

**GENERAL INDEX**

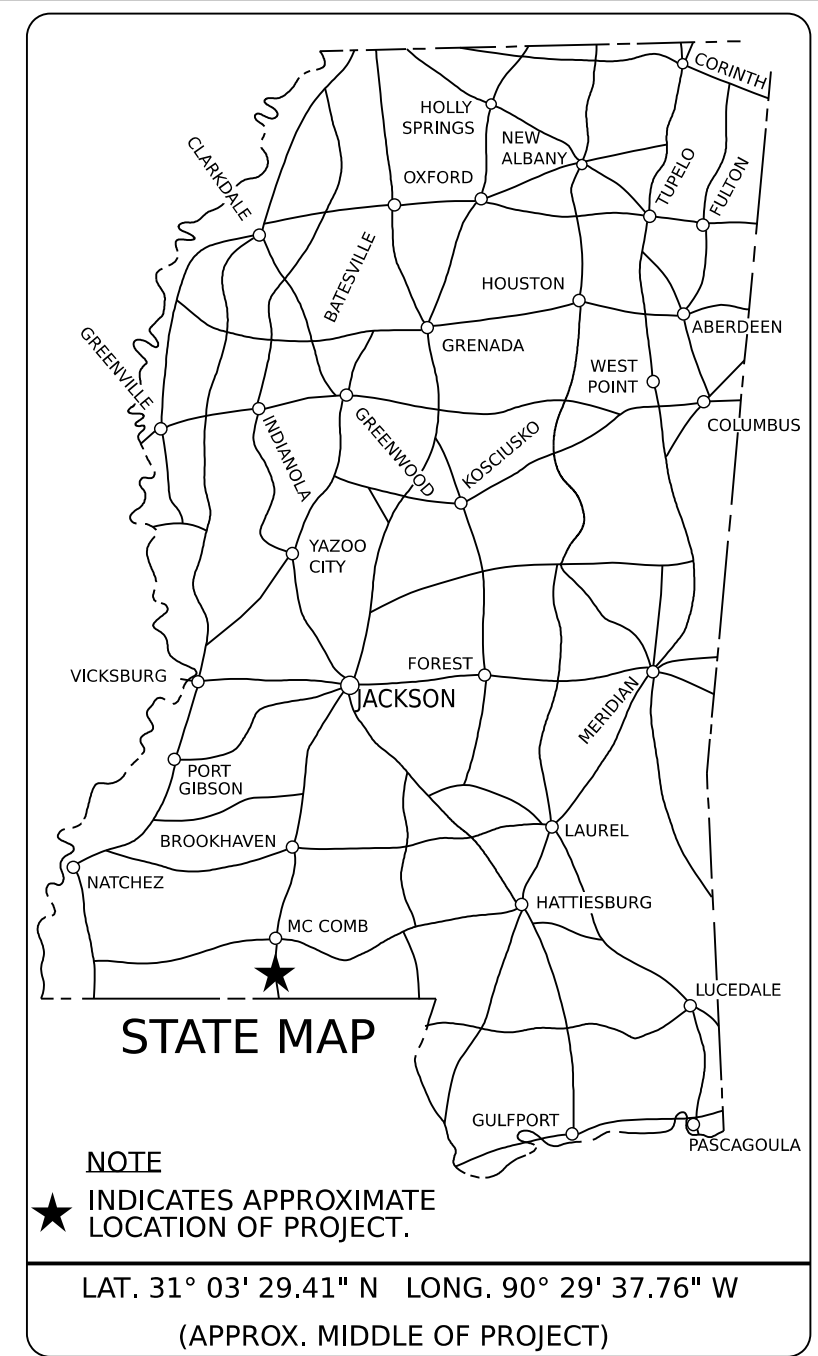
INCLUDED THIS PROJECT	BEGIN WITH SHEET
<input checked="" type="checkbox"/> ROADWAY.....	1
<input type="checkbox"/> PERMANENT SIGNS.....	1001
<input type="checkbox"/> TRAFFIC SIGNALS.....	2001
<input type="checkbox"/> ITS COMPONENTS.....	3001
<input type="checkbox"/> LIGHTING.....	4001
<input type="checkbox"/> (RESERVED).....	5001
<input checked="" type="checkbox"/> ROADWAY STD. DWGS.....	6001
<input type="checkbox"/> BOX CULVERT STD. DWGS (LRFD).....	7001
<input type="checkbox"/> BOX CULVERT STD. DWGS (STD. SPEC.)....	7501
<input type="checkbox"/> BRIDGE.....	8001
<input type="checkbox"/> CROSS SECTIONS.....	9001

BRIDGE STRUCTURES REQ'D.

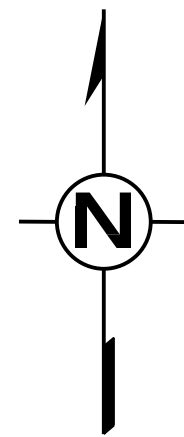
BOX BRIDGES REQ'D.

