR ROTATING BEACONS BOTH FORWARD AND TO THE REAR.
- ON HIGH-SPEED ROADWAYS, A THIRD SHADOW VEHICLE (NOT SHOWN) MAY BE USED WITH SHADOW VEHICLE 1 IN THE CLOSED LANE, SHADOW VEHICLE 2 STRADDLING THE EDGE LINE, AND SHADOW VEHICLE 3 ON THE SHOULDER.
- WHERE ADEQUATE SHOULDER WIDTH IS NOT AVAILABLE, SHADOW VEHICLE 3 MAY ALSO STRADDLE THE EDGE LINE.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
- VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBTURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
- ARROW BOARD TO BE USED IN CAUTION MODE.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

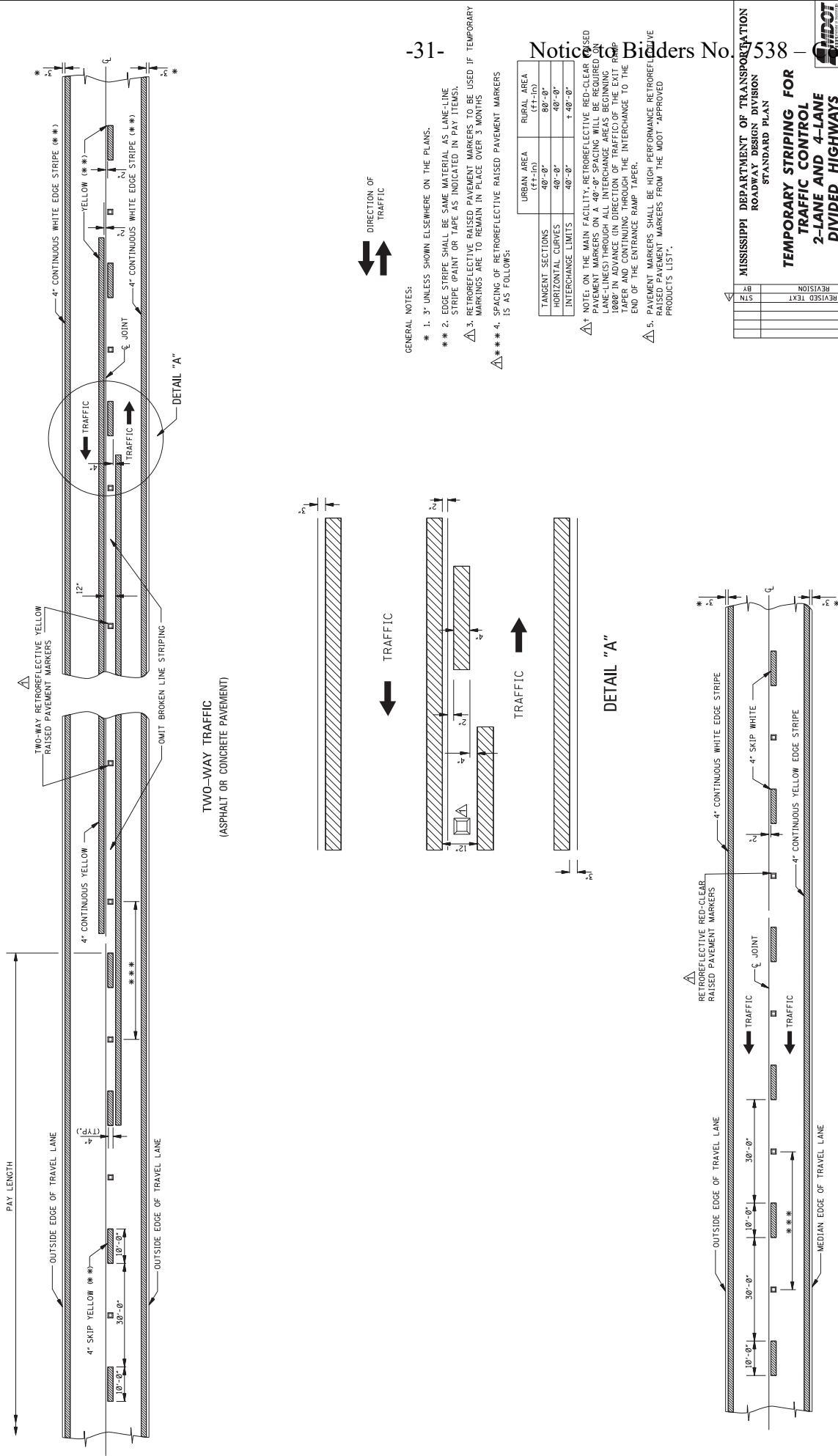
**TRAFFIC CONTROL PLAN
MOBILE OPERATIONS
MULTILANE ROADS
AND
TWO-LANE ROADS**

DATE	BY	REVISION

ISSUE DATE: AUGUST 01, 2017

PROJECT NUMBER: 6359

PLAN NUMBER: TCC-9



GENERAL NOTES:

- * 1. 3" UNLESS SHOWN ELSEWHERE ON THE PLANS.
 - ** 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR TAPE AS INDICATED IN PAY ITEMS).
 - ▲ 3. RETROREFLECTIVE RAISED PAVEMENT MARKERS TO BE USED IF TEMPORARY MARKINGS ARE TO REMAIN IN PLACE OVER 3 MONTHS.
 - ▲▲▲ 4. SPACING OF RETROREFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:
- | TANGENT SECTIONS | URBAN AREA (ft-in) | RURAL AREA (ft-in) |
|--------------------|--------------------|--------------------|
| HORIZONTAL CURVES | 40'-0" | 80'-0" |
| INTERCHANGE LIMITS | 40'-0" | + 40'-0" |
- ▲▲ NOTE: ON THE MAIN FACILITY, RETROREFLECTIVE RED-CLEAR PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINES THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP AND ENDING 1000' BEFORE THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
 - ▲ 5. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RETROREFLECTIVE RAISED PAVEMENT MARKERS FROM THE MOST APPROVED PRODUCTS LIST.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**TEMPORARY STRIPING FOR
TRAFFIC CONTROL
2-LANE AND 4-LANE
DIVIDED HIGHWAYS**

ISSUE DATE: AUGUST 01, 2017

PROJECT NUMBER: 6363

CP-13

REVISIONS:

NO.	DATE	BY	REVISION

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 7539

CODE: (SP)

DATE: 1/28/2026

SUBJECT: Scope of Work Revision

PROJECT: NHPP-0015-01(152) / 109797301 – Lincoln County

Bidders are hereby advised of the following changes to the scope of work:

- MDOT Roadway Design Standard Drawing revisions dated 12/11/2025 have been included in the revised scope of work.

OPERATOR: Bulldozer.....	\$ 13.30	0.00
OPERATOR: Concrete Saw.....	\$ 13.60	0.00
OPERATOR: Crane.....	\$ 16.00	0.00
OPERATOR: Distributor.....	\$ 11.70	0.00
OPERATOR: Drill.....	\$ 19.22	0.00
OPERATOR: Grader/Blade.....	\$ 13.84	0.00
OPERATOR: Loader.....	\$ 11.73	0.00
OPERATOR: Mechanic.....	\$ 16.28	0.00
OPERATOR: Milling Machine.....	\$ 15.38	0.00
OPERATOR: Mixer.....	\$ 14.85	0.00
OPERATOR: Oiler.....	\$ 13.08	0.48
OPERATOR: Paver (Asphalt, Aggregate, and Concrete).....	\$ 13.01	0.00
OPERATOR: Piledriver.....	\$ 15.13	0.00
OPERATOR: Roller (All Types)....	\$ 11.05	0.00
OPERATOR: Scraper.....	\$ 12.63	0.00
OPERATOR: Tractor.....	\$ 9.98	0.00
OPERATOR: Trencher.....	\$ 15.00	0.00
TRUCK DRIVER: Flatbed Truck.....	\$ 13.29	0.00
TRUCK DRIVER: Lowboy Truck.....	\$ 12.49	0.00
TRUCK DRIVER: Mechanic.....	\$ 12.35	0.00
TRUCK DRIVER: Off the Road Truck.....	\$ 12.31	0.00
TRUCK DRIVER: Water Truck.....	\$ 13.15	0.00
TRUCK DRIVER: Dump Truck (All Types).....	\$ 11.64	0.00
TRUCK DRIVER: Semi/Trailer Truck.....	\$ 17.34	0.00

WELDERS - Receive rate prescribed for craft performing operation to which welding is incidental.

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Note: Executive Order (EO) 13706, Establishing Paid Sick Leave for Federal Contractors applies to all contracts subject to the Davis-Bacon Act for which the contract is awarded (and any solicitation was issued) on or after January 1, 2017. If this contract is covered by the EO, the contractor must provide employees with 1 hour of paid sick leave for every 30 hours they work, up to 56 hours of paid sick leave each year.

Employees must be permitted to use paid sick leave for their own illness, injury or other health-related needs, including preventive care; to assist a family member (or person who is like family to the employee) who is ill, injured, or has other health-related needs, including preventive care; or for reasons resulting from, or to assist a family member (or person who is like family to the employee) who is a victim of, domestic violence, sexual assault, or stalking. Additional information on contractor requirements and worker protections under the EO is available at <https://www.dol.gov/agencies/whd/government-contracts>.

Note: Executive Order 13658 generally applies to contracts subject to the Davis-Bacon Act that were awarded on or between January 1, 2015 and January 29, 2022, and that have not been renewed or extended on or after January 30, 2022. Executive Order 13658 does not apply to contracts subject only to the Davis-Bacon Related Acts regardless of when they were awarded. If a contract is subject to Executive Order 13658, the contractor must pay all covered workers at least \$13.30 per hour (or the applicable wage rate listed on this wage determination, if it is higher) for all hours spent performing on the contract in 2025. The applicable Executive Order minimum wage rate will be adjusted annually. Additional information on contractor requirements and worker protections under Executive Order 13658 is available at www.dol.gov/whd/govcontracts.