**STATE OF MISSISSIPPI  
MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

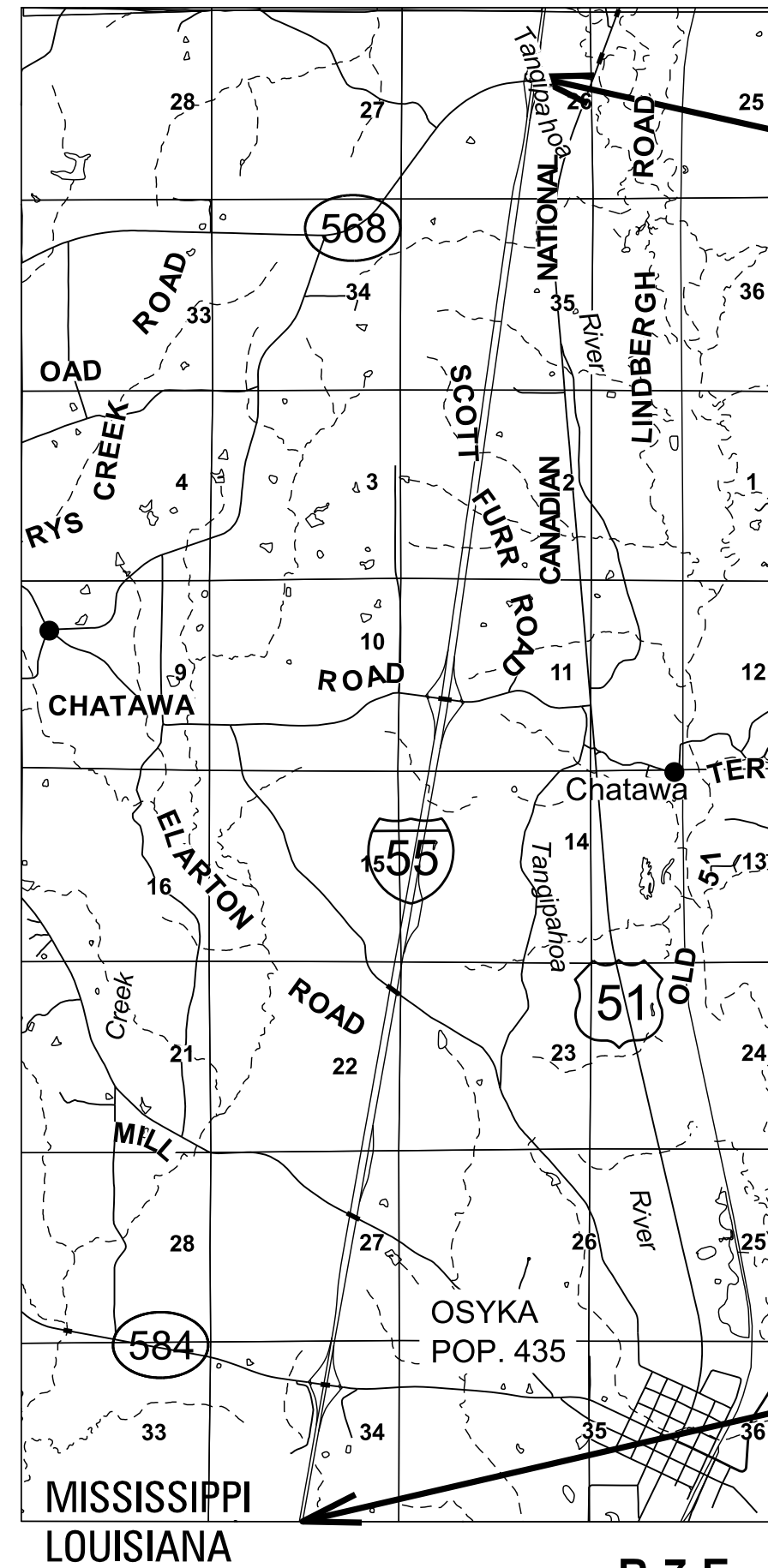
**PLAN AND PROFILE OF  
PROPOSED STATE HIGHWAY  
FEDERAL AID PROJECT NO. IM-0055-01(154)  
I-55 FROM MISSISSIPPI STATE LINE TO MS 568  
PIKE COUNTY**



2.5 MI TO MAGNOLIA ↑ R-7-E | R-8-E



T-2-N  
—  
T-1-N



**E.O.P. STA. 46 + 76**

T-2-N  
—  
T-1-N

**B.O.P. STA. 30 + 00**

R-7-E | R-8-E

SCALES

PLAN 1 IN. = 100 FT.  
PROFILE { HOR. 1 IN. = 100 FT.  
VERT. 1 IN. = 10 FT.  
LAYOUT 1 IN. = 3,043 FT.

EXCEPTIONS

**EQUATIONS  
(LT. LANE)**

106+76.22 = 106+72.44 =	3.78
270+47.82 = 270+55.09 =	-7.27
425+54.37 = 425+50.00 =	4.37
436+04.90 = 25+00.00 =	41,104.90
	<b>41,105.78</b>

**LENGTH DATA**

LENGTH OF ROADWAY	42,781.78 FT.	8.10 MI.
LENGTH OF BRIDGES	FT.	MI.
LENGTH OF PROJECT (NET)	42,781.78 FT.	8.10 MI.
LENGTH OF EXCEPTIONS	FT.	MI.
LENGTH OF PROJECT (GROSS)	42,781.78 FT.	8.10 MI.

**DESIGN CONTROL**

MPH = V (SPEED DESIGN)  
ADT ( ) = : ADT ( ) =  
DHV = : D = % T = %

**PERMITS ACQUIRED BY MDOT**

WETLANDS AND WATERS PERMITS

	WATERS	WETLANDS
NATIONWIDE #14	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
NATIONWIDE (OTHER)*	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
GENERAL*	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
INDIVIDUAL (404)*	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>

STORMWATER PERMIT

Y REQUIRED, CNOI SUBMITTED BY MDOT (DISTURBED AREA=5 ACRES)

S REQUIRED, SCNOI TO BE SUBMITTED BY CONTRACTOR (1 TO 4.99 ACRES)

N NO STORMWATER PERMIT REQUIRED (<1 ACRE)

APPROVED BY: \_\_\_\_\_

DESIGNED BY: DISTRICT 7

**CONSTRUCTION PROJECT DATA**

EXTERNAL PROJECT NUMBER	IM-0055-01(154)
FMS & DETAIL	110061/301000

P S & E DATE:

APPROVED:

DEPUTY EXECUTIVE DIRECTOR / CHIEF ENGINEER

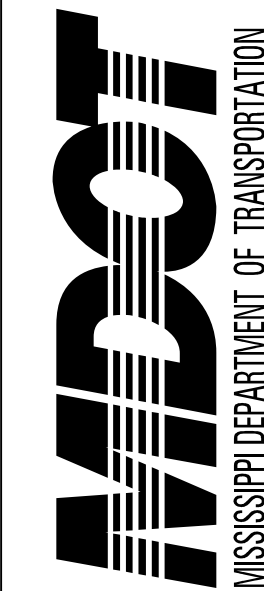
EXECUTIVE DIRECTOR



TITLE SHEET

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DESCRIPTION OF SHEET	WORKING NUMBER SHEET. NO.	
TITLE SHEET		1
DETAILED INDEX	DI-1	2
DETAILED INDEX	DI-2	3
GENERAL NOTES	GN-1	4
TYPICAL SECTION SHEETS (4)		
TYPICAL SECTION - MAINLINE	TS-1	5
TYPICAL SECTION - RAMPS & OVERPASSES	TS-2	6
TYPICAL SECTION - ACCELERATION/DECELERATION LANES	TS-3	7
TYPICAL SECTION - PAVING DETAILS AT RAMPS	TS-4	8
QUANTITY SHEETS (6)		
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ESTIMATED QUANTITIES FOR ASPHALT, MILLING, AND GRANULAR MATERIAL	EQ-1	11
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ESTIMATED QUANTITIES FOR TRAFFIC CONTROL SIGNS	TCPQ-1	14
PLAN & PROFILE SHEETS (34)		
MAINLINE - BOP STA. 30+00 - STA. 44+00	3	15
MAINLINE - STA. 44+00 - STA. 58+00	4	16
MAINLINE - STA. 58+00 - STA. 72+00	5	17
SR 584	5A	18
MAINLINE - STA. 72+00 - STA. 86+00	6	19
MAINLINE - STA. 86+00 - STA. 100+00	7	20
MAINLINE - STA. 100+00 - STA. 114+00	8	21
MAINLINE - STA. 114+00 - STA. 128+00	9	22
MAINLINE - STA. 128+00 - STA. 142+00	10	23
MAINLINE - STA. 142+00 - STA. 156+00	11	24
MAINLINE - STA. 156+00 - STA. 170+00	12	25
MAINLINE - STA. 170+00 - STA. 184+00	13	26
MAINLINE - STA. 184+00 - STA. 198+00	14	27
MAINLINE - STA. 198+00 - STA. 212+00	15	28
MAINLINE - STA. 212+00 - STA. 226+00	16	29
MAINLINE - STA. 226+00 - STA. 240+00	17	30
MAINLINE - STA. 240+00 - STA. 254+00	18	31
MAINLINE - STA. 254+00 - STA. 268+00	19	32
CHATTAWA RD.	19A	33
MAINLINE - STA. 268+00 - STA. 282+00	20	34
MAINLINE - STA. 282+00 - STA. 296+00	21	35
MAINLINE - STA. 296+00 - STA. 310+00	22	36
MAINLINE - STA. 310+00 - STA. 324+00	23	37
MAINLINE - STA. 324+00 - STA. 338+00	24	38
MAINLINE - STA. 338+00 - STA. 352+00	25	39
MAINLINE - STA. 352+00 - STA. 366+00	26	40
MAINLINE - STA. 366+00 - STA. 380+00	27	41
MAINLINE - STA. 380+00 - STA. 394+00	28	42
MAINLINE - STA. 394+00 - STA. 408+00	29	43
MAINLINE - STA. 408+00 - STA. 422+00	30	44
MAINLINE - STA. 422+00 - STA. 436+00	31	45
MAINLINE - STA. 436+00 - STA. 39+00	32	46
SR 568	32A	47
MAINLINE - STA. 39+00 - EOP STA. 46+76	33	48