Unlisted classifications needed for work not included within the scope of the classifications listed may be added after award only as provided in the labor standards contract clauses (29CFR 5.5 (a) (1) (iii)).

The body of each wage determination lists the classifications and wage rates that have been found to be prevailing for the type(s) of construction and geographic area covered by the wage determination. The classifications are listed in alphabetical order under rate identifiers indicating whether the particular rate is a union rate (current union negotiated rate), a survey rate, a weighted union average rate, a state adopted rate, or a supplemental classification rate.

Union Rate Identifiers

A four-letter identifier beginning with characters other than ""SU"", ""UAVG"", ?SA?, or ?SC? denotes that a union rate was prevailing for that classification in the survey. Example: PLUM0198-005 07/01/2024. PLUM is an identifier of the union whose collectively bargained rate prevailed in the survey for this classification, which in this example would be Plumbers. 0198 indicates the local union number or district council number where applicable, i.e., Plumbers Local 0198. The next number, 005 in the example, is an internal number used in processing the wage determination. The date, 07/01/2024 in the example, is the effective date of the most current negotiated rate.

Union prevailing wage rates are updated to reflect all changes over time that are reported to WHD in the rates in the collective bargaining agreement (CBA) governing the classification.

Union Average Rate Identifiers

The UAVG identifier indicates that no single rate prevailed for those classifications, but that 100% of the data reported for the classifications reflected union rates. EXAMPLE:

UAVG-OH-0010 01/01/2024. UAVG indicates that the rate is a weighted union average rate. OH indicates the State of Ohio. The next number, 0010 in the example, is an internal number used in producing the wage determination. The date, 01/01/2024 in the example, indicates the date the wage determination was updated to reflect the most current union average rate.

A UAVG rate will be updated once a year, usually in January, to reflect a weighted average of the current rates in the collective bargaining agreements on which the rate is based.

Survey Rate Identifiers

The "SU" identifier indicates that either a single non-union rate prevailed (as defined in 29 CFR 1.2) for this classification in the survey or that the rate was derived by computing a weighted average rate based on all the rates reported in the survey for that classification. As a weighted average rate includes all rates reported in the survey, it may include both union and non-union rates. Example: SUFL2022-007 6/27/2024. SU indicates the rate is a single non-union prevailing rate or a weighted average of survey data for that classification. FL indicates the State of Florida. 2022 is the year of the survey on which these classifications and rates are based. The next number, 007 in the example, is an internal number used in producing the wage determination. The date, 6/27/2024 in the example, indicates the survey completion date for the classifications and rates under that identifier.

?SU? wage rates typically remain in effect until a new survey is conducted. However, the Wage and Hour Division (WHD) has the discretion to update such rates under 29 CFR 1.6(c)(1).

State Adopted Rate Identifiers

The "SA" identifier indicates that the classifications and prevailing wage rates set by a state (or local) government were adopted under 29 C.F.R 1.3(g)-(h). Example: SAME2023-007 01/03/2024. SA reflects that the rates are state adopted. ME refers to the State of Maine. 2023 is the year during which the state completed the survey on which the listed classifications and rates are based. The next number, 007 in the example, is an internal number used in producing the wage determination. The date, 01/03/2024 in the example, reflects the date on which the classifications and rates under the ?SA? identifier took effect under state law in the state from which the rates were adopted.

WAGE DETERMINATION APPEALS PROCESS

1) Has there been an initial decision in the matter? This can be:

- a) a survey underlying a wage determination
- b) an existing published wage determination
- c) an initial WHD letter setting forth a position on a wage determination matter
- d) an initial conformance (additional classification

and rate) determination

On survey related matters, initial contact, including requests for summaries of surveys, should be directed to the WHD Branch of Wage Surveys. Requests can be submitted via email to davisbaconinfo@dol.gov or by mail to:

Branch of Wage Surveys
Wage and Hour Division
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

Regarding any other wage determination matter such as conformance decisions, requests for initial decisions should be directed to the WHD Branch of Construction Wage Determinations. Requests can be submitted via email to BCWD-Office@dol.gov or by mail to:

Branch of Construction Wage Determinations
Wage and Hour Division
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

2) If an initial decision has been issued, then any interested party (those affected by the action) that disagrees with the decision can request review and reconsideration from the Wage and Hour Administrator (See 29 CFR Part 1.8 and 29 CFR Part 7). Requests for review and reconsideration can be submitted via email to dba.reconsideration@dol.gov or by mail to:

Wage and Hour Administrator
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

The request should be accompanied by a full statement of the interested party's position and any information (wage payment data, project description, area practice material, etc.) that the requestor considers relevant to the issue.

3) If the decision of the Administrator is not favorable, an interested party may appeal directly to the Administrative Review Board (formerly the Wage Appeals Board). Write to:

Administrative Review Board
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210.

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END OF GENERAL DECISION

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