DESIGNED BY:  
 DETAILED BY:  
 CHECKED BY:  
 DATE:

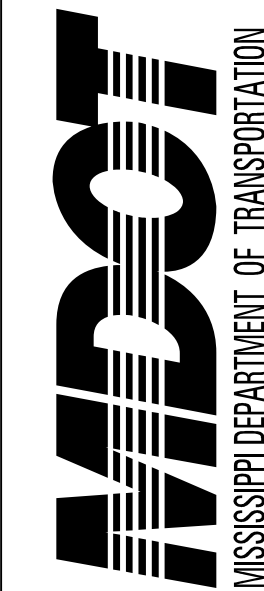
FMS CON: 110061/301000  
 PROJECT NO.: IM-0055-01(154)  
 COUNTY: PIKE

**DETAILED INDEX**

SHEET ID  
**DI-1**  
 SHEET NO.  
**2**

PLAN SHEET

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DESIGNED BY:  
 DETAILED BY:  
 CHECKED BY:  
 DATE:

FMS CON: 110061/301000  
 PROJECT NO.: IM-0055-01(154)  
 COUNTY: PIKE

**DETAILED INDEX**

SHEET ID  
**DI-2**  
 SHEET NO.  
**3**

DESCRIPTION OF SHEET	WORKING NUMBER	SHEET. NO.
<b>SPECIAL DESIGN SHEETS - ROADWAY SHEETS (7)</b>		
DETAIL OF CONSTRUCTION SIGNING	DCS-1	49
CLEARING LIMITS	CL-1	50
GUARDRAIL TYPICAL INSTALLATION AT BRIDGE APPROACHES FOR DIVIDED HIGHWAYS	GR-4A-MOD	51
GUARDRAIL: TYPICAL INSTALLATION FOR ROADSIDE HAZARDS ON DIVIDED HIGHWAYS	GR-4B-MOD	52
GUARDRAIL: POST INSTALLATION IN PAVED AREAS	SDGR-PI	53
TRAFFIC CONTROL DETAILS DRUM PLACEMENT AND SHOULDER CLOSURE	SDTCP-16	54
TRAFFIC RECORDER WIM KISTLER SYSTEM 4-LANE DIVIDED ROADWAY LAYOUT PLAN	TRS-1	55
<b>STANDARD DRAWINGS - ROADWAY SHEETS (23)</b>		
PAVEMENT MARKING DETAILS FOR 2-LANE & 4-LANE DIVIDED ROADWAYS	PM-1	6051
PAVEMENT MARKING DETAILS FOR INTERCHANGE ENTRANCE RAMPS (PARALLEL AND TAPERED)	PM-3	6053
PAVEMENT MARKING DETAILS FOR INTERCHANGE EXIT RAMPS (PARALLEL AND TAPERED)	PM-4	6054
PAVEMENT MARKING LEGEND DETAILS	PM-5	6055
PAVEMENT MARKING LEGEND DETAILS	PM-6	6056
4-LANE TO 2-LANE TRANSITION AT INTERCHANGE	PM-8	6058
PAVEMENT MARKING DETAILS FOR INTERCHANGE WITH LANE DROPS	PM-10	6060
RUMBLE STRIP DETAIL FOR OGFC OR CONCRETE ROADWAY WITH ASPHALT SHOULDERS	RS-3	6066
GUARDRAIL: "W" BEAM (WOOD POSTS)	GR-1	6201
GUARDRAIL: "W" BEAM (STEEL POSTS)	GR-1B	6203
GUARDRAIL: BRIDGE END SECTIONS TYPE "A" & "C"	GR-2	6204
GUARDRAIL: BRIDGE END SECTION TYPE "I"	GR-2F	6210
GUARDRAIL: MILSCCELLANEOUS HARDWARE	GR-HW	6221
TYPICAL GUARDRAIL DELINEATION	SN-8C	6317
SIGNING DETAILS FOR BRIDGE APPROACHES	SN-9	6318
TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT OF 65 OR 70 (INTERSTATES AND OTHER 4-LANE DIVIDED HIGHWAYS) (MEDIAN LANE OR OUTSIDE LANE CLOSURE) (EXTENDED PERIOD)	TCP-4	6354
TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT OF 65 OR 70 (INTERSTATES AND OTHER 4-LANE DIVIDED HIGHWAYS) (MEDIAN LANE OR OUTSIDE LANE CLOSURE) (WORK DAY ONLY)	TCP-5	6355
SHORT DURATION CLOSING OF DIVIDED HIGHWAYS	TCP-7	6357
HIGHWAY SIGN BARRICADE DETAILS FOR CONSTRUCTION PROJECTS	TCP-8	6358
TRAFFIC CONTROL PLAN MOBILE OPERATIONS MULTILANE ROADS AND TWO-LANE ROADS	TCP-9	6359
DETAILS OF OUTSIDE LANE CLOSURE AT EXIT AND ENTRANCE RAMPS	TCP-12	6360
TRAFFIC CONTROL PLANS UNEVEN PAVEMENT DETAILS	TCP-12	6362
TEMPORARY STRIPING FOR TRAFFIC CONTROL 2-LANE AND 4-LANE DIVIDED HIGHWAYS	TCP-13	6363
<b>TOTAL SHEETS (78)</b>		

PS & E PLANS-DATE.....		
FMS CON. # 110061/301000		
REVISIONS		
DATE	SHEET NO.	BY

PLAN SHEET

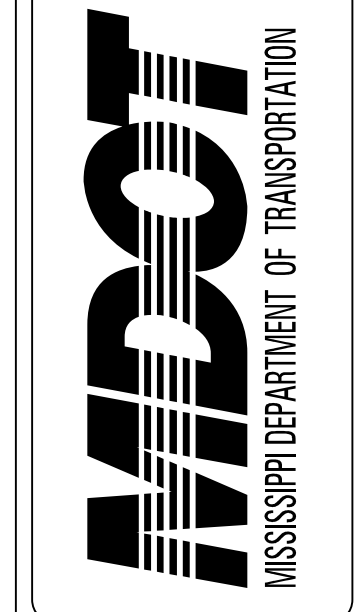
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GENERAL NOTES

- ① THE LOCATION AND SPACING OF SIGNS, SHOWN ON THE TRAFFIC CONTROL PLANS, ARE APPROXIMATE AND MAY BE ADJUSTED AS NECESSARY TO FIT FIELD CONDITIONS.
- ② ALL TRAFFIC CONTROL DEVICES ON THIS PROJECT SHALL COMPLY WITH PART VI OF THE MUTCD (LATEST EDITION).
- ③ ALL PLASTIC DRUMS SHALL HAVE A BALLASTING COLLAR MADE FROM RECYCLED TRUCK TIRES OR OTHER SUITABLE MATERIAL.
- ④ THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING EXISTING STRUCTURES SUCH AS, BUT NOT LIMITED TO, PIPES, INLETS, APRONS, AND BRIDGES FROM DAMAGE WHICH MIGHT OCCUR DURING CONSTRUCTION. THE CONTRACTOR SHALL REPLACE OR REPAIR, AS DIRECTED BY THE ENGINEER, ANY STRUCTURES DAMAGED DURING THE LIFE OF THE CONTRACT. NO PAYMENT WILL BE MADE FOR REPLACEMENT OR REPAIR OF DAMAGED ITEMS.
- ⑤ FLUORESCENT ORANGE SHEETING SHALL BE USED ON ALL CONSTRUCTION AND TRAFFIC CONTROL SIGNS EXCEPT FOR THOSE DESIGNATED ON THE PLANS TO BE BLACK LEGEND AND BORDER ON WHITE BACKGROUND.
- ⑥ VEGETATIVE MATERIAL WILL BE REMOVED PRIOR TO PLACEMENT OF GRANULAR MATERIAL. THE COST OF WHICH SHALL BE ABSORBED IN OTHER ITEMS BID.
- ⑦ THE CONTRACTOR SHALL COVER ANY TEMPORARY TRAFFIC CONTROL SIGNS SHOWN IN THE TRAFFIC CONTROL PLAN THAT DO NOT APPLY TO THE CURRENT WORK OR PHASE.
- ⑧ REMOVAL OF RAISED PAVEMENT MARKERS THAT ARE IN CONFLICT WITH REQUIRED CONSTRUCTION IS NOT CONSIDERED A SEPARATE PAY ITEM. COST TO BE ABSORBED IN OTHER ITEMS BID.
- ⑨ WHERE MILLING IS REQUIRED, THE CONTRACTOR SHALL PROVIDE OUTLETS IN THE EXISTING SHOULDERS AT SUFFICIENT INTERVALS TO PREVENT POOLING OR STANDING WATER ON THE MILLED SURFACE, THE COST OF WHICH SHALL BE ABSORBED IN OTHER ITEMS BID.
- ⑩ TEMPORARY STRIPING SHALL CONFORM TO FINISHED STRIPE SPECIFICATIONS FOR ALIGNMENT, NEATNESS, AND STRAIGHTNESS.
- ⑪ ALL ADDENDA TO THESE PLANS WILL BE POSTED TO WWW.MDOT.MS.GOV UNDER THE PROPOSAL ADDENDA COLUMN. BIDDERS ARE ADVISED THAT HARD COPIES OF ANY ADDENDA FOR THIS PROJECT WILL NOT BE MAILED. IT IS THE BIDDER'S RESPONSIBILITY TO CHECK AND SEE IF ANY ADDENDA HAVE BEEN POSTED FOR THIS PROJECT.
- ⑫ STORAGE OF FLAMMABLE MATERIALS WILL NOT BE ALLOWED UNDER ANY BRIDGE STRUCTURES WITHOUT WRITTEN APPROVAL FROM THE PROJECT ENGINEER. SEE NOTICE TO BIDDERS ENTITLED "MATERIAL STORAGE UNDER BRIDGES" FOR MORE INFORMATION.
- ⑬ THE RETROREFLECTIVE SIGN SHEETING ON RIGID, TEMPORARY TRAFFIC CONTROL (ORANGE) SIGNS SHALL BE MINIMUM TYPE IX.
- ⑭ ALL SIGNS AND DELINEATORS THAT CONFLICT WITH THE CONSTRUCTION OF THIS PROJECT SHALL BE REMOVED AND RESET BY THE CONTRACTOR; COST TO BE ABSORBED IN OTHER PAY ITEMS.
- ⑮ TEMPORARY STRIPING SHALL BE REQUIRED AFTER MILLING, PRELIMINARY LEVELING, AND OVERLAYING OPERATIONS; TEMPORARY STRIPING SHALL BE PLACED IN THE SAME LOCATIONS AND LAYOUT AS PERMANENT STRIPE. ALL CENTERLINE, LANE LINES, EDGE LINES, AND NO-PASSING STRIPES THAT HAVE BEEN REMOVED DURING THE DAY'S OPERATIONS SHALL BE REPLACED WITH TEMPORARY STRIPE BEFORE WORK IS DISCONTINUED FOR THE DAY OR AS SOON THEREAFTER AS WEATHER CONDITIONS WILL PERMIT. EXCEPT THAT:
  - (1) REPLACEMENT OF NO-PASSING STRIPES MAY BE DELAYED FOR A PERIOD NOT TO EXCEED THREE (3) DAYS FOR A TWO OR THREE LANE ROAD
  - (2) TEMPORARY EDGE LINES ON PROJECTS REQUIRING SHOULDERS CONSTRUCTED OF GRANULAR MATERIAL MAY BE DELAYED FOR A PERIOD NOT TO EXCEED THREE (3) DAYS.
- ⑯ ALL ASPHALT AND CONCRETE CURBS ALONG RAMPS, LOCAL ROADS, ETC. FROM B.O.P. TO E.O.P. SHALL BE PAINTED ( TWO APPLICATIONS ) WITH WHITE TRAFFIC PAINT AND TRAFFIC BEADS ; COST TO BE ABSORBED IN OTHER PAY ITEMS.
- ⑰ IF THE ASPHALT CURB ALONG THE LOCAL ROAD IS SEVERELY DAMAGED, THE ENTIRE ASPHALT CURB WILL BE REMOVED AS DIRECTED BY THE ENGINEER; COST TO BE ABSORBED IN OTHER PAY ITEMS.
- ⑱ ALL LOCAL ROADS SHALL BE PAVED TO THE R.O.W. LIMITS OR AS DIRECTED BY THE ENGINEER.
- ⑲ GRANULAR MATERIAL WILL NOT BE ALLOWED TO BE PLACED DIRECTLY ON THE SURFACE LIFT OF ASPHALT, BUT MUST BE PLACED DIRECTLY ON THE GRAVEL SHOULDER OR A ROAD WIDENER MACHINE USED AND APPROVED BY THE PROJECT ENGINEER.
- ⑳ THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING EXISTING BRIDGE JOINT MATERIAL ON ALL BRIDGES WITHIN THE PROJECT LIMITS FROM DIRECT CONTACT WITH THERMOPLASTIC PAVEMENT MARKING, AND WITH ANY OTHER CONSTRUCTION MATERIALS THAT MIGHT DAMAGE THE BRIDGE JOINTS.
- ㉑ VOIDS CREATED BY THE REMOVAL OF, BUT NOT LIMITED TO, POSTS, CONCRETE ANCHORS, AND FOOTINGS SHALL BE BACKFILLED AND TAMPED IN ACCORDANCE WITH SECTION 203 OF THE MISSISSIPPI STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, THE COST OF WHICH WILL BE ABSORBED IN OTHER ITEMS BID.
- ㉒ WORK ON STRUCTURES FOR THIS PROJECT REQUIRES EXCAVATION IN THE IMMEDIATE VICINITY OF TRAFFIC AND ADJACENT PROPERTIES. THEREFORE, THE RISK OF A FAILURE OCCURRING DURING EXCAVATION REQUIRES THAT EXTREME CAUTION BE EXERCISED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PLACING WHAT BRACING, SHORING, OR GROUND SUPPORT SYSTEM THAT IS DEEMED NECESSARY TO PREVENT A FAILURE AND PROTECT THE PERSONS WORKING NEAR THE EXCAVATION, THE PUBLIC THAT MAY BE ABOVE THE EXCAVATION, OR ANY STRUCTURES ADJACENT TO THE EXCAVATION. ALL COSTS FOR DESIGNING, DRAWING, AND CONSTRUCTING THE FACILITY SHALL BE INCLUDED IN THE PRICE BID FOR CONTRACT ITEMS.
- ㉓ THE CONTRACTOR IS RESPONSIBLE FOR FIELD-VERIFICATION OF EXISTING GRADES AND MAKING ADJUSTMENTS AS NECESSARY WITH THE APPROVAL OF THE PROJECT ENGINEER.
- ㉔ IF COLORS ARE USED ON PLAN/PROFILE SHEETS, THEY ARE INTENDED TO VISUALLY EASE THE LOCATION OF ELEMENTS FOR USERS OF THESE DRAWINGS. ALTHOUGH THE INTENT IS TO CATEGORIZE EVERYTHING AS EITHER EXISTING OR PROPOSED, IT IS THE END USER'S RESPONSIBILITY TO ENSURE ALL ELEMENTS ARE INTERPRETED CORRECTLY, REGARDLESS OF COLOR.

GENERAL NOTES (CONT.)

- ㉕ CURB AND GUTTER VERTICAL DIMENSIONS SHOWN IN THE DETAIL DRAWINGS ARE FOR A CURB IN THE "CATCH" CONFIGURATION AND SHALL BE CONSIDERED TO BE MINIMUM DIMENSIONS. THE DIMENSIONS MAY BE MODIFIED AS NECESSARY FOR "SPILL" CURB AND GUTTER, BUT SHALL NOT BE LESS THAN THE MINIMUM SHOWN.
- ㉖ INSTALLATION DATES SHALL BE CLEARLY WRITTEN IN BOLD BLACK MARKINGS ON THE BACK BOTTOM HALF OF ALL SIGNS WITH A PERMANENT MARKING STICK THAT IS WATERPROOF, FADE RESISTANT AND MARKS ON WET OR DRY SURFACES.
- ㉗ ALL POST, PIPE, AND I-BEAM LENGTHS IN THESE PLANS ARE ESTIMATES. POST LENGTHS FOR ALL SIGNS SHALL BE VERIFIED IN THE FIELD BY THE CONTRACTOR PRIOR TO FABRICATION.
- ㉘ ALL EXISTING SIGNS WHICH ARE TO BE REMOVED AS A PART OF THIS PROJECT THAT ARE NOT IN CONFLICT WITH CONSTRUCTION SHALL REMAIN IN PLACE UNTIL NEW SIGNS ARE INSTALLED UNLESS NOTED OR DIRECTED OTHERWISE BY THE PROJECT ENGINEER. ROADWAY SIGNS THAT ARE IN CONFLICT WITH CONSTRUCTION SHALL BE REMOVED AND RELOCATED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER, THE COST OF WHICH SHALL BE ABSORBED IN OTHER ITEMS BID.
- ㉙ DIRECT-APPLIED LEGEND, BORDER, AND/OR SHIELDS ARE TO BE USED ON ALL SIGNS. DIGITALLY PRODUCED SIGN COPY, SHIELDS, LEGEND, SYMBOLS, OR IMAGES WILL NOT BE ALLOWED WITHOUT WRITTEN APPROVAL FROM MDOT'S PROJECT ENGINEER.
- ㉚ ALL PERMANENT SIGNS SHALL CONFORM TO THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- ㉛ ALL SIGN LOCATIONS SHALL BE APPROVED BY THE PROJECT ENGINEER PRIOR TO INSTALLATION.
- ㉜ THE RETROREFLECTIVE SIGN SHEETING ON PERMANENT GROUND-MOUNTED SIGNS SHALL BE AS FOLLOWS: BROWN BACKGROUND SHEETING ON GUIDE SIGNS SHALL BE MINIMUM TYPE VIII; GREEN AND BLUE BACKGROUND SHEETING ON GUIDE SIGNS SHALL BE MINIMUM TYPE IX; ALL WHITE, YELLOW, FLUORESCENT YELLOW AND FLUORESCENT YELLOW/GREEN SHEETING SHALL BE TYPE XI. ALL SIGN SHEETING ON OVERHEAD SIGNS SHALL BE TYPE XI.
- ㉝ ALL EXISTING SIGNS AND SUPPORTS REMOVED UNDER THIS PROJECT SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND ARE NOT A SEPARATE PAY ITEM.
- ㉞ THE CONTRACTOR SHALL COORDINATE AND CONDUCT WORK AT LOCAL ROADS AND DRIVEWAYS IN A MANNER SUCH THAT ACCESS IS NOT INTERRUPTED UNNECESSARILY. ACCESS SHALL BE PRESERVED IN THE BEST MANNER POSSIBLE. COORDINATION AND COMMUNICATION WITH LANDOWNERS MAY BE NECESSARY TO PREVENT INTERRUPTION OF DRIVEWAY ACCESS.
- ㉟ TEMPORARY PAVEMENT JOINTS (PAPER JOINTS) SHALL BE EMPLOYED AT ALL LOCATIONS REQUIRING TRAFFIC TO TRAVERSE AN UNEVEN PAVEMENT JOINT. PAPER JOINTS SHALL BE A MINIMUM OF 9 FEET IN LENGTH AND SHALL BE ADEQUATELY MAINTAINED.
- ㊱ NO TEMPORARY CULVERT STREAM CROSSINGS WILL BE ALLOWED.
- ㊲ SOME WORK IS REQUIRED OUTSIDE OF THE PROJECT LIMITS. NO ADDITIONAL COMPENSATION WILL BE MADE FOR SUCH WORK EXCEPT AS PROVIDED BY SPECIFIC PAY ITEMS IN THE PLANS.
- ㊳ ALL MDOT OWNED RIGHT-OF-WAY ALONG I-55 SHALL BE CLEARED UP TO 70' FROM THE EDGE OF PAVEMENT OR TO THE ROW LINE, WHICHEVER IS LESS. LIMBS SHALL BE TRIMMED TO A HEIGHT OF APPROXIMATELY 70' ABOVE GROUND AT THE EDGE OF THE CLEARING LIMITS. CAREFUL CONSIDERATION SHOULD BE GIVEN TO ANY ABOVE-GROUND UTILITIES WITHIN THE ABOVE-DESCRIBED LIMITS. WITH CLEARING ACTIVITIES NEAR THOSE ADHERING TO ALL APPLICABLE SAFETY STANDARDS. CUT TREES AND TRIMMED LIMBS MAY EITHER BE HAULED AWAY, MULCHED IN PLACE, OR ANOTHER METHOD APPROVED BY THE ENGINEER. ALL EXISTING DOWNED TREES AND WOODY DEBRIS SHALL BE REMOVED AND OR MULCHED AS DESCRIBED. MULCHED MATERIAL SHALL BE SPREAD SUCH THAT NO MORE THAN 4" IN DEPTH OF MATERIAL IS LEFT IN ANY LOCATION TREE STUMPS SHALL BE GROUND FLUSH TO THE GROUND LINE UNLESS DIRECTED OTHERWISE BY THE ENGINEER. MATERIAL MAY NOT BE MULCHED IN RESIDENTIAL AREAS. THIS WORK SHALL BE PAID FOR AS 201-D001: RANDOM CLEARING, PER STATION. EACH SIDE OF THE ROADWAY SHALL BE MEASURED SEPARATELY.
- ㊴ REMOVAL SHOULD BE DONE SO AS NOT TO DISTURB BELOW-GROUND ROOT SYSTEMS THAT MAY CAUSE INSTABILITY OR SLOPE FAILURE, POTENTIALLY DAMAGING THE ROADWAY SUBSTRUCTURE. ANY REMOVAL METHOD ALTERNATIVES TO GRINDING MECHANISMS TO REMOVE REMAINING STUMPS TO GROUND LEVEL SHOULD BE PRE-APPROVED BY THE DISTRICT ENGINEER WITH A PLAN TO ENSURE SUBSTRUCTURE FAILURE IS UNLIKELY TO OCCUR. IF ROADSIDE FOSLOPES ARE STEEPER THAN 3:1 OR INCLUDE OTHER ROADWAY FEATURES THAT WOULD MAKE TREE REMOVAL INFEASIBLE, RISK ASSESSMENT SHOULD BE CONDUCTED BY THE PROJECT ENGINEER AND HIGHWAY SAFETY DIVISION.
- ㊵ THE CONTRACTOR SHALL REPAIR ANY DAMAGE TO THE PAVED OR UNPAVED SHOULDER THAT MIGHT OCCUR DURING CONSTRUCTION. ANY REPAIR TO SHOULDER WILL BE IN ACCORDANCE WITH THE APPLICABLE SECTIONS OF THE MISSISSIPPI STANDARD SPECIFICATIONS FOR ROAD AND AND BRIDGE CONSTRUCTION. NO PAYMENT WILL BE MADE FOR REPAIR OF DAMAGED SHOULDER.



DESIGNED BY:  
 DETAILED BY:  
 CHECKED BY:  
 DATE:

FMS CON: 110061/301000  
 PROJECT NO.: IM-0055-01(154)  
 COUNTY: PIKE

**GENERAL NOTES**

SHEET ID  
**GN-1**  
 SHEET NO.  
**4**

PLAN SHEET

3/2/2026 4:25:16 PM Special Designs.dgn